

BRENT SPENCE
BRIDGE CORRIDOR



BRENT SPENCE BRIDGE CORRIDOR PROJECT

PUBLIC INVOLVEMENT SUMMARY

Part 3

KYTC PROJECT ITEM NO. 6-17 | ODOT PID 89068
JANUARY 11, 2024

This report consists of 4 parts. Click to access [Part 1](#), [Part 2](#), and [Part 4](#).



PUBLIC INVOLVEMENT SUMMARY

This report consists of 4 parts. [Click below to access each part.](#)

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Appendix H:
2012-2021 Comments and Responses



BSB inquiries

01/12/15

Contact Information

Name: mike

Email: mdfairbairn@gmail.com

Comments: as transplants to the metro area, we are curious who had the bright idea of having two major interstates choke point at a single river crossing? from the proposed designs, assume that local traffic will be routed along the existing bridge? (the one noted as being unsafe and apparently neither state is maintaining it due to it being functionally obsolete?)

Response: 01/16/15

Mike,

Thank you for your inquiry regarding the Brent Spence Bridge. The current configuration of Interstate 71 and 75 coming together at the Brent Spence Bridge is certainly problematic today. However, when the bridge opened in 1963 the traffic patterns and volumes were much different. It is due to these changes in volume and subsequent steps taken to address the congestion that have garnered the “unsafe” label. In order to address the additional traffic, travel lane widths and shoulders were narrowed to accommodate a fourth lane on the bridge on both the top and bottom decks.

In addition to the construction of a new bridge and refurbishing the existing Ohio River crossing, the proposed project includes almost eight miles of roadway reconstruction. This project and several others improve and widen I-75 from Northern Kentucky to just north of Dayton, Ohio. In order to address the concern of two major interstates at one crossing, the new configuration (new bridge and existing bridge) will have designated locations for the traffic based on destination, either I-71, I-75 or local. The new double-decked bridge will carry both I-71 and I-75 southbound traffic on the top deck, but will be separated by a barrier wall to prevent weaving of the traffic, thus enhancing safety. The bottom deck of the new structure will carry I-75 northbound and local southbound traffic, also separated by a wall. The existing bridge will have northbound I-71 on the top deck and northbound local traffic on the bottom deck.

Additional information regarding the project can be found via:

<http://www.brentspencebridgecorridor.com>.

Please let me know if you have additional questions or need further information.

Thanks,

Brian Cunningham

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01/11/15

Contact Information

Name: josh combs

Email: jcombs701@yahoo.com

Comments:

Hi, This is mr.combs,will there be a new brent spence bridge built,if not,what will it take to get it built,how much does it cost, if they decide to build a new bridge what year will it be built. How can a person get the design & everything that goes with it,to see all this stuff. Thank you & god bless, To call me, my phone number is 1-606-521-5894. Thanks, Josh Combs

Response: Draft 01/15/15

Mr. Combs,

Thank you for your inquiry regarding the I-71/I-75/Brent Spence Bridge Project. There is no simple answer to your question of when the construction on the project will begin. A major challenge of how to pay the estimated \$2.6 billion cost of this project needs to be determined before we get to the point of when dirt starts moving. Additionally, for every month the project is delayed, an additional \$7 million in project costs is included to account for inflation.

Kentucky and Ohio are currently working through this financing dilemma. A major element of realistically financing this project involves the authorization of using tolls to cover costs that state and federal transportation dollars do not. Ohio has the legislative authority for the use of tolls on the project, however, Kentucky does not.

The Kentucky Transportation Cabinet is currently working with its legislature to pass required legislation that would enable them to levy tolls. Both Ohio and Kentucky continue to work cooperatively in developing the project, so it can move to construction when the financing approach is finalized. If tolling legislation is passed possible construction could occur in 2017.

Specific information regarding the project can be found via:
<http://www.brentspencebridgecorridor.com>.

Please contact me if you need further information or if you have additional questions.

Thanks,
Brian Cunningham

01/11/15

Contact Information

Name: Martin

Email: mmenni2@uic.edu

Comments:

Can you post a copy of this document on your website? Brent Spence Bridge Replacement/Rehabilitation Project: Conceptual Alternatives Solutions (March 2006) The Conceptual Alternatives Solutions document evaluates 25 conceptual alternatives that were developed, including the No Build alternative. Evaluation criteria included congestion

mitigation, safety, engineering, environmental resource impacts, access/accessibility, construction cost, constructability, and subcomponents of each.

Response: 01/15/15

Martin,

The Conceptual Alternatives Analysis is now posted on the website. The link is:
http://www.brentspencebridgecorridor.com/uploads/pdfs/BSBC_ConceptualAlternativeStudy.pdf

Please let me know if you are looking for something different or if you have additional questions.

Thanks,
Brian Cunningham

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01/04/15

Contact Information

Name: jerry howley

Email: jjhowley95@gmail.com

Comments: When is the contract letting for the bridge?

Response: 01/05/15

Jerry,

Thank you for your inquiry regarding the I-71/I-75/Brent Spence Bridge Project. At this time we do not have a date for the contract letting. We continue to work with the Kentucky Transportation Cabinet on the development of the project. This work involves the financing plan, when that has been determined we will have a better idea of when a contract letting will occur.

Please let me know if you need additional information or have further questions.

Thanks,
Brian Cunningham

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11/11/14

Contact Information

Name: crawford cobb

Email: crawfordcobb@Yahoo.com

Comments: what residential dwellings will have to be removed for the project?

Response: 11/13/14

Thank you for your inquiry. The preferred alternative for the I-71/I-75/Brent Spence Bridge project does not impact any residential properties in Ohio. However, in Kentucky there are 40 residential structures that have been identified as complete acquisitions based on impacts by the preferred alternative. The attached list from the 2012 Preferred Alternative Verification Report details the Kentucky properties impacted by the preferred alternative.

If there is a question on a specific property, please contact Stacey Hans at stacey.hans@ky.gov or 859-341-2700 ext 274.

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11/08/14

Contact Information

Name: Lois

Email: lschoborg@gmail.com

Comments: Having read answers to the "myths" and having lived in areas of the country that deal with tolling, what is being told on this page is misleading and downright untrue. I'm not seeing anyplace the corruption that tolling historically has been tied to referenced. I did a Google search "tolling corruption" and got "About 205,000 results". Tolling will do the opposite of what your debunking of myths says, it will cause hardship on low income especially Kentuckians, and it will reinforce the 'great divide' between Northern Kentucky and Cincinnati. To spite your denials, it is just another tax. And no place on this site that I've been able to find says which agency will collect the funds? Whose pockets will be lined with the tolls?

Response: 11/20/14

Thank you for your inquiry. We understand your concerns and why it will be essential that Ohio and Kentucky develop an in-depth, solid managing-contract and are proactive in holding accountable any organization involved with tolling activities.

For more than 60 years the highway trust fund (HTF) worked well in financing the nation's transportation program. However, that is not the case today. The revenue currently being collected does not meet the country's continually growing infrastructure needs. As a result, additional funding tools are required and one of those financing methods is using tolls. Tolls are a true user fee – those that use the product pay for it. Tolling will continue to be used until Congress takes action to increase revenue coming into the HTF or another consistent and stable funding system is created.

Please feel free to contact me if you have additional questions or need further information.

Thank you,
Brian Cunningham

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10/15/14

Contact Information

Name:

Email: mblank@zoomtown.com

Comments: Would the cost of retrofitting/expanding the existing bridge exceed the cost of building a new bridge?

Response: 11/06/14

Thank you for your inquiry and please accept my apologies for the delay in this response. Unfortunately, the bridge structure cannot be expanded to allow for the required additional capacity on the existing structure and is the reason a new bridge is needed. The existing bridge will be refurbished and returned to nearly its original lane configuration, as part of the new I-71/I-75/Brent Spence Bridge project that is currently being developed.

The original bridge had three lanes and a shoulder, it was modified in 1986 by removing the shoulders increasing it from three to four lanes. This modification was done to accommodate for dramatic traffic growth. The original bridge was built in 1963 to handle 80,000 vehicles a day, today we currently have more than 170,000 vehicles using this river crossing daily.

While the actual cost of the river crossings is substantial, the total project cost, estimated at more than \$2.5 billion, includes eight miles of improvements with several rebuilt interchanges, new approach ramps and other vital safety enhancements.

Capacity issues and safety concerns and the fact that the Brent Spence Bridge Corridor is a major economic driver for the region and the nation, make the project a top priority for both Ohio and Kentucky

Please feel free to contact me if you have additional questions or need further information.

Respectfully,

Brian Cunningham

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10/09/14

Contact Information

Name: Chris

Email: critt1990@gmail.com

Comments: When is construction supposed to start?

Response: 10/15/14

Thank you for your inquiry. There is no simple answer to your question of when the construction on the project will begin. A major challenge of how to pay the estimated \$2.6 billion cost of this project needs to be determined before we get to the point of when dirt starts moving. Kentucky and Ohio are currently working through this financing dilemma. A major element of realistically financing this project involves the authorization of using tolls to cover costs that state and federal transportation dollars do not. Ohio has the legislative authority for the use of tolls on the project, however, Kentucky does not. The Kentucky Transportation Cabinet is currently working with its legislature to pass required legislation that would enable them to levy tolls. Both Ohio and Kentucky continue to work cooperatively in developing the project, so it can rapidly move to

construction when the financing approach is finalized. Unfortunately, without the ability to use tolls in conjunction with financing the project, construction it is not likely to start until 2035.

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10/09/14

Contact Information

Name: Devaughn Ragan

Email: kennedy.ragan@yahoo.com

Comments: Who will pay for it, the Federalists or the States?

Response: 10/09/14

Thank you for your inquiry. Ohio and Kentucky are using both state and federal funds to build the I-71/I-75/Brent Spence Bridge project. However, the estimated \$2.5 billion project cost is much higher than is feasible for either state to pay.

This estimated cost is nearly double the combined annual transportation budgets for the two states. The public sources of funding that each state transportation department receives, whether federal or state, will not cover the cost of the project requiring the exploration of additional revenue sources.

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10/05/14

Contact Information

Name: Matt Allen

Email: mattall3n@hotmail.com

Comments: A community of 40,000 residents, Covington, should not bear the brunt of a new interstate bridge on a federal highway. This Corridor serves the state of Ohio, Kentucky and the nation. The taxing system is broken causing the country's infrastructure to wither. We need to change the system in order to maintain the roadways that we have already. Are the Residents of St. Bernard being asked to pay for the construction at the Mitchell Ave. Exit? Why is a bridge something we should put tolls on but not a stretch of highway? Should we toll 75 through Hamilton County? Or would that be too far thus causing us to rethink the taxing plan? And, why do we keep thinking that we can build our way out of traffic? The highway systems original goal was to build our way out of traffic. That didn't work. Are we going to give our children the same short sighted answer to the problems we face today? Let's find another solution to traffic.

Response: 10/20/14

Matt,

Thank you for your inquiry. Billions of dollars in commerce and millions of people cross the bridge every year, sometimes at a crawl. This delay is a significant waste of time, money and fuel. Replacing the bridge is important to the economic vitality of Kentucky, Ohio and the other eight states that are connected by interstates 75, 71 and 74. The Brent Spence Bridge is a chokepoint where the thousands of miles of these three interstates meet. The traditional transportation funding mechanism, the Highway Trust Fund, worked for decades to adequately fund the nation's infrastructure needs. Unfortunately, that is no longer the case. New and

innovative financing methods must be utilized in order to fill the gap between infrastructure needs and available funding. Tolling is one such way to cover costs that state and federal transportation dollars do not. As opposed to other mechanisms, such as special tax districts, tolling the facility captures only those choosing to utilize the new structure. Unless Congress acts to alter how transportation funding is generated, tolling will continue to be used out of necessity.

Respectfully,
Brian Cunningham

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02/09/15

Contact Information

Name: Don Baker

Email: dbaker@blueone.net

Comments: The current controversy around the Brent Spence Bridge is causing me to write this. I have never posted anything before. Maybe no one will read it, but I will feel better. The bridge and the highway were a bad idea right from the start. Politicians wanted them in the downtown area. They got what they asked for and now don't like it. I think it is always wrong to combine interstate traffic with local traffic. They have completely different goals and just get in each other's way. Local drivers complain about trucks on the road, but those trucks are paying the taxes that support the road and they are carrying the products that we want. Very few people travelling from Michigan to Florida want to stop in Cincinnati or Covington. If they have to stop for food or fuel they will stop in Sharonville or Florence. The goal is to get through Cincinnati and Covington. I think the interstate system should be hard to get on and easy to get off. Put an exit at every possible cross street but only put entrances at major highways. Anyone travelling less than 5 miles should stay off the interstate. The proposed new bridge should have dedicated lanes from I-74 in Ohio to I-275 in Kentucky with no entrance ramps, only exits, and be funded with diesel fuel taxes and axle-mile taxes on trucks. Tolls might be acceptable if they completely eliminate the delays caused by the BS Bridge. The original plans for I-71 had it crossing the Ohio River in the Silver Grove area. It has no business on the BS Bridge. I-71 should immediately be routed over I-275, requiring only sign changes, with future plans to go from the split in Boone County directly to I-71 in the Mason area. I am retired now, but when I was working I went from Covington to I-75 NB every day. I did not use the BS Bridge. I used the Bailey bridge at Main Street and got on I-75 at Gest Street. I avoided the frantic lane changes needed to get from the 5th Street entrance to NB I-75. There will never be a better time to upgrade our infrastructure. We are still using buildings, roads, bridges and parks built in the 1930s by WPA. The states and Federal government can now borrow money at near zero interest rates. That can not last forever, but it sure could solve our unemployment problems.

Response: 02/12/15

Mr. Baker,

Thank you for your inquiry regarding the I-71/I-75/Brent Spence Bridge Corridor project. The interstate highway system has been an important national asset for nearly 60 years. Parts of it are now in great need of improvement because of six decades of wear and tear and significant changes in travel patterns.

This nearly eight-mile, \$2.6 billion project is one such project and is why it is one of the top transportation priorities for both Kentucky and Ohio.

The original intent of the interstate system was to move interstate travel and commerce. However, because of the interstate's efficiency it has evolved during the past 60 years and now handles substantial local trips. Because of its effectiveness in moving people and goods it must be repaired, rebuilt and re-energized. These are the exact reasons the I-71/I-75/Brent Spence

Bridge Corridor improvements are economically vital for not only the northern Kentucky/southwest Ohio area but for the eastern region of the United States.

A major challenge is how to pay for this mega-project and needs to be determined before we get to the point of when dirt starts moving. Additionally, for every month the project is delayed, an additional \$7 million in project costs is included to account for inflation.

Kentucky and Ohio are currently working through this financing dilemma. A major element of realistically financing this project involves the authorization of using tolls to cover costs that existing state and federal transportation dollars cannot. Ohio has the legislative authority for the use of tolls on the project, however, Kentucky does not.

The Kentucky Transportation Cabinet is currently working with its legislature to pass required legislation that would enable them to levy tolls. Both Ohio and Kentucky continue to work cooperatively in developing the project, so it can move to construction when the financing approach is finalized. If tolling legislation is passed possible construction could occur in 2017.

Please continue to check the website at <http://www.brentspencebridgecorridor.com> for updates.

Thank you,
Brian Cunningham

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02/08/15

Contact Information

Name: jenny ogden

Email: jenalea39@gmail.com

Comments: why not keep large trucks off this bridge while it is so dangerous?? I know this was done another time, but I can't remember why.

Response: 02/10/15

Jenny,

Thank you for your interest in the I-71/I-75/Brent Spence Bridge corridor project. Due to safety concerns associated with the area known as the I-71/75 "Cut in the Hill" in Northern Kentucky, there was a truck ban previously in place along the corridor that was lifted at the completion of the roadway reconstruction project.

KYTC and ODOT examined the very issue of a truck ban in 2005 as part of the *Brent Spence Bridge Engineering and Feasibility Study*, but concluded that a truck ban would be extremely complicated to enact and enforce. In addition to receive approval from the Federal Highway Administration, enactment of a truck ban would require an agreement by both Kentucky and Ohio and the adoption of the necessary regulations to enforce the ban. Even if such regulations could be enacted, enforcement would be nearly impossible as any truck delivering within the I-

275 loop would be exempt from the ban. In addition verifying delivery of the product within the I-275 loop rather than passing through would significantly stress the resources of state and local police departments.

Additionally, studies have shown that eliminating truck traffic along the corridor in the Northern Kentucky and Cincinnati region does not solve the congestion and safety issues, but simply moves them to other locations. Even with the truck traffic removed, the amount of traffic utilizing the I-71/75 corridor is more than the roadway was originally designed to carry. The steep grade of the roadway contributes to the congestion, but the sheer volume of the passenger vehicles trying to use the roadway also plays a part in the problem. The Ohio-Kentucky-Indiana Regional Council of Governments looked at a truck ban option in 2007 and concluded:

- A ban of through trucks on the northern Kentucky portion of I-71/75 has no substantial benefits. Reduction in severe crashes is expected to be very modest. Operating costs to the trucking industry and additional emissions negatively impact the region. The practical difficulties in the enforcement of a truck ban must also be considered in the deployment of such a ban.

Here is a link to the study if you wish to review it:

http://www.oki.org/studies/pdf/bsb/BSBTruck_Ban_Final.pdf

Please continue to check the website at <http://www.brentspencebridgecorridor.com> for updates.

Thank you,
Brian Cunningham

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02/05/15

Contact Information

Name: Alex

Email: alexherrmann15@gmail.com

Comments: I was curious as to where you retrieved your statistical information that shows at your home page scrolling across the top? I am doing a speech regarding the Brent Spence Bridge in one of my speaking classes at NKU and need accurate sources. I'd appreciate any additional information you could also give me... Thanks!

Response: 02/10/15

Alex,

I have attached the source information you requested involving the I-71/I-75/Brent Spence Corridor project found at: <http://www.brentspencebridgecorridor.com/>

Please continue to monitor the web page for ongoing updates.

Thanks,

Brian Cunningham

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02/03/15

Contact Information

Name: Chris Moriconi

Email: cmoriconi@cityofindependence.org

Comments: I am working on a research paper for NKU's MPA-Research class. Where can I find any environmental impact studies on the proposed bridge project? Also, can you provide what research methodologies were used in the findings as they related to tolls vs no tolls, the fuel wasted waiting in congestion traffic, etc. Thank you!

Response: 02/05/15

Chris,

Thank you for your inquiry regarding the I-71/I-75/Brent Spence Bridge Project.

The environmental documents and baselines are available in the Document Center tab on the website: <http://www.brentspencebridgecorridor.com/>

Here are the links to the two major ones:

2012 Environmental Assessment (EA):

<http://www.brentspencebridgecorridor.com/uploads/pdfs/Brent%20Spence%20Bridge%20Environmental%20Assessment%20March%202012.pdf>

2012 Finding of No Significant Impact (FONSI):

<http://www.brentspencebridgecorridor.com/uploads/pdfs/FONSI.pdf>

We are just beginning to look at the impacts related to tolls, so any findings as they relate to tolls, will not be available for a while.

Please let me know if you need additional information or have further questions.

Thanks,
Brian Cunningham

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02/01/15

Contact Information

Name: Jason Monahan

Email: jason.a.monahan@gmail.com

Response: 02/05/15

Jason,

Thank you for your inquiry. The preferred alternative for the I-71/I-75/Brent Spence Bridge project does not impact any residential properties in Ohio. However, in Kentucky there are 40 residential structures that have been identified as complete acquisitions based on impacts by the preferred alternative.

The attached list from the 2012 Preferred Alternative Verification Report details the Kentucky properties impacted by the preferred alternative. Please note that the plans as presented were developed as part of the Preliminary Engineering and Environmental process; the plans are still preliminary and subject to change as the project moves into the detailed design process.

Please continue to check the website at <http://www.brentspencebridgecorridor.com> for updates.

If there is a question on a specific property, please contact Stacey Hans at stacey.hans@ky.gov or 859-341-2700 ext 274.

Thanks,
Brian Cunningham

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02/01/15

Contact Information

Name: R. Plummer

Email: mickeymouse1@fuse.net

Comments: OBAMA ought to be MADE to PAY FOR THE BRIDGE, since he was ARROGANT ENOUGH to come and MAKE A SPEECH ABOUT IT ! Put his MONEY where his ARROGANT BIG MOUTH is !

Response: 02/05/15

Thank you for your inquiry regarding the I-71/I-75/Brent Spence Bridge Corridor project. This nearly eight-mile, \$2.6 billion project is one of the top transportation priorities for both Kentucky and Ohio.

One of the major challenges is how to pay for this mega-project and needs to be determined before we get to the point of when shovels hit the ground. Additionally, for every month the project is delayed, an additional \$7 million in project costs is added due to inflation.

Kentucky and are currently working through this financing challenge. A major element of realistically financing this project involves the authorization of using tolls to cover costs that state and federal transportation dollars cannot. Ohio has the legislative authority for the use of tolls on the project, however, Kentucky does not.

The Kentucky Transportation Cabinet is currently working with its legislature to pass required legislation that would enable them to utilize Public-Private Partnerships as a financing option. Both Ohio and Kentucky continue to work cooperatively in developing the project, so it can move to construction when the financing approach is finalized. If legislation is passed that would allow tolling as a revenue source, construction could begin as early as 2017.

Please continue to check the website at <http://www.brentspencebridgecorridor.com> for updates.

Thank you,
Brian Cunningham

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From: AT & T <rail@fuse.net>
Sent: Thursday, August 1, 2019 9:02 PM
To: Rowe, Charles <Charles.Rowe@dot.ohio.gov>
Subject: Brent Spence bridge idea

Dear Charlie:

About a year ago, I resigned as a Trustee of the Cincinnati Southern Railway (CSR) due to health and personal reasons. I am pleased to report that things are much better now and find myself resuming my usual on going analysis of rail rights of way in and around Cincinnati. Up to this point, I have concentrated on the possibility of using the rail rights of way heading west along River road as the base for a Brent Spence alternative. A river crossing would be in the vicinity of CVG.

As I have reflected upon my time as a CSR Trustee and the Brent Spence bridge dilemma, it dawned on me that maybe the Cincinnati Southern could play a part in at least easing the pressure.

So, here are my thoughts:

It might be possible to run a non standard no access highway between the Freeman Ave. ramps off I-75 , elevated over Freeman Avenue for about 1600 feet, then SW across various rail lines (which will need extensive adjustment) to the CSR bridge. Then, a new bridge adjacent to the existing RR bridge, to Kentucky and following the CSR right of way connect up to I-75 with new ramps in the Ft. Mitchell area.

Facts:

1. Very little private property acquisition
2. **1 mile shorter route than current I-75 routing**
3. very level and straight.
4. Few major obstacles to resolve.

Rail changes and congestion relief:

1. **Eliminate CSX elevated structure**
2. **New connection from C.U.T. to CSX replacing bridge connection**
3. **New connection from NS (CSR) to riverfront (reestablish route severed when bridge burned years ago.)**
4. **Connect / reestablish CSX riverfront track - allows trains to directly run through Cincinnati from east to west, west to east and avoid having to go into congested CSX Queensgate Yard.**

Please review and let me have your thoughts.

Sincerely,

Tom McOwen

513-703-8055



RESPONSE:

Thank you for your inquiry regarding construction of a new Ohio River crossing at Hamilton County.

The current Brent Spence Bridge is sound and being maintained by the Kentucky Transportation Cabinet. If you have specific questions on the condition, they should be contacted.

The preferred alternative for the Brent Spence Corridor project that was identified by the Ohio Department of Transportation and the Kentucky Transportation Cabinet does build a new bridge just west of the existing Brent Spence Bridge, and the cost estimates to construct a structure and approach roadways sufficient to carry interstate traffic and meet current and future demands are currently \$2.6 billion.

Although we at ODOT have been working to acquire the right of way needed in Ohio for a new bridge, the project is not funded for construction at this time; however, we will continue to work with our partners in Kentucky to identify funding mechanisms so this project can move forward.

Thanks,
Kathleen

Kathleen Fuller, CPM

Public Information Officer

ODOT District 8

505 S.R. 741 South, Lebanon, Ohio 45036

(o) 513-933-6517 (m) 513-439-2292

transportation.ohio.gov



From: Gray Ralphes <gray.ralphes@gmail.com>

Sent: Thursday, April 23, 2020 3:32 PM

To: DOT D08 PIO <D08.PIO@dot.ohio.gov>

Subject:

Hi i would like it if you could answer a few of my questions please.

1. how much would it cost for ohio or Hamilton county to build a new bridge next to the Brent-Spence bridge.
2. wouldn't it be beater to build a new bridge instead of having the old one threatening to collapse and having the budget of Hamilton county be wasted every year on repairs.

Thanks

CAUTION: This is an external email and may not be safe. If the email looks suspicious, please do not click links or open attachments and forward the email to csc@ohio.gov or click the Phish Alert Button if available.

Appendix I:
2022-Present Comments and Responses



Brent Spence Bridge Corridor Project Comments and Responses

Comment Number	Nature of Comment	Comment	Response
001	BSB Companion Bridge	06/01/2022 - ODOT Website Comment - The new companion bridge should include a pedestrian facility, so that people don't have to detour a mile out of their way to cross the river.	The Brent Spence carries limited access vehicle traffic and the current alternative is continuing with this pattern. As such pedestrian facilities are not planned on the freeway components. The region has multiple pedestrian crossings of the Ohio River with the closest being the Clay Wade Bailey bridge 600' east of the existing Brent Spence Bridge.
001A	Roadway/Bridge Design	06/08/2022 - ODOT Website Comment - With plans for a second bridge, consider using 1 for truck traffic and one for automobile traffic, separating the two would make for a more efficient and safe roadway. In fact it would be worth studying in areas where you have the space to make truck only lanes, to keep trucks from making lane changes. and bottlenecking traffic, and keeping cars from cutting in and out of the truck traffic.	Thanks for your comment concerning the Brent Spence Bridge Corridor project. We understand your concerns with the intermixing of two different vehicle classifications; however, we will not be considering independent truck lanes. We are making improvements to separate the traffic wanting to access local destinations with those traveling through the central business districts. Local destination traffic will utilize a collector-distributor system. This should minimize the weaving of traffic in the corridor.
001B	Storm and Sanitary Ft. Wright Ramps	<p>06/12/2022 - Email - I am on the Fort Wright City Council and past member of the Board of Sanitation District No. 1 thus these two points.</p> <p>When it rains the water flow from I75 between Kyles Lane north to the bridge is directed off the expressway into the sewer systems in Covington. This system is a combined sanitary and storm system. This diversion of water causes the system to overflow, creating health hazards and violations of the Consent Decree Northern Kentucky is operating under. These overflows are costly to all Northern Kentucky residents and businesses. A while back, transportation officials had made presentations to citizens, cities, and others including SD1. Dave Rager who was then the Executive Director of SD1 and his engineering team met with those transportation officials. Out of those meetings a verbal commitment was made to Northern Kentucky that waters from the I75 corridor in this area be capture and transported to the Ohio River in storm water piping to be designed into and added to project. No more water was to be directed to Covington. Not sure of this part, maintenance of this piping was to be the responsibility of the transportation cabinet. This note is a reminder of that commitment, so those attributes are included in the design.</p> <p>The second point that needs to be addressed are the proposed exits</p>	<p>KYTC has committed to strive to separate the highway drainage from combined sewer systems as part of the BSB Corridor Project. Where separation is not feasible, adequate detention per KYTC standards will be provided. Furthermore, KYTC committed to participate with Sanitation District No. 1 of Northern Kentucky (SD1) efforts to bring applicable agencies together to discuss, investigate, and evaluate mutually beneficial arrangements.</p> <p>Since those original commitments were made, the City of Covington has reassumed storm water responsibility from SD1. The City is responsible for storm water runoff until it reaches the combined sewer system, at which point it becomes the responsibility of SD1. Given this development, KYTC has been engaged in on-going coordination with both the City of Covington and SD1. These efforts have included several meetings and the preparation of a Willow Run Storm Water Separation Feasibility Study Report (March 2022) to evaluate alternative drainage layouts for storm and sanitary separation. These coordination efforts will be on-going during the projects design, construction, and maintenance phases.</p> <p>The preferred alternative for the Brent Spence Bridge Corridor project will widen I-71/I-75 through Ft. Wright, Kentucky and will rebuild the Kyles Lane and Dixie Highway interchanges. However, the configuration of these interchange will match what currently</p>

Comment Number	Nature of Comment	Comment	Response
001B (cont.)	Storm and Sanitary Ft. Wright Ramps (cont.)	and entrances to I75 at Fort Wright. As we understand these, as drawn, place an economic burden on merchants on Dixie Hwy near Kyles Lane. Additionally, those exits are the main entrance to Fort Wright and need to be convenient and welcoming to motorist traveling along I75. The city with its engineering team would like to meet with the transportation planners so we can, 1) understand the drawings, and 2) arrive at an understanding that negates our concerns about the impact to The City.	exists, and the access to Ft. Wright will be unchanged. Furthermore, value engineering efforts and design refinements have reduced footprint of the project in these areas. The project will be constructed largely within the existing transportation right-of-way. No commercial merchants will be displaced in these areas, although one residence will be displaced. KYTC will continue to coordinate the projects impacts with the City of Ft. Wright through the project's design and construction phases.
002	Bridge Closures	07/02/2022 - Web Comment - I use I-71 from Louisville to get to Columbus Ohio. My last trip I was very excited that the southbound trip over the bridge was no longer detoured. Is the northbound repair work done? I travel from the St Louis area and used to take I-70 to Columbus but it is a royal mess around Indianapolis and I prefer to take 64 to 71. Please let me know as I can take another bridge but it is not as convenient as the Brent Spence Bridge. Thank you.	The Brent Spence Bridge is not under any long-term closures. Travelers can review real-time traffic information to determine if their routes are affected by work zones or other incidents at: https://goky.ky.gov/ and https://ohgo.com/ .
003	Project Mailing List	07/05/2022 - Web Comment - I'd like to receive the BSB updates.	This individual was added to the project mailing list.
004	Project Mailing List	07/06/2022 - Web Comment - Please register me.	This individual was added to the project mailing list.
005	Aesthetics Noise	07/09/2022 - Web Comment - I am on the Fort Wright City Council. These thoughts are in addition to my previous suggestions. 1) When traveling North on I75 say to Dayton several overpasses have been decorated by local cities making them very attractive, welcoming, and pleasing to motorist. An opportunity exists to accomplish the same here in Kentucky. The first overpass south of the Bent Spence Bridge is Kyles Lane. The city would like to discuss enhancing the aesthetics of that overpass and welcoming motorist to the city. An opportunity exists for the state to partner with the city on this upgrade to welcoming motorist to Kentucky. 2) Several Fort Wright residents who live east of I75 approached the city concerning noise abatement of traffic which will be closer to them and increasing. Addressing these concerns is imperative.	A Project Aesthetics Committee was formed as a subgroup of the Project Advisory Committee (PAC) to evaluate aesthetic treatments through the corridor, including corridor themes during the project's initial development stages. The Aesthetics Committee is scheduled to reconvene in 2022 to continue its evaluation of aesthetics throughout the BSB corridor. KYTC is currently conducting a noise analysis to identify any noise impacts associated with the BSB Corridor Project and to identify mitigation measures for any identified impacts. KYTC will continue to coordinate the projects aesthetic and noise components with the City of Ft. Wright through the project's design and construction phases.
006	Construction	07/11/2022 - Web Comment - Is there a site already in place for suppliers to sign up and/or provide capabilities statements?	There is an option on the BSB Corridor Project Procurement Information website to submit contact information and submit comments or questions: www.dot.state.oh.us/Divisions/ConstructionMgt/design-build/bsb-project-procurement/

Comment Number	Nature of Comment	Comment	Response
007	Project Mailing List	07/15/2022 - Web Comment - Please add me to your contact list to receive project updates.	This individual was added to the project mailing list.
008	General Support	07/15/2022 - Web Comment - Looking forward to getting this done.	Comment noted.
009	Project Mailing List	07/15/2022 - Web Comment - Please add me to the mailing for updates. Thanks.	This individual was added to the project mailing list.
010	General Support	07/15/2022 - Web Comment - Thanks.	Comment noted.
011	Project Mailing List	07/15/2022 - Web Comment - Please send updates.	This individual was added to the project mailing list.
012	Pike Street	07/15/2022 - Web Comment - How is the corridor going to affect Pike Street in Covington, KY?	Near Pike Street, Preferred Alternative I (Concept I-W) will widen I-71/I-75 from 7 to 11 lanes and will build two parallel collector-distributor (C-D) roadways to provide access to local streets. Traffic will exit I-71/I-75 and travel down the C-D road to access Pike Street. In addition, portions of Pike Street will be rebuilt to accommodate the widened interstate and C-D roads.
013	Project Mailing List	07/15/2022 - Web Comment - Subscribe.	This individual was added to the project mailing list.
014	Project Mailing List	07/15/2022 - Web Comment - Hello, we are one of 7 state welcome centers for KY. We would like to stay informed about the bridge project.	This email address was added to the project mailing list.
015	BSB Companion Bridge	07/15/2022 - Web Comment - Why are they making the new bridge smaller. That is where a large amount of traffic is.	The Preferred Alternative (Concept I-W) will rehabilitate and reconfigure the existing double decker Brent Spence Bridge (BSB) to reduce the number of lanes on each deck from four to three and increase inside and outside shoulder widths. The existing BSB will serve local traffic. In addition, it will construct a double decker companion bridge with five lanes on each deck west of the existing BSB. The new companion bridge will serve through (interstate) traffic. With both bridges, the total number of lanes across the Ohio River will increase from 8 to 16. Traffic analyses completed for the project show these lanes will restore acceptable traffic flow across the river.
016	Project Mailing List	07/15/2022 - Web Comment - Signing up.	This individual was added to the project mailing list.
017	Project Mailing List	07/15/2022 - Web Comment - Receive updates.	This individual was added to the project mailing list.
018	Project Mailing List	07/15/2022 - Web Comment - Please keep me informed. Thanks.	This individual was added to the project mailing list.

Comment Number	Nature of Comment	Comment	Response
019	Traffic	07/15/2022 - Web Comment - I know everyone is happy the bridge is getting done. My concern is, is it going to be able to handle future traffic 40 yrs from now. I'm a big believer I-71 needs to be pushed around 275. Why is this so hard to understand.	The Brent Spence Bridge Corridor Project has been designed to support traffic projected to travel through the corridor through the year 2050, which is the standard design horizon for large infrastructure projects. Studies conducted by KYTC concluded that bypass options do not postpone the need for a new Brent Spence Bridge or the need for improvements to I-71/I-75.
020	Project Mailing List	07/16/2022 - Web Comment - Please sign me up for updates.	This individual was added to the project mailing list.
021	General Support	07/16/2022 - Web Comment - The new 2022 revision is awesome. Way to go Governor's! I can't wait 'till this is shovel ready! God Speed!	Comment noted.
022	Project Mailing List	07/16/2022 - Web Comment - Sign me up.	This individual was added to the project mailing list.
023	Project Mailing List	07/16/2022 - Web Comment - Please sign me up for regular updates.	This individual was added to the project mailing list.
024	Project Mailing List	07/16/2022 - Web Comment - Trying to sign up for updates in the Brent Spence bridge.	This individual was added to the project mailing list.
025	Project Mailing List	07/16/2022 - Web Comment - Please provide updates on the Brent Spence project.	This individual was added to the project mailing list.
026	Project Mailing List	07/16/2022 - Web Comment - Add me to your update list please.	This individual was added to the project mailing list.
027	Project Mailing List	07/16/2022 - Web Comment - Updates on Brent Spence bridge please.	This individual was added to the project mailing list.
028	Project Mailing List	07/16/2022 - Web Comment - Sign me up	This individual was added to the project mailing list.
029	Project Mailing List	07/16/2022 - Web Comment - Please send bridge updates to my email	This individual was added to the project mailing list.
030	Project Mailing List	07/17/2022 - Web Comment - I'd like to be on email update list.	This individual was added to the project mailing list.
031	Project Mailing List	07/17/2022 - Web Comment - Signing up for emails about the project updates	This individual was added to the project mailing list.
032	Noise Air Quality Right-of-Way	07/17/2022 - Web Comment - As a resident of West Covington who will be impacted by this expansion, what will you be doing to reduce noise levels and pollution that will subsequently come from this proposal? Also, how can I find out what streets will be impacted and potentially fall to eminent domain?	KYTC is currently conducting a noise analysis to identify any noise impacts associated with the BSB Corridor Project and to identify mitigation measures for any identified impacts. Air quality analyses conducted for the project showed reduced vehicle emissions due to reduced traffic in the corridor. Mapping showing relocations associated with the project will be available at the project's public hearing.

Comment Number	Nature of Comment	Comment	Response
033	Project Mailing List	07/17/2022 - Web Comment - I would like to get updates on the project.	This individual was added to the project mailing list.
034	Project Mailing List	07/18/2022 - Web Comment - Please provide updates on the Brent Spence Bridge and associated area improvements.	This individual was added to the project mailing list.
035	Project Mailing List	07/18/2022 - Web Comment - Please send updates.	This individual was added to the project mailing list.
036	Project Mailing List	07/18/2022 - Web Comment - Please sign me up.	This individual was added to the project mailing list.
037	Roadway/Bridge Design	07/18/2022 - Web Comment - I am assuming there will be a divider between the north and southbound lanes on the updated drawing. It doesn't seem very safe to have north and south bound traffic side by side without a barrier between them. People aren't always good at holding their lane. We have enough bad accidents when everybody travels in the same direction. I am also curious what type of protection will be on the outer lanes to prevent vehicles from being jettisoned into the river. It seems to be a common practice to just put a short concrete wall on the edges, which has proven to be deadly for disabled vehicles in the past.	All northbound and southbound traffic on I-71 and I-75 will be separated with a median barrier. The outside lanes on the bridges over the river will be protected by a bridge parapet that meets all current safety standards. In addition, standard shoulders will be provided on the existing and new companion Brent Spence Bridges to provide room to accommodate disabled vehicles.
038	Project Mailing List	07/21/2022 - Web Comment - I wanted to sign up to receive updates on the project.	This individual was added to the project mailing list.
039	Project Mailing List	07/21/2022 - Web Comment - Send updates.	This individual was added to the project mailing list.
040	Project Mailing List	07/21/2022 - Web Comment - I am an acoustic design consultant and working with several clients on potential highway noise from the project. Please send project updates. Thank you.	This individual was added to the project mailing list.
041	Project Mailing List	07/21/2022 - Web Comment - Please include me in any updates, thanks.	This individual was added to the project mailing list.
042	Project Mailing List	07/21/2022 - Web Comment - Looking forward to updates on the new bridge.	This individual was added to the project mailing list.
043	Project Mailing List	07/22/2022 - Web Comment - Sign me up.	This individual was added to the project mailing list.
044	Project Mailing List	07/22/2022 - Web Comment - Please keep me updated on the progress of the bridge project.	This individual was added to the project mailing list.

Comment Number	Nature of Comment	Comment	Response
045	Right-of-Way	07/26/2022 - Web Comment - My daughter's home is located at [REDACTED] Watkins Street, in Covington, which is right next to the 12th Street ramp to Southbound I-71/I-75. Will her home be taken for the bridge expansion?	My apologies in the delay of the response to your inquiry regarding project impacts associated with the Brent Spence Bridge Corridor Project, specific to property located along [REDACTED] in Covington. Based on current plans, the property located at [REDACTED] is a potential partial acquisition (strip taking - a portion of the land, but not the structure) and could be impacted by the project.
046	Contact Information	07/27/2022 - Web Comment - Is there an email address to which public comments can be sent, in lieu of using this online form?	Emailed comments may be directed to: mailto:info@brentspencebridgecorridor.com .
047	Roadway/Bridge Design	07/28/2022 - Web Comment - On the KY side of the 'Through Traffic Bridge' - where is the 'alternate I71/75' ROAD going to be? Which side of I71/75 - East or West - will this new ROAD be? How will this new ROAD connect to the existing I71/75 in Ft. Mitchell?	Preferred Alternative I (Concept I-W) will rehabilitate and reconfigure the existing double decker Brent Spence Bridge to carry local traffic and build a new double decker companion bridge just west of the existing BSB to carry through (interstate) traffic. The I-71/I-75 lanes will be realigned to provide access to the new companion bridge just north and south of the Ohio River. I-71/I-75 will remain along the same alignment for the remainder of the project corridor, including in Ft. Mitchell.
048	Traffic	07/28/2022 – Email – Two main questions. By my calculations, the respective peak hours increase 75% from 2017 to the projection. The respective peak periods (the peak 4 or 5 hours) increase 30%. As you shared, we are assuming the peak periods would be consistent with pro-covid (at worst). This discrepancy creates real design challenges or opportunities for cost savings/design improvements. I can't speak specifically to design capacity of expressway lanes, but the current bridge with 8 narrow lanes with really-really troubling movements can run 91k vehicles at the respective AM/PM periods. The Companion Bridge with really great geometry and very normal movement has 73K vehicles on 10 lanes. If peak flow was consistent pre-covid, could you put 71 and local traffic on the original bridge? It probably all comes down to the black box, so just throwing out some observations.	Under the existing conditions, there are not enough lanes on I-71/I-75 to serve all the traffic attempting to travel through the corridor. As a result, the area serves as a bottleneck that constrains the number of vehicles that can pass through during peak periods, resulting in slowed traffic and backups across the BSB. The proposed improvements will add lanes to I-71/I-75, dramatically reducing these bottlenecks and allowing more traffic to pass through the corridor without slowing or stopping due to congestion. The apparent 75-percent increase in traffic volumes does not necessarily result from an increase in the number of vehicles desiring to travel through the corridor. Rather, it is an increase in the number of vehicles that are actually able to move unrestricted through the corridor during peak travel periods due to a reduction in traffic congestion. The remaining traffic growth is based on the Ohio-Kentucky-Indiana Regional Council of Governments (OKI) travel demand model, which accounts for population and economic growth, among other factors. The preferred alternative for the BSB Corridor Project was analyzed using pre-COVID traffic as a base condition. Traffic volumes for the year 2050 were then forecasted using the OKI regional travel demand model. Based on analyses conducted using these volumes, the original BSB could not accommodate all I-71 and local traffic volumes.

Comment Number	Nature of Comment	Comment	Response
049	Project Mailing List	07/31/2022 - Web Comment - Please give me updates on any meetings open to public concerning the new bridge plans. Is it too late to give opinion on design of new bridge? Why is this important to me? A new exciting design will draw people to the area and show that Ohio/Kentucky are progressive states.	This individual was added to the project mailing list. There will be public involvement activities scheduled to allow interested persons to provide feedback on the project. All individuals included on the project mailing list will be notified of future public involvement activities.
050	Project Schedule	08/09/2022 - Web Comment - What is timeline for the project?	Construction on Phase I of the BSB Corridor Project (Findlay Street to Marshall Avenue) is anticipated to begin in 2028 and be complete in 2031. Construction on Phase II (Linn Street to Findlay Street) is anticipated to begin in 2025 and end in 2029. Construction on Phase III (the remainder of the project corridor, including the companion bridge over the Ohio River) is anticipated to begin in 2023 and be complete in 2029.
051	Project Risk Register	8/9/2022 - Web Comment - Could you email me an Excel or Word version of the Project Risk Register please? It is a very good register! Thank you	The requested information was provided to this individual on September 27, 2022.
052	Roadway/Bridge Design	08/09/2022 - Web Comment - Who is doing the design/engineering for the new bridge?	The project will utilize a Progressive Design-Build method for design and construction. A Request for Proposals is currently scheduled for January 2023. At that time, prospective design-build teams will prepare technical submittals. KYTC and ODOT will review the submittals, compile a "shortlist" of the most highly qualified proposers, conduct interviews, and select one design-build team to deliver the project. More information about the design-build process can be found at: www.dot.state.oh.us/Divisions/ConstructionMgt/design-build/bsb-project-procurement .
053	Roadway/Bridge Design	08/11/2022 - Web Comment - I believe there should be a separate bridge for trucks and cars.	Given the complex geometry of the Covington and Cincinnati approaches to the Brent Spence Bridge, it would not be feasible to construct separate bridges for trucks and cars that maintain all interstate movements and local connections to the Covington and Cincinnati street networks for both trucks and cars.
054	Right-of-Way	08/12/2022 - Web Comment - Hello, I have a rental property on W 12th St in Covington and I'm wondering if it might be taken for the new bridge. How and when will property owners be notified of this? The property is currently empty so I could make some major improvements, but don't want to do so if it will just be torn down in a year or two. The address is [REDACTED].	The property located at 635 W 12th Street in Covington is not within the construction limits of the proposed project and will not need to be acquired. KYTC has begun notifying some of the owners of properties impacted by construction of the BSB Corridor Project by certified letter. We will continue to work through this process and will conduct follow up meetings with individual property owners.

Comment Number	Nature of Comment	Comment	Response
055	Information Request	08/23/2022 - Web Comment - Please make Appendix E to the 2012 EA available. Thanks.	Appendix E was provided to this individual on 9/26/2022
056	Roadway/Bridge Design	08/22/2022 - Web Comment - I thought the new bridge design was a suspension bridge.	The new bridge design may be an arch bridge or a cable-stayed bridge. The decision about the final bridge design will be made during the project's design-build stage.
057	General Support	08/29/2022 - Web Comment - Keep at it.	Comment noted.
058	Project Mailing List	09/13/2022 - Web Comment - Please include me in the distribution list. Thanks.	This individual was added to the project mailing list.
059	Information Request	09/22/2022 - Web Comment - Can you share the engineering study appendices per the documents web page states they are available upon request. Thank you.	The requested information was provided to this individual on 9/26/2022
060	Alternatives	<p>10/08/2022 - Email - [Edited for brevity] The Brent Spence Bridge Corridor Project now has a chance to start because of the Bipartisan Infrastructure Law. However, this project is only the beginning of the necessary interstate infrastructure projects not only in the Cincinnati region but for the interstate system in Ohio, Kentucky, and across the nation, as even this project will reach traffic capacity design limits in the next 20 years, truth both for I-75 and I-70. I am advocating planning for alternatives to Interstate 75 and Interstate 70, both transcontinental routes, that will interconnect Midwestern states to the ports of the Atlantic and Pacific more directly ...</p> <p>Unfortunately, ODOT and KYTC have limited options when it comes to improving I-75 capacity. The Brent Spence Bridge is now 60 years old and functionally obsolete, and MUST be replaced, no question. I-75 will have to be widened by at least one lane in either direction through Cincinnati. However, this will also reach its limit in 20 years. Some proposals like the 4-lane Eastern Bypass are expensive and functionally obsolete from the beginning. Building I-75 subsurface 60 years after it was first build will be excessively expensive and produce a highway with a dangerous steep grade between the bridge and the subsurface section. ODOT was correct to reject both plans ...</p> <p>Three Future Interstate Corridors should be created by Congress as alternative traffic routes for I-75 and I-70: 1) Future I-73 2) Future I-66 3) Future I-50/Future I-60/Future I-70.</p>	<p>Comment noted.</p> <p>Potential future Interstate corridors are beyond the purpose and need and scope of the BSB Corridor Project.</p>

Comment Number	Nature of Comment	Comment	Response
061	Information Request	<p>10/09/2022 - Web Comment - Hello, I am requesting that the public comment paper titled "Working Position Paper: Redesign of the Brent Spence Bridge Project" by Ryan Laber, P.E as provided to ODOT and KYTC via email on 1/17/2022 be provided to me via email and made available on the project website for public review. Currently, the proposition paper is not available under the documents section of this website.</p> <p>It is not possible for one to make an informed opinion of the project team's response ("Ohio Department of Transportation Response to Public Comment: Working Position Paper: Redesign of the Brent Spence Bridge Project", dated approx. 10/1/2022) to this public comment without making the original public comment available.</p>	<p>The "Working Position Paper" was provided as requested.</p> <p>Because the "Working Position Paper" was not prepared by KYTC or ODOT as part of the BSB Corridor Project development but rather was submitted as a public comment, it will not be posted in the document center of the project website. However, it will continue to be provided upon request.</p>
062	Information Request	<p>10/19/2022 - Web Comment - Is it possible for you to send through a copy of the Appendices A to F that support the Design Summary report please?</p>	<p>The appendices to the Design Summary Report were provided as requested.</p>
063	Project Footprint	<p>10/25/2022 - ODOT Web Site - There is a once in a lifetime opportunity to reimagine this corridor. Please listen and hear citizens and community members who are loudly expressing interest in almost any other plan than the current. Giving the land that is current a sprawl of urban highway, back to the city would create some much opportunity for our city. At the very least, please add caps to the current plan. Additional retail and pedestrian space replacing the current mess would lead to so much growth for our city, both in pride and in dollars, similar to the banks. There are unfortunately many who are unaware of this project, and that alternatives actually exist, but who would absolutely cherish new city space.</p>	<p>ODOT has incorporated several features that reduce the project's footprint, including optimizing interchange geometry by utilizing the land formerly occupied by the Dunnhumby building, reducing shoulder widths, and reducing the width of the companion bridge. In addition, 10 acres of excess ODOT property adjacent to Cincinnati's Central Business District can be transferred to the City of Cincinnati upon the project's completion. Finally, ODOT is continuing to coordinate with the City of Cincinnati to identify other refinements that will open up additional land to support Cincinnati in advancing its urban redevelopment goals.</p>
064	Traffic Project Footprint	<p>10/25/2022 - ODOT Web Site - The dual bridge plan will only invite more traffic to the highways thus negating any initial positive gains in how long it takes to get through this section of the city. We need to be using this federal funding to find ways to make traveling off interstates more efficient and accessible. Please rethink this plan and find ways to give back public/private land and encourage public transportation.</p>	<p>The Brent Spence Bridge Corridor Project has been designed to support traffic projected to travel in the corridor through the year 2050, which is the standard design horizon for large infrastructure projects. As currently planned, 10 acres of excess ODOT property adjacent to Cincinnati's Central Business District can be transferred to the City of Cincinnati upon the project's completion. ODOT is continuing to coordinate with the City of Cincinnati to identify other refinements that will open up additional land to support Cincinnati in advancing its urban redevelopment goals. The project will construct sidewalks, shared use paths, and bike lanes that will connect to existing public transit stops.</p>

Comment Number	Nature of Comment	Comment	Response
065	General Opposition	10/25/2022 - ODOT Web Site - The Brent Spence should be sunk; the period in which the Brent Spence was not operating was functional and superior than normal operations. Through trucking should *not* go through the Cincinnati basin Short of this, the bridge should be rail and bike/ped only; there is no valid reason for automobiles and trucks to pass through the city at this juncture.	Your comments have been forwarded to the project team for their consideration and will be included in the public record.
066	Project Footprint Project Development Process	10/25/2022 - ODOT Web Site - Hello! If we're spending \$3 Billion on a vanity project for ODOT, we should at least do something worthwhile with it. This project gives us the opportunity to reduce the footprint of I-75 through downtown Cincinnati, correcting a terrible mistake made decades ago, and reconnecting a neighborhood decimated by racist transit policy. ODOT's refusal to consider any changes to their decades old plan, and refusal to further engage with residents or the City disgusts me as a lifelong Ohioan. Your response to the "Westway Emails" was filled with underhanded disrespect and proves how little you care about the input of regular Ohioans. Ten years is an incredibly long time; needs and wants change. Has ODOT approached the City at all within the past two-years to see if their preference to maintain the Downtown connections to I-75 has changed? Has ODOT truly considered significant deviation from the plans made 10 years ago? If so, I would like for those to be made public. Those alternatives should be presented and considered on equal ground as the current proposal. ODOT's handling of this project has been sorely disappointing. Be better.	<p>ODOT has coordinated closely with the City of Cincinnati during all stages of the project's development, including reductions to the project's footprint, access to downtown, redevelopment goals, aesthetics, multimodal accommodations, and other items. ODOT's partnership with the City of Cincinnati will continue through the project's design and construction.</p> <p>Since 2012, ODOT and KYTC have completed a number of additional studies and incorporated refinements into the preferred alternative for the Brent Spence Bridge Corridor Project. The documentation of these efforts is publicly available on the project website at: www.brentspencebridgecorridor.com. In addition, the public will be provided the opportunity to review project materials and provide formal comments at public hearings currently scheduled for July/August 2023.</p>
067	Traffic	10/25/2022 - ODOT Web Site - The Brent Spencer Bridge could certainly use a facelift, however traffic crossing the bridge has been dropping for several years. It is unclear why an entirely new bridge is needed. If the desire is to improve traffic conditions, why are new designs not considering removing on/off ramps that are often the cause for such congestion. The removal of these ramps would greatly benefit the cities in both sides of the river.	<p>The Brent Spence Bridge Corridor Project has been designed to support traffic projected to travel in the corridor through the year 2050, which is the standard design horizon for large infrastructure projects.</p> <p>While the project must improve traffic flow on I-75, it must also maintain ramps that provide local access and mobility. ODOT is continuing to coordinate with the City of Cincinnati to refine local ramps in support of local accessibility, access, and redevelopment goals.</p>

Comment Number	Nature of Comment	Comment	Response
068	Roadway/Bridge Design	10/25/2022 - ODOT Web Site - I urge the project planners to consider the redesign of the "highway spaghetti monster" proposed by Bridge Forward. This project would cost comparatively the same while increasing the amount of prime real estate available in the downtown Cincinnati area. This is an opportunity cost that could effectively pay for itself over a 50 year period. In my opinion and the opinion of most rational tax payers, this would be the best use of tax dollars in a project of this magnitude and permanence.	ODOT carefully evaluated the designs offered by the Bridge-Forward Cincinnati Coalition. A detailed response is provided on the project website: HERE
069	Project Footprint Quality of Life	10/25/2022 - ODOT Web Site - I would like to see this project do it's best to right the wrongs of highway planning. Reconnecting previously unconnected areas, shrinking the size of the highway and interchange footprint, and possibly adding mass transit to the mixture. I75 is currently a blight on the city of Cincinnati, especially near downtown and the West End, and with this project, it could be transformed to mitigate the negative effects of urban freeways.	<p>While the project's primary needs are to solve transportation problem, ODOT has also pursued other quality of life goals in the project's development. To that end, ODOT has intentionally incorporated features that reduce the project's footprint, promote and improve bicycle and pedestrian connectivity, improve water quality by separating storm water from the sanitary system, balance regional and local traffic needs, and reduce community impacts throughout the project's development.</p> <p>The project will reduce congestion and improve reliability for local bus routes, which use the Brent Spence Bridge for 210 trips every weekday. In addition, improved and new sidewalks, shared use paths, and bicycle lanes will improve connections to existing bus stops. Finally, future accommodation of bus service along the corridor's shoulder will not be precluded by the project.</p>
070	Public Input	10/25/2022 - ODOT Web Site - Is there going to be an opportunity for public input on what the design of the bridge and surrounding interchange is going to be? Many in the community believe an alternate design should be considered that buries the highway and reconnects downtown similar to Fort Washington way and we would like an opportunity to provide our input.	<p>The public will be provided the opportunity to review project materials and provide formal comments at public hearings currently scheduled for July/August 2023.</p> <p>ODOT has evaluated options for depressing I-75 similar to Fort Washington Way based on public comments received to date. Responses to those comments can be viewed on the project's website: HERE and HERE</p>
071	Roadway/Bridge Design	10/25/2022 - ODOT Web Site - Right size and cap the highway between 4th and Ezzard Charles!!! The people of Cincinnati want downtown to be reconnected to our west side!! Most of it is already at the required depth to cap!!!	ODOT has evaluated options for depressing I-75 and constructing an urban street grid in Cincinnati based on public comments received to date. A response to those comments can be viewed on the project's website: HERE

Comment Number	Nature of Comment	Comment	Response
072	Traffic	10/25/2022 - ODOT Web Site - The traffic studies from 2021 for this interchange show that no more than 9% southbound and 5% northbound traffic actually use any of the on or off ramps to downtown. Does this meet any current ODOT minimums for requiring these ramps?	ODOT does not have a minimum required traffic volume for interstate ramps and is continuing to coordinate the project's design with the City of Cincinnati to support local access and mobility needs.
073	Project Footprint	10/25/2022 - ODOT Web Site - Looking over the project there feels like a significantly better way to accomplish the goals set forward while still giving more land back to Cincinnati, I would not proceed with the project, it will be a disaster	Your comments have been forwarded to the project team for their consideration and will be included in the public record.
074	Project Footprint Roadway/Bridge Design	10/26/2022 - ODOT Web Site - Not shrinking the footprint of the project, similar to Fort Washington way would be a multi-generational mistake.	Your comments have been forwarded to the project team for their consideration and will be included in the public record.
075	Project Footprint	10/25/2022 - ODOT Web Site - This plan is a continued overbuild of a highway system through an urban area. We have an opportunity to improve flow AND reduce the footprint of the highway and you are doing neither. It's just more lanes. How many studies prove the uselessness of that until we learn? Scrap it and start over.	Your comments have been forwarded to the project team for their consideration and will be included in the public record.
076	Roadway/Bridge Design	10/26/2022 - ODOT Web Site - I have been following this project for over a decade. I believe that the planning and public outreach for this project has been inadequate from the start. The project has been under development for so long that society's values have changed. The availability of funding is not a good reason to skip proper planning. The potential to return land to the urban core should be studied and a true conversation should be had about the impact of urban highways on the city should be acknowledged and mitigated. Are you actually open to design changes that would improve urban form? Or is this just a box checking exercise? Have modern views on urban highways been incorporated into this decades long project? How?	<p>ODOT has incorporated several features that reduce the project's footprint. As currently planned, 10 acres of excess ODOT property adjacent to Cincinnati's Central Business District will be transferred to the City of Cincinnati upon the project's completion. ODOT is also continuing to coordinate with the City of Cincinnati to identify other refinements that will open up additional land to support Cincinnati in advancing its urban redevelopment goals.</p> <p>In collaboration with the City of Cincinnati, ODOT has also incorporated features that support urban form, such as enhanced streetscape amenities, aesthetic treatments, and bicycle and pedestrian infrastructure to improve connectivity to transit, employment, healthcare, cultural, recreational, and commercial destinations.</p>

Comment Number	Nature of Comment	Comment	Response
077	Transit Multimodal Project Footprint Roadway/Bridge Design	<p>10/26/2022 - ODOT Web Site - 1.Cincinnati has made great gains in growing the pedestrian path network along the Ohio riverfront and this new bridge should include pedestrian/bike facilities also. Adequate protection measures and lighting should be added to the design accordingly. This can absolutely be accomplished on a limited access vehicle bridge ... [edited for brevity]. 2.New bridge should include designated transit lanes for bus and capacity for future rail including designated ROWs for a rail approach on the Ohio & Kentucky sides of the bridge. Transit usage is growing in Cincinnati: the streetcar is setting ridership records every month this year, a BRT plan is being finalized now, the 2021 Infrastructure Investment and Jobs Act included funding for passenger rail expansion which could be used to restart the 3C+D Amtrak rail service, a transit connection between Cincinnati and CVG is consistently the most-requested routes in planning discussions ... [edited for brevity] ... In summary, at the very least it should be demonstrated that the new bridge is future-proofed to add bus & rail transit when those efforts become a priority. I encourage your team to please revisit the proposal set forth by Vision Zero Cincinnati to expand transit in this river crossing. This proposal has been endorsed by leaders of City of Cincinnati and merits consideration. 3.Every effort should be made to reduce the physical footprint of the highway, on/off ramps, and associated structures. The "spaghetti" of highways and fly-over ramps next to downtown Cincinnati is confusing and dangerous for drivers to navigate, a physical boundary to East-West travel & development, a poor use of valuable land, and an eyesore to anyone who experiences it. The reduction of the highway footprint should prioritize returning contiguous, usable, parcels of land back to the city. Small slivers of freed-up property here & there are useless for any new meaningful purpose and will only become more grass for ODOT to mow. I encourage your team to please revisit the proposal set forth by the Bridge Forward Coalition to reduce the highway's physical footprint to return entire blocks of land to the city for new housing, offices, and business uses. This proposal has been endorsed by leaders of City of Cincinnati and merits consideration.</p>	<p>Thank you for submitting your comments regarding this project to the Ohio Department of Transportation. The project will reduce congestion and improve reliability for local bus routes, which use the Brent Spence Bridge for 210 trips every weekday. In addition, improved and new sidewalks, shared use paths, and bicycle lanes will improve connections to existing bus stops. Finally, future accommodation of bus service along the corridor's shoulder will not be precluded by the project. Pedestrian and bicycle accommodations are not planned on the new companion bridge because of the proximity of a reasonable crossing at the Clay Wade Bailey Bridge. Furthermore, any new rail crossings of the Ohio River will need to be evaluated as part of a separate project.</p> <p>ODOT carefully evaluated the designs offered by the Bridge-Forward Cincinnati Coalition. A detailed response is provided on the project website: HERE</p>

Comment Number	Nature of Comment	Comment	Response
078	Roadway/Bridge Design	10/26/2022 - ODOT Web Site - The Bridge Forward design meets and exceeds the proposed plans for the bridge. It has the potential to spend less money, reduces the impact of the highway on our city and environment, and provides better outcomes for the community while still achieving the desired traffic improvements. At the very least concepts from the Bridge Forward design should be reviewed and potentially incorporated.	Thank you for submitting your comments regarding this project to the Ohio Department of Transportation. ODOT carefully evaluated the designs offered by the Bridge-Forward Cincinnati Coalition. A detailed response is provided on the project website: HERE
079	Project Footprint Multimodal	10/26/2022 - ODOT Web Site - I do not think expanding the already massive footprint of the interstate benefits the people of the area or the cities of Cincinnati and Covington. The massive expansion proposed will limit opportunities for growth and further divide Cincinnati's urban core. There is no need for that many entrance and exit ramps. People can drive an extra few blocks to get into the interstate. Also, the new companion bridge or the Brent Spence could have some pedestrian and bicycle infrastructure added. Overall, the project seems to aim to create even more disruption and hostile infrastructure sprawl into the heart of our city, rather than connecting our area in a way that benefits the people who live here.	Thank you for submitting your comments regarding this project to the Ohio Department of Transportation. ODOT has incorporated several features that reduce the project's footprint, including optimizing interchange geometry by utilizing the land formerly occupied by the Dunnhumby building, reducing shoulder widths, and reducing the width of the companion bridge. In addition, 10 acres of excess ODOT property adjacent to Cincinnati's Central Business District will be transferred to the City of Cincinnati upon the project's completion. Finally, ODOT is continuing to coordinate with the City of Cincinnati to refine local ramps in support of local accessibility, access, and redevelopment goals. Pedestrian and bicycle accommodations are not planned on the new companion bridge because of the proximity of a reasonable crossing at the Clay Wade Bailey Bridge.
080	Design-Build Process	10/28/2022 - Web Comment - Hello there! Singh & Associates, Inc. is very interested in participating on this transformational project. As an Ohio Unified Certification Program (UCP) Disadvantaged Business Enterprise (DBE), our firm can help the project team fulfill the DBE participation goals, provide high-quality engineering services for roadway lighting/electrical, and bring new firm expertise and services to the Ohio & Kentucky transportation markets. Please reach out directly if there are ways to position our firm to be on the progressive design-build teams that will be pursuing this once-in-a-generation opportunity.	Information regarding the progressive design-build procurement can be accessed on the BSB Project Procurement Information website: www.dot.state.oh.us/Divisions/ConstructionMgt/design-build/bsb-project-procurement/ Questions and comments can also be submitted on the procurement website.

Comment Number	Nature of Comment	Comment	Response
081	Roadway/Bridge Design Project Footprint	11/1/2022 - ODOT Web Site - It is critical to the future of our city and region that we do not proceed with a plan that doesn't fully leverage the immense amount of tax money being spent on it. Reducing and trenching the proposed highways and ramps to return land to productive use, reconnect the street grid, and put the focus back in people and not vehicles is important to allowing the city to grow for the next 50 years. Reducing the negative impact of vehicles on our cities where we live, work, and play is possible and this project can be transformative in a positive way, but only if we don't keep designing highways like we have in the past.	Thank you for submitting your comments regarding this project to the Ohio Department of Transportation. ODOT has evaluated options for trenching I-75 based on public comments received to date. Responses to those comments can be viewed on the project's website: HERE and HERE
082	General Opposition	11/1/2022 - ODOT Web Site - We don't need a highway interchange downtown. Such a waste of space.	Thank you for submitting your comments regarding this project to the Ohio Department of Transportation. Your comments have been forwarded to the project team for their consideration and will be included in the public record.
083	Roadway/Bridge Design Project Footprint	11/1/2022 - ODOT Web Site - Asking planners to consider the proposal put forward by the Bridge Forward project. This is an historic opportunity to reconnect neighborhoods destroyed as part of the original I-75 build. If a companion bridge is to be built, it should majorly reduce the highway footprint connecting to downtown Cincinnati to allow for a modern, well-connected, and multimodal urban environment. No more "spaghetti mess"!	<p>Thank you for submitting your comments regarding this project to the Ohio Department of Transportation. ODOT carefully evaluated the designs offered by the Bridge-Forward Cincinnati Coalition. A detailed response is provided on the project website: HERE</p> <p>[UPDATED RESPONSE]</p> <p>ODOT has incorporated several features that reduce the project's footprint, including optimizing interchange geometry by utilizing the land formerly occupied by the Dunnhumby building, reducing shoulder widths, lowering design speeds, and reducing the width of the companion bridge. In addition, 10 acres of excess ODOT property adjacent to Cincinnati's Central Business District can be transferred to the City of Cincinnati upon the project's completion. Most recently, ODOT and the City of Cincinnati collaborated on refinements to the Downtown Cincinnati ramps that will open up an additional 9.5 acres of land for potential redevelopment. ODOT will continue to work with the City of Cincinnati throughout the remaining design activities to identify opportunities to incorporate refinements that reclaim land and improve mobility for all modes of transportation.</p> <p>The project will install bicycle and pedestrian infrastructure in and between the CBD, Queensgate, and the West End. Pedestrian and bicycle connections will be included across I-75 on 6th Street, 7th Street, 9th Street, Linn Street, Freeman Avenue, Ezzard Charles, Liberty Street, Findlay Street, Bank Street, and Harrison Avenue. In</p>

Comment Number	Nature of Comment	Comment	Response
083 (cont.)	Roadway/Bridge Design Project Footprint (cont.)		<p>addition, a new shared use path will be constructed along Winchell Avenue between 9th Street and Ezzard Charles Drive, including a pedestrian bridge connection to Freeman Avenue.</p> <p>ODOT is closely coordinating the aesthetic plans for the project with the City of Cincinnati to further its goals to create vibrant urban spaces in locations throughout the corridor.</p>
084	Roadway/Bridge Design Project Footprint	<p>11/1/2022 - ODOT Web Site - I feel very strongly that ODOT needs to do whatever is possible to reclaim land in any Brent Spence Bridge corridor plan. Downtown Cincinnati has effectively been walled-off from westward expansion by the I-75 highway and all of its associated ramps and exits. This is a once in a lifetime opportunity to address past mistakes that led to displacement and disinvestment of the downtown core. Any plans to create a new bridge and traffic pattern should take care to maximize the public benefit, and shaving a few seconds off of travel time, on average, to someone's trip should not be the priority when comparing to the opportunity to make a truly transformational project. The Bridge Forward plan is a great starting point for this.</p>	<p>Thank you for submitting your comments regarding this project to the Ohio Department of Transportation. ODOT carefully evaluated the designs offered by the Bridge-Forward Cincinnati Coalition. A detailed response is provided on the project website: HERE</p> <p>ODOT has incorporated several features that reduce the project's footprint, including optimizing interchange geometry by utilizing the land formerly occupied by the Dunnhumby building, reducing shoulder widths, and reducing the width of the companion bridge. In addition, 10 acres of excess ODOT property adjacent to Cincinnati's Central Business District will be transferred to the City of Cincinnati upon the project's completion. Finally, ODOT is continuing to coordinate with the City of Cincinnati to refine local ramps in support of local access and redevelopment goals.</p>
085	Roadway/Bridge Design Project Footprint	<p>11/1/2022 - ODOT Web Site - The Brent Spence Bridge Corridor Project as it is currently proposed would be a disaster for Cincinnati and for all its residents, and it is absolutely crucial that ODOT reconsider its plans and engage with the local community. The project area, Interstate 75 between the Western Hills Viaduct and the Ohio River, was home to tens of thousands of majority Black and working class residents when the highway was originally built. The construction of the highway displaced nearly all of these people, and turned a once-thriving neighborhood into a wide expanse of concrete, parking lots, and frontage and collector roads. This has had numerous devastating consequences for the citizens of Cincinnati; chief among them is the disastrous effect this has had on the livability of neighborhoods in the basin. The highway serves as a near-impassable moat separating Queensgate and the West End, and the surrounding streets are wide arterials meant to handle high speed traffic coming off the highways. In neighborhoods where a majority of residents do not own a car and rely on walking and biking for transportation, the highway and these roads are major safety barriers</p>	<p>Thank you for submitting your comments regarding this project to the Ohio Department of Transportation. Your comments have been forwarded to the project team for their consideration and will be included in the public record.</p> <p>[UPDATED RESPONSE]</p> <p>In November and December 2022, ODOT and KYTC held 12 small-scale outreach meetings in neighborhoods directly adjacent to the project's construction limits to share updates on the project and to offer residents the opportunity to share feedback with the project team. In December, ODOT and KYTC held one daytime and one evening broad-scale neighborhood outreach meeting in each state to engage neighborhoods that are near the project corridor but will not be directly impacted. All comments received are considered in the project's development, and responses are provided for every comment.</p>

Comment Number	Nature of Comment	Comment	Response
085 (cont.)	Roadway/Bridge Design Project Footprint (cont.)	<p>for accessing jobs and homes. Moreover, the transformation of hundreds of acres of the city's urban core into highway related development has had a dramatic negative impact on the city's economic and social development. Not only does it significantly decrease the amount of revenue-generating land in the city's most valuable real estate, but the city is left to spend millions every year financing the supportive infrastructure for the highway - widening roads and building parking structures to accommodate the increased traffic the highway brings - while not collecting any revenue from the highway itself. All of this, for a highway that exists predominantly to support non-local through-traffic as a shipping corridor. That Cincinnati should bear the burden of supporting such a massive piece of infrastructure to benefit its own citizenry would be one thing, but to bear the burden in support of truckers and through-haulers who largely do not contribute to Cincinnati's local economy at all is ridiculous on its face. Interstate 275 was built as a bypass for a reason - it is only 5-10 minutes longer to get from the north side of the loop to the south side via 275 than it is via 75, a negligible difference for through traffic. If ODOT were serious about proposing economically efficient and safety-conscious solutions to the current traffic regime, they would construct toll gates immediately inside the 275 loop on Interstates 75, 71, and 471, that would toll drivers only after they have passed through both a north and south toll. This would encourage through-traffic to use the 275 Loop to bypass Cincinnati without penalizing local drivers who actually live here and support our local economy. The reduced traffic (largely from freight trucks) would then allow the current Brent Spence Bridge to be re-stripped back down to 3 lanes per deck as it was originally designed, making the bridge safe by current design standards without seeing significant congestion increases. However, as ODOT seems intent on building a new companion bridge come hell or high water, it would be beneficial to at least consider community input before spending billions to dramatically change the landscape of our community. Local engineers, planners, and activists have come together to propose the Bridge Forward plan, a proposal that would allow the companion bridge to be built while also reclaiming usable land for the city and allowing our community to grow and develop in a way that it has not been able to since the highway was built over 50 years ago. While the decision makers at ODOT are largely unelected positions, the Bridge Forward plan has large grassroots support from both</p>	<p>ODOT and KYTC are currently preparing a supplemental Environmental Assessment (EA) that will include a summary of all public involvement activities, including comments received. Once the supplemental EA is made available to the public, hearings will be scheduled to provide additional opportunities for public feedback. More opportunities for public feedback will occur during the project's design-build phase.</p> <p>The project will install bicycle and pedestrian infrastructure in and between the CBD, Queensgate, and the West End. Pedestrian and bicycle connections will be included across I-75 on 6th Street, 7th Street, 9th Street, Linn Street, Freeman Avenue, Ezzard Charles, Liberty Street, Findlay Street, Bank Street, and Harrison Avenue. In addition, a new shared use path will be constructed along Winchell Avenue between 9th Street and Ezzard Charles Drive, including a pedestrian bridge connection to Freeman Avenue. The pedestrian and bicycle facilities incorporated into the project will improve connectivity to transit, employment, healthcare, cultural, recreational, and commercial destinations.</p> <p>The project is designed to accommodate traffic projected to travel through the corridor by the year 2050 based on a regional travel-demand model. The Brent Spence Bridge Corridor incorporates a collector-distributor system specifically designed to improve traffic flow and accessibility for local traffic. Traffic will access downtown Cincinnati on a lower speed (45 mph) collector-distributor system, and the existing Brent Spence Bridge will be returned to three standard lanes with shoulders.. To promote safety for bicycles and pedestrians, the ramp connections with local streets are being designed as lower-speed urban roadways. The design of the intersections of local streets will be in accordance with City of Cincinnati design standards.</p> <p>ODOT has incorporated several features that reduce the project's footprint, including optimizing interchange geometry by utilizing the land formerly occupied by the Dunnhumby building, reducing shoulder widths, lowering design speeds, and reducing the width of the companion bridge. ODOT and the City of Cincinnati collaborated on refinements to the Downtown Cincinnati ramps that will open up an additional 9.5 acres of land for potential redevelopment. ODOT will continue to work with the City of Cincinnati throughout the</p>

Comment Number	Nature of Comment	Comment	Response
085 (cont.)	Roadway/Bridge Design Project Footprint (cont.)	<p>community members and actual elected officials, including the mayor and city council of Cincinnati. It is my understanding that this plan was presented to ODOT during a community feedback period and I must admit I find ODOT's official response to be upsettingly dismissive and flippant. It is clear in their response that ODOT did not seriously consider the plan or its technical aspects, often quoting misleading or outright false aspects of the plan to justify their non-consideration. For example, in their response ODOT cited a proposed 4th Street overpass over the highway as a reason the plan could not be constructed, as the highway grade would not support an overpass at this location. However, Bridge Forward has clarified that the 4th Street overpass was included in promotional material but not in technical material or the presentation given to ODOT. Additionally, many statewide and ODOT officials have claimed that such a proposal would require a complete reworking of environmental review studies and preliminary approval documents, which would unacceptably delay construction timelines. This is also not true - Bridge Forward has completed relevant environmental studies and the plan can be approved while federal funds are still available for construction - but even if it were true, it lies squarely on ODOT that plans for the companion bridge have been sitting in public record for nearly a decade with no public comment, and community feedback was only accepted *after* it has been decided that any significant changes to the proposed plan would be unacceptable. In all, I find the way that ODOT has handled the process for this project to be shameful, destructive, and wholly undemocratic. As it stands, statewide officials would have us spend billions in taxpayer dollars to double-down on the destructive mistakes of the last century and further damage our community for the sake of non local freight benefactors, all while intentionally delaying public comment until it is too late to be meaningful and flippantly dismissing the serious concerns raised by the community when they are given. I am imploring ODOT to reconsider its approach to this project and give serious consideration to the feedback it has received from the residents of this city who want their public infrastructure to reflect the values and interests of their community.</p>	<p>remaining design activities to identify opportunities to incorporate refinements that reclaim land and improve mobility for all modes of transportation.</p> <p>Tolling the I-71/I-75 corridor is not permitted in the State of Kentucky. Reducing congestion and maintaining connections to key regional and national corridors are primary goals of the project. This includes accommodating freight traffic that is using the interstate system. Diversion of trucks would place them on other routes that are also experiencing congestion. Furthermore, origin and destination studies indicate that 30 percent of the trucks in the Brent Spence Bridge corridor travel to and from local routes, serving an important economic link for the City of Cincinnati.</p> <p>ODOT carefully evaluated the designs offered by the Bridge-Forward Cincinnati Coalition with input from a multidisciplinary group of roadway engineers, traffic engineers, and environmental professionals and documented the findings in a detailed response. (provided on the project website HERE.) ODOT has also scheduled a working session with Bridge-Forward to discuss their ideas about the project.</p>

Comment Number	Nature of Comment	Comment	Response
086	Roadway/Bridge Design Project Footprint	11/2/2022 - Email Comment - West End Community Council Support for Bridge Forward Design Recommendations for the Brent Spence Bridge Project and Feasibility Assessments [edited for brevity] Please accept this notice as full documented support from the West End Community Council for the Brent Spence Bridge project design improvements proposed by the Bridge Forward Coalition and for the appropriate feasibility assessments to be conducted.	Thanks for passing on your document concerning the Brent Spence Project design. Our project team is looking forward to discussing the project with your community on November 22nd. We are continuing to work on the Brent Spence Bridge Corridor project with our stakeholders to capture opportunities to address shared community priorities. We look forward to sharing our progress with your community later this month.
087	Project Development Process	11/9/2022 - Web Comment - Cincinnati DOTE recently asked to be a contributing partner with ODOT on this project so that the city's needs are met with this plan. Why were they rejected for this? Sen. Sharrod Brown just released a public statement that he hopes ODOT acts in good faith with the communities bear the BSB corridor. Why are you not listening to the people that will be affected by this??? Why did the public comment section of this website go down????	<p>ODOT has been closely coordinating with the City of Cincinnati, including the Department of Transportation Engineering (DOTE), during all stages of the project's development. Most recently, ODOT and the City collaborated on refinements to the Downtown Cincinnati ramps that will open up an additional 9.5 acres of land for potential redevelopment. ODOT and the City are continuing to collaborate on aesthetic treatments and pedestrian and bicycle facilities in the finished corridor. Furthermore, the City will review the Statements of Qualifications (SOQ) for the progressive design-build team and provide feedback to the project evaluators, ensuring that the priorities and perspectives of local government are reflected in the selection process. ODOT will also collaborate with the City on other key project considerations such as the maintenance of traffic and incident management plans for construction.</p> <p>ODOT had previously established individual websites for the different construction phases of the project. The decision was recently made to redirect all websites to the general project website. This will help to ensure consistent information is shared for the Brent Spence Bridge Corridor Project and to streamline the comment and response process. Comments are now, and will continue to be, accepted on this general project website. The individual ODOT websites will be reactivated to provide up-to-date information for each phase during construction.</p>

<p>088</p>	<p>Public Input Project Development Process Traffic</p>	<p>11/9/2022 - Web Comment - Why is there no public comment on this project? Why are you pushing the city out of the planning of this project? This project is unnecessary because traffic has remained the same for decades on the bridge, by expanding it you will encourage more traffic and we need to reduce cars on the road and encourage alternate forms of transportation, like biking and bus.</p>	<p>Public feedback has been an important part of the development of the Brent Spence Bridge Corridor Project. Several public meetings and two public hearings were held during the development of the preferred alternative. Most recently, ODOT has carefully evaluated and responded to proposals submitted as part of the public comment process. In addition, ODOT and KYTC are currently working to gather feedback about the project at the neighborhood level. In addition, the public will be provided the opportunity to review project materials and provide formal comments at public hearings currently scheduled for July/August 2023.</p> <p>ODOT has coordinated closely with the City of Cincinnati during all stages of the project's development, including reductions to the project's footprint, access to downtown, redevelopment goals, aesthetics, pedestrian and bicycle facilities, and other items. ODOT's partnership with the City of Cincinnati will continue through the project's design and construction.</p> <p>The Brent Spence Bridge Corridor Project has been designed to support traffic projected to travel in the corridor through the year 2050, which is the standard design horizon for large infrastructure projects.</p> <p>[UPDATED RESPONSE]</p> <p>In November and December 2022, ODOT and KYTC held 12 small-scale outreach meetings in neighborhoods directly adjacent to the project's construction limits to share updates on the project and to offer residents the opportunity to share feedback with the project team. In December, ODOT and KYTC held one daytime and one evening broad-scale neighborhood outreach meeting in each state to engage neighborhoods that are near the project corridor but will not be directly impacted. All comments received are considered in the project's development, and responses are provided for every comment. Copies of all comments and responses are available on the project website HERE.</p> <p>ODOT and KYTC are currently preparing a supplemental Environmental Assessment (EA) that will include a summary of all public involvement activities, including comments received. Once the supplemental EA is made available to the public, hearings will be scheduled to provide additional opportunities for public feedback. More opportunities for public feedback will occur during the project's design-build phase.</p>
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Comment Number	Nature of Comment	Comment	Response
089	Traffic	11/10/2022 - Web Comment - Is it possible to design semi-truck only express lanes with barriers starting well north of the new bridge (say before Ezra Charles) and exiting well south (say after Kyles Lane) to limit these trucks mixing in with local traffic while providing the truckers with a quicker route through the bridge congestion?	Origin and destination studies conducted for the Brent Spence Bridge Corridor indicate that trucks traveling in the corridor come from several locations, including I-71, I-75, US-50, I-471, with around 30 percent traveling to and from other local routes. Building barrier-separated truck lanes from Ezzard Charles Drive to Kyles Lane would substantially increase the project's footprint and costs while only accommodating a relatively small portion of the truck traffic in the corridor.
090	Information Request	11/11/2022 - Web Comment - In the Brent Spence Bridge Fast Facts there is an duplicated fact. The first one is the same as the last one.	Thank you for bringing this to our attention. Our communications team has been made aware of the duplication, and it will be corrected.
091	Aesthetics	11/12/2022 - Web Comment - Brent Spence Bridge Corridor Aesthetics Committee - Committee Members Traveling north on I75 and I71 to Dayton and Columbus almost all overpasses have a pleasing welcoming message artfully designed on the overpasses. Traveling south into Kentucky the Kyles Lane overpass is the first a motorist encounters. With your leadership combining State of Kentucky officials and City of Fort Wright officials a very decorative welcoming and pleasing message can be created welcoming motorist to both Kentucky and Fort Wright. This would be a very long-lasting positive impression these motorists will receive. Please consider tackling this endeavor, it will be worth wile for the State and the City.	KYTC has been collaborating with City of Fort Wright officials and the project Aesthetics Committee to develop aesthetic guidelines for the Brent Spence Bridge Corridor. While trying to maintain a consistent theme throughout the corridor, we are working with the City of Fort Wright to establish design items and finishes that also tie into existing elements within the City. Items being explored include landscaping, streetscapes, gateways, and façade treatments for piers/abutments/retaining walls/noise walls. Multiple aesthetics meetings will be held throughout the design process to finalize plans for aesthetics throughout the corridor.
091A	Project Development Process	11/14/2022 - Email Comment - [Pertaining to the response to Comment 087] That 9.5 acres was already on the original plan and from what I've heard from sources in city hall, ODOT has refused the city's request to be a contributing member for this project. Please allow Cincinnati to have a larger say in what happens in our city. That plan unveiled does nothing to help connect downtown to the neighborhood just west of the highway. None of the things that the city has requested have been accepted by ODOT. Please allow Cincinnati and it's residents directly affected are by this to the table. There has been no real public engagement other than online forms. When ODOT meets with the public on Nov 29 it will be very apparent that no one outside of ODOT likes this plan.	Comment noted.

Comment Number	Nature of Comment	Comment	Response
092	Project Mailing List	11/18/2022 - Web Comment - I would like to stay up to date with the project.	This individual was added to the project mailing list.
093	Public Input	11/19/2022 - Email Comment - Thank you for coming to speak to us. I have a couple of quick questions: 1. Can community input still affect the design of the bridge; and 2. If so, how is community input weighted?	<p>Hello! Thank you for allowing us to join Camp Washington's Community Council Meeting on December 12. ODOT and KYTC are currently in the process of meeting with all the neighborhoods that are directly adjacent to the Brent Spence Bridge Corridor to share project details, answer questions, and receive feedback from the residents who reside closest to the project. Larger-scale meetings are also planned for mid-December.</p> <p>ODOT and KYTC consider and evaluate all public comments received for the project, and public comments have influenced many aspects of the design to date. While a preferred alternative for the Brent Spence Bridge Corridor has been identified, there are opportunities to refine the design in response to community concerns. A recent example is the re-working of several ramps to open up additional land for redevelopment in downtown Cincinnati in response comments from the City. Community feedback is evaluated in terms of engineering feasibility, compliance with state and federal laws, costs, impacts, and benefits before any decisions to incorporate changes in the project's design are made.</p>
094	Traffic	11/22/2022 - Web Comment - ODOT has admitted the bridge will not reduce traffic, but rather induce more people to drive. Cincinnati already cannot afford to maintain it's expensive car infrastructure in it's current state. How does building another bridge, encouraging more people to drive, prevent the city from sinking deeper into debt?	<p>The Brent Spence Bridge Corridor Project has been designed to support traffic projected to travel in the corridor through the year 2050, which is the standard design horizon for large infrastructure projects.</p> <p>The Brent Spence Bridge Project will be funded through a combination of federal and state funds, and ODOT and KYTC are responsible for maintaining the interstate infrastructure. None of the cities in the corridor are anticipated to incur debt related to the Brent Spence Bridge Corridor Project.</p>
095	Traffic	11/23/2022 - Web Comment - I work for PVS Chemicals in Sharonville, OH and our drivers are stating that hazmat is not permitted on the bridge now. Our question is if we have a local delivery in Northern KY, do we have to go around on I-275 or other bridges. Please let me know as soon as you can.	We suggest you contact the Department of Vehicle Regulation in Kentucky for any questions related to Hazardous Cargo (HC) hauling. Resources are also available through the Federal Motor Carrier Safety Administration and the Public Utility Commission of Ohio. There are no designated HC routes in Southwest Ohio. Kentucky HC Routes can be viewed here :

Comment Number	Nature of Comment	Comment	Response
096	Noise	11/23/2022 - Web Comment - I live on W 7th St between Bakewell and Philadelphia. I would like to see sound walls brought back into this project again.	Thank you for your comment regarding the installation of noise walls as part of the Brent Spence Bridge Corridor Project. Recognizing that traffic noise is a concern for residents located close to the corridor, KYTC is currently conducting a technical study to further evaluate a system of noise/visual screening walls to help provide noise reduction and to shield views of the highway. Based on the technical feasibility of constructing a beneficial wall and comments received from individuals who reside in this area, KYTC will determine if a wall will be constructed and document that decision in a technical memo. This comment has been noted as part of KYTC's evaluation of noise walls in this area.
097	Noise	11/23/2022 - Web Comment - I would like to give feedback as a resident of the MainStrasse area of Covington, I will be negatively impacted by this project if it moves forward without the noise reduction efforts put in place before it is completed. I would like to see the sound reduction walls and the green spaces installed as part of this plan.	Thank you for your comment regarding the installation of noise walls as part of the Brent Spence Bridge Corridor Project. Recognizing that traffic noise is a concern for residents located close to the corridor, KYTC is currently conducting a technical study to further evaluate a system of noise/visual screening walls to help provide noise reduction for residences in Mainstrasse and to shield views of the highway. Based on the technical feasibility of constructing a beneficial wall and comments received from individuals who reside in this area, KYTC will determine if a wall will be constructed and document that decision in a technical memo. This comment has been noted as part of KYTC's evaluation of noise walls in the Mainstrasse area.
098	Project Mailing List	11/28/2022 - Web Comment - Please add me to information distribution list.	This individual was added to the project mailing list.
099	Project Mailing List	11/29/2022 - Web Comment - I'd like to get updates on the Brent Spence companion bridge.	This individual was added to the project mailing list.

Comment Number	Nature of Comment	Comment	Response
100	Public Input	11/29/2022 - Web Comment - Please update your website to show upcoming public engagement meetings.	<p>ODOT and KYTC are in the process of reaching out to individual impacted communities along the corridor. This targeted outreach is tailored to each neighborhood and is utilizing methods that exist within each community to notify their community members of the opportunity to learn more about the project, ask questions of the project team, and provide feedback.</p> <p>Information about an upcoming broad neighborhood meeting in Ohio can be found here: Information about an upcoming broad neighborhood meeting in Kentucky can be found here:</p> <p>In addition, ODOT and KYTC are in the process of creating a new Public Involvement page for the project website.</p>
101	Information Request	11/30/2022 - Web Comment - I saw that there were DGN files provided in the procurement section of ODOT's site. This file type is very hard to work with outside of microstation. Are you able to provide DWG or SHP files of these to the public? They do not need to be held to the same QC as the DGNs.	The DGN files offered in the procurement section of ODOT's site were provided in DWG format. It is important to note that the project design is developed using the DGN files. ODOT cannot guarantee the accuracy or quality of the DWG files.
102	Information Request	12/2/2022 - Web Comment - I am interested in learning more about the Corridor Project. Is it possible to obtain copies of the applications submitted to the DOT for the Bridge Investment Program and the Multimodal Discretionary Grant Program. I am a resident of the city of Cincinnati.	Electronic copies of the grant applications were provided to this individual on 12/5/2022.

Comment Number	Nature of Comment	Comment	Response
103	Noise	12/7/2022 - Email Comment - Hello, I'm a homeowner at [REDACTED] W. Maple Avenue], Fort Mitchell, KY 41011. I am for a sound wall being installed behind our homes.	<p>Thank you for your comment regarding a sound wall in the West Maple Avenue Vicinity. KYTC evaluated several noise walls and noise wall systems in the Brent Spence Bridge Corridor. Some of them were warranted based on KYTC's Noise Policy. However, in the vicinity of West Maple Avenue, a noise wall system was not found to be cost effective</p> <p>Recognizing that traffic noise is a concern, KYTC is currently conducting a technical study to further evaluate a noise/visual screening wall to help provide about a 3-6 decibel noise reduction for residences near West Maple Avenue and to shield views of the highway. KYTC will document their decision about this noise wall system in a technical memo based on the technical feasibility and public comments. This comment will be considered as part of that process.</p> <p>For any recommended noise walls, KYTC will conduct additional coordination with the people who will benefit from the walls during the project's design-build phase.</p>
104	Project Development Process Roadway/Bridge Design Multimodal	<p>12/7/2022 - Email Comment - Hi. I am writing to you today regarding the Brent Spence Bridge Corridor Project. In November, the Ohio Department of Transportation (ODOT) released an updated plan for removing the 4th street exit ramp to northbound Interstate 75 and freeing up ten acres of land just west of Central Avenue, the western edge of Cincinnati's Central Business District. I commend this effort as it is a good faith effort that ODOT is open to refining the now ten-year-old plan. However, I believe more can be done to achieve federal, regional, state and city objectives for this project.</p> <p>Specifically, those goals include the following:</p> <ul style="list-style-type: none"> - Return the maximum amount of contiguous land to the City's urban core to achieve city and regional objectives; - Maximize the amount of sidewalk-supported street frontage that is conducive to street-facing development within and adjacent to the project footprint; - Maximize the number of pedestrian connections between the CBD, Queensgate, and the West End, and minimize the distance to cross each of those connections; - Embrace the NACTO Urban Street Design Guide in developing 	<p>Thank you for the comments regarding the recent revisions to the project design in the vicinity of the Central Business District.</p> <p>ODOT is continuing to collaborate with the City of Cincinnati to incorporate community priorities into the project. One of these priorities includes maximizing the amount of land returned to the City once the project is complete. The ultimate vision for this land and other areas adjacent to the project footprint - including economic development potential - will be developed by the City of Cincinnati.</p> <p>The project will install bicycle and pedestrian infrastructure in and between the CBD, Queensgate, and the West End. Pedestrian and bicycle connections will be included across I-75 on 6th Street, 7th Street, 9th Street, Linn Street, Freeman Avenue, Ezzard Charles, Liberty Street, Findlay Street, Bank Street, and Harrison Avenue. In addition, a new shared use path will be constructed along Winchell Avenue between 9th Street and Ezzard Charles Drive, including a pedestrian bridge connection to Freeman Avenue. To promote safety for bicycles and pedestrians, the ramp connections with local streets are being designed as lower-speed urban roadways. The design of</p>

Comment Number	Nature of Comment	Comment	Response
<p>104 (cont.)</p>	<p>Project Development Process Roadway/Bridge Design Multimodal (cont.)</p>	<p>streets, blocks, and other elements; - Minimize the number of lane miles using high-speed directional ramps; - Maximize the economic development potential of new real estate located within the urban core, increasing the city's tax base, which can lead to lower taxes.</p> <p>While I am not advocating for a specific design, I strongly encourage all involved agencies to consider the evolution of community priorities around building resilient, environmentally sustainable, pedestrian-focused communities while enhancing non-automobile-based mobility options and repositioning to a place-based economy. These goals are significantly different from the ones the city and region prioritized when substantial planning concluded on the project in 2012/2013.</p> <p>It has come to my attention that the City of Cincinnati is not a "Cooperating Agency" in the NEPA process. This is something that needs to be rectified as it is clear the City of Cincinnati, along with Covington, KY, will bare the physical impacts of the bridge construction. City Council can accomplish the following objectives: - Formally adopt a set of measurable priorities for the Project. - Enter into a formal agreement with ODOT, specifying the rights & powers that the City will have throughout the Project's environmental process and development process. - Formally request to ODOT and the FHWA that design alternatives, guided primarily by the measurable priorities, other than Alternative I-W, be explored and considered during Phase 1A – the proof of concept phase – during the Project's progressive design-build process. - Formally request to ODOT and the FHWA that sufficiently funded, good-faith, independent analyses be conducted immediately of the following: --- The assumptions used in the Project's traffic forecasting projections; --- The feasibility of utilizing alternative design concepts proposed from sources other than ODOT, guided by the measurable priorities; --- The economic impact analysis of the potential expansion of the CBD.</p>	<p>the intersections of local streets will be in accordance with City of Cincinnati design standards. The pedestrian and bicycle infrastructure included in the project will be built in accordance with ODOT's Multimodal Design Guide, which takes into account a broader framework of national design guidance, including the National Association of City Transportation Officials (NACTO) Urban Bikeway Design Guide, the NACTO Urban Street Design Guide, and the NACTO Transit Design Guide.</p> <p>The pedestrian and bicycle facilities incorporated into the project will improve connectivity to transit, employment, healthcare, cultural, recreational, and commercial destinations. In addition, the aesthetic treatments, wide sidewalks or shared-use paths, planters, translucent screen walls incorporated into the overpass bridges will help to foster urban placemaking throughout the project area.</p> <p>Cooperating agencies are defined by federal law and are limited to federal agencies that have jurisdiction by law or special expertise with respect to any environmental impact involved in a proposed project. State and local agencies of similar qualifications can also be designated as cooperating agencies. Several federal agencies have been designated cooperating agencies for the project. In addition, several state and federal agencies have also been designated as participating agencies, which are agencies with an interest in the project but a lower degree of authority than a cooperating agency. While it has not requested to be included as a participating or cooperating agency, the City of Cincinnati has been involved in the Project Advisory Committee and Aesthetics Design Committee since the project's inception. In addition, ODOT regularly coordinates design details directly with the City and has refined the design in several locations in response to feedback received. This collaboration will continue through the procurement and construction phases of the project, with the City of Cincinnati being involved in the evaluation of the design-build teams.</p> <p>ODOT encourages you to share your thoughts regarding the Cincinnati City Council directly with your council representative.</p>

Comment Number	Nature of Comment	Comment	Response
104 (cont.)	Project Development Process Roadway/Bridge Design Multimodal (cont.)	<p>Cincinnati City Council should be encouraged to adopt these policies as soon as possible. Ideally, they should occur before the City of Cincinnati engages with ODOT in selecting a progressive design-build team (DBT), before the City establishes its design-build advisory committee to work with the DBT, and before the Environmental Document is completed and signed by FHWA.</p> <p>None of the items I am advocating for in this letter should impact the Project's funding, budget, or schedule. Instead, by demanding the best possible process, Cincinnati City Council will ensure the best possible product out of this Project for our city for generations to come.</p>	
105	Right-of-Way	12/2/2022 - Web Comment - What is going to happen to property owners whose property lies in the path of this monstrosity? Get told you have to move?	Value engineering and engineering refinements have reduced the amount of residents who will need to relocate because of the project from over 40 to 4, all of which are in Kentucky. Likewise, business relocations throughout the entire corridor have been reduced from 14 to 13. The acquisition of property for right-of-way – including residential and business relocations – has been, and will continue to be, in accordance with the Uniform Relocation Assistance and Real Property Acquisition Policies Act of 1970 (Uniform Act).
106	Traffic	12/4/2022 - Web Comment - Will the only way to access I-71 N from Kentucky be from the current Brent Spence Bridge?	Travelers will be able to access northbound I-71 from both the existing Brent Spence Bridge and the new companion bridge.
107	Project Funding	12/5/2022 - Web Comment - Way past time for Senator McConnell to step and focus on his home state. If anyone can divert the government's funds (pork barrel) Mitch can. Maybe some of the special crop Kentucky leads the nation with. Not getting any younger, Senator. The only way to a long life is aging. :)	Comment noted.
108	Information Request	12/7/2022 - Web Comment - Is it possible for you to email the slides from today's meeting? Also, is it possible to receive a list of all attendees today?	The slides and a list of attendees from the DBE Matchmaker Event on December 7, 2022 were provided to this individual.

Comment Number	Nature of Comment	Comment	Response
109	Noise	12/8/2022 - Web Comment - People who live next to I-75 in Covington ky right now have no sound barriers now so are we going to be living right on the highway in Lewisburg when the new bridge is built? People on 12th street and Hermes ave need information.	<p>KYTC has identified several locations for potential noise walls to mitigate noise impacts in accordance with its noise policy, including in Lewisburg in the vicinity of West 12th Street and Hermes Avenue. KYTC will conduct additional coordination with the people who will benefit from the walls during the project's design-build phase.</p> <p>Please visit www.PublicInput.com/bsbc to view additional materials about the project, including a presentation from a Lewisburg/Botany Hills neighborhood meeting that took place on December 6, 2022.</p> <p>More meetings for Kentucky neighborhoods are scheduled on December 14, 2022. The meetings will occur from 12pm-2pm and 5pm-7pm at the Kenton County Fiscal Court (Riedlin Schott Community Room), 1840 Simon Kenton Way, Covington, KY. The meetings will be open-house format with a short presentation at 12:30pm and 5:30pm.</p>
110	Project Footprint Noise	12/8/2022 - Web Comment - I live at [REDACTED] Western Avenue in Covington and am curious how the project will impact my residence. I see the bridge from my house everytime I look out my window and live just to the west of the current structure. Will the new companion bridge to the west bring traffic closer to my house ?? How will it affect my property value, noise, etc.	<p>Once constructed, the new companion bridge will be closer to Western Avenue than the existing highway. KYTC has identified several locations for potential noise walls to mitigate noise impacts in accordance with its noise policy, including in Lewisburg near Western Avenue. The recommended noise wall will reduce predicted noise levels by 1 to 4 decibels at your residence. KYTC will conduct additional coordination with the people who will benefit from sound walls during the project's design-build phase. Because KYTC will not be acquiring any residences along Western Avenue, determinations of property values were not prepared.</p> <p>Additional meetings for Kentucky neighborhoods are scheduled on December 14, 2022. The meetings will occur from 12pm-2pm and 5pm-7pm at the Kenton County Fiscal Court (Riedlin Schott Community Room), 1840 Simon Kenton Way, Covington, KY. The meetings will be open-house format with a short presentation at 12:30pm and 5:30pm.</p>

Comment Number	Nature of Comment	Comment	Response
111	Traffic	12/8/2022 - Web Comment - This additional bridge is unnecessary. You need to direct all thru truck traffic to the I-275 loop. Even a trucker who wishes to use the Sharonville rest area must use I-275, regardless of direction. Only trucks making freight deliveries will then be allowed on I-75 inside the loop. Northbound trucks needing I-71 will also be on the bridge, but for that reason only. This will decrease overall truck traffic on the bridge. Let's stop wasting taxpayer money because we think construction is a sport.	Reducing congestion and maintaining connections to key regional and national corridors are primary goals of the project. This includes accommodating freight traffic that is using the interstate system. Diversion of trucks would place them on other routes that are also experiencing congestion and would not be possible to enforce given the need to provide access for local truck traffic. The proposed design meets the project's identified goals while also safely accommodating all types of vehicles.
112	Incident Management	12/8/2022 - Web Comment - As a former first responder I'm concerned about access for not only emergency equipment by clean up and removal services from an accident scene. The sooner these responders can get in and out of an emergency area the sooner the traffic flow can continue.	ODOT and KYTC are coordinating with first responders in Cincinnati, Covington, Fort Wright, and Fort Mitchel to ensure that the project's design accommodates access for incident management. In addition, KYTC and ODOT will coordinate with first responders to develop an incident management plan during construction of the project.
112A	Project Development Process	<p>12/12/2022 - Email Comment - At a community outreach meeting, ODOT stated that this would be moving forward as a reviewed EA. With it progressing forward as a reviewed EA, I'm worried there will not be enough community/public input on this project. I looked into what is required public input wise for an EA and it appears to be at the discretion of ODOT on how much they want to take in. I have reached out to ODOT with questions and comments about the project, but they have yet to give clear answers or reply in some cases.</p> <p>Will FHWA be allowing for public comments directly to them for this project? I understand the timeline for this project is very fast paced, but I feel that a project with this large of an impact and severity needs to have more than a few one hour meetings with the community.</p> <p>I'm a bit confused on how the EIS to EA occurred for this project as a whole. The FONSI posted in 2012 states that since it's remaining in the "mainline" of the I75/71 footprint it is an EA, is this the only reason why it received an EA rather than an EIS? I'm not an expert by any means, but doesn't this fall under 23 CFR 771.115(a)(2)?</p>	<p>The Ohio and Kentucky Divisions of FHWA in cooperation with ODOT and KYTC are continuing National Environmental Policy Act (NEPA) studies for the proposed Brent Spence Bridge (BSB) Corridor project carrying Interstates 71 & 75 over the Ohio River in the cities of Cincinnati and Covington. As you are aware, in 2012 after completion of the EA, FHWA determined Alternative I will have no significant impact on the human or natural environment. Since the Finding of No Significant Impact (FONSI) in 2012, Alternative I has been refined and a supplemental environmental assessment (EA) is being prepared to evaluate impacts of these refinements (Concept I-W).</p> <p>FHWA concluded that a supplemental EA is the proper level of review for this project under the National Environmental Policy Act. An EA is appropriate when a proposed action is not classified as a categorical exclusion, but does not clearly require an environmental impact statement (EIS). Preparation of the supplemental EA is currently underway and has included/will continue to include different opportunities for public involvement (meetings, emailed or mailed comments). The supplemental EA findings will allow FHWA, ODOT and KYTC to determine if an EIS is warranted, as you suggested. If not, FHWA will issue a Finding of No Significant Impact (FONSI). FHWA encourages your participation in the public involvement process. FHWA, as well as ODOT and KYTC are all accepting public comments for this project. All comments received will be addressed prior to the NEPA determination.</p>

Comment Number	Nature of Comment	Comment	Response
113	Traffic	12/13/2022 – Email Comment - [Pertaining to the response to Comment 111] But, you're wrong. Did you forget how to put up signs? You already have a sign directing HAZMAT trucks to use I-71. You put up electronic signs at every construction site, and you have the (sometimes accurate) overhead ones, giving traffic estimates. Is it really a stretch of strained resources putting up a sign diverting thru truck traffic--trucks which are not making stops within the I-275 loop--onto I-275 at both of its junctions with I-75? Or, do you simply have too much money you can't allocate otherwise?	Comment noted.
114	Project Mailing List	12/12/2022 - Web Comment - It was GREAT meeting those of you on the project that I did. I am very interested and would like to keep updated on this project to bid.	You have been added to the project mailing list. In addition, please visit https://www.dot.state.oh.us/Divisions/ConstructionMgt/design-build/bsb-project-procurement/Pages/default.aspx for project procurement information.
115	Traffic	12/14/2022 - Web Comment - Here's a quick, inexpensive, easy solution. Make all vehicles that weigh over 10K GVWR take I-275. Piggyback onto the existing electronic weigh station bypass systems to allow local deliveries.	Reducing congestion and maintaining connections to key regional and national corridors are primary goals of the project. This includes accommodating freight traffic that is using the interstate system. Diversion of trucks would place them on other routes that are also experiencing congestion and would not be possible to enforce given the need to provide access for local truck traffic. The proposed design meets the project's identified goals while also safely accommodating all types of vehicles.
116	Project Schedule	12/14/2022 - Web Comment - when is construction to start?	If the necessary funding is secured, the project's groundbreaking is scheduled for November 2023 with construction beginning in earnest in 2024.
117	Information Request	12/14/2022 - Web Comment - I live on west 11th Street - will it be closed?	Upon project completion, West 11th Street in Covington will continue to operate similar as it does today, connecting Bullock Street and Hermes Avenue. However, during construction, there will be intermittent times where the roadway will be closed for construction activities.
118	General Support	12/15/2022 - Web Comment - THANKS	Comment noted.
119	Information Request	12/15/2022 - Web Comment - Please e-mail me the Final Design Summary Appendix with the maps/drawings that illustrate the design of the Brent Spence new bridge and the approach ramps.	This requested information was provided to this individual.

Comment Number	Nature of Comment	Comment	Response
120	Information Request	<p>12/16/2022 - Web Comment - I would greatly appreciate it if you could please provide the following. • BSB Concept I-W Plan (Appendices) • BSB Concept I-M Plan • Appendix E: BSB Potential Design Exceptions. E-1: Concept I-W Design Documentation – Map; E-2: Concept I-W Design Documentation – Table; E-3: Concept I-M Design Documentation – KY Map; E-4: Concept I-M Design Documentation – OH Map; E-5: Concept I-M Design Documentation – Table</p>	<p>The requested information was provided to this individual.</p>
121	Traffic	<p>12/16/2022 - Web Comment - I live in Norwood and work in Florence, so I regularly drive this corridor. I'm concerned that a companion bridge will only bring a small and temporary speed-up to traffic through Cincinnati, after spending billions of dollars and causing years of slow traffic due to construction closures. A new bridge will draw more traffic to the region and encourage more locals to drive across it. Study after study of enlarged or new highways show that the extra capacity they create is quickly consumed, and bottlenecks return.</p> <p>Driving on the Brent Spence on a rainy night feels precarious, that's true. The lane lines are difficult to see, and the lanes have been striped to be very narrow to fit four lanes across. So restripe it to three lanes and add more signage to guide people to the lane(s) they should use to continue on I-75 or I-71.</p> <p>Of course the Brent Spence is only one of the bottlenecks on this corridor. A new companion bridge won't ease the grade on the cut in the hill or the curve at Dixie Highway or simplify the many on- and off-ramps west of downtown. It won't smooth the slowdowns from vehicles merging onto I-75 or I-71 from the Norwood Lateral. I-71 between the Lateral and Ronald Reagan will still be as slow as a restaurant drive-thru during rush hour.</p> <p>The only way to actually reduce traffic congestion is to take cars off the road, and the best way to do that is to expand and improve mass transit. I know this companion bridge project has been in the work for many years, and it represents a once-in-a-generation investment in the Cincinnati/Northern Kentucky region. But it's the wrong project for the people who live in region and for those transporting goods through the Midwest. I hope you'll reconsider building a companion bridge.</p>	<p>The project is designed to accommodate traffic projected to travel through the corridor by the year 2050, which is the standard design horizon for large infrastructure projects. The traffic projections are based on a regional travel-demand model which assigns routes used by travelers based on available capacity, projected trips, and calculated travel times. Although some travel patterns may change with improved capacity on I-71 and I-75, the project is not anticipated to create additional travel demand that is not already anticipated to exist by the year 2050.</p> <p>The Brent Spence Bridge Corridor project will restripe the existing Brent Spence Bridge to return it to three standard lanes with shoulders. In addition, the existing bridge will provide access for local traffic only at a reduced 45-mph speed. The project will incorporate signing to guide travelers along I-71 and I-75 and to local destinations.</p> <p>While the project will not substantially change the grade on the cut-in-the-hill, additional capacity on I-71/I-75 will help to improve traffic flow through this area. In addition, the project will allow movements to and from the ramps in Cincinnati to occur off the highway, on lower-speed collector-distributor roads, improving traffic flow and safety through the Central Business District. Problems on I-71 east of Cincinnati would need to be addressed as part of a separate project.</p> <p>The project will reduce congestion and improve reliability for local bus routes, which use the Brent Spence Bridge for 210 trips every weekday. In addition, improved and new sidewalks, shared use paths, and bicycle lanes will improve connections to existing bus stops. Finally, future accommodation of bus service along the corridor's shoulder will not be precluded by the project. Other transit improvements, such as rail transit will need to be evaluated as part of a separate project.</p>

Comment Number	Nature of Comment	Comment	Response
121A	Project Development Process	<p>12/16/2022 - Email Comment - I'd like to ask that the FHWA please begin proactively reaching out to all 10 of the City of Cincinnati's elected officials, as well as all relevant Hamilton County elected officials. As evidenced by the Mayor's request to ODOT that the City become a Cooperating Agency, there is serious interest in the Brent Spence project among local elected leaders. However, many are first-time office holders, and they do not have experience navigating the complex processes associated with a multi-billion dollar infrastructure project being delivered by a state agency. I can tell you that elected officials in City Hall want to get engaged on this project, and they want to drive the outcome, but they do not know how. My understanding is that the project's design direction and Progressive Design-Build procurement and delivery should be substantially steered by feedback collected during the Supplemental EA process and by the City's own adopted priorities for the project. However, many in City Hall do not understand the opportunity to substantially shape the outcome of this project.</p> <p>You shared that the FHWA is the lead agency for this project. I would ask that -- just as ODOT through the SEA process has been asked to reach out to residents and businesses in the vicinity of the project in order to educate them about the project and collect their feedback -- so should FHWA reach out to City of Cincinnati and Hamilton County elected officials, educating them about the environmental process, progressive design-build delivery, and opportunities for municipal involvement. As the Environmental Program Manager for this project, you are familiar with the ways that NEPA / CEQ have contemplated significant municipal involvement in projects like this one and indeed ways that NEPA / CEQ have codified avenues for municipal involvement, such as Cooperating Agency, Participating Agency, etc. The elected officials in Cincinnati and Hamilton County are not familiar with these opportunities. And, just as neighbors of the project deserve to be engaged in a way that is accessible for them, our elected officials, too, should be approached where they are, in language they understand, in a way that addresses their concerns and desires. They are, afterall, the people's representatives.</p> <p>To be clear, FHWA is the correct agency to make this outreach, and it should be made as soon as possible. Unfortunately, to date, ODOT has provided the City with dissembled information regarding its</p>	<p>The City of Cincinnati has been engaged with this project throughout its development. FHWA staff have been and continue to be available to the elected officials of the City to help all interested parties understand the environmental process. FHWA is following the established policies and procedures for implementing the National Environmental Policy Act of 1969 as amended (NEPA) and for project procurement. Coordination with the City will continue as the project design progresses and throughout construction.</p> <p>Thank you for your interest in this project.</p>

Comment Number	Nature of Comment	Comment	Response
121A (cont.)	Project Development Process (cont.)	<p>opportunity to shape the project (i.e. become a Cooperating Agency), and months have been squandered as a result. This cannot be allowed to happen again.</p> <p>While I earnestly appreciate your reaching out to me with this information, Bridge Forward should not be relied upon by the FHWA to educate elected leaders. Bridge Forward is a volunteer, grassroots organization. Bridge Forward is not viewed as an authoritative source on NEPA, CEQ, or infrastructure delivery. Additionally, we know that local leaders have been told, perhaps even by ODOT, to stop talking to Bridge Forward, or else they risk jeopardizing the project's funding and schedule.</p> <p>Despite that, Bridge Forward has identified in the community a groundswell of interest in and support for making this interstate project as good of a product for the urban core of Cincinnati as it possibly can be. And, for a \$3 billion, decade-old plan that was pulled off the shelf and teed up for construction as soon as the BIL / IIJA was signed (notwithstanding two no-hurdle FONSI extensions), Bridge Forward has acted in an extremely responsible way. We have not opposed this project, but rather made good-faith efforts to work within the project's development and environmental processes to allow for needed design updates and for as much local control through elected leadership (not simply department of transportation staff) as possible. We feel this responsible approach, including but not limited to the following, focused on education of opportunities for local control, should be verified and communicated to all local elected leadership, by FHWA:</p> <ul style="list-style-type: none"> • Opportunity for local jurisdictions to become a Cooperating Agency (or, Participating Agency, etc.); • Opportunity, through an RFQ addendum and/or through contract language, to codify the expectation for a very robust Phase 1A - Proof of Concept Phase - and Phase 1B - Project Development Phase - for the project, including: <ul style="list-style-type: none"> o Incentivization of innovative design concepts; o Re-evaluation of assumptions used in the Project's traffic forecasting projections by a detached third-party; o Use of alternative design concepts proposed from sources other than BSMT; and o Investigation of adding additional scopes of work and/or 	

Comment Number	Nature of Comment	Comment	Response
121A (cont.)	Project Development Process (cont.)	<p>additional features to the Project, the associated pools of federal funding which may be available for additional scopes, and the likelihood of securing said federal funding;</p> <ul style="list-style-type: none"> • Opportunity to ensure that the design-build team (DBT) is selected in large-part based on its demonstrated ability to deliver a project in a context-sensitive urban project while working closely with a municipality to develop innovative design direction during a PDB; and • Opportunity for the City to submit priorities to the BSMT, which would shape all phases of the PDB, which we anticipate will consist of the following: <ul style="list-style-type: none"> o Maximize the amount of returned land contiguous to the City's urban core; o Maximize the amount of walkable street frontage that is conducive to street-facing development within and adjacent to the project footprint; o Maximize the number of pedestrian connections between the CBD, Queensgate, and the West End, and minimize the walking distance of each of those connections; o Maximize the number of local streets that are designed according to the NACTO Urban Street Design Guide; o Minimize the number of lane miles using high-speed directional ramps; and o Achieve best expenditure of public dollars measured in life-cycle return on investment. <p>The forthcoming announcement of funding for this project will be a milestone for the region. We need to make sure the funding is deployed correctly and the best possible project is delivered, according to the stated priorities of the federal funding sources themselves, and according to law.</p>	

Comment Number	Nature of Comment	Comment	Response
122	Information Request	12/21/2022 - Web Comment - KDG is an MBE with extensive CM/CI and project controls experience. Any pre-qualification requirements and process information will be much appreciated.	<p>Anticipated prequalification requirements for the Brent Spence Bridge Corridor Project are located here: https://www.dot.state.oh.us/Divisions/ConstructionMgt/design-build/bsb-project-procurement/Documents/20221216-UpdateInfo/BSBC_Prequalifications%20Announcement.pdf</p> <p>Information on the prequalification process can be found in this presentation: https://brentspencebridgecorridor.com/wp-content/uploads/2022/12/2022-12-07-BSBC-Event-Slides_Combined.pdf</p> <p>Please visit the project procurement website to access the most up-to-date procurement information and to sign up for email alerts: https://www.dot.state.oh.us/Divisions/ConstructionMgt/design-build/bsb-project-procurement/Pages/default.aspx</p>
123	Right-of-Way	12/21/2022 - Web Comment - will any buildings on Crescent Ave., Baker St., or Lewis St. in Covington, Ky be taken for the new bridge construction?	No buildings on Crescent Avenue, Baker Street, or Lewis Street will be impacted by the Brent Spence Bridge Corridor Project.
124	Noise	12/23/2022 - Web Comment - Hello. My name is [REDACTED] and I am the Condo Association President of "The Views" off Dixie Highway and on behalf of the entire community, we strongly support noise barrier walls and an ordinance to outlaw engine braking for trucks traveling north on I71/75 down the cut in the hill.	<p>Thank you for your recent email regarding the Brent Spence Bridge Corridor Project, specifically the support for noise walls. KYTC evaluated several noise walls and noise wall systems along the Brent Spence Bridge Corridor. Noise walls along Bullock Street in Lewisburg - east of "The Views" - were found to be warranted based on KYTC's Noise Policy. KYTC will conduct additional coordination with the people who will benefit from the walls during the project's design-build phase.</p> <p>Regarding the request for an ordinance to outlaw engine braking for trucks, KYTC does not restrict the use of engine breaks on interstate highways as they are considered a safety feature for trucks.</p>

Comment Number	Nature of Comment	Comment	Response
125	BSB Companion Bridge Project Schedule	12/24/2022 - Web Comment - Why have no time frames been given for the phases particularly the bridge over the Ohio portion. Is the Brent Spence bridge being removed and replaced with a whole new bridge or repaired with a so-called companion bridge being built. The information is very confusing to time and scope.	A new double-decker companion bridge that is five lanes in each direction will be built to carry through-traveling interstate traffic. The existing Brent Spence Bridge will not be removed. Only the decks (the surface that vehicles drive on) will be replaced, and some of the structural steel in the bridge will be repaired. The existing bridge will be restriped to three lanes in each direction with standard shoulders and will carry local traffic traveling to and from Covington and downtown Cincinnati. The project will be built in three phases. Construction on Phase III, which includes the new companion bridge and the existing Brent Spence Bridge, will begin in late 2023/early 2024 and will last five years. A graphic showing the different project phases and the anticipated construction timeline can be accessed here: https://brentspencebridgecorridor.com/ohio/
126	Project Footprint	12/27/2022 - Web Comment - I think reducing the ramps and space used on the Ohio side would take advantage of this opportunity the best. So much wasted area in a key part of the community.	ODOT has incorporated several features that reduce the project's footprint, including optimizing interchange geometry by utilizing the land formerly occupied by the Dunnhumby building, reducing shoulder widths, and reducing the width of the new companion bridge. In addition, 10 acres of excess ODOT property adjacent to Cincinnati's Central Business District can be transferred to the City of Cincinnati upon the project's completion. Most recently, ODOT and the City of Cincinnati collaborated on refinements to the downtown Cincinnati ramps that will open up an additional 9.5 acres of land for potential redevelopment. ODOT will continue to work with the City of Cincinnati throughout the remaining design activities to identify opportunities to incorporate refinements that reclaim additional land.
127	BSB Companion Bridge	12/29/2022 - Web Comment - Can you send me pictures of the new bridge's design.	The design of the new companion bridge is not yet determined. It may be a tied arch design or a cable stayed design. Pictures showing what the tied arch design might look like can be found on the project website: https://brentspencebridgecorridor.com/wp-content/uploads/2022/12/Renderings-for-Boards-Reduced.pdf . A picture showing what the cable stayed design might look like is attached to this message.

Comment Number	Nature of Comment	Comment	Response
128	Multimodal Project Footprint	<p>12/29/2022 - Web Comment - Dear responsible parties at ODOT and KYTC for the I-71/75 corridor improvement,</p> <p>Thank you for your work so far in the development of plans to update and improve the I-71/75 corridor along with the Brent Spence Bridge Replacement. The need to invest and improve our transportation infrastructure is paramount to both a healthy economy/supply chain and safety of the public. Also, thank you for your consideration of public feedback. As a resident of Cincinnati, in the Over-The-Rhine district, I would like to submit the following points for consideration:</p> <p>First, on the Brent Spence bridge renderings and lane assignments, I agree that a new bridge should be built to support the current traffic lanes in a new structure. However, in the local/collector lanes the addition of bus, bicycle/shared path, and rail should be included in the design. The Brent Spence bridge was built in 1960, and now 60-70 years later we are updating its design. The new corridor should be built for the transportation infrastructure we (as residents of the Cincinnati/Northern Kentucky area) want and will be utilizing decades in the future, not the status quo. There are currently only 3 possible ways to cross on bicycle/shared path and 2 by rail in the Cincinnati area (with both bridges dating back to the late 1800s and rebuilt in the 1920s). Adding additional capacity in both rail and bicycle/shared path is critical to building a sustainable transportation future.</p> <p>Second, on the total land area taken up by the I-71/75 interchange and I-75 corridor on the Ohio side of the project, I agree that it can't be completely reduced to its previous zero-impact as the corridor does need to exist. I propose that the reduction of access to the interstates would benefit the local community more through the reduction of traffic and reduction of land needed for collector roads and on/off ramps. As a resident of OTR, I want to encourage people to explore the neighborhood (including attending events at TQL stadium) through non-individual automobile ways. We need to be building a better system of public transportation in and around our neighborhoods to encourage people not to use their automobile.</p> <p>Third, specifically regarding the overpass of I-75 on Ezzard Charles Drive, I would encourage the development of an overpass connects the Cincinnati Union Terminal on one end Music Hall on the other.</p>	<p>The project has not incorporated rail into the design because it is not supported by the project's purpose and need, and there are no current plans for new rail in the region. New rail facilities would need to be evaluated as part of a separate project.</p> <p>[UPDATED RESPONSE] <i>New rail facilities were not considered a feasible alternative for this project. Since there are no regional plans for new rail facilities in the foreseeable future, provisions for inclusion of a rail corridor have not been a part of the Brent Spence project. New rail facilities would need to be evaluated as part of a separate project. However, the current design will not preclude the future consideration of a rail corridor,</i></p> <p>In Ohio, pedestrian and bicycle connections will be included across I-75 on 6th Street, 7th Street, 9th Street, Linn Street, Freeman Avenue, Ezzard Charles, Liberty Street, Findlay Street, Bank Street, and Harrison Avenue. In addition, a new shared use path will be constructed along Winchell Avenue between 9th Street and Ezzard Charles Drive, including a pedestrian bridge connection to Freeman Avenue.</p> <p>In Kentucky, new shared use paths and sidewalks will be included along the frontage roads. Also new and rebuilt sidewalks will be included under the MLK/West 12th Street, Pike Street, West 9th Street, West 5th Street, and West 3rd Street bridges. A new shared use path will be built under the West 9th Street and West 5th Street bridges, which will tie into the shared use paths in Goebel Park. The shared use path will be extended along Crescent Avenue to connect to the existing shared use path along the river.</p> <p>[UPDATED RESPONSE] Pedestrian and bicycle accommodations are not planned on the new companion bridge or the existing Brent Spence bridge because of the proximity of a reasonable crossing at the Clay Wade Bailey Bridge. Preliminary investigations indicate that adding bike lanes to the Clay Wade Bailey Bridge may be feasible. KYTC and ODOT will further evaluate adding bike lanes to the Clay Wade Bailey Bridge during the project's progressive design-build phase.</p>

Comment Number	Nature of Comment	Comment	Response
128 (cont.)	Multimodal Project Footprint (cont.)	<p>This should be done by making the interstate less apparent/visible using green space and dedicated bicycle/shared paths (reducing the focus of automobile traffic). Also, the overpass should be designed with the potential to support a tram line in the future.</p> <p>Finally, thank you for your consideration. I know that these processes take time and resources but listening to the communities in and around these projects is vital to developing a greater Cincinnati Area that we all will be proud to call home in the next 50 years.</p>	<p>The project will reduce congestion and improve reliability for local bus routes, which use the Brent Spence Bridge for 210 trips every weekday. In addition, improved and new sidewalks, shared use paths, and bicycle lanes will improve connections to existing bus stops. Finally, future accommodation of bus service along the corridor's shoulder will not be precluded by the project. Pedestrian and bicycle accommodations are not planned on the new companion bridge or the existing Brent Spence Bridge because of the proximity of a reasonable crossing at the Clay Wade Bailey Bridge.</p> <p>ODOT and the City of Cincinnati have collaborated to reduce the project's footprint in downtown Cincinnati. The number of lanes and ramps provided were determined based on traffic operational analysis of existing and future traffic volumes traveling through the area. Further reductions in the number of lanes and/or ramps would negatively affect traffic flow, connectivity, and access to and from downtown Cincinnati.</p> <p>[UPDATED RESPONSE] ODOT has incorporated several features that reduce the project's footprint, including optimizing interchange geometry by utilizing the land formerly occupied by the Dunnhumby building, reducing shoulder widths, and reducing the width of the new companion bridge. Most recently, ODOT and the City of Cincinnati collaborated on refinements to the downtown Cincinnati ramps that will open up an additional 9.5 acres of land for potential redevelopment. ODOT will continue to work with the City of Cincinnati throughout the progressive design-build process to identify opportunities to incorporate refinements to further reduce the project footprint and reclaim additional land.</p>

Comment Number	Nature of Comment	Comment	Response
129	Project Schedule	12/30/2022 - Web Comment - The document about says the project will start in the north with Phase 1 but the timeframe listed of 2028 thru 2031 is significantly later than the time frame given for Phases 2 and 3.	<p>The project phases are numbered from north to south. However, they will not be constructed in that order. As currently planned, Phase I will be constructed last. However, the timeframe for this particular phase will be adjusted to coordinate with the City of Cincinnati's Western Hills Viaduct Project.</p> <p>The Western Hills Viaduct project and the Brent Spence Bridge project overlap and coordination is on-going to develop the construction phasing and schedule for this area of the project.</p>
130	Right-of-Way	12/30/2022 - Web Comment - How do I find out if my house is one that will be affected by this project? I live at [REDACTED] in Fort Mitchell- just NE of the Dixie Hwy S- bound exit ramp. It's a concern because I have 3 children in the Beechwood School District.	<p>Thank you for your recent inquiry regarding potential impacts from the Brent Spence Bridge Corridor Project. Given the address provided in your email, your property will not be impacted by the project based on current plans.</p> <p>Thank you again for your interest in the project and I encourage you to continue to follow via the website: https://brentspencebridgecorridor.com/</p>
131	Information Request	12/30/2022 - Web Comment - Please send me the appendices to the DESIGN SUMMARY REPORT dated AUGUST 23, 2022.	The requested information was provided to this individual.
132	Project Mailing List	01/02/2023 - Web Comment - Please place me on the B—S Bridge update email list.	This individual was added to the project mailing list.

Comment Number	Nature of Comment	Comment	Response
132A	Public Input	<p>01/02/2023 - Email Comment - I am a resident of the Cincinnati Central Business District (CBD) neighborhood and live within a half-block of the project area (the 300 block of W 4th St). A CBD neighborhood outreach meeting was held on 11/29/2022. Despite being a resident of the target neighborhood for the meeting and taking adequate steps to stay informed on the project, I did not receive any communication from ODOT/the project team that a CBD neighborhood outreach meeting was being held. My knowledge of the meeting came from a colleague who has no affiliation to the project team or its partners. Had it not been for that colleague, I would not have known the meeting was happening. Thus, the purpose of this letter is to document in the public comment register key facts that demonstrate the project team has circumnavigated their own communication tools regarding the impacted neighborhood outreach period which has resulted in a disingenuous public outreach where not all residents of impacted neighborhoods were aware of the engagement opportunity.</p> <p>To begin, the project's E-newsletter, the means for communicating project updates and news, failed to adequately advertise the outreach meetings. I signed up for and have received the project's newsletters beginning with the 10/21/2022 edition as well as the 11/10 and 12/5 editions. The 11/10 newsletter is the first communication regarding the neighborhood outreach; however, no communication was given for the meeting dates, times, or locations. The newsletter did not even indicate which neighborhoods the project team would be visiting nor was any information provided on where an individual could go to learn more of the neighborhood meetings or view the self-service comment website (www.publicinput.com/bsbc). This lack of notice comes despite text on the project website's newsletter sign up form indicating notice of upcoming meetings would be provided by signing up. The full text of the 11/10 newsletter is provided in Appendix A and the website sign up message is provided in Appendix B.</p> <p>The 12/5 edition did include the remaining events; however, when comparing with the full list of outreach meetings (Appendix C), 5 KY meetings were already held and 2 OH meetings were also already held (an additional KY meeting was held on 12/5- same day notice is hardly effective public engagement). While the newsletter references past meetings, there was no mention of which neighborhoods were</p>	<p>Thank you for sharing your concerns to the Brent Spence Bridge Corridor web site regarding the public involvement process for the Brent Spence Bridge Corridor Project. ODOT and KYTC value feedback as an important part of the project development process.</p> <p>ODOT and KYTC conducted the neighborhood outreach and other public involvement activities in accordance with the Public Engagement Plan developed for the project. The meetings were part of a targeted outreach plan tailored to each, specific neighborhood to provide community members within the neighborhood the opportunity to learn more about the project, ask questions of the project team, and provide feedback. The specific neighborhoods were chosen because a demographic analysis showed that they contained underserved populations and are directly adjacent to the project, where community impacts would be most likely to occur.</p> <p>In Ohio, individual, neighborhood meetings were held in November and December in the CUF (Clifton Heights-University Heights-Fairview), Central Business District (CBD), Camp Washington, and West End neighborhoods. Respectively, the meetings were held November 15, November 29, December 12, and December 20. These public involvement meetings were conducted during the established community council meetings in which community leaders hold regular meetings to provide their residents with information specific their neighborhood.</p> <p>Similarly, KYTC held meetings targeted to neighborhoods that are within the corridor between Covington and the Dixie Highway interchange; due the greater number of affected neighborhoods, it was deemed necessary to hold more meetings in Kentucky. The outreach strategy was outlined in an Environmental Justice Methodology memo and the project Public Engagement Plan, both of which were reviewed by FHWA.</p> <p>For those meetings held in Ohio, the decision on how to notify the neighborhood groups was based on discussions with city of Cincinnati, which is most familiar with the community councils and could provide contact information. Community Councils are recognized by the City and, historically, ODOT has engaged these councils as part of our outreach on projects we sponsor. We then</p>

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132A (cont.)	Public Input (cont.)	<p>already visited. Additionally, no message accompanied that list inviting members of already-visited neighborhoods to attend a future meeting outside of their neighborhood.</p> <p>Since the 12/5 newsletter did not even reference the self-service input website (www.pulicinput.com/bsbc), members of already visited neighborhoods who missed their neighborhood meeting as a result of the lack of communication by the project team are left to think their next opportunity to engage is the public hearings in July/August. By only publicizing the outreach meetings mid-way through the scheduled calendar, the project team has conceded that they ineffectively notified the public of the outreach period which has resulted in citizens missing their opportunity to engage. The 12/5 newsletter is available in Appendix D.</p> <p>The next logical place to look for updates, the project’s website (www.brentspencebridgecorridor.com), did not list the public comment period until the outreach meetings were well underway. Using the Internet Archive Wayback Machine (a service that archives partial snapshots of websites at random intervals), bounds can be placed on when the first mention of the public outreach period is made. The “Public Involvement and Comments” page, which contains a summary of the outreach meeting neighborhoods and dates, was not published until sometime after December 12th, 2022 and December 23rd, 2022. This is known because the snapshot taken on December 12th does not have a link in the home page menu to the “Public Involvement and Comments” page; the snapshot on December 23rd does have the link. Assuming the best case of a website update just after the December 12th snapshot, 8 KY meetings and 2 OH meetings already occurred (an additional 1 meeting occurred on 12/12 in each state). Moreover, the current language on the “Public Involvement and Comments” page does not even contain the time or location, nor does it reference the self-service input website (pulicinput.com/bsbc). Again, the website content has not been kept up to date to inform the public in a timely manner of the opportunities to engage and provide input to the project which demonstrates the project team is not using their established communication tools to engage with impacted neighborhoods. The website snapshots are provided in Appendix E.</p>	<p>worked with the community councils to promote the meetings via their established procedures and communication methods. Depending on the neighborhood, these advertising methods included neighborhood web sites, social media (Facebook), announcements at prior meetings, and email notifications. Information about the availability of project materials and the opportunity to comment online through PublicInput.com and the web site was also distributed through the community councils. As these meetings were for targeted audiences and not the public at large, the project team did not feel that placing the information in the Brent Spence Bridge Corridor e-newsletter would be appropriate. However, responding to feedback received during the neighborhood outreach, ODOT chose to advertise the Camp Washington and West End meetings, as well as Kentucky neighborhood meetings, in the December 5 project e-newsletter. In addition, the project web site’s newly created Public Involvement page included the dates for all the neighborhood meetings, past and future.</p> <p>In addition to the neighborhood-specific meetings, ODOT conducted two, larger community public involvement meetings on December 13 as well as similar meeting in Kentucky on December 14. These were an extended part of this targeted outreach to reach neighborhoods that were near the project but not directly adjacent to the corridor or to capture community members unable to attend their specific neighborhood meeting. For this meeting, fliers advertising the meetings were sent to the following community councils: Lower Price Hill, Mount Auburn, Over the Rhine, Mount Auburn-Walnut Hills, and Pendleton. The meeting was promoted on the project website, the project’s December e-newsletter, ODOT District 8’s events page, Facebook, Twitter, Nextdoor.com, and several media outlets. Information was also provided to Cincinnati City council, and the city shared information regarding these meetings on their social media platforms as well.</p> <p>The PublicInput.com site was opened for public comments concurrent with the first neighborhood meeting on November 15, and this comment period was concluded January 5, sixteen days after the last neighborhood meeting. This was to ensure that comments from the targeted neighborhood meetings were captured and that adequate time existed to fully evaluate and respond to feedback</p>

Comment Number	Nature of Comment	Comment	Response
132A (cont.)	Public Input (cont.)	<p>In light of the aforementioned observations, the project team was questioned during the CBD meeting how the outreach meetings were communicated to the public. Given there was no information in the project newsletter nor the website prior to the CBD meeting, I questioned how the meetings were being publicized. The response from the presenter, Mr. Stefan Spinoso, was that the project team reached out to the neighborhood community councils to distribute the information. While the community councils are often viewed as the voice of the community, membership is voluntary, and their leadership is voluntary. When seeking public input, the entire public of the impacted neighborhoods needs to be made aware of their opportunity to provide input. Reaching out to community councils, because of their voluntary makeup, only addressed a portion of the targeted neighborhoods. It was irresponsible for the project team to put the responsibility of publicizing the outreach meetings on the community councils, especially in place of using the project team's own communication tools.</p> <p>In conclusion, the facts contained herein demonstrate the haphazard, figure-it-out-as-we-go communication plan being carried out by the project team to seek input from the impacted neighborhoods. The project team's own communication tools, the project website and newsletter, failed to communicate the dates, times, locations, or impacted neighborhoods until several meetings had already been held. The project team relied on the community councils to publicize the meeting information which did not reach every citizen of the impacted neighborhoods. As a result, the impacted neighborhood public outreach period did not effectively provide an opportunity for every citizen of the impacted neighborhoods to engage and provide feedback to the project team.</p> <p>Therefore, to ensure that the entire public of the impacted neighborhoods has been made aware of their opportunity to learn about the project and comment, I request that the impacted neighborhood comment period be repeated with a communication plan that guarantees the entire public of the impacted neighborhoods has been made aware of their opportunity to engage.</p>	<p>received - including making decisions regarding mitigation and enhancement measures - prior to finalizing the Environmental Justice Analysis Report and the supplemental Environmental Assessment (EA), submitted to FHWA on February 16, 2023.</p> <p>Additionally, anyone may submit comments on the project website Brent Spence Bridge Corridor at any time. Those comments and questions are captured and addressed accordingly, and then being shared on the project website each month. It is important to note that the project website is a very dynamic, evolving site, and it is being updated regularly with information pertaining to project development, public involvement information, meetings, and procurement.</p> <p>In addition to the comments you have already shared with the project team and the Federal Highway Administration, you may continue to submit comments via the project web site as listed above, as well as during the public hearings which will be held this summer. Although a date has yet to be established, these meetings will be announced through the project web site, both agencies' web sites, the e-newsletter, social and traditional media, our partners at the cities of Cincinnati and Covington, and other identified resources to assure that we engage with our communities on both sides of the Ohio River.</p> <p>Again, we thank you for your comments, your attendance at the November 29th, 2022 CBD meeting, and your participation in the public involvement process for this monumental project. As we move forward with this major new construction project, we will continue following the Public Engagement Plan while also evolving our process to meet the needs of the community. It is the goal of the project team to provide all members of the general public multiple opportunities and resources to comment on the project.</p>

Comment Number	Nature of Comment	Comment	Response
133	Construction	01/03/2023 - Web Comment - What is the projected advertisement for construction bid date? 2023? 2024?	The draft Request for Proposals (RFP) for progressive design-build teams will be released on January 13, 2023. The final RFP will be advertised on February 17, 2023. Additional information can be found on the project's procurement website: https://www.dot.state.oh.us/Divisions/ConstructionMgt/design-build/bsb-project-procurement/Pages/default.aspx
134	Historic Resources	01/03/2023 - Web Comment - I was looking over the BS Bridge Corridor PDFs and there are some placed marked with a purple star. The legend says these are "NRHP Listed or Eligible Site" what does that mean to the structures? Are they going to be taken or might be taken?	On the project mapping, a purple star means that a structure (such as a building or a bridge) has been identified as either being listed on or eligible for listing on the National Register of Historic Places (NRHP). In addition, historic districts are shown on the project mapping with purple hatching. The purple stars and hatching only indicate that historic resources are present in and near the project area. Impacts to historic resources has been avoided and minimized to the greatest extent possible. The project will impact some areas within the Lewisburg Historic District (KY), and it will remove approximately 204 feet of Longworth Hall (OH), which is listed on the NRHP. KYTC is currently developing a Memorandum of Agreement to mitigate the impacts to the Lewisburg Historic District. In addition, ODOT entered into a Memorandum of Agreement committing to completing various repair, upgrade, restoration, enhancement, and refurbishment measures to be completed at Longworth Hall to mitigate impacts from the project.
135	Construction	01/03/2023 - Web Comment - Good afternoon, looking for a plan holders list for the bridge project. Poseidon Barge fabricates, sells, and rents portable sectional barges for floating cranes, excavators, drill rigs and other types of construction equipment. Thank you for your help!	ODOT and KYTC do not have a list of contractors for the Brent Spence Bridge Corridor Project. Proposals will be due from design-build teams on March 31, 2023. Please visit https://www.dot.state.oh.us/Divisions/ConstructionMgt/design-build/bsb-project-procurement/Pages/default.aspx for information on the project's procurement process. This web site also includes a form to sign up for email alerts to be notified of any future networking opportunities.

Comment Number	Nature of Comment	Comment	Response
135A	Alternatives	<p>01/03/2023 - Email Comment - So I'm just confused as an outside observer looking to move to Cincinnati or Covington because of the opportunity for urban growth. In Cincinnati, a city famous for unbelievable destruction of a city's urban fabric in the name of highways, the plan is to create more lanes? With 1.6 or however many billion dollars there is no plan to at least add rail or public transit option? This is not the future younger generations want. We want to correct the mistakes of the past. At least have a street car option or something. Have you ever heard of induced demand? Evidence based policy? So disappointed in this city.</p>	<p>Traffic projections are based on a regional travel-demand model which assigns routes used by travelers based on detailed information for individuals, households, number of lanes, projected trips, and calculated travel times. Traffic projections show that adding lanes will increase traffic volumes in the Brent Spence Bridge Corridor. Some of that increase is due to travelers shifting trips they were already making from other congested routes. In addition, some travelers will make new trips they would not have made without the highway improvements (induced trips). The Brent Spence Bridge Corridor Project is designed to accommodate all trips (including induced trips) projected by the Ohio-Kentucky-Indiana (OKI) regional travel demand model through the year 2050.</p> <p>In 2004, the Ohio-Kentucky-Indiana Regional Council of Governments (OKI) and the Miami Valley Regional Planning Commission (MVRPC) completed a major planning study known as the North South Transportation Initiative (Initiative) that considered highway improvements in addition to transit improvements such as express bus, commuter rail, and others. The Initiative concluded that a highway improvement project was necessary to address capacity issues on I-75, including the Brent Spence Bridge Corridor. While the original findings of the Initiative called for four lanes in each direction on I-75, traffic analyses completed as part of ODOT's Millcreek Expressway and Thru the Valley projects determined that five lanes were needed south of the I-74/I-75 interchange. This change was approved by OKI. The Brent Spence Bridge Corridor Project addresses the highway component of the Initiative by improving interchanges and providing the number of lanes previously approved by OKI. The Brent Spence Bridge Corridor Project will address traffic congestion, substantially improve safety, and enhance travel for other modes of transportation. The transit component included in the Initiative must be developed and championed regionally, and ODOT and KYTC are ready to support this when it is advanced at a regional level. The North South Transportation Initiative planning study can be accessed here: www.oki.org/studies/pdf/northsouth/ns-complete.pdf.</p>

Comment Number	Nature of Comment	Comment	Response
136	Alternatives	01/04/2023 - Web Comment - How about making that bridge light rail ready to link downtown to the airport? It would bring business downtown to and from the airport, have stops along the way to drop and pick-up people off at their places of work in and around the airport and increase the potential for economic growth. This would give us alternatives that would reduce the pollution of all the cars going back and forth. We could use the light rail already in the city of Cincinnati.	The project has not incorporated light rail into the design because it is not supported by the project's purpose and need, and there are no current plans for new rail in the region. New light rail facilities would need to be evaluated as part of a separate project.
136A	Alternatives	<p>01/04/2023 - Email Comment - I hope you are well. I write to you today to express my dissatisfaction with the current Brent Spence Bridge companion proposal. While I (as well as many other Cincinnati residents) have outwardly expressed their dissatisfaction over the past year and more, it seems to have fallen on deaf ears — whether that be at in-person meetings, in official public comments, or even just on social media. My comments today will be rather lengthy. I honestly would prefer to keep the feedback short and sweet, but at this point in time I see no other way to get the attention of ODOT and those planning for our city's future. Additionally, I will try my best to include references/URLs within this public comment to support the points laid out.</p> <p>First and foremost, it is of the utmost importance that something be done about the Brent Spence Bridge. It has been in official need of a replacement since 1998 according to the Federal Highway Administration (https://www.oki.org/brentspencebridge/). I am 100% in favor of repairs, a replacement, or a companion to the current bridge, and I'm sure those who utilize it regularly and see the first-hand dilapidation would agree. If the companion bridge is to be built, so be it. It would provide another link within the greater Cincinnati and larger Midwest regions that will prove invaluable in the coming years as Cincinnati continues to urbanize and densify. The issues begin to arise when we examine who (drivers) and what (cars and trucks) will utilize the new span, and the opportunity cost of what instead could be built in its place.</p> <p>The current proposed companion bridge (https://www.cincinnati.com/story/news/traffic/2022/08/29/how-much-traffic-will-brent-spence-companion-bridge/65407695007/) allots 10 new lanes to exclusively car and truck traffic. While this bridge is set</p>	<p>Traffic projections are based on a regional travel-demand model which assigns routes used by travelers based on detailed information for individuals, households, number of lanes, projected trips, and calculated travel times. Traffic projections show that adding lanes will increase traffic volumes in the Brent Spence Bridge Corridor. Some of that increase is due to travelers shifting trips they were already making from other congested routes. In addition, some travelers will make new trips they would not have made without the highway improvements (induced trips). The Brent Spence Bridge Corridor Project is designed to accommodate all trips (including induced trips) projected by the Ohio-Kentucky-Indiana (OKI) regional travel demand model through the year 2050.</p> <p>ODOT thoughtfully considered the designs offered by the Bridge-Forward Cincinnati Coalition, which included burying the freeway. A detailed response is provided on the project website: https://brentspencebridgecorridor.com/wp-content/uploads/2022/10/BF-Response-to-Public-Comment_w-Executive-Summary-1.pdf. ODOT is also coordinating with Bridge-Forward to schedule a working session to discuss their ideas about the project.</p> <p>The current design will open up an additional 9.5 acres of land for potential redevelopment adjacent to Cincinnati's Central Business District. ODOT will continue to work with the City of Cincinnati throughout the remaining design activities to identify opportunities to incorporate refinements that reclaim additional land.</p> <p>In 2004, the Ohio-Kentucky-Indiana Regional Council of Governments (OKI) and the Miami Valley Regional Planning Commission (MVRPC) completed a major planning study known as</p>

Comment Number	Nature of Comment	Comment	Response
136A (cont.)	Alternatives (cont.)	<p>aside for “express traffic,” ODOT will effectively be doubling the number of 71/75 lanes across the Ohio River, inducing demand for car and truck travel. As I’m sure ODOT is aware, more highway lanes (especially without tolls) lead to more drivers utilizing the lanes (example from Houston with additional hyperlinks embedded: https://brokensidewalk.com/2015/induced-demand-in-action/). See this link for further evidence: https://t4america.org/wp-content/uploads/2020/03/Congestion-Report-2020-FINAL.pdf. Whether or not these cars or trucks are just passing through via the “express companion bridge” or entering the city via the “local Brent Spence bridge,” they are polluting our downtown and taking up valuable space that could be allotted to people who live in the city and tax-generating real-estate. Moreover, this problem does not go away with electric cars. Sure they do not with CO2 emissions, but electric cars take up just as much space/volume as regular cars, actually weigh significantly more than ICE-powered cars, and still pollute via tire degradation: https://www.theguardian.com/environment/2022/jun/03/car-tyres-produce-more-particle-pollution-than-exhausts-tests-show. What ODOT is doing with their outdated logic by inviting this enormous amount of additional car traffic into downtown Cincinnati will hurt businesses in the Central Business District and residents who live car-free or car-light (of which a significant portion of Cincinnatians are doing, and it’s only increasing: https://usa.streetsblog.org/2021/10/22/u-s-cities-have-more-car-free-households-than-you-think/) with increased pollution and congestion.</p> <p>Speaking more on the “space” portion of my above comment, the acreage dedicated to Ohio approach interchange is immense. While the proposed footprint is smaller than the current interchange on the West Side, it is a marginal decrease at best. Originally in the 1950s-60s, ODOT bulldozed and raised the West End of Cincinnati to make way for the interstate. More than 10,000 homes, businesses, and buildings were essentially deleted from existence to make way for what stands today. An area the size of Over the Rhine occupying the flat, sprawling plain west of downtown was simply wiped clean in the name of highway construction, what planning documents call “slum clearance” and a landing spot for light industry (https://www.citybeat.com/news/echoes-of-a-lost-west-end-12219411). 25,000 people lived in the 297 acre neighborhood of</p>	<p>the North South Transportation Initiative (Initiative) that considered highway improvements in addition to transit improvements such as express bus, commuter rail, and others. The Initiative concluded that a highway improvement project was necessary to address capacity issues on I-75, including the Brent Spence Bridge Corridor. While the original findings of the Initiative called for four lanes in each direction on I-75, traffic analyses completed as part of ODOT’s Millcreek Expressway and Thru the Valley projects determined that five lanes were needed south of the I-74/I-75 interchange. This change was approved by OKI. The Brent Spence Bridge Corridor Project addresses the highway component of the Initiative by improving interchanges and providing the number of lanes previously approved by OKI. The Brent Spence Bridge Corridor Project will address traffic congestion, substantially improve safety, and enhance travel for other modes of transportation. The transit component included in the Initiative must be developed and championed regionally, and ODOT and KYTC are ready to support this when it is advanced at a regional level. The North South Transportation Initiative planning study can be accessed here: www.oki.org/studies/pdf/northsouth/ns-complete.pdf.</p> <p>The project will reduce congestion and improve reliability for local bus routes, which use the Brent Spence Bridge for 210 trips every weekday. In addition, improved and new sidewalks, shared use paths, and bicycle lanes will improve connections to existing bus stops. Finally, future accommodation of bus service along the corridor’s shoulder will not be precluded by the project. Pedestrian and bicycle accommodations are not planned on the new companion bridge because of the proximity of a reasonable crossing at the Clay Wade Bailey Bridge. Furthermore, any new rail crossings of the Ohio River will need to be evaluated as part of a separate project.</p> <p>In addition to ODOT, there are multiple stakeholders in the tri-state region when it comes to transportation. There are regional mass transit agencies, local governments, an airport authority, and two additional states that all play a role in addressing the transportation needs of the region. As we have for decades, we’ll work hand-in-hand with them to project and meet the current and future needs of the region.</p> <p>KYTC and ODOT are cognizant of historic freeway construction that</p>

Comment Number	Nature of Comment	Comment	Response
136A (cont.)	Alternatives (cont.)	<p>Kenyon-Barr (images: https://twitter.com/JoshJunker2/status/1608548164784762882). Today the “neighborhood” (if you can call it that) is known as Queensgate — home to a total of 120 people at the time of this article (2017).</p> <p>With the opportunity to right the wrongs of the past and create a forward-looking, interconnected multi-modal city, ODOT can amend their plans for the Ohio approach to follow that of Bridge Forward Cincinnati (https://www.bridge-forward.org/). Bridge Forward puts forth a plan that still calls for the construction of a companion bridge, but buries the Ohio approach so that over 30 acres of land can be reclaimed. In adopting this approach, ODOT could effectively create 30 acres for businesses and high density homes to be developed — all of which would repay taxes to the city and state governments. This would be an incredible investment in the city’s future while still fixing the issues at hand with the current Brent Spence bridge.</p> <p>There is, however, one issue with the plan — it calls for a >5% grade on part of the approach (https://local12.com/news/local/i-75-interchange-odot-review-plan-30-new-acres-city-blocks-not-feasible-downtown-cincinnati-queensgate-brent-spence-covington-northern-kentucky-nky-bridge-forward-local-12-wkrc-tristate-ohio-ky-indiana-news) which is larger than allowed by federal interstate highway standards (https://en.wikipedia.org/wiki/Interstate_Highway_standards). I would urge ODOT to not let this one sticking point, which they call out as being THE blocker here, to de-rail a once-in-a-generation opportunity to develop western downtown and reconnect Cincinnati with its West End. Exceptions can be made, plans can be altered; however, ODOT has not even assembled a Bridge Forward-inspired alternative for us to compare or analyze. Should they put in the work, I believe that ODOT could find a way to fit most or all of Bridge Forward’s plan into their proposal while still following federal guidelines for interstate construction. Again, I want to emphasize that this would be more than just a fix for the bridge — it would be an investment in the city’s future — a future that will be shaped by more than just cars.</p> <p>With all that said, I pose the following questions to ODOT and request written, detailed responses to each one. Please see below:</p>	<p>has impacted communities and have incorporated several features that reduce the project’s footprint, including optimizing interchange geometry by utilizing the land formerly occupied by the Dunnhumby building, lowering design speeds, reducing shoulder widths, implementing retaining walls, and reducing the width of the companion bridge. In West End, the project will be built almost entirely within the existing highway footprint, and the project will not relocate any residents in West End. Furthermore, the project includes features to enhance connections in existing neighborhoods, such as installing bicycle and pedestrian infrastructure in and between the CBD, Queensgate, and the West End. Pedestrian and bicycle connections will be included across I-75 on 6th Street, 7th Street, 9th Street, Linn Street, Freeman Avenue, Ezzard Charles, Liberty Street, Findlay Street, Bank Street, and Harrison Avenue. In addition, a new shared use path will be constructed along Winchell Avenue between 9th Street and Ezzard Charles Drive, including a pedestrian bridge connection to Freeman Avenue.</p>

Comment Number	Nature of Comment	Comment	Response
136A (cont.)	Alternatives (cont.)	<p>Questions:</p> <ul style="list-style-type: none"> • Will ODOT amend their proposal or even create a new alternative for the Ohio approach to reconnect downtown Cincinnati with the West Side, bury the freeway, and reclaim 30 acres of land that could be used to generate tax revenue and further fund improvements to our city? • What is ODOT doing to ensure forward-compatibility of the new companion bridge with other modes of transit such as, but not limited to: bus rapid transit, streetcar/tram, light rail, heavy rail, inter-city rail, and high speed rail? • What is ODOT's outlook on cars as the dominant mode of transportation in the Cincinnati/Northern Kentucky region? Are they projecting any mode-changes within the next decade or so? • Will ODOT ever consider funding large-scale rail projects? • What is ODOT doing to rectify the razing of the West End in the 50s and 60s? Are there currently efforts to reconnect the neighborhoods affected by the proposal ODOT has put forward? <p>All of the above, from the additional bridge to the Ohio approach and everything in between, can be summarized with the following sentence: this is not a 21st century solution. What ODOT has proposed is a 20th century solution. Their proposal exasperates past mistakes and injustices to those who lived in the path of the current Ohio-side interchange and does nothing to rectify that all while ignoring their own numbers which say traffic across the bridge has been decreasing steadily since 2014: https://odot.public.ms2soft.com/tcds/tsearch.asp?loc=odot. Their proposal is focused solely on cars and trucks with zero provisions for future rail options whether those be local, regional, or inter-city. It should be noted that the Cincinnati Connector has regained its ridership faster than every city but Atlanta's rail system and has posted record-high ridership totals for 4 months in a row (https://twitter.com/bradleywthomas/status/1600861667554103297?s=20&t=cXJSVR5tXjINKXROeBD10Q).</p> <p>Therefore, I urge ODOT to reconsider their plans for the Ohio approach and amend them to incorporate Bridge Forward's basic outline of burying the companion bridge's Ohio-side approach to reclaim 30 acres of tax revenue-generating land. Trucks and cars, especially as we transition to electric vehicles, will forever be a part</p>	

Comment Number	Nature of Comment	Comment	Response
136A (cont.)	Alternatives (cont.)	<p>of our region and transportation plan. The times of dedicating acres and acres of land to them rather than people and residents of the city, though, are over. This project is scheduled to be complete in 2030 — a 2030 that will look drastically different from the 2023 we are just getting to know today. Will ODOT build a project based on the mistakes and mis-logic of the past? Or will they work to be innovators in the space of 21st century highway reconstruction and help propel Cincinnati to be one of the country's leaders when it comes to urban design and people-oriented cities?</p> <p>In closing, I again urge ODOT to work with Bridge Forward to construct an Ohio approach and companion bridge that will benefit all of Cincinnati for decades and decades to come.</p>	
137	Project Mailing List	01/04/2023 - Web Comment - Keep me posted. Thanks	This individual was added to the project mailing list.
138	Alternatives	<p>01/05/2023 - Web Comment -</p> <ol style="list-style-type: none"> 1. COMPANION BRIDGE WILL NOT WORK 2. HILLSIDE IS NOT STABLE ENOUGH 3. WILL REQUIRE EMINITE DOMAINE OF LAND NEAR 140 FEET WIDE. 4. CONSTRUCTION COSTS WILL MOVE IT TO COMPLETION NEAR \$7B. 5. TRUCKS WILL CONTINUE TO SLOW BECAUSE OF THE GRADE RISE. <p>CONCLUSION.. NOT FEASEABLE 1960 THEY NEW THERE WOULD BE A PROBLEM.</p> <ol style="list-style-type: none"> 1. CANTALEVER DESIGN LIMITED CAPACITIY 2. GRADE LEVEL WOULD SLOW TRUCK TRAFFIC <p>ANSWER. . . THIRD DECK 100 FEET WIDE, BYPASSING DOWNTOWN EXITS. 5* GRADE, FROM HOPPLE STREET TO KYLES LANE. TIME TO COMPLETION LESS THAN FOUR YEARS. COST THEN \$20MM, TODAY <\$2B. THIRD DECK ADDS NEW PILLINGS FIFTEEN FEET OUT IN RIVER (REQUIRES BARGES TO BE A LITTLE THINNER) AND ADDS MORE SUPPORT STRENGTH TO THE CANTELIVER.</p>	<p>The alternatives evaluation process and engineering study conducted for the project has shown that the new companion bridge is feasible and the preferred alternative for addressing the traffic flow and safety problems in the corridor. The Brent Spence Bridge Corridor Project (including the new companion bridge) is estimated to cost \$3.6 billion. While additional land will be required to build the project, ODOT and KYTC have worked to reduce the project's footprint. The project will not change the grade on the cut-in-the-hill in Kentucky. Adding a third deck to the existing Brent Spence Bridge would not satisfy the project's purpose and need, because it would not provide enough lanes to carry the traffic projected to travel through the corridor.</p>

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139	Construction	01/05/2023 - Web Comment - Ladies and Gentlemen This is [REDACTED] from Hebetec Engineering Ltd www.hebetec.com in Switzerland. We are a small company, part of the worldwide known Vinci group. Our passion is lifting, lowering, and hauling of heavy and very heavy loads. Among others we are specialized in Works related with building and demolishing bridges. Within the past years we took part on several demolition projects in the US such as Oakland Bay Bridge CA or Tappan Zee Bridge NJ. We also do large sliding projects, e.g., Lennetal Bridge in Germany, where we slid an entire, pre-constructed concrete bride of over 3.200ft length and 33.000tons weight, laterally by 65ft in one piece. Seeing your landmark project Brent Spence Bridge Rehabilitation, we would be keen to support you or your contractors with our expertise.	Please visit https://www.dot.state.oh.us/Divisions/ConstructionMgt/design-build/bsb-project-procurement/Pages/default.aspx for information on the project's procurement process. This web site also includes a form to sign up for email alerts to be notified of any future match-making opportunities.
140	Information Request	01/05/2023 - Web Comment - I'd like to speak to the public relations person who is handling the Brent Spence Bridge communications to the public. What's Happening! in Boone, Campbell, and Kenton Counties would like to keep the citizens apprised of the plans for the Brent Spence Bridge Project and how it will impact their lives, commutes, and quality of life. For the past 33 years, we have mailed a semi-annual, full-color publication to every household in the NKY corridor. Please email or call me at [REDACTED] to discuss partnering with us. Thank you! [REDACTED]	This individual was contacted by a member of the project team.
141	Construction	01/05/2023 - Web Comment - I am interested in receiving information about bidding this project as a supplier. We are a DBE certified company that specializes in drilling, heavy construction equipment, perm and temp casing, tooling, etc. for sale or rental. The size of our equipment ranges from 2'(610mm) to 14'(4268mm) in diameter.	Please visit https://www.dot.state.oh.us/Divisions/ConstructionMgt/design-build/bsb-project-procurement/Pages/default.aspx for information on the project's procurement process. This web site also includes a form to sign up for email alerts to be notified of any future networking opportunities.
142	Construction	01/05/2023 - Web Comment - Has this project been awarded to a GC yet	The draft Request for Proposals (RFP) for progressive design-build teams will be released on January 13, 2023. The final RFP will be advertised on February 17, 2023. The design-build contract will be awarded on May 31, 2023. Additional information can be found on the project's procurement website: https://www.dot.state.oh.us/Divisions/ConstructionMgt/design-build/bsb-project-procurement/Pages/default.aspx
143	N/A	01/05/2023 - Web Comment - MAP OF CINCINNATI & COVINGTON	Comment noted.

Comment Number	Nature of Comment	Comment	Response
143A	Alternatives	<p>01/05/2023 - Email Comment - Please see the attached file of petitions regarding the Brent Spence Corridor Project. Please enter these into the public record.</p> <p>Text from attached letters: I'm writing to you today in hopes that you will oppose the Brent Spence Bridge Corridor expansion project. The project won't solve the bridge's traffic and safety problems, but it will harm local communities, cost taxpayers billions of dollars, and only heightens our reliance on cars and fossil fuels.</p> <p>Highway expansion projects harm already vulnerable populations. This expansion project will promulgate plumes of dust and fumes, worsening air pollution and health outcomes in nearby communities in the long term while interrupting people's everyday lives in the short term. After months of negotiation, Covington residents are still giving up over a dozen properties to the state to build this highway. And despite all of this, the project likely will not even solve the problems it is setting out to fix. Research strongly suggests that expanding highway capacity attracts more vehicles and increases traffic, the reverse effect of what KYTC's proposed plan is supposed to achieve.</p> <p>Our region carries the burden from hundreds of trucks crossing the bridge corridor daily, but most of the freight crossing the bridge is neither made here nor delivered here. The chronic truck congestion on the bridge makes it extremely unsafe. Rather than spend billions on an un-aesthetic, unnecessary highway, Kentucky and Ohio should be investing in the livelihoods of its residents. Funding should be shifted to expanding transit options and multi-modal transportation projects while reducing non-local truck traffic. We should be reducing air pollution and greenhouse gas emissions, not creating opportunities to worsen them.</p> <p>We cannot remain fully dependent on cars as transportation. As we have seen recently, the financial precariousness of car ownership places a mounting burden on American families. Investing in better public transit systems will reduce traffic congestion, reduce the cost to taxpayers while seeing a more beneficial use of their money, and improve the quality of life in neighboring communities.</p> <p>I am hoping that you will not support expanding the Brent Spence highway but rather invest in our communities, our transportation system, and our health and safety.</p>	<p>Thank you for your comment regarding the Brent Spence Bridge Corridor Project. The provided petition and accompanying form letters have been included in the project record.</p> <p>The Brent Spence Bridge Corridor Project is a direct outcome of a major planning study known as the North South Transportation Initiative (Initiative) that was conducted by the Ohio-Kentucky-Indiana Regional Council of Governments (OKI) and the Miami Valley Regional Planning Commission (MVRPC) in 2004. The study considered highway improvements in addition to transit improvements such as express bus, commuter rail, and others. The study and subsequent update concluded that additional highway capacity (five lanes) were needed south of the I-74/I-75 interchange, including the Brent Spence Bridge.</p> <p>The agencies (ODOT and KYTC) then established the Brent Spence Bridge Corridor Project to address the highway capacity needs. At the onset of the project and continuing through today, the identified goals of the Brent Spence Bridge Corridor Project are:</p> <ul style="list-style-type: none"> • Improve traffic flow and level of service • Improve safety • Correct geometric deficiencies • Maintain connections to key regional and national transportation corridors <p>The Brent Spence Bridge Corridor Project will address traffic congestion, substantially improve safety, and seek to enhance travel for other modes of transportation, including bicycle and pedestrian, within the project corridor. In addition to engineering feasibility studies, the project has followed and will continue to follow the National Environmental Policy Act (NEPA) to evaluate environmental impacts from the project. As such, the project team has worked to reduce the project footprint and impacts, specifically to properties located within Covington.</p>

Comment Number	Nature of Comment	Comment	Response
143B	Public Input	01/05/2023 - Email Comment - Please see attached for a progress report taken today of results from a survey that Bridge Forward Cincinnati has been circulating in the community, here. Each response is from a unique respondent. Final results and an analysis will be forwarded when the survey period is complete.	Comment noted.
143C	Alternatives	01/05/2023 - Email Comment - Please also see attached for letters of support for Bridge Forward's design direction goals to date from local Community Councils. More will follow. The final attachment is procurement language that Bridge Forward is requesting be included in the initial RFQ or as an addendum to the RFQ. [Attached letters from: Camp Washington Community Council (undated), Over the Rhine Community Council (12/20/2022), West End Community Council (10/25/2021 and 10/28/2022).]	The provided letters of support for Bridge Forward's design direction goals have been added to the project record. We are continuing to work on the Brent Spence Bridge Corridor Project with our stakeholders to capture opportunities to address shared community priorities. The specific language in the progressive design-build contract documents is developed and vetted through a rigorous process led by a multidisciplinary team of industry and legal professionals. The draft contract documents released on January 13, 2023 include requirements for the design-build team to develop draft alternative design concepts and an innovation plan that improve project quality, reduce costs, shorten schedule, and support the project goals and objectives. Stated objectives in the draft contract documents include: building a good project by building a project with a context sensitive design that fits within the community, minimizing footprint to maximize public investment and potential for developable space, improving neighborhood connectivity across the interstate, providing strong aesthetic value, minimizing physical intrusion, and designing for sustained quality of life, among others. Draft alternative concepts will be evaluated through a collaborative effort with ODOT and KYTC and other community partners, as appropriate, and will consider long term maintenance and operation costs. The draft contract documents also require adherence to all environmental commitments in the supplemental EA.
144	Construction	01/06/2023 - Web Comment - When are RFPs due?	The draft Request for Proposals (RFP) for progressive design-build teams will be released on January 13, 2023. The final RFP will be advertised on February 17, 2023. Proposals from design-build teams are due on March 31, 2023. The design-build contract will be awarded on May 31, 2023. Additional information can be found on the project's procurement website: https://www.dot.state.oh.us/Divisions/ConstructionMgt/design-build/bsb-project-procurement/Pages/default.aspx

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145	Construction	01/06/2023 - Web Comment - I am a licensed professional Civil Engineer in Ohio. I would be thrilled to work on the new bridge project.	Please visit https://www.dot.state.oh.us/Divisions/ConstructionMgt/design-build/bsb-project-procurement/Pages/default.aspx for information on the project's procurement process. This web site also includes a form to sign up for email alerts to be notified of any future networking opportunities.
146	Historic Resource	01/06/2023 - Web Comment - I have noticed, after looking at the renderings of the new bridge, that a major portion of the B&O Freight Terminal will be demolished. I am not sure if anyone is aware or not, but this building is listed on the National Register of Historic Places. As a supporter of historic preservation, I implore you to consider the historic aspects of this building and find a way to avoid demolishing this historic building. As a starting point, may I direct you to the Golden Gate Bridge? When it was initially designed, Fort Point, a historic fortification on the shores of San Francisco Bay, was to be demolished. But, after a redesign of the bridge, an arch span was incorporated into one of the approaches that straddled the fort. Hopefully, this may be an option to preserve this historic freight terminal.	ODOT and KYTC are aware of Longworth Hall's historic significance. The alternatives analysis for the Brent Spence Bridge Corridor Project evaluated ways to avoid impacts to Longworth Hall. Out of 25 alternatives that were studied, only one alternative was found to meet the project's purpose and need while avoiding impacts to Longworth Hall. This alternative would have resulted in substantially greater residential and business impacts, including the largest employer in the Queensgate area. This alternative was also projected to cost substantially more than other alternatives under consideration. In addition, the cities of Covington and Cincinnati strongly opposed this alternative. As a result, impacts to 204 feet of Longworth Hall were found to be unavoidable. To mitigate the impacts, ODOT entered into a Memorandum of Agreement committing to completing various repair, upgrade, restoration, enhancement, and refurbishment measures to be completed at Longworth Hall as part of the project.

Comment Number	Nature of Comment	Comment	Response
147	Roadway/Bridge Design	<p>01/09/2023 - Web Comment - I was reviewing the documents for the KY aesthetics committee from August of 2022. The proposed street designs and under pass designs around the bridge concern me. Firstly, does the highway off-ramp to 5th street need to have two lanes? Currently the off-ramp has only one lane and it seems more than sufficient. Eliminating the second lane would reduce the project's footprint and likely lead to a reduction in costs even if minimal.</p> <p>Secondly, turning radii of the proposed intersections along 5th and Crescent look very generous, which will likely encourage cars to speed. Please consider tightening the turning radii as this will greatly improve the safety of streets for all users. Vehicles will move slower. Pedestrians will have less distance to travel across the roads leaving less time interacting with vehicles in the street.</p> <p>Another thing I would encourage you all to look at is the necessity of slip lanes, as well as the four lane intersection of Crescent and West 3rd. Given those are the streets that enter Botany Hills and the Kenton Hills neighborhoods, the intersection should be more welcoming. The current and proposed designs create a confusing intersection that is hostile to drivers and pedestrians. Please consider reducing the number of lanes and "dieting" the intersection.</p>	<p>Regarding the number of lanes on the 5th Street off-ramp, there is only one lane exiting the collector-distributor northbound to 5th Street. The second lane at the 5th Street intersection is from the local street connecting 9th Street and 5th Street. The number of lanes on the 5th Street ramp is based on traffic operational analyses completed for the project, which consider the traffic projected to travel in the corridor through the year 2050. To promote safety for bicycles and pedestrians, the ramp connections with local streets are being designed as lower-speed urban roadways. The design of the intersections of local streets will be in accordance with City of Covington design standards.</p> <p>For your comment regarding the intersection of 5th and Crescent, the plans shown in the exhibits are still preliminary. Once on board, a Design Build Team will develop more detailed project plans that will take a better look at the intersections within the project limits. To promote safety for bicycles and pedestrians, the ramp connections and other intersections with local streets are being designed as lower-speed urban roadways. The design of the intersections of local streets will be in accordance with City of Covington design standards.</p>

Comment Number	Nature of Comment	Comment	Response
148	Roadway/Bridge Design	01/09/2023 - Web Comment - How come the new bridge can't be a bridge for traffic taking I75 (4/5 lanes) while Brent Spence Bridge (reduced to 3 lanes) is a bridge solely for traffic going from I71/I75 onto Fort Washington Way and onto Columbus? Both bridges could still have exits/on ramp to/from downtown Cincinnati thus meaning the insane spaghetti looking ramps of an interchange on the Cincinnati side would likely take up less? I feel this idea should very much be considered since we only get to do this right once!	<p>The project will build a new double-decker companion bridge that will carry through (Interstate) traffic. The project will also build a collector-distributor system that will provide access to and from local destinations. The existing Brent Spence Bridge will be used for local traffic as part of the collector-distributor system. Access to I-71 will be provided via the new companion bridge and the existing Brent Spence Bridge. Ramps to and from the Interstate will provide access to the collector-distributor system, from which travelers will be able to access local destinations. Adding additional exits/on ramps to and from downtown Cincinnati from the through (Interstate) lanes would increase the number of ramps on the Cincinnati side and would take up substantially more space than the current design.</p> <p>Various configurations of lanes at the crossing of the Ohio River have been considered throughout the project's development, including suggestions similar to what the commenter describes. The preferred solution, Concept I-W, has been found to best meet the goals identified for the project.</p>
149	Public Input	01/10/2023 - Web Comment - what is the public participation process for the BS bridge and the corridor?	<p>KYTC and ODOT completed a series of 16 targeted neighborhood outreach meetings for the project in November and December 2023. The public is also able to submit comments via the project website (www.brentspencebridgecorridor.com). Public hearings scheduled in July/August of 2023 will provide another opportunity for public participation. The public can also access information about the project by visiting the project website, signing up for project updates, and viewing the project's social media accounts: https://www.facebook.com/BrentSpenceInfo and https://twitter.com/brentspenceinfo.</p>
150	Construction	01/10/2023 - Web Comment - I simply wanted to know if this contract has been awarded. I also would like to know who the contract was awarded to. Our company has several deck barges and spud barges that may be able to help with the project. I appreciate your time.	<p>The draft Request for Proposals (RFP) for progressive design-build teams was released on January 13, 2023. The final RFP will be advertised on February 17, 2023. Proposals from design-build teams are due on March 31, 2023. The design-build contract will be awarded on May 31, 2023. Additional information can be found on the project's procurement website: https://www.dot.state.oh.us/Divisions/ConstructionMgt/design-build/bsb-project-procurement/Pages/default.aspx</p>

Comment Number	Nature of Comment	Comment	Response
151	Roadway/Bridge Design	01/10/2023 - Web Comment - Will the NEW Brent Spence Bridge for I-71-75 also be a double-decker bridge?	Yes. The new companion bridge will be a double-decker structure. The top deck will carry five lanes of northbound traffic, and the bottom deck will carry five lanes of southbound traffic.
152	Construction	01/10/2023 - Web Comment - --Could you send me information on who is doing the work on this bridge? I heard you needed welders, etc. I have bridge building experience. Have supervised total construction of bridges, experienced welder, etc. Just need to know who I need to contact to get more information on working on this project. Thanks for you help any help is greatly appreciated.	Please visit https://www.dot.state.oh.us/Divisions/ConstructionMgt/design-build/bsb-project-procurement/Pages/default.aspx for information on the project's procurement process. This web site also includes a form to sign up for email alerts to be notified of any future networking opportunities.
153	Noise	01/11/2023 - Web Comment - I live on the corridor very close to the highway and want to do everything I can to get the walls built so that it isn't as loud for the residents.	KYTC has identified several locations for potential noise walls to mitigate noise impacts in accordance with its noise policy. In addition, both KYTC and ODOT are evaluating additional walls or other features to further reduce noise and provide visual screening to the highway in noise sensitive areas that did not warrant noise walls according to their noise policies.
154	Noise	01/11/2023 - Web Comment - I, along with the members in my household, are in full support of a sound wall to improve the quality of living on W Maple as well as avoid potential problems involved. <ul style="list-style-type: none"> * Property values on West Maple will, undoubtedly, go down should noise levels increase. * Currently as a resident of West Maple, we can't hold a conversation in our front yard with friends and neighbors given the intense noise levels. * West Maple currently has the highest noise level reading in all of Fort Mitchell. This will only get worse with this project, and even with a sound wall, this West Maple Avenue will still be the highest noise level reading in all of Fort Mitchell. * Even before this project, the noise from the traffic and trucks, specifically, wake us up in the middle of the night. This will only get worse as traffic increases. * The sound wall along West Maple has been deemed unreasonable, meaning it doesn't affect enough people to make it cost effective; however, the taxes paid by the residents of West Maple are significant given the home values associated with this area. We pay WAY to much to the city to not be considered for this sound wall. It affects our lives!	<p>You provided a comment regarding a noise wall near West Maple Avenue. As part of the environmental work for the project, the Kentucky Transportation Cabinet (KYTC) evaluated several noise walls and noise wall systems along the Brent Spence Bridge Corridor. Some were found to be warranted based on KYTC's Noise Policy. However, a noise wall was not found to be cost effective in the vicinity of West Maple Avenue.</p> <p>Recognizing that traffic noise is a concern, KYTC conducted a technical study to further evaluate a noise/visual screening wall to help provide about a 3-6 decibel noise reduction for residences near West Maple Avenue and to shield views of the highway. Based on the technical feasibility and public comments, KYTC is currently recommending a noise/visual screening wall at this location. KYTC and the project team will conduct additional coordination with the people who will benefit from the walls during the project's design-build phase.</p>

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155	Noise	<p>01/11/2023 - Web Comment - I, along with the members in my household, are in full support of a sound wall to improve the quality of living on W Maple as well as avoid potential problems involved.</p> <ul style="list-style-type: none"> * Property values on West Maple will, undoubtedly, go down should noise levels increase. * Currently as a resident of West Maple, we can't hold a conversation in our front yard with friends and neighbors given the intense noise levels. * West Maple currently has the highest noise level reading in all of Fort Mitchell. This will only get worse with this project, and even with a sound wall, this West Maple Avenue will still be the highest noise level reading in all of Fort Mitchell. * Even before this project, the noise from the traffic and trucks, specifically, wake us up in the middle of the night. This will only get worse as traffic increases. * The sound wall along West Maple has been deemed unreasonable, meaning it doesn't affect enough people to make it cost effective; however, the taxes paid by the residents of West Maple are significant given the home values associated with this area. We pay WAY to much to the city to not be considered for this sound wall. It affects our lives! 	<p>You provided a comment regarding a noise wall near West Maple Avenue. As part of the environmental work for the project, the Kentucky Transportation Cabinet (KYTC) evaluated several noise walls and noise wall systems along the Brent Spence Bridge Corridor. Some were found to be warranted based on KYTC's Noise Policy. However, a noise wall was not found to be cost effective in the vicinity of West Maple Avenue.</p> <p>Recognizing that traffic noise is a concern, KYTC conducted a technical study to further evaluate a noise/visual screening wall to help provide about a 3-6 decibel noise reduction for residences near West Maple Avenue and to shield views of the highway. Based on the technical feasibility and public comments, KYTC is currently recommending a noise/visual screening wall at this location. KYTC and the project team will conduct additional coordination with the people who will benefit from the walls during the project's design-build phase.</p>

Comment Number	Nature of Comment	Comment	Response
156	Noise	<p>01/11/2023 - Web Comment - I, along with the members in my household, are in full support of a sound wall to improve the quality of living on W Maple as well as avoid potential problems involved.</p> <ul style="list-style-type: none"> * Property values on West Maple will, undoubtedly, go down should noise levels increase. * Currently as a resident of West Maple, we can't hold a conversation in our front yard with friends and neighbors given the intense noise levels. * West Maple currently has the highest noise level reading in all of Fort Mitchell. This will only get worse with this project, and even with a sound wall, this West Maple Avenue will still be the highest noise level reading in all of Fort Mitchell. * Even before this project, the noise from the traffic and trucks, specifically, wake us up in the middle of the night. This will only get worse as traffic increases. * The sound wall along West Maple has been deemed unreasonable, meaning it doesn't affect enough people to make it cost effective; however, the taxes paid by the residents of West Maple are significant given the home values associated with this area. We pay WAY too much to the city to not be considered for this sound wall. It affects our lives! 	<p>You provided a comment regarding a noise wall near West Maple Avenue. As part of the environmental work for the project, the Kentucky Transportation Cabinet (KYTC) evaluated several noise walls and noise wall systems along the Brent Spence Bridge Corridor. Some were found to be warranted based on KYTC's Noise Policy. However, a noise wall was not found to be cost effective in the vicinity of West Maple Avenue.</p> <p>Recognizing that traffic noise is a concern, KYTC conducted a technical study to further evaluate a noise/visual screening wall to help provide about a 3-6 decibel noise reduction for residences near West Maple Avenue and to shield views of the highway. Based on the technical feasibility and public comments, KYTC is currently recommending a noise/visual screening wall at this location. KYTC and the project team will conduct additional coordination with the people who will benefit from the walls during the project's design-build phase.</p>
157	Noise	<p>01/11/2023 - Web Comment - Hello, I reside on W Maple Ave, and am very hopeful that this team can help us resolve the noise pollution that exists on the interstate immediately behind our street. Driving to work today, I noticed that there are sound walls just north of Dixie Hwy on the interstate. Any help you can offer would be tremendously appreciated. The cars and mostly trucks driving south on I-75 wake me up routinely throughout the night. Thank you for listening.</p>	<p>You provided a comment regarding a noise wall near West Maple Avenue. As part of the environmental work for the project, the Kentucky Transportation Cabinet (KYTC) evaluated several noise walls and noise wall systems along the Brent Spence Bridge Corridor. Some were found to be warranted based on KYTC's Noise Policy. However, a noise wall was not found to be cost effective in the vicinity of West Maple Avenue.</p> <p>Recognizing that traffic noise is a concern, KYTC conducted a technical study to further evaluate a noise/visual screening wall to help provide about a 3-6 decibel noise reduction for residences near West Maple Avenue and to shield views of the highway. Based on the technical feasibility and public comments, KYTC is currently recommending a noise/visual screening wall at this location. KYTC and the project team will conduct additional coordination with the people who will benefit from the walls during the project's design-build phase.</p>

Comment Number	Nature of Comment	Comment	Response
158	Noise	01/11/2023 - Web Comment - I am in favor of a sound wall or other device to minimize I-75 traffic noise on West Maple Ave in Fort Mitchell, Ky.	<p>You provided a comment regarding a noise wall near West Maple Avenue. As part of the environmental work for the project, the Kentucky Transportation Cabinet (KYTC) evaluated several noise walls and noise wall systems along the Brent Spence Bridge Corridor. Some were found to be warranted based on KYTC's Noise Policy. However, a noise wall was not found to be cost effective in the vicinity of West Maple Avenue.</p> <p>Recognizing that traffic noise is a concern, KYTC conducted a technical study to further evaluate a noise/visual screening wall to help provide about a 3-6 decibel noise reduction for residences near West Maple Avenue and to shield views of the highway. Based on the technical feasibility and public comments, KYTC is currently recommending a noise/visual screening wall at this location. KYTC and the project team will conduct additional coordination with the people who will benefit from the walls during the project's design-build phase.</p>
159	BSB Companion Bridge	01/12/2023 - Web Comment - How will a name for the new bridge be chosen?	The process for naming the new bridge has not been established.
160	Noise	<p>01/12/2023 - Web Comment - I am writing this to express my strong support for a Sound/Noise Wall along the I-71/75 South Expressway running along West Maple Avenue. We have lived on West Maple for roughly 8 years, and it is frightening how much the noise has increased since we moved in.</p> <p>CURRENTLY, WEST MAPLE HAS THE HIGHEST NOISE LEVEL READING IN ALL OF FORT MITCHELL. We are not able to hold a conversation with neighbors and/or entertain in our front or side yards given the significant disruption/noise levels in the area. I am not exaggerating when I say that my dog, Huey, can't even here me call him from the porch when he is in the front yard. We also have sound proof windows on the entire front of the house, and the noise from the traffic/trucks on the expressway (especially at night) is disrupting and wakes us up often several times a night. It's frightening to think about how much worse the noise levels will get as the Expressway expands and traffic increases.</p> <p>Between the increase in additional traffic noise and the potential reverberation from the Sound Wall running along the opposite side of</p>	<p>You provided a comment regarding a noise wall near West Maple Avenue. As part of the environmental work for the project, the Kentucky Transportation Cabinet (KYTC) evaluated several noise walls and noise wall systems along the Brent Spence Bridge Corridor. Some were found to be warranted based on KYTC's Noise Policy. However, a noise wall was not found to be cost effective in the vicinity of West Maple Avenue.</p> <p>Recognizing that traffic noise is a concern, KYTC conducted a technical study to further evaluate a noise/visual screening wall to help provide about a 3-6 decibel noise reduction for residences near West Maple Avenue and to shield views of the highway. Based on the technical feasibility and public comments, KYTC is currently recommending a noise/visual screening wall at this location. KYTC and the project team will conduct additional coordination with the people who will benefit from the walls during the project's design-build phase.</p>

Comment Number	Nature of Comment	Comment	Response
160 (cont.)	Noise (cont.)	<p>the Expressway (near Beechwood School), the noise levels will be unbearable.</p> <p>It is extremely upsetting that a Sound Wall in our area is not currently a part of the recommendation based, primarily, on the number of residents living on our street. As residents of Fort Mitchell, we pay significant state, county and city taxes and deserve the same quality of life as others living in the cities affected by this project. There is no question that if we do not secure a Sound Wall for this area, there will be significant negative implications to those of us living on this street (i.e., our quality of life will decline, our homes will become less desirable and; therefore, our home values will decrease).</p> <p>Based on the future (post project) Noise Level Reading, EVEN WITH A SOUND WALL, WEST MAPLE WILL STILL HAVE THE HIGHEST NOISE LEVEL READING IN ALL OF FORT MITCHELL. This is simply crazy and unacceptable. Given all of this, not providing support for this Sound Wall is simply unacceptable.</p> <p>In closing, I respectfully ask that you revise your proposal/draft to include a Sound Wall along I-75/71 South Expressway along West Maple Avenue in Fort Mitchell.</p>	
161	Noise	<p>01/12/2023 - Web Comment - I am writing to voice my concern about the expansion of I75 through Ft. Mitchell. I understand the reasoning behind this and the expansion itself is not my issue. My issue is the fact that there is no plan to put a sound wall in on the "Old Fort Mitchell" side of the expressway. From my understanding, there is a plan in place to put a sound wall on the Beechwood School side, however not our side, which will make it even worse. It will bring all of our property values down if the noise gets any worse than it already is. Please rethink this and put up a sound wall!</p>	<p>You provided a comment regarding a noise wall near West Maple Avenue. As part of the environmental work for the project, the Kentucky Transportation Cabinet (KYTC) evaluated several noise walls and noise wall systems along the Brent Spence Bridge Corridor. Some were found to be warranted based on KYTC's Noise Policy. However, a noise wall was not found to be cost effective in the vicinity of West Maple Avenue.</p> <p>Recognizing that traffic noise is a concern, KYTC conducted a technical study to further evaluate a noise/visual screening wall to help provide about a 3-6 decibel noise reduction for residences near West Maple Avenue and to shield views of the highway. Based on the technical feasibility and public comments, KYTC is currently recommending a noise/visual screening wall at this location. KYTC and the project team will conduct additional coordination with the people who will benefit from the walls during the project's design-build phase.</p>

Comment Number	Nature of Comment	Comment	Response
162	Project Mailing List	01/12/2023 - Web Comment - I would like to receive updates on this project.	This individual was added to the project mailing list.
163	Construction	01/13/2023 - Web Comment - I'm with United Rentals. I'm looking to learn who the general contractors will be. I can help with the equipment that will be required to perform coffer dams	<p>The draft Request for Proposals (RFP) for progressive design-build teams was released on January 13, 2023. The final RFP will be advertised on February 17, 2023. Proposals from design-build teams are due on March 31, 2023. The design-build contract will be awarded on May 31, 2023.</p> <p>Please visit https://www.dot.state.oh.us/Divisions/ConstructionMgt/design-build/bsb-project-procurement/Pages/default.aspx for information on the project's procurement process. This web site also includes a form to sign up for email alerts to be notified of any future networking opportunities.</p>
164	Noise	<p>01/14/2023 - Web Comment - Thank you for seeking feedback on the perceived benefit of including a sound wall to the Brent Spence Corridor Project serving the residents of Ft. Mitchell, KY.</p> <p>I have a good and longtime friend who lives 100 yards from Interstate 71 in Montgomery, Ohio, approximately 20 miles from the proposed project. When Interstate 71 was improved many years ago a sound wall was built which he can see today from his home.</p> <p>He told me this week that the sound wall was of immense benefit to him and his neighbors as it greatly reduced the sounds of traffic 24/7, improving their quality of life.</p> <p>I believe in similar fashion the residents of the corridor in KY leading to the bridge who will be affected by any roadway additions would benefit from a sound wall as did the residents of Interstate 71 in Cincinnati. I am thus asking for similar consideration. Thank you.</p>	<p>Thank you for your comment regarding the Brent Spence Bridge Corridor Project. You provided a comment regarding noise walls in Kentucky. KYTC has identified several locations along the Brent Spence Bridge Corridor for potential noise walls to mitigate noise impacts in accordance with its noise policy, including several areas in Fort Mitchell. In addition, both ODOT and KYTC are evaluating additional walls or other features to further reduce noise and provide visual screening to the highway in noise sensitive areas that did not warrant noise walls according to their noise policies.</p> <p>In Fort Mitchell, KYTC conducted a technical study to further evaluate a noise/visual screening wall to help provide about a 3-6 decibel noise reduction for residences near West Maple Avenue and to shield views of the highway. Based on the technical feasibility and public comments, KYTC is currently recommending a noise/visual screening wall at this location. KYTC and the project team will conduct additional coordination with the people who will benefit from the walls during the project's design-build phase.</p>

Comment Number	Nature of Comment	Comment	Response
165	Roadway/Bridge Design	<p>01/15/2023 - Web Comment - I know the plans have been agreed to and changes to it have zero percent of happening. But. I'd like to know who came up with No facelift to the existing bridge and who thought up using the same Blueprint from the big Mac bridge. The steel of the 75 bridge isn't getting any younger and down the road Will have issues you should try to avoid now. Doing so the old bridge SHOULD look the same as the new bride. As for the new bride. I'm guessing the designer has no imagination at all. Do they want to paint it blue and yellow also? When the plans were presented and agreed on, BOTH people or groups of people should have been fired. If you'll notice, no to buildings on either side of the river look the same. Why would you COPY an existing bridge and not replace any of the steel on the old? It's another case of people in charge have no clue. Just a bunch of Gumby's. Which equals SAD COMMENTARY.</p>	<p>Alternatives that removed the existing Brent Spence Bridge were evaluated during the project's development. Those analyses concluded that the existing bridge has a long life, and removing it to build a wider companion bridge was not cost effective. However, the proposed project will include a deck replacement and steel repairs on the existing bridge.</p> <p>The design of the new companion bridge is not yet determined. It may be an arch design or a cable stayed design. The decision about bridge types was made in close coordination with the project Aesthetic Committee, which will continue to have input on the appearance of the new companion bridge throughout the project's design-build phase.</p>
166	Noise	<p>01/16/2023 - Web Comment - As a resident of [REDACTED] West Maple Ave. in Ft. Mitchell, we can't hold a conversation in our front yard with friends and neighbors given the intense noise levels. Even before this project, the noise from the traffic and trucks, specifically, wake us up in the middle of the night. This will only get worse as traffic increases and becomes closer to our properties. Please install sound barrier along West Maple Ave.</p>	<p>Thank you for your comment regarding the Brent Spence Bridge Corridor Project. You provided a comment regarding a noise wall near West Maple Avenue. As part of the environmental work for the project, the Kentucky Transportation Cabinet (KYTC) evaluated several noise walls and noise wall systems along the Brent Spence Bridge Corridor. Some were found to be warranted based on KYTC's Noise Policy. However, a noise wall was not found to be cost effective in the vicinity of West Maple Avenue.</p> <p>Recognizing that traffic noise is a concern, KYTC conducted a technical study to further evaluate a noise/visual screening wall to help provide about a 3-6 decibel noise reduction for residences near West Maple Avenue and to shield views of the highway. Based on the technical feasibility and public comments, KYTC is currently recommending a noise/visual screening wall at this location. KYTC and the project team will conduct additional coordination with the people who will benefit from the walls during the project's design-build phase.</p>

Comment Number	Nature of Comment	Comment	Response
167	Noise	<p>01/16/2023 - Web Comment - I live at [REDACTED] W. Maple Avenue in Fort Mitchell with my wife, [REDACTED], my 6 year old son, [REDACTED], and my 3 year old twin boys, [REDACTED]. As you might imagine, the boys LOVE to play outside. Unfortunately, there is not much of a back yard as the homes are built on a steep hill, so they are forced to play in the front yard. The noise coming from the interstate is constantly an issue for us. If the boys get too far from the house, they can't hear us yell at them to come back. It is difficult for my wife and I to carry on a conversation out front. Even sitting on the back deck at night, the noise is relentless. After hearing that a sound wall has been proposed across from us, we were thrilled!!! After looking into this possibility more, I'm getting the impression that because our street doesn't have dozens of homes, that the sound wall isn't reasonable. I would beg that this be reconsidered. My mother lives in one of the condos down the street, they butt up against the interstate and it is way worse. And there are dozens and dozens of residents down that way. If there is anything that can be done reconsider the feasibility and reasonableness of this wall, please let me know. If i need to attend meetings or write to my Congressman, that is what I will do. Even the slightest decrease in the noise pollution would greatly improve our lives.</p>	<p>Thank you for your comment regarding the Brent Spence Bridge Corridor Project. You provided a comment regarding a noise wall near West Maple Avenue. As part of the environmental work for the project, the Kentucky Transportation Cabinet (KYTC) evaluated several noise walls and noise wall systems along the Brent Spence Bridge Corridor. Some were found to be warranted based on KYTC's Noise Policy. However, a noise wall was not found to be cost effective in the vicinity of West Maple Avenue.</p> <p>Recognizing that traffic noise is a concern, KYTC conducted a technical study to further evaluate a noise/visual screening wall to help provide about a 3-6 decibel noise reduction for residences near West Maple Avenue and to shield views of the highway. Based on the technical feasibility and public comments, KYTC is currently recommending a noise/visual screening wall at this location. KYTC and the project team will conduct additional coordination with the people who will benefit from the walls during the project's design-build phase.</p>
168	Noise	<p>01/16/2023 - Web Comment - I live on West Maple and am in strong support of a wall to help reduce the noise. It is already difficult to have a conversation outside with neighbors and my 3 children cannot hear their friends when playing in our front yard.</p> <p>Added noise with additional traffic would be terrible. Please put up a wal to block the noise.</p>	<p>Thank you for your comment regarding the Brent Spence Bridge Corridor Project. You provided a comment regarding a noise wall near West Maple Avenue. As part of the environmental work for the project, the Kentucky Transportation Cabinet (KYTC) evaluated several noise walls and noise wall systems along the Brent Spence Bridge Corridor. Some were found to be warranted based on KYTC's Noise Policy. However, a noise wall was not found to be cost effective in the vicinity of West Maple Avenue.</p> <p>Recognizing that traffic noise is a concern, KYTC conducted a technical study to further evaluate a noise/visual screening wall to help provide about a 3-6 decibel noise reduction for residences near West Maple Avenue and to shield views of the highway. Based on the technical feasibility and public comments, KYTC is currently recommending a noise/visual screening wall at this location. KYTC and the project team will conduct additional coordination with the people who will benefit from the walls during the project's design-build phase.</p>

Comment Number	Nature of Comment	Comment	Response
169	Noise	01/17/2023 - Web Comment - I am a resident of West Maple Avenue in Fort Mitchell, KY. While I am excited about the upcoming changes to our city regarding the bridge and expressway updates, I am concerned with how this will impact the quality of life in my neighborhood. While a wonderful street to live on, West Maple Avenue is already VERY noisy, and I fear that the expressway expansion with no sound wall will further interrupt our everyday lives. Current state, the noise from the expressway often times wakes me and my family up in the middle of the night, and we are unable to gather on our front porch without struggling to hear each other speak. As do many of our neighbors, we spend ample amounts of time during the spring and summer months in the back yard and on our back deck. We already hear a lot of noise from the expressway, and I am sure this will only get much worse with no sound wall. Lastly, my neighbors and I are extremely nervous of how the changes to the expressway with no sound wall will impact the value of our homes down the road, which we would not be able to control. All of this being said, please consider the addition of a sound wall to the expressway near West Maple Avenue so that the residents of our beloved street do not have to bear any further noise or distraction. Thank you so much!	<p>Thank you for your comment regarding the Brent Spence Bridge Corridor Project. You provided a comment regarding a noise wall near West Maple Avenue. As part of the environmental work for the project, the Kentucky Transportation Cabinet (KYTC) evaluated several noise walls and noise wall systems along the Brent Spence Bridge Corridor. Some were found to be warranted based on KYTC's Noise Policy. However, a noise wall was not found to be cost effective in the vicinity of West Maple Avenue.</p> <p>Recognizing that traffic noise is a concern, KYTC conducted a technical study to further evaluate a noise/visual screening wall to help provide about a 3-6 decibel noise reduction for residences near West Maple Avenue and to shield views of the highway. Based on the technical feasibility and public comments, KYTC is currently recommending a noise/visual screening wall at this location. KYTC and the project team will conduct additional coordination with the people who will benefit from the walls during the project's design-build phase.</p>
170	Construction	01/17/2023 - Web Comment - What day in the 1st week in Feb with they announce companies that are doing the work on the bridge. [REDACTED]	<p>The draft Request for Proposals (RFP) for progressive design-build teams was released on January 13, 2023. The final RFP will be advertised on February 17, 2023. Proposals from design-build teams are due on March 31, 2023. The design-build contract will be awarded on May 31, 2023.</p> <p>Please visit https://www.dot.state.oh.us/Divisions/ConstructionMgt/design-build/bsb-project-procurement/Pages/default.aspx for information on the project's procurement process. This web site also includes a form to sign up for email alerts to be notified of any future networking opportunities.</p>
171	Project Mailing List	01/19/2023 - Web Comment - Trying to sign up for updates only. Thank you.	This individual was added to the project mailing list.

Comment Number	Nature of Comment	Comment	Response
172	Multimodal	01/19/2023 - Web Comment - Seeing as Covington and Cincinnati are currently connected for local traffic via the Clay Wade Bailey Bridge and the current plan calls for the existing Brent Spence Bridge to serve as an additional local traffic bridge, would the project consider protected bicycle lanes to be added to the Brent Spence Bridge ?	Pedestrian and bicycle accommodations are not planned on the new companion bridge or the existing Brent Spence bridge because of the proximity of a reasonable crossing at the Clay Wade Bailey Bridge. Preliminary investigations indicate that adding bike lanes to the Clay Wade Bailey Bridge may be feasible. KYTC and ODOT will further evaluate adding bike lanes to the Clay Wade Bailey Bridge during the project's progressive design-build phase.
173	Noise	01/19/2023 - Web Comment - I am the owner of [REDACTED] W Maple Avenue, Ft Mitchell, KY. I am writing to plead for a sound wall along this stretch of 71/75. The noise from the highway now is very bad (the highest in the whole corridor already). With the expansion it will increase, by your own studies, even more. Please remember that though apparently we do not have quite enough people affected, however, the homes on W Maple are higher value than much of Ft Mitchel and the property taxes as well as personal taxes are significantly higher. Property values (and taxes) will go down with the higher noise levels.	<p>Thank you for your comment regarding the Brent Spence Bridge Corridor Project. You provided a comment regarding a noise wall near West Maple Avenue. As part of the environmental work for the project, the Kentucky Transportation Cabinet (KYTC) evaluated several noise walls and noise wall systems along the Brent Spence Bridge Corridor. Some were found to be warranted based on KYTC's Noise Policy. However, a noise wall was not found to be cost effective in the vicinity of West Maple Avenue.</p> <p>Recognizing that traffic noise is a concern, KYTC conducted a technical study to further evaluate a noise/visual screening wall to help provide about a 3-6 decibel noise reduction for residences near West Maple Avenue and to shield views of the highway. Based on the technical feasibility and public comments, KYTC is currently recommending a noise/visual screening wall at this location. KYTC and the project team will conduct additional coordination with the people who will benefit from the walls during the project's design-build phase.</p>
174	Right-of-Way	<p>01/19/2023 - Web Comment - On the Ohio side, is there a property acquisition Map showing parcels to be purchased for the I-75 project? Will the mid rise hotel in Queensgate be acquired and removed? Part of the power structure along the Ohio River will have to be purchased and demolished for the project. Has this been acquired ?</p> <p>Has property purchase begun on the KY side?</p> <p>I am a former Cincinnati resident now living in California and have been following this proposed project for years - glad it is finally moving forward !!!</p>	<p>Maps showing proposed right-of-way limits (where properties will be impacted) for the project can be accessed here: https://brentspencebridgecorridor.com/wp-content/uploads/2022/12/2022-12-06_Corridor-Exhibit.pdf.</p> <p>The mid-rise hotel in Queensgate will not be acquired or removed as part of the Brent Spence Bridge Corridor Project. ODOT has already acquired the majority of the property required for the project. Duke Energy facilities near the Ohio River were relocated in 2019. KYTC began purchasing property in Kentucky in 2022.</p>

Comment Number	Nature of Comment	Comment	Response
175	Noise	01/20/2023 - Web Comment - I am a resident at [REDACTED] w maple ave. Ft Mitchell, KY. I have spoken with neighbors and agree that we are in need of a sound wall to be added to the I-75 project as it can get very loud behind our home.	<p>Thank you for your comment regarding the Brent Spence Bridge Corridor Project. You provided a comment regarding a noise wall near West Maple Avenue. As part of the environmental work for the project, the Kentucky Transportation Cabinet (KYTC) evaluated several noise walls and noise wall systems along the Brent Spence Bridge Corridor. Some were found to be warranted based on KYTC's Noise Policy. However, a noise wall was not found to be cost effective in the vicinity of West Maple Avenue.</p> <p>Recognizing that traffic noise is a concern, KYTC conducted a technical study to further evaluate a noise/visual screening wall to help provide about a 3-6 decibel noise reduction for residences near West Maple Avenue and to shield views of the highway. Based on the technical feasibility and public comments, KYTC is currently recommending a noise/visual screening wall at this location. KYTC and the project team will conduct additional coordination with the people who will benefit from the walls during the project's design-build phase.</p>
176	Noise	<p>01/20/2023 - Email Comment - We were at the Ft. Mitchell KY information meeting and spoke with the staff afterward. My wife [REDACTED] subsequently spoke with Mr. Craig Craig to discuss noise abatement.</p> <p>I know that in the next few days there were several noise measurements taken in the neighborhood (not at peak noise times, but nonetheless measurements were taken).</p> <p>We wondered if the process is moving forward and if need to provide any further input. Appreciate any follow-up that you or Mr., Craig can provide.</p>	<p>Thank you for your comment regarding the Brent Spence Bridge Corridor Project.</p> <p>You requested information regarding a noise wall near your residence on Summit Lane. The Kentucky Transportation Cabinet's (KYTC's) noise policy considers abatement for areas within 500 feet of the proposed edge of pavement for the highway, and Summit Lane is outside of that distance. However, based on requests we received at the neighborhood outreach meeting in Fort Mitchell, KYTC conducted additional analysis to study extending the recommended noise wall north and west of Dixie Highway. Based on that analysis, KYTC determined that constructing a noise wall along I 71/I-75 would not substantially reduce noise in the vicinity of Summit Lane, and is therefore, not recommending a noise wall in this location.</p>

Comment Number	Nature of Comment	Comment	Response
177	Noise	01/23/2024 - Email Comment - I live right behind the Mercedes dealer and would see it [a noise wall] as a helpful deterrent to more noise. Please keep us posted on what is needed with this project.	<p>Thank you for the clarification. The Kentucky Transportation Cabinet (KYTC) analyzed the residences north of the Mercedes dealership in Fort Mitchell and did not identify noise impacts in this area. Future noise levels were predicted to be below thresholds established by KYTC's noise policy. Therefore, KYTC is not recommending a noise wall in this area.</p> <p>Also, you may direct future questions about the BSB Corridor Project in Kentucky to Stacey Hans, the KYTC project manager. Her email address is Stacey.hans@ky.gov.</p>
178	Right-of-Way	I currently live on 11st st in Covington, KY to the west of I-71/75. I see in the rendering that my street is shown in the phase 3 portion. I am wondering when/if I will hear whether I will be required to relocate. Additionally, if I do not need to relocate, how will the widening of the interstate affect my current home and those closer to the interstate? Will the interstate be expanded closer to my home?	<p>The Brent Spence Bridge Project will widen I-71/I-75 near 11th Street in Covington. Two homes on 11th Street in Kentucky will be required to relocate due to impacts from the project. Without knowing the exact location of your residence, I can't determine specific impacts to your property. However, mapping on the project website shows the limits of the new interstate and homes that will be relocated: https://brentspencebridgecorridor.com/wp-content/uploads/2022/12/2022-12-06_Corridor-Exhibit.pdf.</p> <p>The Kentucky Transportation Cabinet (KYTC) will formally notify property owners and residents in this area about the relocation process after the project's supplemental Environmental Assessment is approved. Please keep in mind that if you are impacted, you are entitled to compensation for the property (if you own) but also relocation expenses (if you own or rent). Please don't hesitate to reach out if you have any additional questions.</p>
179	Public Input	01/26/2023 - Email Comment - Please video tape all future meetings so they can be viewed online. Many interested parties cannot attend in person, for various reasons such as being disabled, lack of transportation, out of town and 100 other reasons so please, tape these meetings to truly serve our whole community.	<p>Summaries of all the neighborhood outreach meetings, including the Q&A, presentations, and exhibits from each meeting are posted on the project website: https://brentspencebridgecorridor.com/public-involvement-and-comments/. The public hearings currently scheduled for July/August 2023 will have a virtual option for those who can't attend in-person.</p>

Comment Number	Nature of Comment	Comment	Response
180	Construction	01/23/2023 - Web Comment - Good afternoon. My name is [REDACTED] with Hercules Machinery. We are a pile driving equipment supplier located in Indiana. I was wondering if there is a bidders list for the project? If so, could you possibly help me to locate it. Thank you in advance for your time.	<p>The draft Request for Proposals (RFP) for progressive design-build teams was released on January 13, 2023. The final RFP will be advertised on February 17, 2023. Proposals from design-build teams are due on March 31, 2023. The design-build contract will be awarded on May 31, 2023.</p> <p>Please visit https://www.dot.state.oh.us/Divisions/ConstructionMgt/design-build/bsb-project-procurement/Pages/default.aspx for information on the project's procurement process. This web site also includes a form to sign up for email alerts to be notified of any future networking opportunities.</p>
181	Noise	<p>01/24/2023 - Web Comment - Please allow this letter to serve as my formal request for you to include a wall when expanding the I75 bridge project.</p> <p>I recently moved to 125 W Maple Ave FtMitchell,Ky. I have never lived in a condo but never expected the noise i would experience. I cannot use my deck or patio due to the expressway noise and i am awakened from large trucks and motorcycles during the night. Anything you can do to get approval for this wall will be greatly appreciated.</p> <p>Another lane on the expressway will make my property value decrease. This was to be my home for retirement.</p>	<p>Thank you for your comment regarding the Brent Spence Bridge Corridor Project. You provided a comment regarding a noise wall near West Maple Avenue. As part of the environmental work for the project, the Kentucky Transportation Cabinet (KYTC) evaluated several noise walls and noise wall systems along the Brent Spence Bridge Corridor. Some were found to be warranted based on KYTC's Noise Policy. However, a noise wall was not found to be cost effective in the vicinity of West Maple Avenue.</p> <p>Recognizing that traffic noise is a concern, KYTC conducted a technical study to further evaluate a noise/visual screening wall to help provide about a 3-6 decibel noise reduction for residences near West Maple Avenue and to shield views of the highway. Based on the technical feasibility and public comments, KYTC is currently recommending a noise/visual screening wall at this location. KYTC and the project team will conduct additional coordination with the people who will benefit from the walls during the project's design-build phase.</p>
182	Information Request	01/26/2023 - Web Comment - I would like to schedule a call with the project team please.	A member of the project team contacted this individual.

Comment Number	Nature of Comment	Comment	Response
183	Noise	01/26/2023 - Web Comment - I want to know how my house will be affected. I live at [REDACTED] Kyles Lane in Ft. Wright, KY. I already hear all of the traffic. How much worse is it going to be?	<p>Thank you for your inquiry regarding the Brent Spence Bridge Corridor Project. Based on current design, the project will not directly impact this residence. Kyles Lane and the ramp from northbound I-71/I-75 will be rebuilt in the existing location. Exhibits showing the current corridor configuration can be found here: https://brentspencebridgecorridor.com/wp-content/uploads/2022/12/2022-12-06_Corridor-Exhibit.pdf.</p> <p>Noise walls recommended along I-71/I-75 will lower noise levels at this location in future conditions. Detailed maps showing predicted noise levels near Kyles Lane can be found in the presentation from the Ft. Wright neighborhood meeting, which is included in the meeting summary on the project website: https://brentspencebridgecorridor.com/wp-content/uploads/2023/01/2022-12-01_Fort-Wright-Neighborhood-Meeting-Summary.pdf.</p>
184	Information Request	01/26/2023 - Web Comment - Hi is there anyway someone can contact me about a picture on your website? We would like to use it but need permission. Thanks!	A member of the project team contacted this individual.
185	Project Mailing List	01/26/2023 - Web Comment - Please send updates	This individual was added to the project mailing list.
186	Project Mailing List	01/27/2023 - Web Comment - [REDACTED]	This individual was added to the project mailing list.
187	Project Mailing List	01/29/2023 - Web Comment - Thank you for the opportunity to keep abreast of this vital project.	This individual was added to the project mailing list.
188	Project Mailing List	01/30/2023 - Web Comment - Thanks for adding me to the email/updates list!	This individual was added to the project mailing list.
189	Construction	01/31/2023 - Web Comment - I would like the link and or advice on prequalification. Our company is an MBE that specializes in Construction Management (CM) and project control (Cost Schedule and document control)	<p>Thank you for your interest in the Brent Spence Bridge Corridor Project. Information about prequalification for both ODOT and KYTC is provided in the presentation from the DBE Information Session held on December 7, 2022. That presentation can be accessed here (beginning with slide 27): https://brentspencebridgecorridor.com/wp-content/uploads/2022/12/2022-12-07-BSBC-Event-Slides_Combined.pdf</p> <p>Contact information for specific prequalification questions is also provided in the presentation.</p>

Comment Number	Nature of Comment	Comment	Response
190	Construction	02/03/2023 - Web Comment - What is the timeline for this new bridge? When is construction expected to start and when will it be finished?	Thank you for your interest in the Brent Spence Bridge Project. The groundbreaking for the Brent Spence Bridge Corridor Project is anticipated to occur in late 2023 with construction beginning in earnest in 2024. The sequence of construction will not be determined until after the design-build team is selected and develops plans, but construction on the new companion bridge is not anticipated to begin until late 2024 or 2025. The project is expected to be substantially complete in 2029.
191	Historic Preservation	01/28/2023 - Online submission to Governor Mike DeWine - Mr. DeWine, Sir, I know the new bridge to replace or supplement the Brent Spence I-75 / I-71 Bridge over the Ohio River from Cincinnati to Newport Ky and its upcoming replacement. There are concerns that I think should be addressed. The building IS on the National Register of Historic Places as of 1986. Link attached. https://catalog.archives.gov/id/71988967 The B&O Freight and Warehouse saignage on the end of the building next to the Brent Spence Bridge. Another sign like that is on the opposite side as well and that end of the structure will have to be removed to make room for the new bridge. Thoughts are to have the signs, on both sides of the building moved to the west end of that structure and remounted as a best scenario. Otherwise would like to salvage the signs but they are large letters. Looks like at 4 or 5 feet tall, each. Not sure what else to do but remount them. In addition to the building we feel the signage is also as important as it reflects our transportation history. It means a lot to many in the area. The B&O was the Nations first railroad so the signage should be viewed as a reminder. Local railroad clubs and individuals have limited funds to say the least and would not be able to afford a project like this so I would like to ask that the State for help in saving that bit of history for us and future generations. It would seem that funding the new bridge would allow a small portion of those funds and equipment to help move the signs, PLEASE. Please reply by email Sir. It would be appreciated greatly and Thank You for your help and consideration in this matter.	<p>Thank you for your feedback about the Brent Spence Bridge Corridor Project and your interest in historic preservation.</p> <p>The project will demolish 204 feet of the B&O Freight Terminal/Longworth Hall building located along Pete Rose Way in Cincinnati. This structure, constructed as a warehouse for the Baltimore & Ohio Railroad, currently operates as a mixed-use office and retail space. To mitigate the impacts to Longworth Hall, ODOT committed to completing various repair, upgrade, restoration, enhancement, and refurbishment measures on the remaining structure. These commitments were documented in a Memorandum of Agreement (MOA) between FHWA, ODOT, and Ohio Historic Preservation Office (OHPO) executed on June 28, 2012. A First Amendment to the MOA was executed on June 22, 2017, and a Second Amendment was executed on May 24, 2022. Both amendments extended the period of the MOA.</p> <p>The mitigation measures included in the MOA include refurbishing the original lettering across the top of the building. The mitigation measures also include installing exterior storm windows, restoring the east wall to an approximation of its original appearance using materials salvaged during demolition, adding a cornerstone commemorating the date of construction on one side and the date of the renovation on the other side, repairing masonry, constructing a plaque/interpretive signing describing change to the property that have occurred over time, and returning removed materials that retain historic integrity and nature to the building owner to be used in future repairs or expansion.</p>

Comment Number	Nature of Comment	Comment	Response
192	Information Request	02/08/2023 - Web Comment - How much freight in dollars/day is transported across the Brent Spence Bridge?	Thank you for your interest in the Brent Spence Bridge (BSB) Project. The BSB corridor forms a critical freight route connecting Canada to Florida, carrying more than \$2 billion of freight every day and more than \$700 billion of freight every year.
193	Construction	02/08/2023 - Web Comment - I work for Nelson Stud Welding and I want to ensure that our studs are used in this bridge project. Our studs are made in Ohio with American steel and meet all welding standards. The best part about using our studs is our service....I will be on jobsite to assist all stud Welding to ensure the job is on schedule. None of our competitors do this. Please feel free to contact me.	Thank you for your interest in the Brent Spence Bridge Corridor Project. Please visit https://www.dot.state.oh.us/Divisions/ConstructionMgt/design-build/bsb-project-procurement/Pages/default.aspx for information on the project's procurement process. This web site also includes a form to sign up for email alerts to be notified of any future networking opportunities.
194	Construction	02/08/2023 - Web Comment - I was hoping to connect with someone about the Brent Spence Bridge Corridor project. Is there a plan-holders list available?	Thank you for your interest in the Brent Spence Bridge Corridor Project. ODOT and KYTC do not have a list of contractors for the Brent Spence Bridge Corridor Project. Proposals will be due from design-build teams on March 31, 2023. Please visit https://www.dot.state.oh.us/Divisions/ConstructionMgt/design-build/bsb-project-procurement/Pages/default.aspx for information on the project's procurement process. This web site also includes a form to sign up for email alerts to be notified of any future networking opportunities.
195	Construction	02/08/2023 - Web Comment - Please be kind to send more information on new planned bridge. works description bid opportunities SBA opportunities	<p>Thank you for your interest in the Brent Spence Bridge Corridor Project. Please visit https://www.dot.state.oh.us/Divisions/ConstructionMgt/design-build/bsb-project-procurement/Pages/default.aspx for information on the project's work description and procurement process. This web site also includes a form to sign up for email alerts to be notified of any future networking opportunities.</p> <p>Information about SBA opportunities is provided in recent Diversity and Inclusion presentations for the project. Those presentations can be accessed using the following links: https://brentspencebridgecorridor.com/wp-content/uploads/2022/11/2022-11-07-Final-BSBC-Slides.pdf, https://brentspencebridgecorridor.com/wp-content/uploads/2022/12/2022-12-07-BSBC-Event-Slides_Combined.pdf, https://brentspencebridgecorridor.com/wp-content/uploads/2023/01/DI-Committee-Slides-2023_01_25.pdf.</p>

Comment Number	Nature of Comment	Comment	Response
196	Construction	02/08/2023 - Web Comment - As a domestic manufacturer of fence and security products, Ameristar offers design input, drawings, specifications, budget numbers, and samples to assist in the design process. If you have an immediate need we can meet virtually to discuss and review your project. With time we can schedule an in person meeting for your team and I will be happy to make arrangements to visit and review the full range of Ameristar solutions. Please let me know what works best for the Brent Spence team.	Thank you for your interest in the Brent Spence Bridge Corridor Project. Please visit https://www.dot.state.oh.us/Divisions/ConstructionMgt/design-build/bsb-project-procurement/Pages/default.aspx for information on the project's procurement process. This web site also includes a form to sign up for email alerts to be notified of any future networking opportunities.
197	Project Mailing List	02/10/2023 - Web Comment - I'm interested to get updates on how the project progresses	This individual was added to the project mailing list.
198	Project Mailing List	02/10/2023 - Web Comment - Up to date we own property along Dixie Highway	This individual was added to the project mailing list.
199	Construction	02/11/2023 - Web Comment - Could you please share how West Covington will be impacted while the building of the new bridge plans to take years. It feels as this area of Covington has been forgotten. Also, where will the staging areas for construction and materials be located?	Thank you for your interest in the BSB Corridor Project and potential impacts to West Covington. KYTC is working with the City of Covington to mitigate impacts during construction. During construction, KYTC will develop an overall Traffic Management Plan and a detailed MOT plan to maintain traffic operations through the corridor and minimize disruption to the surrounding communities. When preparing the MOT plan, KYTC will work to minimize impacts to local businesses; evaluate impacts to public transportation and develop measures to maintain existing services; evaluate temporary detours to limit impacts created by redirecting traffic through community sensitive areas; establish an Incident Management Plan to minimize diversion resulting from incidents that occur during construction in the corridor; include provisions for communicating with trucking companies and mapping services to provide information about re-routing and delays; and provide for adequate signing during construction. The City of Covington, and all relevant agencies within the city will have an opportunity to review and provide input into all aspects of MOT planning, plan development, and construction operations affecting the city. MOT and Incident Management Plans will also be coordinated with first responders, transit agencies, and the Regional Incident Management Task Force. Staging areas for construction and materials have not yet been identified. The Design-Build team will work to identify construction staging areas as they develop the detailed design plans for the project. The design-build team will be selected in May 2023 and will begin working on the detailed design at that time.

Comment Number	Nature of Comment	Comment	Response
200	Construction	02/13/2023 - Web Comment - I am with Quinn Consulting Services, Inc. and we are an ODOT DBE certified firm that provides Construction Management and Inspection services. We are a VA based firm that just completed the \$3.2B VDOT I-66 Outside the Beltway Express Toll Lanes project. We have provided lead QAM and QA inspection services for this project. We are interested in providing CM/CEI services for the Brent Spence Bridge project. When/how can we register for the upcoming DBE matchmaking event that is supposed to take place in March?	Thank you for your interest in the Brent Spence Bridge Corridor Project. You can sign up for the ODOT/KYTC Brent Spence Bridge Corridor Project Networking Meet and Greet even on March 7, 2023 using this link: https://www.eventbrite.com/e/odot-kytc-brent-spence-bridge-corridor-project-networking-meet-greet-tickets-529800857627
201	Construction	02/14/2023 - Web Comment - I represent a company called Uneath. We provide a map-based communication and collaboration tool. We work with both owners such as PG&E and Duke, and contractors such as Kiewit and Aldridge Electric. We've been hearing from our customers that our platform would be useful on large-scale infrastructure projects, as it allows users to geolocate and layer their drawings, aerial images, photos, documents, etc. on a live map. From there, users can walk around in their drawings via the mobile app and see where data is located relative to their current location. We've also heard from some of our customers that Uneath is "sort of like a combination of Bluebeam and Google Earth." Do you think a map-based communication tool might be worth exploring for this project? We'd be happy to meet with a couple of members of the team to give a brief overview/demonstration of the platform, to see if it's something that may provide value.	A member of the project team contacted this individual to discuss the opportunity.
202	Information Request	02/14/2023 - Web Comment - Hi, my name is [REDACTED] and I am a student at the University of Cincinnati. I am reaching out to see if someone from the Project Team or Owners Group would be interested in speaking to my club, American Society of Civil Engineers. This would be during the Fall Semester. Thankyou in advance.	A member of the project team contacted this individual to discuss the speaking opportunity.

Comment Number	Nature of Comment	Comment	Response
203	Construction	02/16/2023 - Web Comment - Where can I find a list of all the items o be purchased for the bridge project?	Thank you for your interest in the Brent Spence Bridge Corridor Project. A list of all the items to be purchased for the project has not been prepared, as the design has not yet progressed to the point that such a list can be prepared. For more information, including project's Request for Proposals and Technical Requirements, please visit the project's procurement website: https://www.eventbrite.com/e/odot-kytc-brent-spence-bridge-corridor-project-networking-meet-greet-tickets-529800857627
204	Construction	02/17/2023 - Web Comment - Where or how can I find names of the companies that are awarded the contracts to do the work on the Brent Spence Bridge Corridor? I would like to know who they are so I can reach out to them about staffing. Thank you!	<p>Thank you for your interest in the Brent Spence Bridge Corridor Project. ODOT and KYTC do not have a list of contractors for the project. Proposals will be due from design-build teams on March 31, 2023. Please visit https://www.dot.state.oh.us/Divisions/ConstructionMgt/design-build/bsb-project-procurement/Pages/default.aspx for information on the project's procurement process. This web site also includes a form to sign up for email alerts to be notified of any future networking opportunities.</p> <p>In addition, the project website has information for those interested in working on the project: https://brentspencebridgecorridor.com/work-with-us/</p> <p>You may also sign up for a networking event on March 7, 2023 using this link: https://www.eventbrite.com/e/odot-kytc-brent-spence-bridge-corridor-project-networking-meet-greet-tickets-529800857627</p>
205	Construction	02/17/2023 - Web Comment - When is this project estimated to begin?	Thank you for your interest in the Brent Spence Bridge Project. The groundbreaking for the project is anticipated to occur in late 2023 with construction beginning in earnest in 2024. The project is expected to be substantially complete in 2029.
206	Public Input	02/21/2023 - Email Comment - The below email was sent on 1/4/2023 which included the attached letter [Comment 132A]. Over a month has passed since it was sent. When can I expect a response?	A response to the original email/letter (Comment 132A) was sent on February 24, 2023.

Comment Number	Nature of Comment	Comment	Response
207	Construction	<p>02/24/2023 - Email Comment - I'm [REDACTED] from China, we are a professional lighting manufacturer.</p> <p>We have developed a new LED flexible light strip now, which has a low operating voltage and will not cause safety hazards. It also has stable shockproof performance, focuses on waterproofing, and has a longer lifespan.</p> <p>This lamp is suitable for decoration and lighting on buildings, bridges, gardens, furniture, signboards, commercial, etc., it can adding endless joy and festivals to various festivals such as Christmas, Halloween, Valentine's Day, Easter, National Day, etc.</p> <p>If it is possible, can I send you more details of this LED flexible light strip for your reference please?</p>	<p>Thank you for your interest in the Brent Spence Bridge Corridor Project. ODOT and KYTC have not yet selected the design-build team for the project. Proposals will be due from design-build teams on March 31, 2023. Please visit https://www.dot.state.oh.us/Divisions/ConstructionMgt/design-build/bsb-project-procurement/Pages/default.aspx for information on the project's procurement process. This web site also includes a form to sign up for email alerts to be notified of any future networking opportunities.</p> <p>In addition, the project website has information for those interested in working on the project: You may also email your product information to the following address: BSBProcurement@dot.ohio.gov</p>
208	Information Request	<p>02/21/2023 - Web Comment - Wondering if you would be willing to forward the BIP grant application that you submitted for this with appendices? I'm working with FDOT down in Florida and we're considering a large-scale project like this. I'm happy to submit a public records request if you need me to. If you're able to forward electronically, that would be perfect.</p>	<p>A copy of the BIP grant application was forwarded to this individual.</p>
209	Information Request	<p>02/21/2023 - Web Comment - I am very interested in the new bridge and any information I can get. I am a senior at Clark Montessori and doing a project on the bridge. Is there a way I can virtually watch any meetings?</p>	<p>A member of the project team contacted this individual.</p>
210	Construction	<p>02/22/2023 - Web Comment - Looking for a listing of the Pre-qualified Design Build teams identified for this project.</p> <p>Also, did ODOT advertise a Request for Qualifications before the RFP. Please advise.</p>	<p>Thank you for your interest in the Brent Spence Bridge Corridor Project. Because the project is following a progressive design-build process, ODOT and KYTC did not advertise a separate Request for Qualification before the Request for Proposals was released. ODOT and KYTC do not have a list of pre-qualified design-build teams for the project. Proposals will be due from design-build teams on March 31, 2023.</p> <p>Please visit https://www.dot.state.oh.us/Divisions/ConstructionMgt/design-build/bsb-project-procurement/Pages/default.aspx for information on the project's procurement process. This web site also includes a form to sign up for email alerts to be notified of any future networking opportunities.</p>

Comment Number	Nature of Comment	Comment	Response
211	Miscellaneous	02/22/2023 - Web Comment - I understand you are trying to keep this project local, but we would appreciate the opportunity to help with anyone with extended stay needs. I am the Area Director of Sales for Sonesta. My Sonesta ES Suites Cincinnati Blue Ash location has fully equipped kitchens that include ovens. We are the perfect home away from home. We provide a nice, safe, yet affordable place to stay. I can send you a virtual tour of our property, if interested.	Comment noted. A member of the project team returned a call received on this issue from the commenter.
212	Project Mailing List	02/23/2023 - Web Comment - Just wanted to be included in the information website	This individual was added to the project mailing list.
213	Public Input	02/26/2023 - Email Comment - Please see attached for additional survey results from unique respondents submitted between 1/5/23 and 2/21/23 (with PII removed), and for a letter from the Mt. Aururn Community Council.	Comment noted.
214	Information Request	03/02/2023 - Email Comment - I am looking at the 2006 Planning Study Report available at https://brentspencebridgecorridor.com/documents/ , but I can't find the appendices anywhere online. Can you either send me a link or the actual appendices themselves, and especially Appendix D, which lists all of the 25 or so alternatives that were initially considered? I would very much appreciate it.	The requested information was provided to this individual.
215	Construction	02/28/2023 - Web Comment - I would like to find out the names of the contracting teams that have expressed an interest in this project. We are expansion joint fabricators and would like to reach out to them.	<p>Thank you for your interest in the Brent Spence Bridge Corridor Project. Due to confidentiality concerns, ODOT and KYTC are not able to provide a list of contracting teams that have expressed an interest in the project. Proposals will be due from design-build teams on March 31, 2023. Please visit https://www.dot.state.oh.us/Divisions/ConstructionMgt/design-build/bsb-project-procurement/Pages/default.aspx for information on the project's procurement process. This web site also includes a form to sign up for email alerts to be notified of any future networking opportunities.</p> <p>In addition, the project website has information for those interested in working on the project: https://brentspencebridgecorridor.com/work-with-us/</p> <p>You may also email your product information to the following address: BSBProcurement@dot.ohio.gov</p>

Comment Number	Nature of Comment	Comment	Response
216	Alternatives	02/28/2023 - Web Comment - Our downtown is landlocked between the hills, river, and I-75. I believe that we should consider Bridge Forward's (https://www.bridge-forward.org/) ideas to reclaim land around the Ohio side of the I-75 approach to the proposed bridge. This reclaimed land along with their other ideas could be the catalyst to re-development of the entire West End / Queen's Gate area. Now is the time to slow this thing down and get it right since we will have to live with the result for generations.	<p>Thank you for your interest in the Brent Spence Bridge (BSB) Corridor Project. ODOT thoughtfully considered the designs offered by the Bridge-Forward Cincinnati Coalition. A detailed response is provided on the project website: https://brentspencebridgecorridor.com/wp-content/uploads/2022/10/BF-Response-to-Public-Comment_w-Executive-Summary-1.pdf.</p> <p>ODOT and the City of Cincinnati have also held multiple working sessions with Bridge Forward to discuss their ideas about the BSB Corridor Project, West End, and Queensgate. The City of Cincinnati has also invited a representative of Bridge Forward to participate in an advisory committee it is forming to represent local concerns in the development and construction of the BSB Corridor Project.</p>
217	Project Mailing List	03/01/2023 - Web Comment - I would like to obtain notifications on this important project.	This individual was added to the project mailing list.
218	Construction	03/01/2023 - Web Comment - Is there a way to know who is bidding on the design-build? Is there a copy of the RFP available? We are trying to submit bids to subs for aggregates.	<p>Thank you for your interest in the Brent Spence Bridge Corridor Project. ODOT and KYTC do not have a list of teams bidding on the progressive design-build project. Proposals will be due from design-build teams on March 31, 2023. Please visit https://www.dot.state.oh.us/Divisions/ConstructionMgt/design-build/bsb-project-procurement/Pages/default.aspx for information on the project's procurement process, including links to the request for proposals (RFP). This web site also includes a form to sign up for email alerts to be notified of any future networking opportunities.</p> <p>In addition, the project website has information for those interested in working on the project: https://brentspencebridgecorridor.com/work-with-us/</p> <p>You may also email your product information to the following address: BSBProcurement@dot.ohio.gov</p>
219	BSB Companion Bridge	03/02/2023 - Web Comment - A COMPANION BRIDGE IS A FAILURE. THE ANSWER LIES BACK IN 1960.	Thank you for your interest in the Brent Spence Bridge Corridor Project. Your comment has been included in the project record.

Comment Number	Nature of Comment	Comment	Response
220	Stakeholder Input	03/03/2023 - Web Comment - I would like to know why certain economic development organizations were not included on the list. For instance, the Greater Cincinnati & Northern Kentucky African American Chamber of Commerce was not included. Representatives of that organization attended at least two meetings and offered to sponsor outreach programs. There are other organizations that are working on this as well. It appears to be an oversight which may be indicative of how inclusion will be addressed in this project.	Thank you for your feedback about the Brent Spence Bridge Corridor Project and for your participation on the Diversity and Inclusion Committee. The membership on this committee is dynamic and has grown since the original list was published on the project website. ODOT and KYTC agree that the most current membership of the Diversity and Inclusion Committee should be reflected on the project website and will update the list accordingly. We will also work to keep the list updated moving forward. Thank you again for your feedback, and we look forward to collaborating with you as the project moves forward.
221	Stakeholder Input	03/03/2023 - Web Comment - The Greater Cincinnati & Northern Kentucky African American Chamber are on the Outreach Advisory Committee. We have attended and actively participated in the meetings, yet we were not listed. Why? Answer that question.	Thank you for your feedback about the Brent Spence Bridge Corridor Project and for your participation on the Diversity and Inclusion Committee. The membership on this committee is dynamic and has grown since the original list was published on the project website. ODOT and KYTC agree that the most current membership of the Diversity and Inclusion Committee should be reflected on the project website and will update the list accordingly. We will also work to keep the list updated moving forward. Thank you again for your feedback, and we look forward to collaborating with you as the project moves forward.
222	Construction	03/06/2023 - Web Comment - ***Contractor Housing*** Hello we are pre-leasing housing for the upcoming bridge project. Please let us know is we can help any construction companies secure their housing needs.	Thank you for your interest in the Brent Spence Bridge Corridor Project. ODOT and KYTC anticipate that the design-build team will be announced in May 2023 and under contract by June 2023. You may reach out to the selected design-build team at that time.
223	Construction	03/06/2023 - Web Comment - Hello, my name is [REDACTED] and I am the Northern Kentucky Regional Procurement Consultant for the KY APEX Accelerator (formerly known as the KYPTAC). We assist small businesses in obtaining local, state, and federal government contracts and assist in the completion of socio-economic small business certification application, including DBE. I am writing to offer our support in helping to meet the DBE goals for the Brent Spence Bridge project. Would someone be available to participate in a 45-minute Zoom meeting to discuss the type of subcontracting opportunities that are available?	A member of the project team contacted this individual.

Comment Number	Nature of Comment	Comment	Response
224	Construction	03/07/2023 - Web Comment - Looking for the latest on the bridge project. Will there be associated structures for maintenance, etc.?	<p>Thank you for your interest in the Brent Spence Bridge Corridor Project. The technical requirements are provided on the project's procurement website: https://www.dot.state.oh.us/Divisions/ConstructionMgt/design-build/bsb-project-procurement/Pages/default.aspx</p> <p>ODOT and KYTC have longstanding maintenance agreements in place with respects to bridges over the Ohio River. The maintenance of the new companion bridge will be covered by the Interstate Cooperative Agreement: https://brentspencebridgecorridor.com/wp-content/uploads/2023/02/2023.2.17-OH-KY-Interstate-Cooperative-Agreement_Fully-Executed.pdf</p>
225	Construction	03/08/2023 - Web Comment - I am certain that the team that has been assembled for the design of the bridge is very capable and efficient, but I would like to offer a suggestion. The Weirton-Steubenville Bridge (Veterans Memorial Bridge) crossing the Ohio River is nationally recognized. This was designed by the T.Y. Linn Group. The Walsh Group out of Chicago is one of the largest bridge designers in the country. Would it make sense to get some perspective from a different source?	<p>Thank you for your interest in the Brent Spence Bridge Corridor Project. ODOT and KYTC are currently requesting proposals from design-build teams interested in working on the project. This request for proposals is open to all firms that meet the qualifications outlined in the procurement documents, which are available here: https://www.dot.state.oh.us/Divisions/ConstructionMgt/design-build/bsb-project-procurement/Pages/default.aspxopen. Proposals from prospective design-build teams are due on March 31, 2023.</p> <p>An extensive bridge type selection process was undertaken by the project team that included public involvement and input from the project Aesthetics Committee. See the Executive Summary of the process here: https://brentspencebridgecorridor.com/wp-content/uploads/2022/10/Bridge-Type-Selection-Executive-Summary-1.pdf. As the design continues to be refined, the Aesthetics Committee will continue to be engaged.</p>
226	Project Mailing List	03/15/2023 - Web Comment - Representing Camp Washington and SpringDot.	This individual was added to the project mailing list.
227	Construction	03/17/2023 - Web Comment - When is the construction going to start?	Thank you for your interest in the Brent Spence Bridge Project. The groundbreaking for the project is anticipated to occur in late 2023 with construction beginning in earnest in 2024. The project is expected to be substantially complete in 2030.

Comment Number	Nature of Comment	Comment	Response
228	Multimodal	03/17/2023 - Web Comment - Please consider going to 2 lanes and having a nice, protected bike lane. This would be an important addition to connecting the community.	<p>The project is designed to accommodate traffic projected to travel through the corridor by the year 2050, which is the standard design horizon for large infrastructure projects. The traffic projections are based on a regional travel-demand model which assigns routes used by travelers based on available capacity, projected trips, and calculated travel times. Reducing the number of lanes on the freeway would not meet the project's primary purpose, which includes improving traffic flow and safety in the I-71/I-75 corridor.</p> <p>Pedestrian and bicycle accommodations are not planned on I-71/I-75, the new companion bridge, or the existing Brent Spence bridge because of the proximity of other reasonable accommodations, including a crossing of the Ohio River at the Clay Wade Bailey Bridge. Preliminary investigations indicate that adding bike lanes to the Clay Wade Bailey Bridge may be feasible. KYTC and ODOT will further evaluate adding bike lanes to the Clay Wade Bailey Bridge during the project's progressive design-build phase.</p> <p>Although not provided on I-71/I-75, the project will incorporate pedestrian and bicycle facilities along local streets to improve connectivity to transit, employment, healthcare, cultural, recreational, and commercial destinations. In Ohio, pedestrian and bicycle connections will be included across I-75 on 6th Street, 7th Street, 9th Street, Linn Street, Freeman Avenue, Ezzard Charles, Liberty Street, Findlay Street, Bank Street, and Harrison Avenue. In addition, a new shared use path will be constructed along Winchell Avenue between 9th Street and Ezzard Charles Drive, including a pedestrian bridge connection to Freeman Avenue.</p> <p>In Kentucky, new shared use paths and sidewalks will be included along the frontage roads. Also new and rebuilt sidewalks will be included under the MLK/West 12th Street, Pike Street, West 9th Street, West 5th Street, and West 3rd Street bridges. A new shared use path will be built under the West 9th Street and West 5th Street bridges, which will tie into the shared use paths in Goebel Park. The shared use path will be extended along Crescent Avenue to connect to the existing shared use path along the river.</p>

Comment Number	Nature of Comment	Comment	Response
229	Information Request	03/28/2023 - Web Comment - I'm a reporter with WCPO 9 News. I was hoping to speak with someone on the Diversity & Inclusion committee about minority contractors being included in the construction of the Brent Spence Corridor Project. My deadline to have this interview completed is Tuesday March 28 at 2pm.	A member of the project team contacted this individual.
230	Construction	03/28/2023 - Web Comment - when will it start?	Thank you for your interest in the Brent Spence Bridge Corridor Project. Construction on the project is expected to begin in earnest in 2024. The portion of the project south of Linn St. in Ohio is expected to be essentially complete in 2030. Two phases north of Linn St. will start in 2025 and 2028 and will take 3 to 4 years each to complete.
231	Diversity and Inclusion	03/29/2023 - Web Comment - You retards. Building a bridge properly has nothing to do with civil rights unless you don't want to build it properly.	Comment noted.
232	Diversity and Inclusion	03/29/2023 - Web Comment - As a female minority of the Jewish culture, I represent a minority of construction workers. As much as I enjoy seeing minorities given more opportunities, I also think all minorities need representation and a chance to further their careers into a positive future. Especially as a Jewish woman. We represent the smallest amount of actual construction workers. I just want a fair shake and think all minorities deserve a fair opportunity too	Thank you for your interest in the Brent Spence Bridge Corridor Project. Please visit https://www.dot.state.oh.us/Divisions/ConstructionMgt/design-build/bsb-project-procurement/Pages/default.aspx , which includes a form to sign up for email alerts to be notified of any future networking opportunities. In addition, the project website has information for those interested in working on the project: https://brentspencebridgecorridor.com/work-with-us/
233	Diversity and Inclusion	03/29/2023 - Web Comment - This ridiculous. The project is a bridge. The most qualified engineers, contractors and workers should be employed regardless of what they may be if on a bridge that fails I wouldn't be consoled by learning that standards and qualifications were forwarded for diversity Assuming I survive of course	Thank you for your feedback about the Brent Spence Bridge Corridor Project. KYTC and ODOT are committed to hiring qualified professionals to work on the BSB project while also promoting and growing a diverse and inclusive workforce. We firmly believe that we can accomplish both. Any entity that works on the project will be required to adhere to all federal, state, and local design standards, specifications, and construction practices.
234	Diversity and Inclusion	03/29/2023 - Web Comment - I want the best company's working on the job. My family will be using the bridge on a daily commute and all I care about is that the most qualified people work on it and design it.	Thank you for your feedback about the Brent Spence Bridge Corridor Project. KYTC and ODOT are committed to hiring qualified professionals to work on the BSB project while also promoting and growing a diverse and inclusive workforce. We firmly believe that we can accomplish both while building a project that will benefit you and your family during your daily commutes.

Comment Number	Nature of Comment	Comment	Response
235	Diversity and Inclusion	03/30/2023 - Web Comment - Regarding the Brent Spence Bridge Project and "diversity and inclusion" as far as the contracts go, I feel that it is only fair that black businesses and workers have an equal shot at the project. It's only right. BUT I want you to make sure that the contracts you give out, the workers on the project, DESERVE the selection. This is a bridge over water we're talking about. Just giving out work to businesses and workers solely based on their skin color and a rule to include all races, is absurd and outrageous. I believe everyone deserves a fair shot. But not based on color. Base it on COMPETENCE. Having this bridge collapse full of vehicles all because you had to pick someone of a certain race is a very real concern, I hope you take into account. COMPETENCE, not COLOR.	Thank you for your feedback about the Brent Spence Bridge Corridor Project. KYTC and ODOT are committed to hiring qualified professionals to work on the BSB project while also promoting and growing a diverse and inclusive workforce. We firmly believe that we can accomplish both. Any entity that works on the project will be required to adhere to all federal, state, and local design standards, specifications, and construction practices.
236	Construction	03/30/2023 - Web Comment - We are a Belzona distributor, an international manufacturer of solid based epoxies for coating steel and concrete to prevent long term corrosion. Our 2 products for steel would be Belzona 5811 (Immersion Grade) and topped off with a Urethane called Belzona 5115. Both are non-solvent materials. We would like to know where to start? Who is the governing engineering firm we should talk with?	<p>Thank you for your interest in the Brent Spence Bridge Corridor Project. ODOT and KYTC are currently soliciting proposals from teams bidding on the progressive design-build project. Proposals will be due from design-build teams on April 14, 2023. Please visit https://www.dot.state.oh.us/Divisions/ConstructionMgt/design-build/bsb-project-procurement/Pages/default.aspx for information on the project's procurement process, including links to the request for proposals (RFP). This web site also includes a form to sign up for email alerts to be notified of any future networking opportunities.</p> <p>In addition, the project website has information for those interested in working on the project: https://brentspencebridgecorridor.com/work-with-us/</p> <p>You may also visit https://www.dot.state.oh.us/Divisions/ConstructionMgt/Materials/new-products/Pages/default.aspx for information on how to get materials approved for use on ODOT projects.</p>
237	Project Mailing List	03/31/2023 - Web Comment - I live directly west of the Brent Spence at 405 Western Avenue, Covington. I wish receive updates as they become available. Thank You !!	This individual was added to the project mailing list.
238	BSB Companion Bridge	03/31/2023 - Web Comment - I noticed the email stating the companion bridge has been reduced to 84', but the total bridge width is 107'. Is this 107' smaller than the previous footprint?	Thank you for your interest in the Brent Spence Bridge Corridor Project. The original total width for the companion bridge was 172 feet. Because of refinements incorporated into the design, the total width has been reduced to 107 feet.

Comment Number	Nature of Comment	Comment	Response
239	Information Request	03/31/2023 - Email Comment - The Cincinnati Herald would like to use this article and photos in our edition for next week. Please advise since it has a copyright.	A member of the project team authorized the use of the referenced article and photos.
240	Construction	04/01/2023 - Web Comment - Hi my name is [REDACTED] and I'm the owner of Good Hand Hauling llc. Our primary focus is hauling away cement, asphalt sand gravel and other bulk items. On March 7 I went to the meet and greet hosted by ODOT and was advised to filled out paperwork with Walsh and kokosing. at the time I was in the process of getting my approval for my DBE Certification I filled out the paperwork with the companies and never heard anything from those companies I have several dump trucks and I would like to be part of the project.	<p>Thank you for your interest in the Brent Spence Bridge Corridor Project. We encourage you to reach out directly to any firms with which you have ongoing coordination to determine the status of those activities.</p> <p>In addition, the project website has information for those interested in working on the project: https://brentspencebridgecorridor.com/work-with-us/</p>
241	BSB Companion Bridge	04/01/2023 - Web Comment - Is the new bridge going to have a name or just be called the. "Companion Bridge?"... Thanks	Thank you for your interest in the Brent Spence Bridge Corridor Project. The new companion bridge is anticipated to be formally named, but process for naming the new bridge has not yet been established.
242	General Support	04/01/2023 - Web Comment - Great to hear about new bridge	Comment noted.
243	Construction	04/03/2023 - Web Comment - The company I work for owns and manages the Holiday Inn Cincinnati-Riverfront at 600 W 3rd St. Covington, KY. Of course, we are interested in providing accommodations for crews during this project when they are in need of securing lodging needs. Do you happen to have a point of contact for whom I can reach out to regarding this matter when the time comes?	Thank you for your interest in the Brent Spence Bridge Corridor Project. ODOT and KYTC anticipate that the design-build team will be announced in June 2023 and under contract by July 2023. You may reach out to the selected design-build team at that time.
244	Information Request	04/05/2023 - Web Comment - To whom this may concern, my name is [REDACTED], and I am a current senior at Mariemont High School, located in Cincinnati. I have been tasked with a "Civic Engagement Project" for my AP Government and Politics class. I decided I wanted to research and write about the Brent Spence Bridge. While it is definitely controversial, I would like to highlight the importance of it. It would be an honor to include primary insight in my research. Is there someone who would be willing to answer a couple of questions about the project, and other aspects of the bridge? Over the phone, or via email, whichever is preferred.	A member of the project team contacted this individual.

Comment Number	Nature of Comment	Comment	Response
245	Construction	04/06/2023 - Web Comment - Hi, I am a regional sales manager for Champion Fiberglass. We manufacture fiberglass conduit and bridge hangers, fiberglass bridge drain, along with fiberglass strut. I would like to reach out to your team to possibly meet for a product demonstration and presentation. We have successfully done these with a lunch and learn format if you are interested or any other time would work for me. It would typically take 1 hour however I have shared a 30-minute version if this would be the time you all could give me. Please get back with me via email or phone [REDACTED] if I could meet with the people in your group that would be interested. Thank you and have a great day.	<p>Thank you for your interest in the Brent Spence Bridge Corridor Project. The project is still in active procurement. We expect to announce a design-build team in May. Here is a link to the procurement website, and you can sign up for notification emails through this site: https://www.dot.state.oh.us/Divisions/ConstructionMgt/design-build/bsb-project-procurement/Pages/default.aspx</p> <p>Also, The following link will give you information and contacts on how ODOT can incorporate new products into its work: https://www.dot.state.oh.us/Divisions/ConstructionMgt/Materials/new-products/Pages/default.aspx</p>
246	Construction	04/06/2023 - Web Comment - Hope all is well I was wondering if they had the transportation part filled. I am An MBE and edge holder just seeing if they may need some tractor trailers or dump trucks, please let me know I would like to be a part of this project Thank you for your time have great day.	<p>Thank you for your interest in the Brent Spence Bridge Corridor Project. ODOT and KYTC are currently soliciting proposals from teams bidding on the progressive design-build project. Proposals will be due from design-build teams on April 14, 2023. Please visit https://www.dot.state.oh.us/Divisions/ConstructionMgt/design-build/bsb-project-procurement/Pages/default.aspx for information on the project's procurement process, including links to the request for proposals (RFP). This web site also includes a form to sign up for email alerts to be notified of any future networking opportunities.</p> <p>In addition, the project website has information for those interested in working on the project: https://brentspencebridgecorridor.com/work-with-us/</p> <p>You may also visit https://www.dot.state.oh.us/Divisions/ConstructionMgt/Materials/new-products/Pages/default.aspx for information on how to get materials approved for use on ODOT projects.</p>

Comment Number	Nature of Comment	Comment	Response
247	Construction	<p>04/06/2023 – Email Comment - I was hoping I could get your assistance as I'm trying to gather information on the Brent Spence Bridge Project which has gradually been moving forward. Champion Fiberglass manufactures an electrical conduit system that is utilized on bridges all across the country, so we are very interested in being a part of this project. With our products light weight yet rigid design, Champion Fiberglass could provide a substantial savings to not only material costs but also with installation and labor costs. I understand that the project is still in the early stages however I wanted to be diligent in making our interests in the project known, our main targets would be the firm(s) responsible for the electrical engineering portion of the project.</p> <p>Any information you could share at this time would be appreciated. Just to give you an idea of how our product is utilized in bridge applications below is an image of a bridge in Seattle, WA. where our conduit is being used. Should you have any questions on the material I have provided please let me know. Thank you in advance for your help.</p>	<p>Thank you for your interest in the Brent Spence Bridge Corridor Project. The project is still in active procurement. We expect to announce a design-build team in May. Here is a link to the procurement website, and you can sign up for notification emails through this site: https://www.dot.state.oh.us/Divisions/ConstructionMgt/design-build/bsb-project-procurement/Pages/default.aspx</p> <p>Also, The following link will give you information and contacts on how ODOT can incorporate new products into its work. https://www.dot.state.oh.us/Divisions/ConstructionMgt/Materials/new-products/Pages/default.aspx</p>
248	Construction	<p>04/07/2023 - Web Comment - I represent the Courtyard Cincinnati Covington and would love the opportunity to host any upcoming meetings or updates related to the Brent Spence Corridor project. We would also welcome the opportunity to host any construction crews or project management teams in the area. Are you able to connect me with someone who handles this?</p>	<p>Thank you for your interest in the Brent Spence Bridge Corridor Project. ODOT and KYTC anticipate that the design-build team will be announced in June 2023 and under contract by July 2023. You may reach out to the selected design-build team at that time.</p>
249	Construction/ Design	<p>03/27/2023 - Email - Thanks for the response. So I'm a bit confused now that I look more into the project. So my understanding is there will be a new companion bridge with be I-71/I-75, correct? This will be 5 lanes of traffic in either direction. Then, the old bridge will be 3 lanes in either direction. Then, what is going to happen with the railroad bridge? Is car-traffic being added to the railroad bridge?</p>	<p>The design for the new, companion bridge, which will carry I-71/75 traffic, calls for five travel lanes in each direction, with two, 12-foot shoulders on each deck. The existing Brent Spence Bridge will remain in service, and it will be reconfigured to carry three lanes of local or commuter traffic in each direction.</p> <p>As indicated below, preliminary studies have indicated that adding bike lanes to the Clay Wade Bailey Bridge may be feasible. This structure carries U.S. Route 42 and U.S. Route 127 across the river between Cincinnati and Covington, adding bike lanes to this bridge will be evaluated during the project's progressive design-build phase. Vehicular traffic will not be added to the C&O Railroad Bridge, which runs alongside the Clay Wade Bailey Bridge.</p>

Comment Number	Nature of Comment	Comment	Response
250	Right-of-Way	04/09/2023 - Web Comment - Hello , We live in Ludlow, Ky & have interests in west Covington .I was wondering what structures in West Covington well the new Bridge alienate, I can not find any plan on the net.	<p>Maps showing proposed right-of-way limits (where properties will be impacted) for the project can be accessed here: https://brentspencebridgecorridor.com/wp-content/uploads/2022/12/2022-12-06_Corridor-Exhibit.pdf. Impacted structures are shown with red hatching on the exhibit.</p> <p>The Kentucky Transportation Cabinet (KYTC) is currently in the process of acquiring the property needed to build the project and will formally notify property owners and residents as part of the relocation process. Please keep in mind that if you are impacted, you are entitled to compensation for the property (if you own) but also relocation expenses (if you own or rent). Please don't hesitate to reach out if you have any additional questions.</p>
251	Construction	04/11/2023 - Web Comment - Happy Tuesday! I have an existing construction company. How do I get connected with the Prime contractors for this project? I'd love to connect with them!	<p>Thank you for your interest in the Brent Spence Bridge Corridor Project. ODOT and KYTC do not have a list of contractors for the Brent Spence Bridge Corridor Project, as the project is still in active procurement. We expect to announce a design-build team in June. Here is a link to the procurement website, and you can sign up for notification emails through this site: https://www.dot.state.oh.us/Divisions/ConstructionMgt/design-build/bsb-project-procurement/Pages/default.aspx</p> <p>In addition, the project website has information for those interested in working on the project: https://brentspencebridgecorridor.com/work-with-us/</p>
252	Construction	04/12/2023 - Web Comment - Hello - I was wondering if there is a plan-holder list for this project. Thanks	<p>Thank you for your interest in the Brent Spence Bridge Corridor Project. ODOT and KYTC do not have a list of contractors for the Brent Spence Bridge Corridor Project, as the project is still in active procurement. We expect to announce a design-build team in June. Here is a link to the procurement website, and you can sign up for notification emails through this site: https://www.dot.state.oh.us/Divisions/ConstructionMgt/design-build/bsb-project-procurement/Pages/default.aspx</p> <p>In addition, the project website has information for those interested in working on the project: https://brentspencebridgecorridor.com/work-with-us/</p>

Comment Number	Nature of Comment	Comment	Response
253	Construction	04/14/2023 - Web Comment - I am a Logistics Account Executive at Total Quality Logistics. With expertise in Oversize-Overweight solution development here at the Lexington, KY office. I am already in talks with a company in Maysville, KY about finding solutions for them to tear down the current bridge associated with this project near CVG. If you would like to talk to me about how I may be able to assist with this project, please send me an email or call my cell at [REDACTED].	<p>Thank you for your interest in the Brent Spence Bridge Corridor Project. The project is still in active procurement. We expect to announce a design-build team in June. Here is a link to the procurement website, and you can sign up for notification emails to be notified of any future networking opportunities through this site: https://www.dot.state.oh.us/Divisions/ConstructionMgt/design-build/bsb-project-procurement/Pages/default.aspx</p> <p>The design-build team will be responsible for developing means and methods for delivery and removal of oversize-overweight items, so they would be the appropriate group to work with once selected.</p>
254	Project Schedule	04/20/2023 - Web Comment - Where can I find a schedule of construction for the project?	<p>Thank you for your interest in the Brent Spence Bridge Corridor Project. Construction on Phase I of the BSB Corridor Project (Findlay Street to Marshall Avenue) is anticipated to begin in 2028 and be complete in 2031. Construction on Phase II (Linn Street to Findlay Street) is anticipated to begin in 2025 and end in 2029. Construction on Phase III (the remainder of the project corridor, including the companion bridge over the Ohio River) is anticipated to begin in 2023 and be complete in 2030.</p>
255	Miscellaneous	<p>04/21/2023 - Web Comment - I saw your website and I wanted to reach out because I think I can be of great assistance.</p> <p>I provide Virtual Assistants to Business Owners. We help aid with their tedious day-to-day tasks like; prospecting, administrative tasks, cold calling, database management, social media branding, content writing, and accounting work.</p> <p>Are you currently looking for any help?</p>	<p>Thank you for your interest in the Brent Spence Bridge Corridor Project. The project is still in active procurement. We expect to announce a design-build team in June. Here is a link to the procurement website, and you can sign up for notification emails to be notified of any future networking opportunities through this site: https://www.dot.state.oh.us/Divisions/ConstructionMgt/design-build/bsb-project-procurement/Pages/default.aspx</p> <p>The design-build team will be responsible for day-to-day tasks, so they would be the appropriate group to work with once selected.</p>

Comment Number	Nature of Comment	Comment	Response
256	Construction	<p>4/21/2023 - Web Comment - Hello, I am filling out this field because I am interested in providing logistics/transportation needs for the construction of the Brent Spence Bridge Corridor Project. TQL (Total Quality Logistics) is a locally owned company with our HQ in Cincinnati. My team and I are located in the Lexington, KY office, so we have assets in both states. We are confident we can provide any and everything you need as far as project transportation for the construction of the project.</p>	<p>Thank you for your interest in the Brent Spence Bridge Corridor Project. The project is still in active procurement. We expect to announce a design-build team in June. Here is a link to the procurement website, and you can sign up for notification emails to be notified of any future networking opportunities through this site: https://www.dot.state.oh.us/Divisions/ConstructionMgt/design-build/bsb-project-procurement/Pages/default.aspx</p> <p>The design-build team will be responsible for overseeing logistics/transportation needs for the project, so they would be the appropriate group to work with once selected.</p>
257	Public Input	<p>4/24/2023 - Email Comment - My name is [REDACTED], and I'm a volunteer with Bridge Forward Cincinnati. I'm writing today to share with you a digital copy of the Bridge Forward Community Engagement & Participation Report 2023. Several hard copies of this report will also be sent to the Ohio FHWA office and ODOT, those should be received in the next few days.</p> <p>Bridge Forward Cincinnati is a grassroots non-profit organized around making the Brent Spence Project as beneficial to the local community as possible. There have been reports recently about a group that wants to stop the Brent Spence Project. That is not our position, and we are not affiliated with that group.</p>	<p>Thank you for your interest in the Brent Spence Bridge Corridor Project. Your comments and provided information will be included in the project record.</p>
258	Roadway/Bridge Design	<p>4/22/2023 - Web Comment - Where can I view a drawing of the new bridge and surrounding area. Not on-line, it's too small. I'd like to see how it may affect my home.</p>	<p>Thank you for your interest in the Brent Spence Bridge Corridor Project. Maps showing the new companion bridge and surrounding area can be accessed here: https://brentspencebridgecorridor.com/wp-content/uploads/2023/04/2023-04-24_Corridor-Exhibit.pdf. You can access renderings showing what the project might look like from various locations here: https://brentspencebridgecorridor.com/wp-content/uploads/2023/02/Renderings-for-Boards-Reduced.pdf.</p> <p>The maps and drawings linked above have enough resolution for you to zoom in to look more closely at specific locations. If you would prefer hard copies of project mapping, please provide your address, and we would be happy to mail them to you.</p>

Comment Number	Nature of Comment	Comment	Response
259	Construction	<p>04/24/2023 - Web Comment - I work at TQL and I am wanting to get in touch with a project manager for the Brent Spence Bridge Corridor Project. Just wanting to extend any services necessary for this project. Whether it is OSOW, FTL, drop-trailer, or warehousing, TQL and I can assist with the best service in logistics.</p> <p>Feel free to reach out to my work line [REDACTED] or my personal line [REDACTED] and I would love to help out in any way.</p>	<p>Thank you for your interest in the Brent Spence Bridge Corridor Project. The project is still in active procurement. We expect to announce a design-build team in June. Here is a link to the procurement website, and you can sign up for notification emails to be notified of any future networking opportunities through this site: https://www.dot.state.oh.us/Divisions/ConstructionMgt/design-build/bsb-project-procurement/Pages/default.aspx</p> <p>The design-build team will be responsible for overseeing logistics/transportation needs for the project, so they would be the appropriate group to work with once selected.</p>
260	Roadway/Bridge Design	<p>04/25/2023 - Web Comment - I noted that there would be a move of the us 50 entrance ramp westward ... what effect will that have on the buildings directly off the Linn exit ramp? Will they be demolished in the construction? Thank you for your response. If there is a map that I could view as well, I would be interested if you could tell me how to view it. I could not tell much by the overview.</p>	<p>Thank you for your interest in the Brent Spence Bridge Corridor Project. Based on coordination with the City of Cincinnati, the northbound I-75 exit to 5th street will be reconfigured to create a new signalized intersection with the US 50 ramp approximately 300 feet west of Central Avenue. This change will take place in the area that is already occupied by the highway and will not impact on any existing buildings. Maps showing the proposed project can be accessed here: https://brentspencebridgecorridor.com/wp-content/uploads/2023/04/2023-04-24_Corridor-Exhibit.pdf.</p>
261	Information Request	<p>04/27/2023 - Web Comment - Would like additional information on how to be in touch with contractors bidding project.</p>	<p>Thank you for your interest in the Brent Spence Bridge Corridor Project. The project is still in active procurement. We expect to announce a design-build team in June. Here is a link to the procurement website, and you can sign up for notification emails to be notified of any future networking opportunities through this site: https://www.dot.state.oh.us/Divisions/ConstructionMgt/design-build/bsb-project-procurement/Pages/default.aspx</p>
262	Project Mailing List	<p>04/29/2023 - Web Comment - I would like to get the project updates.</p>	<p>This individual was added to the project mailing list.</p>

Comment Number	Nature of Comment	Comment	Response
263	Right-of-Way	<p>04/29/2023 - Web Comment - I don't know why you guys aren't taking the back end of Hermes Avenue in Covington there are a lot of empty run down homes the noise is deafening now, water from Dixie highway runs off the hill because all of the condos they have built behind it; this street could connect Pike Street and the express way and ease traffic from the 12th street on ramp. There's no places to park which causes tempers to flair. It should be used to ease traffic congestion.</p>	<p>Thank you for your interest in the Brent Spence Bridge Corridor Project and your inquiry regarding property along Hermes Avenue. In accordance with State and Federal laws, KYTC will only acquire properties that are directly impacted by the project. The Brent Spence Bridge Corridor Project does not impact any homes along Hemes Avenue; therefore, they will not be acquired. In addition, KYTC is proposing noise walls that will help to reduce noise levels along Hermes Avenue. The issues raised about local development patterns, stormwater management, parking, and traffic patterns are outside of the scope of this project and would be best addressed by local agencies.</p> <p>Maps showing proposed right-of-way limits (where properties will be impacted) for the project can be accessed here. Impacted structures are shown with red hatching on the exhibit. This exhibit also shows the locations of proposed noise walls.</p>
264	Alternatives	<p>04/29/2023 - Web Comment - YOU NEED TO THINK IN THE 1960 BOX!!!!!!!!!!!!!!!!!!!!!!</p> <p>A. CURRENT DESIGN 1. THE UNDERGROUND RIVER PROBLEM 2. B/S IS CANTILEVERED 3. LANES ARE SMALL 4. COMPLETION HAD A COST FACTOR BELOW NEED 5. GRADE DEGREE SLOWS HEAVY VEHICALS</p> <p>B. FUTURE 1. CORRECT GRADE TOO FLOW THRU TRAFIC AT DESIGN SPEED 2. RESTRUCTURE CURRENT BS BRIDGE TO LESSON CANTALEVER 3. REDUCE PROPERTY ACQUASSION TOO A MINIMUM. 4. REDUCE TOTAL COST TOO BELOW THE \$3B PRICE</p> <p>C. CONSIDER CORRECTION OF 1960 1. SET NEW PILINGS ABOUT 20 FEET OUT IN RIVER ON EACH SIDE 2. CONNACT CURRENT DECKS ON EXISTING PILINGS AND NEW PILINGS (REDUCES CANTILEVER PROMBEM) 3. CONSTRUCT A THIRD DECK (BYPASSING CINCY AND COVINGTON EXITS) AT APPX 100 FEET WIDE 4. CONSTRUCT ELEVELATED HIGHWAY OVER EXISTING ROADWAY STARTING BETWEEN OHIO HOPPLE / WESTERN HILLS EXITS AND KENTUCKY KEYLES LANE. 5. ESTIMATED COST BASEED ON CALIFORNIA ELEVATED HIGHWAY PRICES < \$2.4B.</p> <p>!!!!!!!!!!!!!!YOUR IDEA WILL KEEP THE PROBLEM IN PLAY!!!!!!!!!!!!!!</p>	<p>Thank you for your interest in the Brent Spence Bridge Corridor Project. The alternatives evaluation process and engineering study conducted for the project has shown that rehabilitating the existing Brent Spence Bridge and building a new companion bridge is feasible and the preferred alternative for addressing the traffic flow and safety problems in the corridor. KYTC and ODOT will work with the selected design-build team to prepare detailed design plans for the new companion bridge. The project will not change the grade on the cut-in-the-hill in Kentucky. Adding a third deck to the existing Brent Spence Bridge would not satisfy the project's purpose and need, because it would not provide enough lanes to carry the traffic projected to travel through the corridor. Likewise, constructing an elevated highway above the existing roadway through the majority of the 8-mile corridor would substantially increase the project's costs and impacts and is not required to satisfy the project's purpose and need.</p> <p>KYTC and ODOT have incorporated several refinements (including refinements to the design of the new companion bridge) to minimize project costs and will continue to work with the selected design-build team to identify additional cost-saving measures. While additional land will be required to build the project, ODOT and KYTC have worked to substantially reduce the project's footprint, including reducing the residential relocations from over 40 to 4.</p>

Comment Number	Nature of Comment	Comment	Response
265	Project Schedule	04/29/2023 - Web Comment - Completion date. ???	Thank you for your interest in the Brent Spence Bridge Corridor Project. Construction on Phase I of the BSB Corridor Project (Findlay Street to Marshall Avenue) is anticipated to begin in 2028 and be complete in 2031. Construction on Phase II (Linn Street to Findlay Street) is anticipated to begin in 2025 and end in 2029. Construction on Phase III (the remainder of the project corridor, including the companion bridge over the Ohio River) is anticipated to begin in 2023 and be complete in 2030.
266	Construction	05/03/2023 - Web Comment - We were at the Tri-state conference last week. We are a KYTC DBE/SBE certified company. We also are certified in Indiana. Can someone help me navigate the Brent Spence Bridge Corridor project. We heard that DBE's from Kentucky can bid on projects. I'd like to better understand that and what are the first steps we need to do.	Thank you for your interest in the Brent Spence Bridge Corridor Project. The project is still in active procurement. We expect to announce a design-build team in June. Please visit the "Work With Us" page on the project website for links to resources for firms that want to work on the project: https://brentspencebridgecorridor.com/work-with-us/ In addition, here is a link to the procurement website, and you can sign up for notification emails to be notified of any future networking opportunities through this site: https://www.dot.state.oh.us/Divisions/ConstructionMgt/design-build/bsb-project-procurement/Pages/default.aspx
267	Construction	05/05/2023 - Web Comment - I am reaching out to find out how I go about applying for appointment and an opportunity to work on this project, may I get a list of contractors who will be on job site? If I'm not mistaken this would be considered Union work? Should go to Union Hall in Kentucky or Ohio to seek employment? I'm assuming Ohio cuz that's where I resign? Any information would be greatly appreciated not too savings with trying to work this site. Thank you for taking the time out to read and gather information in regard to my question.	Thank you for your interest in the Brent Spence Bridge Corridor Project. The project is still in active procurement. We expect to announce a design-build team in June. Please visit the "Work With Us" page on the project website for links to resources for individuals that want to work on the project: https://brentspencebridgecorridor.com/work-with-us/
268	Construction	05/10/2023 - Web Comment - Has their already been a bid meeting?	Thank you for your interest in the Brent Spence Bridge Corridor Project. The project is still in active procurement. Proposals have been received and are being evaluated by ODOT and KYTC. We expect to announce a design-build team in June. Here is a link to the procurement website, and you can sign up for notification emails to be notified of any future announcements about the project's procurement activities: https://www.dot.state.oh.us/Divisions/ConstructionMgt/design-build/bsb-project-procurement/Pages/default.aspx

Comment Number	Nature of Comment	Comment	Response
269	Miscellaneous	05/11/2023 - Web Comment - Good morning. I am the Immigrant Liaison at Hamilton County Community Action Agency. We are interested in joining the Diversity and Inclusion committee for the Brent Spence Bridge Corridor project. We believe our presence with the immigrant communities in the region will be instrumental in your outreach efforts to diverse entrepreneurs and workforce development trainees. Please let us know if we are able to join your efforts at this point, and how do I go about doing that. Thank you in advance for your response. Best regards.	Thank you for your request to join the Diversity & Inclusion Outreach Committee. The ODOT Office of Opportunity, Diversity, and Inclusion will reach out to the Hamilton County Community Action Agency with a formal invitation.
270	Construction	05/26/2023 - Web Comment - I just wanted to reach out to see if it was possible to lend a hand with this project. I work for a logistics company called Total Quality Logistics! We can provide Crane and Forklift rentals along with rigging crews. We can also assist with drop trailer, warehouse storage and transportation of materials and equipment if needed! I am currently located in Evansville, IN. However, we have capacity in all 50 states for all of these services. So location is never an issue with any of the projects that I help with. If anything comes up that I can help with, please feel free to reach out at anytime!	<p>Thank you for your interest in the Brent Spence Bridge Corridor Project. The project is still in active procurement. We expect to announce a design-build team in June. Here is a link to the procurement website, and you can sign up for notification emails to be notified of any future networking opportunities through this site: https://www.dot.state.oh.us/Divisions/ConstructionMgt/design-build/bsb-project-procurement/Pages/default.aspx</p> <p>The design-build team will be responsible for overseeing logistics/transportation needs for the project, so they would be the appropriate group to work with once selected.</p>
271	Construction	05/31/2023 - Web Comment - We are a bridge AISC certified fabricator. Do you know when this project will go out for bid? Would you know who might be bidding on this project?	<p>Thank you for your interest in the Brent Spence Bridge Corridor Project. The project is still in active procurement. We expect to announce a design-build team in June. Here is a link to the procurement website, and you can sign up for notification emails to be notified of any future networking opportunities through this site: https://www.dot.state.oh.us/Divisions/ConstructionMgt/design-build/bsb-project-procurement/Pages/default.aspx</p> <p>The design-build team will be responsible for developing means and methods for construction of the project, so they would be the appropriate group to work with once selected.</p> <p>You may also visit https://www.dot.state.oh.us/Divisions/ConstructionMgt/Materials/new-products/Pages/default.aspx for information on how to get materials approved for use on ODOT projects.</p>

Comment Number	Nature of Comment	Comment	Response
272	BSB Companion Bridge	05/31/2023 - Web Comment - Think BOLD, not boring! Consider this type of cable bridge: https://www.roadtraffic-technology.com/projects/i-280/ The design is already in the box and ready to be utilized.	Thank you for your interest in the Brent Spence Bridge Corridor Project. The design of the new companion bridge is not yet determined. It may be an arch design or a cable stayed design. Pictures showing what the arch or cable stayed design might look like can be found on the project website: https://brentspencebridgecorridor.com/wp-content/uploads/2023/02/Renderings-for-Boards-Reduced.pdf . These bridge types were selected through an extensive bridge type selection process that was undertaken by the project team that involved public involvement and input from the project Aesthetics Committee. An Executive Summary of this process can be accessed here: https://brentspencebridgecorridor.com/wp-content/uploads/2022/10/Bridge-Type-Selection-Executive-Summary-1.pdf . KYTC and ODOT are committed to building an iconic, aesthetically pleasing bridge. KYTC and ODOT will determine the final bridge type for the new companion bridge based on a technical evaluation performed by the design-build team. Once the bridge type is determined, that information will be made available to the public, and the project Aesthetics Committee will be engaged to provide feedback on the aesthetic elements of the new companion bridge and the existing BSB.
273	Project Mailing List	06/06/2023 - Web Comment - Please keep me updated on public hearings.	Thank you for your interest in the Brent Spence Bridge Corridor Project. You have been added to the project mailing list and will receive future project updates, including information about the public hearings, when they are scheduled.
274	Right-of-Way	06/09/2023 - Web Comment - I have a client on the Ohio side of the River that owns a large tract of land near the River that appears to be in the construction path, but he indicates that no one has contacted him. Is there someone I can speak to and explore this question? again, property on the Ohio side.	A member of the project team contacted this individual.

Comment Number	Nature of Comment	Comment	Response
275	Right-of-Way	<p>06/09/2023 - Web Comment - I own [REDACTED] in Covington and when we met in the fall, I was told property procurement for my area would begin in the spring and was wondering if you have a date when that may begin? My tenant was asking me, and I told him I would reach out to you.</p>	<p>Thank you for your inquiry regarding the Brent Spence Bridge Corridor Project, and more specifically, the timeline associated with the acquisition of your property located within the project area.</p> <p>Due to the location of your property within the historic district, initiating the Right of Way process for you (as the property owner) and your tenant is contingent upon completing the update to the environmental documentation for the project. Unfortunately, that process is taking longer than originally anticipated and was not completed as planned. We continue to be actively engaged in the environmental process, but current schedule shows a delay of approximately a year. However, we continue to work to accelerate the timeline.</p> <p>I understand the uncertainty in the timeline for you and your tenant is difficult. Please feel free to reach out to me directly (contact information below) with any additional questions or periodic updates.</p>
276	Alternatives	<p>06/15/2023 - Email Comment - I just thought that there's a local effort to try to have the highway department change their alignment on expanding the Brent Spence Bridge and this effort is an angle that they're playing as Leverage. The group is called Bridge Forward they could be blackmailing the municipality like this. Is that legal??</p> <p>Are they creating any liability for ODOT or USDOT? Please forward to me email addresses for the US.councils all they got is their phone numbers I would like to register what is attached to them also. I don't need to tell you but this city is out of control and the governor should take over like they do in other states their MSD is running wild among everything else.</p> <p>They are \$400 million deferred maintenance for Paving and another \$100 million for deferred facilities and that's only what I know about.</p> <p>I worked in the expressway design office in this time period As a draftsman/technician working on all the part takes and whole takes for IH75-IH74 and IH71.</p>	<p>I am in receipt of your correspondence to ODOT's Office of Chief Legal Counsel regarding the Brent Spence Bridge Corridor project, and as with all new construction projects, we are reviewing community input as part of our public involvement process. For more information about the project, please visit the project website at Brent Spence Bridge, where you can learn more about the current status of the project, sign up for our newsletter, submit comments, and review documents.</p>

Comment Number	Nature of Comment	Comment	Response
277	Diversity and Inclusion	<p>06/09/2023 - Web Comment - I am a principal at 7PM Group LLC (7PM). 7PM is a DBE and MBE certified consulting company, and we led the team that wrote and managed the Diversity, Equity and Inclusion (DEI) component for the \$4 billion dollar Louisville Southern Indiana Ohio River Bridges Project (LSIORBP), which included us physically traveling to all 120 counties in Kentucky, 8 counties in southern Indiana and 2 counties in southern Ohio. We participated in Public Involvement, Community Engagement, built strategic databases and managed PR and media relations as it related to DEI. We have grown in expertise, technical capability and overall capacity and would like to work with the BSBC Team. Currently we are working around the state as the Owners' Rep as it relates to DEI (some clients include, Norton Hospital, JCPS, FCPS, Hosparus, WKU and Gray Construction). We would love to speak with someone about the possibility of joining the BSBC Team and sharing lessons learned on the previous twin bridges project, as well as new initiatives and insights gained on current projects. We work all over the state and have established satellite offices in Indianapolis, Cincinnati and Nashville in the past to execute contracts. I can be contacted personally by email or cell @ [REDACTED].</p>	<p>Thank you for your interest in the Brent Spence Bridge Corridor Project. The project is still in active procurement. We expect to announce a design-build team in June. Here is a link to the procurement website, and you can sign up for notification emails to be notified of any future networking opportunities through this site: https://www.dot.state.oh.us/Divisions/ConstructionMgt/design-build/bsb-project-procurement/Pages/default.aspx.</p> <p>The design-build team will be responsible for implementing the DEI component of the project, so they would be the appropriate group to work with once selected.</p>
277A	Project Development Process	<p>06/13/2023 - Email Comment - The Downtown Residents' Council (DRC) represents the interests of Downtown Cincinnati residents and businesses. DRC recognizes the importance of the impending Brent Spence Bridge Corridor Project (BSBCP) to our community and the need to emphasize some critical priorities that we believe should be incorporated into the BSBCP. The BSBCP, a joint project by the Ohio Department of Transportation (ODOT) and the Kentucky Transportation Cabinet (KYTC), has publicly committed to building the project using a "progressive design-build process." We believe that such a process calls for ODOT / KYTC, the design-build team (DBT), impacted municipalities, and the local community to collaborate through the entire project lifecycle, especially now during what is being called Phase IA or the "proof of concept phase."</p> <p>DRC submits that the progressive design build process should be leveraged to the greatest extent possible to benefit the Downtown urban environment, the substantial impact that the BSBCP will have on Downtown, and the transformational opportunity that the BSBCP</p>	<p>Thank you for your interest in the Brent Spence Bridge Corridor Project. We received the letter you sent on behalf of the Downtown Residents Council. We will consider the concerns that were discussed in the letter and ensure they are considered prior to making a NEPA determination.</p>

Comment Number	Nature of Comment	Comment	Response
277A (cont.)	Project Development Process (cont.)	<p>presents for Downtown. For that purpose, we request that the following priorities be involved in guiding the project's design development so as to improve the quality of Downtown's urban environment adjacent to the project:</p> <ul style="list-style-type: none"> • Maximize the amount of pedestrian connection between the Central Business District, Queens gate, and the West End, minimizing the walking distance of each of those connections; • Maximize the number of local streets that are designed for maximum beneficial use and flow; • Minimize the number of lane miles using high-speed directional ramps; and • Achieve best expenditure of public dollars measured in life-cycle return on investment. <p>Specifically, Phase 1A of the project should proactively require robust alternative design concepts, that are specifically guided by these community priorities. DRC asks that the City, County, and ODOT each play a role in ensuring that these community design priorities be substantially incorporated into the project during Phase 1A.</p> <p>Please accept this notice as documented support for the community design priorities stated herein for Phase 1A.</p>	
278	Construction	<p>06/15/2023 - Web Comment - Regarding the construction of the noise wall in particular, where may I see the geological impact studies and what engineering methods will be used to prevent the landslide on our street from getting worse during construction? Is there a contingency plan if the construction affects our properties?</p>	<p>Thank you for your inquiry regarding the Brent Spence Bridge Corridor Project specific to the construction of noise walls.</p> <p>In evaluating opportunities for noise walls within the corridor, the project team utilizes both geotechnical information specific to this project and historical data collected along the corridor. The historical data can be located through an interactive map on the Kentucky Transportation Cabinet's (KYTC) website located at: https://transportation.ky.gov/StructuralDesign/Pages/Geotechnical-Projects.aspx.</p> <p>For the independent noise wall project along Crescent Avenue, a significant component of the project is the construction of a retaining wall along the eastern side of Crescent Avenue (closest to the interstate). The retaining wall will be constructed and secured; the noise barrier system will then be constructed on top of the retaining wall area. All design and construction work will be conducted to current engineering standards.</p>

Comment Number	Nature of Comment	Comment	Response
279	Miscellaneous	06/15/2023 - Web Comment - https://twitter.com/JoshJunker2/status/1668957389784064006?t=CMJpyUN37epoQwK2F0vYA&s=19 . I will suggest that the link above will be used as blackmail to the city of Cincinnati to force you to redesign the connections to front Spence Bridge. FYI. Actually, I should say it's the beginning of the blackmail with the city and your legal Beagles and District 8 pi didn't pay attention to me that's right I got in-laws to do that.	Comment noted.
280	Information Request	06/15/2023 - Web Comment - Each NKY city is wondering how the new twin bridge will effect each of our cities. Crescent Springs is often back-up on a daily basis. Many wrecks have occurred North & South. Crescent Springs access to West I-275 is a difficult access as travelers often speed posing safety concerns as they speed forward to CVG. Those wanting to access South bound lanes find it difficult to move into those lanes. Has KYDOT planned to make access lanes to remediate these concerns?	<p>Thank you for submitting a comment on the Brent Spence Bridge Corridor Project (BSBCP) website. With the southern limit of the BSBCP being just south of the Dixie Highway Interchange, the City of Crescent Springs is located south of the project limits and is not anticipated to be directly affected by the project.</p> <p>As part of a separate effort that is independent of the Brent Spence Bridge Corridor Project, the Kentucky Transportation Cabinet (KYTC) is currently studying ways to address transportation problems in the vicinity of I-75/I-275 interchange, the area specified in your comment. You can access information and provide comments specific to that separate project here: https://www.75275interchange.org/</p>
281	Miscellaneous	<p>06/19/2023 - Web Comment - I'd appreciate the opportunity to contribute an article to your website. My goal in writing the piece is to assist parents of special needs children in assessing their level of fatigue and create a self-care "treatment plan" that is unique to their needs. While the doctor is still the expert on the best type of treatment, having your own self-care routine helps a great deal. [REDACTED] of Mightymoms.net.</p> <p>~Interested in my guest post proposal, just not on the topic I mentioned? That's not a problem! Kindly reply to let me know and we can come up with an alternative topic. Or, if you're not interested in an article at all, I'd respect your wishes. Let me know, please.</p>	<p>We received your offer to contribute an article to the www.brentspencebridgecorridor.com website. The content on the Brent Spence Bridge Corridor Project website is focused exclusively on the aforementioned transportation improvement project, and the content is developed by the Ohio Department of Transportation (ODOT) and the Kentucky Transportation Cabinet (KYTC).</p>
282	Information Request	06/21/2023 - Web Comment - I am wondering if a design firm and contractor has been selected or announced yet?	Thank you for your interest in the Brent Spence Bridge Corridor Project. The project is still in active procurement. We expect to announce a design-build team in July.

Comment Number	Nature of Comment	Comment	Response
283	Alternatives	06/23/2023 - Email Comment - Please find an economic study examining the benefits of the Bridge Forward Concept. Also, please find attached a binder of the "Reconnecting Cincinnati Westway Design Improvements" letters sent from 8/22/22 through 12/22/22. And lastly, an op-ed for inclusion in the SEA / administrative record.	Thank you for your interest in the Brent Spence Bridge Corridor Project. The provided information has been included in the project record.
284	General Support	06/29/2023 - Web Comment - Great to know this all will happen. I am sure it will be great . . . Thanks! For much needed work.	Comment noted.
285	Design-Build Process	07/05/2023 - Email Comment - Thanks for the information – do you know when this project will be officially awarded? Also, what companies have showed interest and have bid on this?	Thank you for your interest in the Brent Spence Bridge Corridor Project. The project is still in active procurement. We expect to announce a design-build team in July. Due to confidentiality concerns, ODOT and KYTC are not able to provide a list of contracting teams that have expressed an interest in the project.
286	Design-Build Process	06/30/2023 - Web Comment - Who did you award as the design team / construction?	Thank you for your interest in the Brent Spence Bridge Corridor Project. The project is still in active procurement. We expect to announce a design-build team in July.
287	Project Schedule	07/01/2023 - Web Comment - Appears to me more analyzing and discussions and surveys and studies to kick the can down the road for another 30 years. This project will never come to fruition and is probably the biggest and longest boondoggle in the nation's infrastructure history.	Thank you for your interest in the Brent Spence Bridge Corridor Project. Construction on the project is expected to begin in earnest in 2024 and be completed by 2030.
288	Design-Build Process	07/05/2023 - Web Comment - Had a design team been awarded or a GC?	Thank you for your interest in the Brent Spence Bridge Corridor Project. The project is still in active procurement. We expect to announce a design-build team in July.
289	Roadway/Bridge Design	07/06/2023 - Web Comment - Would you please include in your plans the proper storm drainage systems to remove the ponding that occurs in the first two miles of road as one enters or leaves Ohio (Lake ODOT)? The existing poor drainage has existed for at least 50 years and has been responsible for many deaths and injuries to both travelers and commuters. Lake ODOT appears design related and may have existed since conception.	Thank you for your interest in the Brent Spence Bridge Corridor Project. The project drainage system will be designed in accordance with the applicable federal and state standards and will be adequately sized to capture project storm water runoff from the highway pavement.
290	Project Mailing List	07/07/2023 - Web Comment - Updates	This individual was added to the project mailing list.

Comment Number	Nature of Comment	Comment	Response
291	Information Request	07/10/2023 - Web Comment - when will the public meetings be scheduled and when will we see the draft environmental documents?	<p>Thank you for your interest in the Brent Spence Bridge Corridor Project. The Ohio Department of Transportation (ODOT) and the Kentucky Transportation Cabinet (KYTC) will be hosting two open houses in August – Aug. 22 and 23 – to update the public on progress toward completion of the bi-state Brent Spence Bridge Corridor Project. Additional details about those meetings will be released in the near future.</p> <p>Recent developments in the project include an alteration of the environmental schedule that affords more time to ensure compliance with all environmental laws and new guidance issued by the Federal Highway Administration (FHWA). Both states will continue to work together to ensure construction is completed on-time. The environmental documents are currently scheduled to be available for public review and comment in February 2024.</p> <p>[UPDATED RESPONSE] Thank you for your interest in the Brent Spence Bridge Corridor Project. We previously responded to a question you posed about the schedule for draft environmental documents for the project. Since we sent that response, ODOT, KYTC, and the Federal Highway Administration have identified opportunities to compress the project schedule. Based on the current project schedule, the environmental documents are anticipated to be available for public review and comment in late 2023.</p> <p>Also, public open houses are currently scheduled for Aug. 23 (Kentucky) and Aug. 24 (Ohio). Please visit the project website for additional information about the open houses. https://brentspencebridgecorridor.com/public-involvement-and-comments/#notice</p>
292	Construction	07/18/2023 - Web Comment - My name is [REDACTED] and I am President/CEO of Capital Transportation Inc. I am a certified DBE with ODOT and I am interested in participating in the Spencer Bridge project. Can I get more information about the project or have someone contact me about the project. Also, I would like to get updates on any pre-bid conference or information meeting about the project. I can be reached at office number [REDACTED]. Please contact me at your earliest convenience.	<p>Thank you for your interest in the Brent Spence Bridge Corridor Project. The project is still in active procurement. We expect to announce a design-build team in July. Please visit the "Work With Us" page on the project website for links to resources for firms that want to work on the project.</p> <p>In addition, please visit the "DBE/D&I Resources" page on the project website for a list of upcoming networking events.</p>

Comment Number	Nature of Comment	Comment	Response
293	Construction	<p>07/19/2023 - Web Comment - We are a Kentucky DBE. Is there a form to fill out for the Brent Spence project?</p> <p>If so, where can I find the form to apply or is there a reciprocity agreement? If there is a reciprocity agreement, what do we need to do to be registered as a qualified DBE.</p>	<p>Thank you for your interest in the Brent Spence Bridge Corridor Project. Please visit the "DBE/D&I resources" page on the project website for links to resources for firms that want to work on the project and for a list of upcoming networking events: https://brentspencebridgecorridor.com/work-with-us/</p>
294	Miscellaneous	<p>07/23/2023 - Web Comment - Thank you for your diligence on this project. It's good to adjust plans in response to concerns that are reasonable. However, the vast majority of negative concerns I see are based on anecdotal, myopic observations. Selfish, short-term perspectives never recognize the fact this project is long overdue, and that has had long term impact on health, safety, and regional, if not national, economic concerns.</p> <p>Covington Mayor Meyer focused on "devastating" impact of two events but neglected to note that dire predictions about changing access to Covington were proven baseless, during work to build the Texas Turnaround. Destination traffic concern is more of a marketing matter than an engineering concern. Also, keeping (inevitable) traffic moving safely is better for the environment than jammed up traffic.</p> <p>Covington needs to address its own existing traffic woes as we indulge fear of impact from the bridge project - poor parking management is a major culprit here. Lack of strategic loading zones promotes double parking and lane hazards from delivery trucks, for starters.</p> <p>I appreciate the broad, studied planning provided by our state road management professionals. Concessions allowed for good reason can benefit us all, but please don't allow political pressure to undermine the long-term goals.</p>	<p>Thank you for submitting a comment on the Brent Spence Bridge Corridor Project (BSBCP) website. Your concerns are noted. The project team will continue to develop concepts that meet the goals and objectives of the project and that benefit the entire region.</p>
295	General Support	<p>07/23/2023 - Web Comment - Let's get this thing going!</p>	<p>Comment noted.</p>

Comment Number	Nature of Comment	Comment	Response
296	Design-Build Process	07/24/2023 - Web Comment - I am inquiring about any 3rd party QA/QC inspection work that Bureau Veritas can provide a proposal. We have extensive experience providing 3rd party QA services to fabrication shops for infrastructure projects, working with 39 State DOT's. We also have extensive experience in field inspection on design-build projects similar to the Brent Spence project. One such current project is on the Gordy Howe Bridge across the Detroit River into Canada. We would appreciate any opportunities to provide a proposal. Thank you.	Thank you for your interest in the Brent Spence Bridge Corridor Project. The Bi-State Management Team recently selected a team through ODOT's consultant services for construction, engineering, and inspection services.
297	Information Request	07/25/2023 - Web Comment - I'm from a community (over the Rhine) where neighbors are constantly asking me how can they get prepared to join a team for employment with the bridge, and I don't really have an answer for them. Can you give me some suggestions that I can pass on to the people who live in the community on what they can do to start getting prepared for employment on the brent spence bridge project? Thank you	Thank you for your interest in the Brent Spence Bridge Corridor Project. Please visit the "Work With Us" page on the project website for links to resources for firms that want to work on the project: https://brentspencebridgecorridor.com/work-with-us/
298	Construction	07/27/2023 - Web Comment - Why will it take SIX YEARS for construction?	Thank you for your interest in the Brent Spence Bridge Corridor Project. A \$3.6 billion dollar project that covers 8 miles of interstate highway is an extensive undertaking that requires balancing production and impacts. The current estimated construction time frame is based on historical production rates for similar projects.
299	Information Request	07/28/2023 - Web Comment - Where may I find the PDF for the design request for proposal?	The Request for Proposal can be found at the following link: https://ftp.dot.state.oh.us/pub/Districts/D08/116649/RFP/
300	Construction	07/28/2023 - Web Comment - Would Love to chat with someone about the needs of Deep Foundation support on this project. Look forward t talking with someone soon.	Thank you for your interest in the Brent Spence Bridge Corridor Project. Please visit the "Work With Us" page on the project website for links to resources for firms that want to work on the project: https://brentspencebridgecorridor.com/work-with-us/ A member of the project team will reach out to the commenter.
301	Construction	07/28/2023 - Web Comment - I am interested in working on the Brent Spence bridge. We have a dump truck company. Please contact us.	Thank you for your interest in the Brent Spence Bridge Corridor Project. Please visit the "Work With Us" page on the project website for links to resources for firms that want to work on the project: https://brentspencebridgecorridor.com/work-with-us/

Comment Number	Nature of Comment	Comment	Response
302	Aesthetics	<p>07/28/2023 - Web Comment - I was horrified to find out the one design that EVERYONE HATES, the ugly monstrosity arch bridge (we already hate the MCdonalds bridge)</p> <p>Please tell me there will be public input on the choice. I have been following this closely for years and it seems the decision as to the design wasn't really published. Who makes this decision? Can we have a public debate? I would be heartbroken if our beautiful our city is made ugly by this horrible clashing architecture that is completely WRONG for the city. The other bridge was ugly and we all had hop that it's replacement wouldn't be this .. I'm horrified that people would be this tasteless. I guess the designer doesn't live here and just doesn't care that it's just WRONG for the city.</p>	<p>Thank you for your interest in the Brent Spence Bridge Corridor Project. An extensive bridge type selection process was undertaken by the project team that included public involvement and input from the project Aesthetics Committee. See the Executive Summary of the process here.</p> <p>The design of the new companion bridge has not yet been determined. The new bridge will be either an arch or a cable-stayed design. KYTC and ODOT are committed to building an iconic, aesthetically pleasing bridge. KYTC and ODOT will determine the final bridge type for the new companion bridge based on a technical evaluation performed by the design-build team. Once the bridge type is determined that information will be made available to the public, and the project Aesthetics Committee will be engaged to provide feedback on the aesthetic elements of the new companion bridge and the existing BSB. You can view renderings of what the different bridge designs might look like on the project website.</p>
303	Alternatives	<p>07/29/2023 - Web Comment - Can a streamlined toll system like Chicago loop be developed to add a minor tax for members and a swift pay station for nonmembers. Benefits to be split to both Ohio and KY. Specifically in Cincinnati to help pay for new covered stadium for football and future venues. This could be a boon to local economy as well as invest in other light rail extensions from Cincinnati to CVG. Helps to reduce traffic flow issues on Cincinnati to local KY as well as the/from suburbs.</p>	<p>Thank you for your interest in the Brent Spence Bridge Corridor Project. Tolling the I-71/I-75 corridor is not permitted in the State of Kentucky.</p>
304	Alternatives	<p>07/29/2023 - Web Comment - Since I-471 already crosses the Ohio River, why not convert I-471 to I-71 then route I-71 along I-275 to I-75. Then the current freeway between I-71 and I-75 can be removed and replaced with a boulevard that reconnects the Riverfront area to the downtown. That eliminates the current I-71/I-75 interchange downtown that makes the new Brent Spence Bridges much simpler without the wide footprint.</p>	<p>Thank you for your interest in the Brent Spence Bridge Corridor Project. Reducing congestion and maintaining connections to key regional and national corridors are primary goals of the project. The corridor forms a critical freight route connecting Canada to Florida, carrying more than \$1 billion of freight every day and more than \$400 billion of freight every year. I-71/I-75 also provide regional and local access to the Cincinnati and Northern Kentucky regions. Diverting traffic away from I-71/I-75 would place additional vehicles on other routes that are also experiencing congestion and would not meet the established purpose of the project.</p>

Comment Number	Nature of Comment	Comment	Response
305	Construction	07/30/2023 - Web Comment - We are a small business and DBE certified with Ohio State DOT. We bid on this project, but we did not see Walsh. Do you have a contact person from Walsh Group to send them our cost estimate? They might still need our vibration monitoring services.	<p>Thank you for your interest in the Brent Spence Bridge Corridor Project. Please visit the "Work With Us" page on the project website for links to resources for firms that want to work on the project: https://brentspencebridgecorridor.com/work-with-us/</p> <p>In addition, please visit the "DBE/D&I Resources" page on the project website for a list of upcoming networking events: https://brentspencebridgecorridor.com/work-with-us/dbe-di-resources/</p>
306	Construction	07/31/2023 - Web Comment - Hopefully during construction they will not detour. Any traffic on Dixie highway/Pike Street.	<p>Thank you for your interest in the Brent Spence Bridge Corridor Project and potential traffic impacts to Covington. The Kentucky Transportation Cabinet (KYTC) is working with the City of Covington to mitigate impacts during construction.</p> <p>During construction, KYTC will develop an overall Traffic Management Plan and a detailed Maintenance of Traffic (MOT) plan to maintain traffic operations through the corridor and minimize disruption to the surrounding communities. When preparing the MOT plan, KYTC will work to minimize impacts to local businesses; evaluate impacts to public transportation and develop measures to maintain existing services; evaluate temporary detours to limit impacts created by redirecting traffic through community sensitive areas; establish an Incident Management Plan to minimize diversion resulting from incidents that occur during construction in the corridor; include provisions for communicating with trucking companies and mapping services to provide information about re-routing and delays; and provide for adequate signing during construction. The City of Covington, and all relevant agencies within the city, will have an opportunity to review and provide input into all aspects of MOT planning, plan development, and construction operations affecting the city. MOT and Incident Management Plans will also be coordinated with first responders, transit agencies, and the Regional Incident Management Task Force.</p>
307	Construction	07/31/2023 - Web Comment - I represent two companies both with MBE/DBE STATE CERTIFIED MBE COMPANY NAME BSI TRUCKING AS WELL AS A WBE TRUCKING COMPANY D AND H TRUCKING COMPANY BOTH ARE LOCALLY OWNED AND OPERATED I HAVE 45 TRUCKS AND CAN HELP THEM FILL THE TRUCKING ORDERS THANK YOU PLEASE FEEL FREE TO CONTACT ME [REDACTED]	<p>Thank you for your interest in the Brent Spence Bridge Corridor Project. Please visit the "Work With Us" page on the project website for links to resources for firms that want to work on the project.</p> <p>In addition, please visit the "DBE/D&I Resources" page on the project website for a list of upcoming networking events.</p>

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308	Construction	07/31/2023 - Web Comment - PLEASE FEEL FREE TO CONTACT [REDACTED] WITH ASHCRAFT SAN AND GRAVEL FOR TRUCKING PLEASE I ALSO HAVE MBE/DBE/WBE PETICIPATION THRU SOME OWNER OPERATORS THAT WORK WITH OUR COMPANY PLEASE FEEL FREE TO REACH OUT TO ME IF I CAN BE OF ANY HELP THANK YOU	<p>Thank you for your interest in the Brent Spence Bridge Corridor Project. Please visit the "Work With Us" page on the project website for links to resources for firms that want to work on the project: https://brentspencebridgecorridor.com/work-with-us/</p> <p>In addition, please visit the "DBE/D&I Resources" page on the project website for a list of upcoming networking events: https://brentspencebridgecorridor.com/work-with-us/dbe-di-resources/</p>
309	Right-of-Way Noise	07/31/2023 - Web Comment - What houses on Ashton Rd in Ft Mitchell will have to go? And will there be a sound wall?	<p>Thank you for your interest in the Brent Spence Bridge Corridor Project and your inquiry regarding property along Ashton Road. In accordance with State and Federal laws, KYTC will only acquire properties that are directly impacted by the project. The Brent Spence Bridge Corridor Project does not impact any homes along Ashton Road; therefore, they will not be acquired. In addition, KYTC is proposing a noise wall that will help to reduce noise levels along Ashton Road.</p> <p>Maps showing proposed right-of-way limits (where properties will be impacted) for the project can be accessed here: https://brentspencebridgecorridor.com/wp-content/uploads/2022/12/2022-12-06_Corridor-Exhibit.pdf. Impacted structures are shown with red hatching on the exhibit. This exhibit also shows the locations of proposed noise walls.</p>
310	Project Mailing List	08/01/2023 - Web Comment - Just want updates	This individual was added to the project mailing list.
311	Alternatives	08/01/2023 - Web Comment - Which lane has the train from the airport to downtown?	Thank you for your interest in the Brent Spence Bridge Corridor Project. The project addresses the highway needs along the corridor. Transit improvements must be developed and championed regionally, and ODOT and KYTC are ready to support these improvements when they are advanced at a regional level.

Comment Number	Nature of Comment	Comment	Response
312	Construction	08/01/2023 - Web Comment - Hello, I own a property on [REDACTED] Hermes in Covington and am wondering how this project will affect my home.	<p>Thank you for your interest in the Brent Spence Bridge Corridor Project and your inquiry regarding property along Hermes Avenue. In accordance with State and Federal laws, KYTC will only acquire properties that are directly impacted by the project. The Brent Spence Bridge Corridor Project does not impact any homes along Hermes Avenue; therefore, they will not be acquired. In addition, KYTC is proposing noise walls that will help to reduce noise levels along Hermes Avenue.</p> <p>Maps showing proposed right-of-way limits (where properties will be impacted) for the project can be accessed here: https://brentspencebridgecorridor.com/wp-content/uploads/2022/12/2022-12-06_Corridor-Exhibit.pdf. Impacted structures are shown with red hatching on the exhibit. This exhibit also shows the locations of proposed noise walls.</p>
313	Construction	08/03/2023 - Web Comment - We have office space available directly near the project site on both the Ohio and Kentucky side of the river. Would welcome the opportunity to connect and understand if I can be of assistance.	<p>Thank you for your interest in the Brent Spence Bridge Corridor Project. KYTC and ODOT announced that the Kokosing-Walsh team was awarded the progressive design-build contract on July 27, 2023. You may reach out to the selected design-build team regarding office space needs.</p>
314	Construction	08/03/2023 - Web Comment - We are interested in providing pricing for the project. We are a minority, woman-owned disadvantaged business enterprise heavy-highway contractor.	<p>Thank you for your interest in the Brent Spence Bridge Corridor Project. Please visit the "Work With Us" page on the project website for links to resources for firms that want to work on the project: https://brentspencebridgecorridor.com/work-with-us/</p> <p>In addition, please visit the "DBE/D&I Resources" page on the project website for a list of upcoming networking events: https://brentspencebridgecorridor.com/work-with-us/dbe-di-resources/</p>

Comment Number	Nature of Comment	Comment	Response
315	Construction	<p>08/04/2023 - Web Comment - I just want to get in touch with you about the job vacancies for steel deck fabrication/erection, PT systems, the cable structures.</p> <p>Frankly, I am a Civil Engineer and Welding Engineer with 12 years of experience in Civil / Bridge Engineering across the UK, Europe (EU), Turkey, and North Africa. Such as: Braila Bridge/ Romania, 1915 canakkale Bridge / Turkey Osmangazi bridge/ Turkey, Currently working at the HS2 Project in Birmingham which is one of the most famous projects in the UK.</p> <p>If you will share an email address, I can share my cv for evaluation.</p>	<p>Thank you for your interest in the Brent Spence Bridge Corridor Project. Please visit the "Work With Us" page on the project website for links to resources for firms that want to work on the project: https://brentspencebridgecorridor.com/work-with-us/</p>
316	Construction	<p>08/08/2023 - Web Comment - We have a fully fenced lot off of Evans st about a mile from the bridge. Would make a great job trailer or staging lot. Please contact if interested. Thank you</p>	<p>Thank you for your interest in the Brent Spence Bridge Corridor Project. You may reach out directly to the design-build team regarding opportunities to work on the project using the following email address: WalshKokosingBrentSpence@walshgroup.com. You can also visit the Walsh Kokosing Design-Build Team website at https://walshkokosing.com/.</p> <p>Additional links to resources for firms that want to work on the project are available on the "Work With Us" page on the project website: https://brentspencebridgecorridor.com/work-with-us/</p>
317	BSB Companion Bridge	<p>08/08/2023 - Web Comment - Approximately how long will it be before the bridge type (arch versus cable-stayed) for the new BSB? (I'm hoping for an arch.) Thanks!</p>	<p>Thank you for your interest in the Brent Spence Bridge Corridor Project. KYTC and ODOT will determine the final bridge type for the new companion bridge based on a technical evaluation performed by the design-build team. Once the bridge type is determined that information will be made available to the public, and the project Aesthetics Committee will be engaged to provide feedback on the aesthetic elements of the new companion bridge and the existing BSB.</p> <p>The design-build team has been selected and is in the process of developing their design and construction schedule. Specific dates and activities are still being developed and have not been finalized. This schedule is expected to be finalized in the next month and will be shared on the project website at that time.</p>

Comment Number	Nature of Comment	Comment	Response
318	Alternatives	<p>08/09/2023 - Email - I knew that the concept of rerouting I-71 and removing the section along the Riverfront was not an option ODOT was even considering, but I wanted an "official" response to verify this as a lot of people believe removing freeways is the answer to a better life that will magically end crime in the big city.</p> <p>Since I-75, I-71, I-74, I-471, and I-275 are heavily congested (as you stated in your response), and traffic study after traffic study across the nation shows that widening highways eventually increases congestion, the I-75 transcontinental corridor will not be capable of handling the increasing freight and general congestion within the next 10 to 20 years through widening alone. Therefore, NOW is the time to start planning an alternative future interstate corridor that completely bypasses Cincinnati. The Ohio Valley Regional Planning Commission is still planning a feasibility of the I-73/I-74 corridor and the Ohio Legislature passed a resolution for ODOT to begin this feasibility study even though both the I-74 extension and I-73 were cancelled in 2000. The people of Cincinnati opposed extending I-74 east of the city and the people of Columbus opposed building I-73 to and around the city. Also, the current routing of I-73 south of Portsmouth through southern West Virginia along U.S. 52 will never be funded by the State of West Virginia. The Virginia DOT also cancelled further study in the state. North Carolina is the only place that build both I-74 and I-73, and South Carolina is not able to fund I-73 to Myrtle Beach. None of this routing of I-73 offers any traffic benefit to I-75. Extending I-74 east would only add to Cincinnati's congestion. Even proposed concepts like the Eastern Cincy Bypass have been studied and rejected at unfeasible. Further widening of I-75 will only raise more and more opposition with little traffic benefit after the next 10-20 yrs.</p> <p>That leave only one option not explored, building a north-south future interstate corridor as an alternative to I-75, a Future I-73 (or Future I-67). This new route for I-73 begins at I-75 in Findlay, then run along the U.S. 68 corridor to the U.S. 23 corridor either following OH 73 to Portsmouth or U.S. 35 to U.S. 23 in Chillicothe. From Portsmouth, the new Future I-73 concept would follow U.S. 23 through Kentucky and Virginia to connect to I-26 in Kingsport Tennessee to form a major freight corridor from Findlay Ohio to the port at Charleston South Carolina, relieving a lot of traffic congestion from I-75 from Cincinnati to Knoxville. This should be the focus of the I-73 feasibility study that the Ohio State Legislature directed ODOT to conduct.</p>	<p>Thank you for your continued interest in the project. The project is designed to accommodate traffic projected to travel through the corridor by the year 2050, which is the standard design horizon for large infrastructure projects. The traffic projections are based on a regional travel-demand model which assigns routes used by travelers based on available capacity, projected trips, and calculated travel times. Traffic projections show that adding lanes will increase traffic volumes in the Brent Spence Bridge Corridor. Some of that increase is due to travelers shifting trips they were already making from other congested routes. In addition, some travelers will make new trips they would not have made without the highway improvements (induced trips). The Brent Spence Bridge Corridor Project is designed to accommodate all trips (including induced trips) projected by the Ohio-Kentucky-Indiana (OKI) regional travel demand model through the year 2050.</p> <p>Your comments about other potential projects are noted. Other regional and statewide improvements will be independently studied and evaluated as appropriate as ODOT continues to manage the existing and future transportation needs in the state.</p> <p>More information on ODOT's Statewide Planning can be found here: https://www.transportation.ohio.gov/programs/statewide-planning-research/welcome</p>

Comment Number	Nature of Comment	Comment	Response
319	Construction	08/11/2023 - Web Comment - I am interested in having a virtual or in-person discussion to share details with you on our access solutions. Could someone from the team please email me back to coordinate a time? Feel free to call my cell.	<p>Thank you for your interest in the Brent Spence Bridge Corridor Project. You may reach out directly to the design-build team regarding opportunities to work on the project using the following email address: WalshKokosingBrentSpence@walshgroup.com. You can also visit the Walsh Kokosing Design-Build Team website at https://walshkokosing.com/.</p> <p>Additional links to resources for firms that want to work on the project are available on the "Work With Us" page on the project website: https://brentspencebridgecorridor.com/work-with-us/</p>
<p>Responses to all comments received between 08/11/2023 and 09/08/2023 were addressed separately as part of the comment period for the open-house project update meetings held on August 23 and August 24, 2023 and can be accessed HERE.</p>			
320	Construction	09/12/2023 - Web Comment - Rivera & Associates LLC is an award winning public relations and marketing firm in Milwaukee Wisconsin. We are DBE, MBE, SBE, SWMBE, NCMSDC, and SBA HUBZONE Certified. We would like to work with you. Please visit our website www.riveraprfirm.com .	<p>Thank you for your interest in the Brent Spence Bridge Corridor Project. KYTC and ODOT already have vendors under contract to support the services described.</p>
321	Construction	09/21/2023 - Web Comment - I'm a journeyman carpenter with an extensive resume. I live in Cincinnati and I would like to be a part of this project.	<p>Thank you for your interest in the Brent Spence Bridge Corridor Project. You may reach out directly to the design-build team regarding opportunities to work on the project using the following email address: WalshKokosingBrentSpence@walshgroup.com. You can also visit the Walsh Kokosing Design-Build Team website at https://walshkokosing.com/.</p> <p>Additional links to resources for firms that want to work on the project are available on the "Explore Construction Careers" page on the project website: https://brentspencebridgecorridor.com/work-with-us/</p>

Comment Number	Nature of Comment	Comment	Response
322	Historic Resources	09/21/2023 - Web Comment - How and when will the project effect Longworth Hall and it's parking area?	<p>Thank you for your interest in the Brent Spence Bridge Corridor Project.</p> <p>The project will remove 204 feet of the B&O Freight Terminal/Longworth Hall building located along Pete Rose Way in Cincinnati. This structure, constructed as a warehouse for the Baltimore & Ohio Railroad, currently operates as a mixed-use office, retail, and event space. ODOT has committed to mitigating the impacts to historic Longworth Hall. The mitigation measures include installing exterior storm windows on the exterior of the building, restoring the east wall to an approximation of its original appearance using materials salvaged from the impacted portion of the structure, restoring windows that are removed and using them in the east wall reconstruction or returning them to the building owner, adding a cornerstone commemorating the date of construction on one side and the date of the renovation on the other side, repairing masonry, refurbishing the original lettering across the top of the building, and constructing a plaque/interpretive signing describing changes to the property that have occurred over time.</p> <p>While the timeline has not yet been established, it is anticipated that the work described above is anticipated to begin later in 2024 at the earliest. The remaining portions of Longworth Hall will continue to be utilized as mixed-use office, retail, and event space during the reconstruction and mitigation efforts.</p>
324	Construction	09/24/2023 - Web Comment - I have 10 or 12 years left to work. I'm strong and healthy and have an abundance of skills. I'd love my last experience to be helping build something I can be proud of and tell my grandchildren.	<p>Thank you for your interest in the Brent Spence Bridge Corridor Project. You may reach out directly to the design-build team regarding opportunities to work on the project using the following email address: WalshKokosingBrentSpence@walshgroup.com. You can also visit the Walsh Kokosing Design-Build Team website at https://walshkokosing.com/.</p> <p>Additional links to resources for individuals who want to work on the project are available on the "Explore Construction Careers" page on the project website: https://brentspencebridgecorridor.com/work-with-us/</p>

Comment Number	Nature of Comment	Comment	Response
325	Multimodal	<p>09/25/2023 - Web Comment - Has there been consideration to include pedestrian and rolling (wheelchair, scooter, bike) accommodations on the bridge of the Brent Spence or in the companion bridge to connect people who don't drive to Cov and Downtown Cincy?</p> <p>I am a homeowner in the neighborhood of West Covington. I do not own a car and choose to take transit, walk and ride bikes instead.</p> <p>I was recently traveling in Pittsburgh and got an opportunity to walk alongside a pedestrian bridge built along the Fort Duquesne bridge which carries I-279. https://www.flickr.com/photos/radunzel/8435926717 It's a 4 lane double decker built in late 1950s very similar to the Brent Spence.</p> <p>I know of the bike connections considered for under the bridge, but given the poor infrastructure on the Clay Wade Bailey for pedestrians and people rolling (wheelchair or by bike or scooter) has there been consideration to include a pedestrian and cycling accommodations in the expansion of the Brent Spence or in the companion bridge?</p>	<p>Thank you for your interest in the Brent Spence Bridge Corridor Project. Pedestrian and bicycle accommodations are not planned on the new companion bridge or the existing Brent Spence Bridge because of the proximity of a reasonable crossing at the Clay Wade Bailey Bridge. Based on public feedback, during the progressive design-build process, KYTC and ODOT will evaluate reconfiguring the lanes on the Clay Wade Bailey Bridge to add bicycle lanes.</p>
326	Construction	<p>09/26/2023 - Web Comment - When is it estimated that phase III would begin? Would Longworth Hall be affected during that phase or earlier?</p>	<p>Thank you for your interest in the Brent Spence Bridge Corridor Project. The construction Phase III is scheduled to begin in 2025, although some limited construction activities may begin in 2024.</p> <p>While the timeline has not yet been established, it is anticipated that the work affecting Longworth Hall will begin in late 2024 at the earliest.</p>
327	Construction	<p>10/01/2023 - Web Comment - Hello! My name is [REDACTED]. I am 56 years of age and still healthy and strong. I have 20 plus years of industrial maintenance experience as well as construction and labor experience. Also have an OSHA ten hour certification. I have 12 years or so left to work and would love the opportunity to be a part of this historic achievement. As well as the steady good paycheck to end my working career. I would be an experienced asset to any workforce and would appreciate the chance more than you could know. Thank you so much and hope to hear from you soon.</p>	<p>Thank you for your interest in the Brent Spence Bridge Corridor Project. You may reach out directly to the design-build team regarding opportunities to work on the project using the following email address: WalshKokosingBrentSpence@walshgroup.com. You can also visit the Walsh Kokosing Design-Build Team website at https://walshkokosing.com/.</p> <p>Additional links to resources for individuals who want to work on the project are available on the "Explore Construction Careers" page on the project website: https://brentspencebridgecorridor.com/work-with-us/</p>

Comment Number	Nature of Comment	Comment	Response
328	Construction	10/03/2023 - Web Comment - Asphalt milling, chip seal, paving, VRAM installer from Cleveland area. Willing to travel.	<p>Thank you for your interest in the Brent Spence Bridge Corridor Project. You may reach out directly to the design-build team regarding opportunities to work on the project using the following email address: WalshKokosingBrentSpence@walshgroup.com. You can also visit the Walsh Kokosing Design-Build Team website at https://walshkokosing.com/.</p> <p>Additional links to resources for firms that want to work on the project are available on the "Explore Construction Careers" page on the project website: https://brentspencebridgecorridor.com/work-with-us/</p>
329	Multimodal	10/03/2023 - Web Comment - What are 'new shared-use paths'?	<p>Thank you for your interest in the Brent Spence Bridge Corridor Project. A shared-use path is a 10-foot paved path that can be shared by pedestrians, cyclists, and other non-motorized users. Renderings of what the shared-use paths might look like on 6th Street and Ezzard Charles Drive in Ohio are shown in Slides 23-28 of this link: https://brentspencebridgecorridor.com/wp-content/uploads/2023/07/Renderings-for-Boards-Open-House.pdf. The shared-use paths are shown on the north (right) side in these renderings.</p>
330	Miscellaneous	10/03/2023 - Web Comment - What time does the tunnel at the East End Bridge Close. I go that way every day and do not want to be stuck on the IN side. Please change the signs that state there will be nightly closures and in stead tell us when they start I cannot find this information on your website thanks	<p>Thank you for your comment. The website you used to submit your comment is dedicated to the Brent Spence Bridge Corridor Project, which is in the Kentucky Transportation Cabinet's (KYTC's) District 6 area. The East End Tunnel is located in KYTC District 5. The East End Tunnel was recently scheduled for nightly closures from October 1 through October 3. You may read more about those closures here: https://transportation.ky.gov/DistrictFive/Pages/News-Archive.aspx</p> <p>In addition, you may sign up to receive press releases about future traffic closures in KYTC District 5 here: https://public.govdelivery.com/accounts/KYTC/signup/13673</p>

Comment Number	Nature of Comment	Comment	Response
331	Construction	<p>10/03/2023 - Web Comment - I would love to join your team</p> <p>I'm a very hard and dedicated worker</p> <p>I'm 21 and willing to learn</p> <p>LET ME WORK</p>	<p>Thank you for your interest in the Brent Spence Bridge Corridor Project. You may reach out directly to the design-build team regarding opportunities to work on the project using the following email address: WalshKokosingBrentSpence@walshgroup.com. You can also visit the Walsh Kokosing Design-Build Team website at https://walshkokosing.com/.</p> <p>Additional links to resources for individuals who want to work on the project are available on the "Explore Construction Careers" page on the project website: https://brentspencebridgecorridor.com/work-with-us/</p>
332	Construction	<p>10/05/2023 - Web Comment - Are you planning to Re-design or build a new website completely? We propose to create a new website design for your business that will improve the user experience, increase engagement, and boost conversions. We are committed to delivering a website that meets your business needs and goals.</p> <p>Please let us know if you would like to discuss other options or have any questions.</p>	<p>Thank you for your interest in the Brent Spence Bridge Corridor Project. KYTC and ODOT have already contracted for website support services.</p>
333	Construction	<p>10/07/2023 - Web Comment - I am a service-connected disabled veteran who would like to be a part of this project. I bid on several government contracts shortly after returning from Iraq tour. However, I was not successful at that time. Are any opportunities available for veterans in my situation.</p>	<p>Thank you for your interest in the Brent Spence Bridge Corridor Project. You may reach out directly to the design-build team regarding opportunities to work on the project using the following email address: WalshKokosingBrentSpence@walshgroup.com. You can also visit the Walsh Kokosing Design-Build Team website at https://walshkokosing.com/.</p> <p>Additional links to resources for individuals who want to work on the project are available on the "Explore Construction Careers" page on the project website: https://brentspencebridgecorridor.com/work-with-us/.</p>
334	Construction	<p>10/09/2023 - Web Comment - This is [REDACTED] with the Holiday Inn Cincinnati Riverfront. We would like to know how we can get in touch with the Contractors, Subcontractors to offer special lodging discounts for their out of town crew members when they start working at the Brent Spence Bridge.</p>	<p>Thank you for your interest in the Brent Spence Bridge Corridor Project. You may reach out directly to the design-build team using the following email address: WalshKokosingBrentSpence@walshgroup.com. You can also visit the Walsh Kokosing Design-Build Team website at https://walshkokosing.com/.</p>

Comment Number	Nature of Comment	Comment	Response
335	Construction/ Design	10/17/2023 - Web Comment - I am the Director of the Seamen's Church Institute's Center for Maritime Education. I am happy to provide an overview of our feasibility and navigation study capabilities. Our facility in Paducah, KY has previously assisted in over 25 major port infrastructure, and highway bridge projects, and has assisted project planners to build consensus among stakeholders on the best layout of proposed bridge crossings, pier placements, traffic analysis, and dredge profile analysis, for lower cost than traditional methods. By leveraging a multi-bridge maritime simulator, we can quickly get feedback from Captains who can navigate different types of tows, for a variety of environmental factors. We have the Cincinatti database and updated tugs, tug and barge combinations, and vessels in our training library and hope you will keep us in mind for testing and providing feedback for this project.	Thank you for your interest in the Brent Spence Bridge Corridor Project. You may reach out directly to the design-build team using the following email address: WalshKokosingBrentSpence@walshgroup.com . You can also visit the Walsh Kokosing Design-Build Team website at https://walshkokosing.com/ .
336	Alternatives	10/20/2023 - Web Comment - Why not reroute the corridor to the west of its current location do a much better job for the next 15-20 years	Thank you for your interest in the Brent Spence Bridge Corridor Project. Rerouting I-71/I-75 west of its current location was evaluated as a conceptual alternative during early project development activities. This conceptual alternative resulted in adverse impacts to communities, residences, businesses, regulated materials sites, and utilities, which were substantially higher than other alternatives under consideration. In addition, the alternative that moved I-71/I-75 to the west had substantially greater overall complexity, constructability risk, and cost when compared to other alternatives. Finally, the concept was strongly opposed by both the City of Cincinnati (Ohio) and the City of Covington (Kentucky). Based on these factors, it was dismissed from further study.
337	BSB Companion Bridge	10/23/2023 - Web Comment - I think that it should be called the Gateway Bridge as it is the gateway to the North and to the South.	Thank you for your interest in the Brent Spence Bridge Corridor Project and for your ideas about a name for the new companion bridge. While the new companion bridge may be formally named, the process for naming the new bridge has not yet been established.
338	Construction	10/23/2023 - Web Comment - The Brent Spence bridge project looks exciting for all involved. I noticed that renderings include the potential for transparent noise walls. The company I work for provides transparent plastic materials used for noise wall applications. We are headquartered in the U.S. with a location in Cincinnati as well as Lexington. I would like to know if you can point me in the right direction to participate in the bidding or quoting process.	Thank you for your interest in the Brent Spence Bridge Corridor Project. You may reach out directly to the design-build team using the following email address: WalshKokosingBrentSpence@walshgroup.com . You can also visit the Walsh Kokosing Design-Build Team website at https://walshkokosing.com/ .

Comment Number	Nature of Comment	Comment	Response
339	Construction	<p>10/30/2023 - Email Comment - Just wanted to circle back after seeing the latest Cincinnati.com article on the construction project. In the article it states the project is "expected to break ground early in 2024". Is it possible you can confirm?</p> <p>Girls on the Run Greater Cincinnati has an agreement with Longworth Hall to host our 30th anniversary 5K event on the East end of the parking lot with our 5K course running out and back from the Pete Rose Way entrance (under the bridge). Therefore, construction would directly impact our event and we would need to secure another venue A.S.A.P.. Appreciate any assistance you may provide!</p> <p>[Note: Comment provided as follow up to Comment 322.]</p>	<p>Thank you for providing this additional information. With a date of 5/11/2024, there is no conflict with any work planned for the Brent Spence Bridge Corridor project. We are working on the purchase of the building, aiming to complete that transaction by the end of the calendar year. Once complete, the State of Ohio (Ohio Department of Transportation) will be the owner. We are also working on hiring a property management firm to manage the property when we take over ownership. I checked in with our real estate team working on the project, and we believe that the property management firm will be able to coordinate with you on the event. So, long story short, we believe that you can proceed with using Longworth Hall. If you have any questions or wish to discuss, please let us know. In the meantime, let's plan to touch base in January to check-in on preparations.</p>
340	Bridge Forward Concepts	<p>10/30/2023 - Email Comment - Please see attached for a binder of letters submitted to date supporting the Bridge Forward street grid concept (local urban roads added on both sides of the interstate; east-west connection at 5th Street; etc.). This binder of letters is separate and apart from the "Reconnecting Westway" letter binder that I previously sent to this list of recipients on 6/23/23. Please ensure this is included in the project record.</p> <p>[Note: Provided materials included three versions of form letters advocating for concepts developed by Bridge Forward sent to the City of Cincinnati Mayor and/or City Council members between 12/22/2022 and 10/23/2023.]</p>	<p>Thank you for your interest in the Brent Spence Bridge Corridor Project. The provided information has been included in the project record.</p>

Comment Number	Nature of Comment	Comment	Response
341	Innovation Process	<p>10/31/2023 – Email Comment – The Greg Fischer Management Professional Support Team, comprised of Kent Ahrenholtz with Kaskaskia Engineering Group, Diana Martin of RL RECORD LLC, Fred Wagner of Venable LLP, and Annie White of Karp Strategies, has provided significant well researched and documented support for its position that there is a better transportation, social, economic and environmental solution. It is extremely unusual for private resources to be committed to technical analysis to the extent seen here. This local investment should be respected and given significant attention and analysis by the progressive design build team. During our due diligence, we evaluated a number of options that we dismissed as not meeting ODOT’s publicly stated goals (for example separation of local and through traffic provided by a collector-distributor system). Please note that these possibilities are not excluded by the 2012 Purpose and Need Statement. Significant among these options is one designated east-east/west-west, which more closely resembles ODOT Alternative I, while falling squarely within the NEPA envelope currently under evaluation. Separately furnished to HNTB are Open Roads dgn. files in accordance with our practice of providing all of our data transparently. These were developed to a horizontal, but not vertical level of design. We are also forwarding a modification of the I-75/US 50 interchange at Freeman Ave. This was developed at the concept level only. Included below are descriptions of these concepts. We are not advocating for these solutions, but think they deserve evaluation by your progressive design-build team of creative engineers. Thus, we provide them as a starting point to stimulate thought. The following provides a brief description of the two additional concepts for consideration by the Bi-State Management Team and their Progressive Design/Build Team.</p> <p><u>Modification of I-75/US 50/Freeman Interchange</u> This concept would effectively remove the US 50 ramp movements from the vicinity of the I-75 Downtown Cincinnati interchange ramps, between 5th and 7th, and provide all of the ramps at the existing I-75/US 50/Freeman Road Interchange. The existing ramps provide a direct connection to and from US 50 to I-75 to the north. New ramps to and from I-75 to the south would be added to those existing ramps. See attached exhibit.</p>	<p>Following up on my previous email, thank you for your interest in the Brent Spence Bridge Corridor Project. ODOT has received the concepts provided by the Greg Fischer Management Professional Support Team and has forwarded them to the design-build team for further evaluation as part of the innovation process. ODOT will continue to work with the City of Cincinnati and Hamilton County during the innovation process and provide them the opportunity for review and comment on any recommendations within their jurisdictions. In addition, the City of Cincinnati has established an advisory group to provide feedback on innovations affecting the City.</p>

Comment Number	Nature of Comment	Comment	Response
341 (cont.)	Innovation Process (cont.)	<p>One main advantage of this concept is removal of the US 50 ramp movements from the highly congested I-71/I-75 Interchange. A disadvantage is the additional "out of direction" travel for US 50 traffic that is destined for both I-71 to the east and I-71/I-75 to the south. This travel is less than one mile, but that traffic would exit existing US 50 and travel north to the Freeman Interchange.</p> <p><u>East-East/West-West Alternative Concept</u> This concept would eliminate the Collector-Distributor (C/D) system of roadways on the current preferred alternative I-W and place I-75 on the new Ohio River Bridge and I-71 on the rehabilitated existing structures. The ramps to and from Downtown Cincinnati would thus connect to I-71 and I-75 mainline, instead of the C/D roads. See attached exhibits.</p> <p>A potentially large advantage of this approach would be the reduction in cost through removal of several directional ramps on the Ohio side of the river that connected the CD roadways to the existing Ohio River bridge and consolidation of access on the Kentucky side. It also reduces the footprint of the project on the Kentucky side, similar to what has been done with the I-W Street Grid concepts on the Ohio side.</p> <p>One possible disadvantage is the safety of connecting the lower-speed local access ramps directly to the high-speed mainline lanes. It also has the potential of causing congestion on the mainline lanes from backup on the ramps, where this would occur on the C/D roadways, rather than the mainline, in the other concepts. There is also no direct ramp to I-75 northbound from 4th St in Kentucky, although that traffic can access I-75 north by crossing the Wade Bailey Bridge and the ramps on the Ohio side. Plus, more ramps are converging on 5th Street in Kentucky, so the signal or intersection spacing may become an issue. In addition, there may be a need to add a 4th lane to I-75 NB and SB through the ramps on the Ohio side, since this concept removes the C/D roadways entirely.</p> <p>[Note: PDFs and OpenRoads files of the concepts described above were provided under separate cover on 10/31/2023.]</p>	

Comment Number	Nature of Comment	Comment	Response
342	Innovation Process	<p>11/01/2023 – Email Comment – As a co-chair of the CRBC Regional Assets & Community Improvement Committee, I would like to thank you for your time last week discussing the bridge design with Keven, Jeff and Greg, including opportunities and challenges around the street grid concept. I apologize that I could not join the conversation, but wanted to let you know that we really value the partnership and engagement with ODOT. As was discussed on the call, we are extremely appreciative of the opportunity presented by the Brent Spence Bridge Corridor Project and are excited about the City of Cincinnati’s recent proposal. We plan to continue to advocate for an enhanced outcome, including the possibility of reducing the footprint and expanded street grid options, if not a full street grid, that connect both sides of the interstate during the innovation period. A reliable and flexible, human scale street grid strengthens opportunities for improving the value of reclaimed and adjacent lands, and most importantly connects Queensgate to the Central Business District for long-term community redevelopment. We are excited to hear that you are already looking at various options and look forward to more engagement during the innovation period. If necessary, the local community should have the option to fund any cost difference and optimize the return on investment. Importantly, we will continue to advocate that any adaptations do not cause delay or have negative impacts on traffic flow. Thanks again for your partnership. We look forward to continuing to work with you.</p>	<p>Thank you for writing to express your views regarding options to enhance local connectivity and to provide support for options that do not cause delay or have negative impacts on traffic flow. Ohio and Kentucky have worked closely with our local partners to strike that balance with our base design, and we are now working diligently with the Design Build Team, which consists of national expert contractors and designers, to seek further opportunities. Thank you for your partnership as well, and I look forward to continuing to work together to deliver this project.</p>
343	Right-of-Way	<p>10/30/2023 - Web Comment - I own property on Willow run and have yet to hear from anyone regarding what will be occurring there. My tenant requires constant access to the building and the space it leases must be licensed by the state of Kentucky and wants to know what is going on. Please advise.</p>	<p>Thank you for your recent inquiry regarding the Brent Spence Bridge Corridor Project and potential impact to property located on Willow Run Road.</p> <p>Without knowing the specific parcel you own, I will try to address your concern on a broader scale. In the area of Willow Run Road, the project team tried to limit any impacts outside of existing state-owned property. Based on current design, there is not an intent to purchase additional property along Willow Run Road in the block north of Fry Street. For proposed project impacts and lane configurations, I encourage you to visit the project website. Additionally, the project's construction documents will require the contractor to maintain access to all properties during construction activities.</p>

Comment Number	Nature of Comment	Comment	Response
344	BSB Companion Bridge	10/30/2023 - Web Comment - I would like to suggest a name for the new bridge - how about a woman for a change ??? What about The (Margaret)"Garner Freedom Bridge" in memory of the Garner Family who crossed the Ohio River at this point seeking Freedom from slavery.	Thank you for your interest in the Brent Spence Bridge Corridor Project and for your ideas about a name for the new companion bridge. While the new companion bridge may be formally named, the process for naming the new bridge has not yet been established.
345	Right-of-Way	10/30/2023 - Web Comment - What will happen to the properties around Carr?	Thank you for your interest in the Brent Spence Bridge Project. The proposed improvements will be centered on existing I-75 in Cincinnati and will not impact any properties on Carr Street.
346	Alternatives Aesthetics Quality of Life	10/30/2023 - Web Comment - Re.: Ezzard Charles Bridge. Having seen the impact of building exterior structures with landscaping, in Brazil can say that (1) very few people will use a noisy bridge to socialize or relax. (2) Planting trees and landscaping on a concrete bridge will increase the risk of water infiltration from roots seeking and widening cracks, (3) Landscaping increases wind stress on the structure. may I suggest a trellis on the exterior of the bridge with climbing plants and increasing the number of trees on adjacent streets leading to the bridge? This will shade pedestrians walking toward it and from it thus providing incentive to walk.	Thank you for your interest in the Brent Spence Bridge Corridor Project. The Ohio Department of Transportation (ODOT) has designed the Brent Spence Bridge Corridor Project to avoid the removal of mature trees along the Ezzard Charles Drive approaches to the bridge. ODOT has also committed to building a wider bridge on Ezzard Charles Drive over I-75. The widened bridge will provide an additional 50 feet of green space on each side that could support potential future civic space or retail development to be determined and implemented by the City of Cincinnati. We have shared your feedback with the City of Cincinnati Department of Transportation and Engineering for their consideration in future planning for the widened bridge area.
347	Design Details	11/02/2023 - Web Comment - I'm wondering if road lighting can be solar powered? And also request lighting to be directed downward only to the roadway to preserve or restore a dark night sky.	Thank you for your interest in the Brent Spence Bridge Corridor Project. The Kentucky Transportation Cabinet (KYTC) and the Ohio Department of Transportation (ODOT) do not have the infrastructure to support solar power for interstate lighting in the project area. However, the project will install energy efficient LED lighting in accordance with KYTC and ODOT specifications. The lighting of the roadways in the project area will be in accordance with KYTC, ODOT, and local standards. The lighting design process takes into account surrounding land uses and with the goal of providing roadway lighting that balances the safety of the traveling public with local concerns, such as light pollution.

Comment Number	Nature of Comment	Comment	Response
348	Historic Resources	10/31/2023 - Email - I have heard that the State of Ohio plans to purchase the former B&O Freight House, now Longworth Hall as part of the project. You had told me nearly a year ago that 200 feet of the east end would be removed and the end wall reconstructed and signage relocated. If Ohio buys Longworth Hall, what are the plans for the building? Will the tenants that are there now be staying or will they need to vacate. Just curious as to what may transpire.	Thank you for your interest in the Brent Spence Bridge Corridor project and your inquiry regarding the project's impact on the former Baltimore & Ohio Railroad warehouse, now known as Longworth Hall. While the project will require 204 feet of the building to be removed along the eastern portion, ODOT has committed to mitigating the impacts to historic Longworth Hall. The mitigation measures include installing exterior storm windows on the exterior of the building, restoring the east wall to an approximation of its original appearance using materials salvaged from the impacted portion of the structure, restoring windows that are removed and using them in the east wall reconstruction or returning them to the building owner, adding a cornerstone commemorating the date of construction on one side and the date of the renovation on the other side, repairing masonry, refurbishing the original lettering across the top of the building, and constructing a interpretive plaque/signing describing changes to the property that have occurred over time. The building will remain occupied, and only businesses directly impacted by the removal of 204 feet from the building's east end will be relocated. Tenants currently renting space within Longworth whose units are affected by the removal have been notified and offered relocation assistance, with some choosing to relocate to other areas within Longworth Hall. ODOT may use interior space or the exterior grounds surrounding the building during the project's construction, but no impacts to the building's continued use for commercial office, retail, and event space are anticipated.
349	Historic Resources	11/05/2023 - Web Comment - When you begin demolition of the east side of Longworth Hall, would there be any chance of getting some of those bricks? My wife and I met in a dance club called Oscars in 2000 on that end of the building. We have been happily married since 2002. So that end has a lot of sentimental value in our lives. We would love to have a piece of that history from when our lives began together. Any info you could give me on this would be greatly appreciated. Thank you so much.	Thank you for your inquiry about Longworth Hall. Because Longworth Hall is a historic structure listed on the National Register of Historic Places, ODOT has entered into a programmatic agreement that will govern the work on structure. Materials from the demolition will be salvaged and used in the restoration of the east wall. All other salvaged materials must be returned to the building owner to be used in future repairs or expansion.

Comment Number	Nature of Comment	Comment	Response
350	Construction	11/08/2023 - Web Comment - Great news, what is the projected completion?	Thank you for your interest in the Brent Spence Bridge Corridor Project. The project will be completed in three phases. Construction of the portion from Dixie Highway (KY) to Ezzard Charles Drive (OH) is anticipated to begin in 2025 and be substantially complete by 2030, although some limited construction activities may begin in 2024. Construction of the portion from Linn Street to Findlay Street (OH) is anticipated to begin in 2026 and be substantially complete in 2031. Construction of the portion from Findlay Street to the Western Hills Viaduct (OH) is anticipated to begin in 2029 and be substantially complete in 2032.
351	Construction	11/08/2023 - Web Comment - When is work expected to begin, and be completed?	Thank you for your interest in the Brent Spence Bridge Corridor Project. The project will be completed in three phases. Construction of the portion from Dixie Highway (KY) to Ezzard Charles Drive (OH) is anticipated to begin in 2025 and be substantially complete by 2030, although some limited construction activities may begin in 2024. Construction of the portion from Linn Street to Findlay Street (OH) is anticipated to begin in 2026 and be substantially complete in 2031. Construction of the portion from Findlay Street to the Western Hills Viaduct (OH) is anticipated to begin in 2029 and be substantially complete in 2032.
352	Innovation Process	11/10/2023 - Web Comment - Love the caps at Ezzard Charles. Suggestion for DBT during this innovation period, look at if you can widen caps by 15-20 feet. I think it's more possible to do so on Kenner side than Hopkins with existing 75 elevation. This brings flexibility that there could be development and park mixture instead of one or other. It's great work just would like to see if 50 feet on either side can be increased.	ODOT has committed to building a wider bridge on Ezzard Charles Drive over I-75. The dimensions of the new bridge were determined in coordination with the City of Cincinnati. Though there is potential for additional width, ODOT and the City agreed upon the proposed 50 feet taking into account future plans by the City, including cost constraints.

Comment Number	Nature of Comment	Comment	Response
353	Noise	<p>11/10/2023 - Web Comment - I have looked at the Project Corridor Map and see a number of places where there is a Noise Wall. My property sits at the end of Scenic Drive (map #4. On looking at that map, the right of way is just below my property [REDACTED]. I do not see any indication that there will be a Noise Wall anywhere on that hillside, yet there is before and after.</p> <p>Can you explain why the noise wall ends Just after the Alberta Historic District and starts up again close to West Pike street.</p>	<p>Thank you for your recent inquiry on the Brent Spence Bridge Corridor Project specific to the selection of noise wall locations.</p> <p>The locations of the proposed noise walls are based on the results of noise analyses prepared for the project. Several factors influence the placement of noise walls, including proximity to the highway, traffic volumes and topography. KYTC's noise policy considers noise walls when the projected noise levels based on proposed traffic at a residential dwelling or area of outdoor use are 66 decibels and above or if there is a 10 decibel or greater increase in sound levels as a result of the project. Where those parameters are met, KYTC recommends noise walls at those locations.</p> <p>Noise modeling completed for the project determined that the noise levels at your residence will be 54.1 decibels after the project is built. In addition, the project is only anticipated to increase noise levels at your residence by 1.2 decibels. Because sound is measured on a logarithmic scale, an increase of 1.2 decibels is generally not perceptible to the human ear. Based on the results of the noise study, the projected sound levels at your residence do not meet the minimum thresholds for a noise wall, and KYTC is not proposing a noise wall in this location.</p>
354	Noise	<p>11/9/2023 – Email Comment - Thank you for taking the time to respond to my comment [Comment 352]. I would recommend that prior to any final decision, that the planners go to the current bridge on Ezzard Charles and attempt a normal conversation at conversational levels. My guess is it will be difficult due to noise from I75. If that is the case the selection of the bridge for civic interaction is suspect.</p>	<p>Thank you for your follow up email. Your comments have been forwarded to the City of Cincinnati for their consideration.</p>

Comment Number	Nature of Comment	Comment	Response
355	Noise	<p>11/13/2023 - Web Comment - I am Pastor of the Central Church of the Nazarene located at 2006 Pieck Drive in Ft Wright. Our church will be greatly effected by the proposed sound wall. The obscurity will be a tremendous detriment to our congregation. We have learned the potential for suitable signage will be a difficult process without guarantee. I asked early on about a clear sound wall section in front of our existing sign. Possibly 150' to 200' of clear wall total. I see the Governor spoke of clear sound walls in Covington. I would like to discuss the possibility of having the same directly behind the church.</p> <p>Please contact me at [REDACTED].</p>	A member of the project team contacted this individual as requested.
356	Construction/ Design	<p>11/15/2023 - Web Comment - So groundbreaking is listed as November 2023, yet a) there is no final bridge design that has been shared, and b) there is no groundbreaking date set that I know of. Net, feels like government has lied to us once more. Not interested in excuses, just truth for once.</p>	<p>Thank you for your interest in the Brent Spence Bridge Corridor Project. The project will be completed in three phases. Construction of the portion from Dixie Highway (KY) to Ezzard Charles Drive (OH), which includes the new companion bridge across the Ohio River, is anticipated to begin in 2025 and be substantially complete by 2030, although some limited construction activities may begin in 2024. The project schedule was updated this past summer. This update reflected that there would not be a groundbreaking in 2023.</p> <p>KYTC and ODOT are working with the design build team to evaluate options for the design of the new companion bridge. Once final decisions about the bridge design have been made, that information will be shared with the public.</p>
357	Construction	<p>11/20/2023 - Web Comment - Has a CE&I consultant been selected yet?</p>	<p>Thank you for your interest in the Brent Spence Bridge Corridor Project. KYTC and ODOT have already contracted for construction, engineering, and inspection services.</p>
358	Construction	<p>12/01/2023 - Email - [Related to Comment 357.] Could you provide me with the firms name?</p>	<p>The WSP team was selected to provide construction, engineering, and inspection services for the BSB Corridor Project.</p>
359	Miscellaneous	<p>11/26/2023 - Web Comment - What is best internet site for info.?</p>	<p>Thank you for your interest in the Brent Spence Bridge Corridor Project. The project website is maintained and updated regularly to provide the most current information about the project: www.brentspencebridgecorridor.com.</p>

Comment Number	Nature of Comment	Comment	Response
360	Project Funding	11/30/2023 - Web Comment - We all know that the final cost will be significantly more than the projected cost and the grant of \$ 1.6B - who's paying for all the overages / extra's / inflation increases ?	Thank you for your interest in the Brent Spence Bridge Corridor Project. The total project cost estimate is \$3.6 billion, which includes all costs required to deliver the project, including but not limited to planning, design, property acquisition, construction, construction management services, and agency labor. The cost of the companion bridge will be split 50/50 between Kentucky and Ohio, and each state will pay for the approach work on their respective ends of the bridge. In December 2022, KYTC and ODOT received \$1.635 billion in federal funding grants under programs created by the Bipartisan Infrastructure Law. The Kentucky General Assembly passed, and Governor Beshear signed, a budget bill that included funding to fulfill state match requirements for large projects. Ohio is using a combination of other federal funding and state funding from the motor fuel tax and bonding.
361	Construction	11/30/2023 - Web Comment - Is there a place that I can view road closures/alternate routes for the affected areas once construction starts? If not now, do you know how far this information will be available in advance?	The project website, www.brentspencebridgecorridor.com , will have current information about road closures and alternate routes once construction starts. The project is still in the design phase, and these details have not yet been determined. Construction is anticipated to start in earnest in 2025 with some work occurring in late 2024. KYTC and ODOT have committed to providing regular project updates regarding maintenance of traffic plans, current traffic patterns, upcoming changes, etc. Information about construction sequencing, project highlights, and construction schedules will also be shared with the public through the project website, social media, e-newsletters, and local media.
362	Project Mailing List	11/30/2023 - Web Comment - Interested in keeping up to date about the project.	This individual was added to the project mailing list.
363	Construction	12/01/2023 - Web Comment - Excited to see this project will begin soon! My name is [REDACTED], I run a housing solution business and would love to work with this project and provide housing for out of town workers. I am local to the greater Cincinnati area and look forward to doing business with you.	Thank you for your interest in the Brent Spence Bridge Corridor Project. You may reach out directly to the design-build team regarding potential housing accommodations using the following email address: WalshKokosingBrentSpence@walshgroup.com . You can also visit the Walsh Kokosing Design-Build Team website at https://walshkokosing.com/ .

Comment Number	Nature of Comment	Comment	Response
364	Construction	12/01/2023 - Web Comment - DNK is interested in providing architectural services for the renovation of Longworth Hall and any other portions of the project that will require architectural (vertical) services.	Thank you for your interest in the Brent Spence Bridge Corridor Project. The Walsh Kokosing Design-Build Team will be responsible for contracting architectural services. You may reach out directly to the design-build team regarding work on the project using the following email address: WalshKokosingBrentSpence@walshgroup.com . You can also visit the Walsh Kokosing Design-Build Team website at https://walshkokosing.com/ .
365	Miscellaneous	12/01/2023 - Web Comment - none	Comment noted.
366	Construction	<p>12/01/2023 - Web Comment - As a domestic manufacturer of fence and security products, Ameristar offers design input, drawings, specifications, budget numbers, and samples to assist in the design process.</p> <p>Aegis II Xtreme was developed to meet the requirements of AASHTO LRFD as a pedestrian railing. It offers the protection of a railing and the aesthetic of an ornamental barrier. The benefit of specifying/using Aegis II Xtreme is domestically manufactured, variety of options, superior coating, and warranty.</p> <p>Ameristar's Aegis II Xtreme has been approved and installed for the Utah DOT, Michigan DOT, Arkansas DOT, New York DOT, Colorado DOT, and Tennessee DOT.</p> <p>If LRFD is not a requirement we have several product solutions that meet IBC code as well.</p> <p>We are happy to schedule a meeting to review how we can assist the project team with fence and security. What works best for your schedules in 2024?</p>	<p>Thank you for your interest in the Brent Spence Bridge Corridor Project. You may reach out directly to the design-build team regarding work on the project using the following email address: WalshKokosingBrentSpence@walshgroup.com. You can also visit the Walsh Kokosing Design-Build Team website at https://walshkokosing.com/.</p> <p>Also, the following link will give you information and contacts on how ODOT can incorporate new products into its work. https://www.transportation.ohio.gov/working/construction/materials/overview/new-products</p>

Comment Number	Nature of Comment	Comment	Response
367	Construction	12/01/2023 - Web Comment - I would be interested in general labor position ,could you keep my email & notify me when opening or start of building the bridge.	<p>Thank you for your interest in the Brent Spence Bridge Corridor Project. You may reach out directly to the design-build team regarding work on the project using the following email address: WalshKokosingBrentSpence@walshgroup.com. You can also visit the Walsh Kokosing Design-Build Team website at https://walshkokosing.com/.</p> <p>You can also visit the "Work With Us" page on the project website for links to resources for individuals that want to work on the project: https://brentspencebridgecorridor.com/work-with-us/explore-construction-careers/</p> <p>We've also added you to the project mailing list so you can receive regular updates about the project.</p>
368	General Support	12/02/2023 - Web Comment - Thanks ! For all updates . I am eager to see the results when completed. I am sure it will be one great attraction when completed and will serve the area in a great way. Thanks ! Again.	Comment noted.
369	General Support	12/03/2023 - Web Comment - Interesting project	Comment noted.
370	General Support	12/04/2023 - Web Comment - Good morning, can you please tell me about the project and how my firm www.oneatlas.com can register as a vendor?	<p>Thank you for your interest in the Brent Spence Bridge Corridor Project. You can find all the latest information about the project on the project website: www.brentspencebridgecorridor.com.</p> <p>You may reach out directly to the design-build team regarding work on the project using the following email address: WalshKokosingBrentSpence@walshgroup.com. You can also visit the Walsh Kokosing Design-Build Team website at https://walshkokosing.com/.</p> <p>Also, the following link will give you information and contacts on how ODOT can incorporate new products into its work. https://www.transportation.ohio.gov/working/construction/materials/overview/new-products</p>

Comment Number	Nature of Comment	Comment	Response
371	Multimodal Traffic	12/05/2023 - Web Comment - Will there be a place for walkers and bikers on one of these bridges? The bridge could be such a great connection to KY and OH as well as a redesign of the areas around the highway to make it nice for people. You say you are looking out for local people but if we can't live around the highway and walk on the new bridge then it is only disconnecting people more. We can't be 100% focused on the cars, what about the people? I am a local in the area for 5 years and I want to stay and see positive changes that can connect the community's. Please reach out to me. I would love to help in anyway I can and hear from you.	<p>Thank you for your interest in the Brent Spence Bridge Corridor Project. Pedestrian and bicycle accommodations are not planned on the existing Brent Spence bridge or the new companion bridge because of the proximity of a reasonable crossing at the Clay Wade Bailey Bridge.</p> <p>The project will build new and improved sidewalks, shared-use paths, bike lanes, and a new pedestrian bridge in the West End Community. The proposed improvements will increase the options available to pedestrians and bicyclists in the project area.</p> <p>Preliminary investigations have shown that adding bike lanes to the Clay Wade Bailey Bridge may be feasible. KYTC and ODOT will further evaluate adding bike lanes to the Clay Wade Bailey Bridge during the project's progressive design-build phase.</p>
372	BSB Companion Bridge	12/06/2023 - Web Comment - I produced two stories for The Cincinnati Enquirer related to the naming of the BSB "companion." The first asked readers to suggest names. The second shared results of that unofficial survey. "OH-KY" or some version of it won the most mentions. Here's the story to add to the hopper of ideas from the public. https://www.cincinnati.com/story/news/2023/11/26/what-should-brent-spence-companion-bridge-be-named/71597226007/	Thank you for your interest in the Brent Spence Bridge Corridor Project and for sharing ideas about a name for the new companion bridge.
373	Alternatives	12/08/2023 - Web Comment - I always thought it was stupid that Kentucky never put in a faster route to get across the Licking River. It takes going through 13 lights at least just to get from one side of Kentucky to the other. Why don't we buy a few of those homes on Martin Luther King and make that easier for people to get there instead of using the Brent Spence Bridge as the the GPS has us do.	Thank you for your interest in the Brent Spence Bridge Corridor Project. The purpose and need for the project is to improve traffic flow and safety in the I-71/I-75 corridor while maintaining key regional connections. Building a new route across the Licking River is beyond the purpose and need for this project and would substantially increase the project's impacts and costs.
374	Construction	12/08/2023 - Web Comment - My Name is [REDACTED] I am a Woman owned business you can handle many different aspects for the bridge project. Hauling, Material, Excavating. Please contact me as I would like to register for the project.	Thank you for your interest in the Brent Spence Bridge Corridor Project. You may reach out directly to the design-build team regarding work on the project using the following email address: WalshKokosingBrentSpence@walshgroup.com . You can also visit the Walsh Kokosing Design-Build Team website at https://walshkokosing.com/ .

Comment Number	Nature of Comment	Comment	Response
375	Schedule	12/11/2023 - Web Comment - Hello, do you have any updates on this project to share or timeline? It appears that the project is delayed according to dates on the website. I am very excited for improvements of the bridge and the surrounding area. Thank you	<p>Thank you for your interest in the Brent Spence Bridge Corridor Project. The project will be completed in three phases. Construction of the portion from Dixie Highway (KY) to Ezzard Charles Drive (OH) is anticipated to begin in 2025 and be substantially complete by 2030, although some limited construction activities may begin in 2024. Construction of the portion from Linn Street to Findlay Street (OH) is anticipated to begin in 2026 and be substantially complete in 2031. Construction of the portion from Findlay Street to the Western Hills Viaduct (OH) is anticipated to begin in 2029 and be substantially complete in 2032.</p> <p>You can continue to access the most current information about the project on the project website: https://brentspencebridgecorridor.com/. You can also sign up to receive the project e-newsletters at: http://brentspencebridgecorridor.com/public-involvement/#footer.</p>
376	Project Mailing List	12/13/2023 - Web Comment - Kindly include us on your email list.	This individual was added to the project mailing list.
377	Multimodal Traffic	12/14/2023 - Web Comment - For the cross streets that will be "improved", bicyclists need bike lanes, not so-called "multi-use" paths which are simply glorified sidewalks which force cyclists to constantly dodge pedestrians and stop at every crossroad. Of course, it will be inevitable that serious cyclists will get screwed over by the project. There is not one bridge across the Ohio River that has bicycle facilities. Sidewalks do not count!	Thank you for your interest in the Brent Spence Bridge Corridor Project. The project will build or replace bike lanes on Liberty Street, Findlay Street, Bank Street, and Harrison Avenue in Cincinnati. The locations of shared-use paths and bike lanes were determined in coordination with the City of Cincinnati and the City of Covington. --- Email was returned as undeliverable
378	Miscellaneous	12/17/2023 - Web Comment - I run a Science Cafe and I am looking for someone to come and talk about the Brent Spence Bridge and the new project. Please contact me for details.	A member of the project team contacted this individual as requested.
379	Construction	12/21/2023 - Web Comment - Greetings - How does an individual apply for employment seeking an opportunity to obtain a job with company conducting work on the Brent Spence Bridge? Have skills/capabilities in cement masonry, carpentry, electrical, plastering, painting, plumbing. Thank you.	<p>Thank you for your interest in the Brent Spence Bridge Corridor Project. You may reach out directly to the design-build team regarding work on the project using the following email address: WalshKokosingBrentSpence@walshgroup.com. You can also visit the Walsh Kokosing Design-Build Team website at https://walshkokosing.com/.</p> <p>You can also visit the "Work With Us" page on the project website for links to resources for individuals that want to work on the project: https://brentspencebridgecorridor.com/work-with-us/explore-construction-careers/</p>

Comment Number	Nature of Comment	Comment	Response
380	Construction Traffic	12/21/2023 - Web Comment - I sell a billboard on I75/71 in Covington at Pike Street. What kind of traffic impact can we expect once the project construction starts in April? Will all traffic still be passing by our board or being rerouted in other directions? Is construction on schedule to start in April?	Thank you for your interest in the Brent Spence Bridge Corridor Project and your question about traffic on I-71/I-75 near Pike Street during construction. Traffic on I-71/I-75 is anticipated to be maintained throughout construction of the project, although some temporary detours of local streets and ramps may be required. During construction, KYTC will develop an overall Traffic Management Plan and a detailed Maintenance of Traffic (MOT) plan to maintain traffic operations through the corridor and minimize disruption to the surrounding communities. Construction in Kentucky is anticipated to begin in 2025 and be substantially complete by 2030, although some limited construction activities may begin in late 2024.
381	General Support	12/22/2023 - Web Comment - Exciting to be a part of the team.	Comment noted.
382	Historic Resources	12/28/2023 - Web Comment - Will the existing Baltimore and Ohio Railroad terminal building have to be demolished?	<p>Thank you for your interest in the Brent Spence Bridge Corridor project and your inquiry regarding the project's impact on the former Baltimore & Ohio Railroad warehouse, now known as Longworth Hall. The project will require 204 feet of the building to be removed along the eastern portion, and ODOT has committed to mitigating the impacts to historic Longworth Hall. The mitigation measures include installing exterior storm windows on the exterior of the building, restoring the east wall to an approximation of its original appearance using materials salvaged from the impacted portion of the structure, restoring windows that are removed and using them in the east wall reconstruction or returning them to the building owner, adding a cornerstone commemorating the date of construction on one side and the date of the renovation on the other side, repairing masonry, refurbishing the original lettering across the top of the building, and constructing an interpretive plaque/signing describing changes to the property that have occurred over time.</p> <p>As a result of the right-of-way process, ODOT has agreed to purchase the full Longworth Hall building from the current owner. Although the ownership will change, the building will remain occupied, and only businesses directly impacted by the removal of 204 feet from the building's east end will be relocated. Tenants currently renting space within Longworth whose units are affected by the removal have been notified and offered relocation assistance, with some choosing to relocate to other areas within Longworth Hall. ODOT may use interior space or the exterior grounds surrounding the building during the project's construction, but no impacts to the building's continued use for commercial office, retail, and event space are anticipated.</p>

Comment Number	Nature of Comment	Comment	Response
383	Miscellaneous	12/28/2023 - Web Comment - Hello	Comment noted.
384	Traffic	12/28/2023 - Web Comment - Will this design interfere with traffic on Kyle's Lane in Covington KY?	<p>Thank you for your interest in the Brent Spence Bridge Corridor Project. Once complete, the project will not negatively impact traffic on Kyles Lane and is anticipated to improve traffic flow and safety when entering and exiting the interstate. The Kyles Lane bridge over I-71/I-75 and the highway ramps will be rebuilt as part of the project. In addition, current plans call for a new collector-distributor road to be built between Dixie Highway and Kyles Lane to reduce weaving movements and improve safety on I-71/I-75.</p> <p>During the project's construction, there may be some short-term traffic impacts while the Kyles Lane bridge and ramps are being rebuilt. The Kentucky Transportation Cabinet (KYTC) will develop an overall Traffic Management Plan and a detailed Maintenance of Traffic (MOT) plan to maintain traffic operations through the corridor, including Kyles Lane, and minimize disruption to the surrounding communities.</p>
385	Existing Bridge	12/28/2023 - Web Comment - The project was originally approved under the premise that the existing bridge was obsolete and more importantly unsafe. Now all of a sudden it's listed as structurally sound. Is that really true? Are any other upgrades to this bridge planned?	<p>Thank you for your interest in the Brent Spence Bridge Corridor Project. The purpose and need for the project was first established in 2006 and has not changed. It includes improving traffic flow and level of service, improving safety, correcting geometric deficiencies, and maintaining connections to key regional and national transportation corridors.</p> <p>The existing Brent Spence Bridge remains safe and structurally sound. Some parts of the existing Brent Spence Bridge do not meet current design standards, such as the lack of shoulders. Heavy traffic congestion coupled with a lack of shoulders contribute to crashes that are occurring on the existing bridge. The Brent Spence Bridge Corridor Project will construct a new 5-lane, double-decker companion bridge to carry interstate traffic across the river. The existing Brent Spence Bridge will only carry local traffic, which will reduce traffic on the structure. The existing bridge will also be restriped to provide the originally designed three lanes and shoulders. These improvements will improve traffic flow and safety on the existing Brent Spence Bridge.</p> <p>In addition, the existing bridge will be rehabilitated as part of the project. The bridge decks will be replaced, and spot repairs of the steel structure will be completed, as needed, to ensure that the bridge remains in good repair.</p>

Comment Number	Nature of Comment	Comment	Response
386	Alternatives Climate Change	<p>12/29/2023 - Web Comment - Why are you spending so much money on more roadways when this could be put towards better public transportation, like buses or creating a train line within the city? We have a duty to address climate change issues that are a severe problem already, and only getting worse, and encouraging more cars to be on the road is horribly counterintuitive for the health of our world, let alone our local communities. While I understand the concern for the bridge being a bottleneck for trucks as it is, there would be much less bottlenecking if there were better public transit options available for people. You may look at my zip code and think I don't have much right to say anything, but there are still people who live in my town that remember the train line running between the hamlet of Kings Mills and Cincinnati, providing valuable transportation to people both commuting and visiting either location. Bringing back train lines is the most ideal in my eyes because it more reliably connects further distances and tends to be more timely while allowing people to help fund it with train ticket sales, and we all know the subways tunnels still exist and continue to cost us maintenance for largely-unused space, but even having significant bus line improvements within the city would help the people in the city immensely, the latter of which surely wouldn't even cost taxpayers as much as this current plan does. Worse, if this has anything to do with car manufacturers trying to influence the decision to create more space for cars and, thus, further car sales, we all have a right to be livid about it, the least of which being that independent driving is not an option for MANY people for a myriad of reasons.</p> <p>I find it very hard to believe that adding more car lanes is really what's best for the people of Cincinnati, and you have a duty to do what's best for them. Priorities are better off going towards reducing all traffic and pollution, allowing better public transit that enables more self-sufficiency for maintenance purposes, and giving non-drivers better choices for navigating the city.</p>	<p>Thank you for your interest in the Brent Spence Bridge Corridor project. The project is designed to accommodate traffic projected to travel through the corridor by the year 2050, which is the standard design horizon for large infrastructure projects. The traffic projections are based on a regional travel-demand model which assigns routes used by travelers based on available capacity, projected trips, and calculated travel times. The Brent Spence Bridge Corridor Project is designed to accommodate all trips projected by the Ohio-Kentucky-Indiana (OKI) regional travel demand model through the year 2050.</p> <p>In 2004, OKI and the Miami Valley Regional Planning Commission (MVRPC) completed a major planning study known as the North South Transportation Initiative (Initiative) that considered highway improvements in addition to transit improvements such as express bus, commuter rail, and others. The Initiative concluded that a highway improvement project was necessary to address capacity issues on I-75, including the Brent Spence Bridge Corridor. The Brent Spence Bridge Corridor Project addresses the highway component of the Initiative. The transit component included in the Initiative must be developed and championed regionally, and ODOT and KYTC are ready to support this when it is advanced at a regional level. The North South Transportation Initiative planning study can be accessed here: www.oki.org/studies/pdf/northsouth/ns-complete.pdf.</p> <p>The project will reduce congestion and improve reliability for local bus routes, which use the Brent Spence Bridge for 210 trips every weekday. In addition, improved and new sidewalks, shared use paths, and bicycle lanes will improve connections to existing bus stops. Finally, future accommodation of bus service along the corridor's shoulder will not be precluded by the project.</p> <p>Finally, KYTC and ODOT are evaluating the project's potential effects on greenhouse gas emissions and climate change in a Supplemental Environmental Assessment being prepared for the project. The Supplemental Environmental Assessment is anticipated to be made available for public review and comment in February 2024.</p>

Comment Number	Nature of Comment	Comment	Response
387	General Opposition	12/29/2023 - Web Comment - WHY CAN YOU NOT SEE: YOUR BILLIONS DOLLAR MISTAKE; THE UN ACCEPTICAL TIME TO COMPLETION AND THE UNNESSARY LAND GRAB. YOU WILL BE BACK TOO THE DESIGN TABLE A WEEK AFTER COMPLECTION AND WANTING TOO HIDE FROM THE PUBLIC.	Thank you for your comment pertaining to the Brent Spence Bridge Corridor Project. Your comment has been included in the project record.
388	Miscellaneous	12/29/2023 - Web Comment - Keep me updated.	This individual was added to the project mailing list.
389	Traffic	12/29/2023 - Web Comment - Will the 4th and 5th street access to /from the bridge from Kentucky be restored?	<p>Thank you for your interest in the Brent Spence Bridge Corridor Project.</p> <p>As part of the project, the Texas turnaround at Pike Street will be removed and replaced by a collector-distributor roadway system, which will provide local access to and from the interstate. West 5th Street into Covington will be accessed via the southbound collector-distributor system. The project will include a West 4th Street ramp to the northbound collector-distributor system, which will then continue on to I-71 and I-75. The current restriction that limits the West 4th Street ramp to the existing Brent Spence Bridge to emergency vehicles only will be removed, and the ramp will be open to all vehicles.</p>
390	Construction	01/03/2024 - Web Comment - Partnership with Rumpke for open top construction dumpsters for this project	<p>Thank you for your interest in the Brent Spence Bridge Corridor Project. You may reach out directly to the design-build team regarding work on the project using the following email address: WalshKokosingBrentSpence@walshgroup.com. You can also visit the Walsh Kokosing Design-Build Team website at https://walshkokosing.com/.</p> <p>You can also visit the "Work With Us" page on the project website for links to resources for individuals that want to work on the project: https://brentspencebridgecorridor.com/work-with-us/explore-construction-careers/</p>

Comment Number	Nature of Comment	Comment	Response
391	Construction	01/04/2023 - Web Comment - Partnership with Rumpke for open top construction containers for the project.	<p>Thank you for your interest in the Brent Spence Bridge Corridor Project. You may reach out directly to the design-build team regarding work on the project using the following email address: WalshKokosingBrentSpence@walshgroup.com. You can also visit the Walsh Kokosing Design-Build Team website at https://walshkokosing.com/.</p> <p>You can also visit the "Work With Us" page on the project website for links to resources for individuals that want to work on the project: https://brentspencebridgecorridor.com/work-with-us/explore-construction-careers/</p>

Brent Spence Bridge Corridor Project Social Media Comments and Responses



David Cecil

What about local HAZMAT loads on new companion bridge such as local gas deliveries to facilities located at base of bridge... Will they be allowed or will we be required to take I471 around like we do now. If new companion bridge is being built like the I471 bridge, then HAZMAT should be allowed

Like Reply Hide 6w



Author

Brent Spence Bridge Corridor

David Cecil Thank you for your question. We appreciate your patience while we looked into how best to address your comment. Decisions about Hazardous Cargo (HC) on I-71/-175 north of I-275 in Northern Kentucky are reviewed periodically based on several factors, including the severity of the hill leading up to the Ohio border. We suggest you contact the Department of Vehicle Regulation in Kentucky for any questions related to Hazardous Cargo (HC) hauling. Resources are also available through the Public Utility Commission of Ohio. If you would like to submit additional questions to the project team, please visit the project website at www.BrentSpenceBridgeCorridor.com and click on the form at the bottom of the page.



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Like Reply Remove Preview 1m



Marc Howard

Good updates. Anything more on timelines overall, start date, or final design?

Like Reply Hide 1w



Author

Brent Spence Bridge Corridor

Marc Howard Thanks, we will continue to post updates here as well as on our Twitter account, BSBCorridor. You can also visit the project website -- <https://brentspencebridgecorridor.com/> -- where you can learn more and sign up to receive periodic project updates, including notification of upcoming public meetings. Groundbreaking is expected in late 2023. Substantial completion is expected by 2030.



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Michael Click
"Error: Unable to find URL to redirect to."

Like Reply Hide 2w

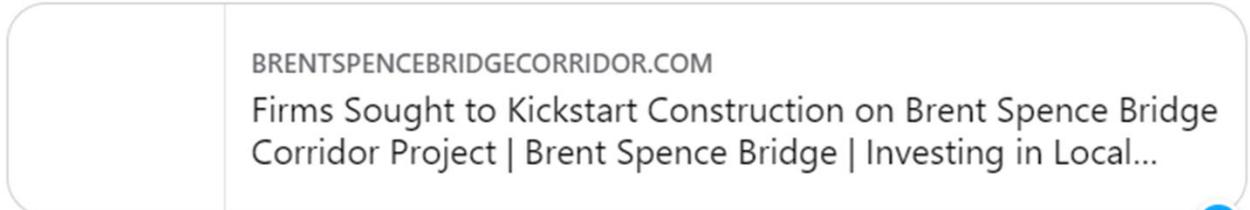


Author

Brent Spence Bridge Corridor

Michael Click Sorry about that -- here is the link:

<https://brentspencebridgecorridor.com/firms-sought-to.../>



Like Reply Remove Preview 2w



Sam Mangano

When will you announce the D-B teams that submitted proposals and what types of bridges they proposed?

Like Reply Hide Send Message 20h



Author

Brent Spence Bridge Corridor

Sam Mangano The plan is to announce the selection of the Design Build Team (design firm and contractor) in early June. Proposed bridge type designs will be determined as part of the design process once the team is in place. We anticipate this to be sometime later this year or early next year.

Like Reply 7m



Gigi-Sammi Princess Palumbo

Aren't all the changes going to cost more money? ...

Like Reply 3w



Author

Brent Spence Bridge Corridor

Gigi-Sammi Princess Palumbo Thank you for your interest in the project. Revisions and changes to the current concept will be considered, developed and analyzed by the project team. Design revisions that meet the project goals and minimize cost and schedule impacts will be carried forward for consideration. Costs are a factor in all decisions. ...

Like Reply 3w



Megan Seiter

What refurbishing is going to be done to the existing bridge? Just the reconfiguration of on/off ramps or something else?

3d Like Reply [Send message](#) Hide



 Author

Brent Spence Bridge Corridor

Megan Seiter Thank you for your question. The existing Brent Spence Bridge will be reconfigured to three lanes instead of four on each deck with emergency shoulders on each side to improve safety. It will carry local traffic between Covington and Cincinnati while the new companion bridge will carry I-71 and I-75.

The Brent Spence Bridge was cleaned and painted in 2022 as part of its regular maintenance schedule. As part of the reconfiguration of the bridge, crews will:

- Replace the bridge decks;
- Complete spot repairs of the steel structure, as needed; and
- Restripe the decks to restore the bridge's originally-designed three lanes of travel in each direction.

4m Like Reply

Brent Spence Bridge Corridor Project Community Council Letters



Camp Washington Community Council

2951 Sidney Avenue

Cincinnati, OH 45225

communitycouncil@camp-washington.org

To the City of Cincinnati, Cincinnati Department of Transportation and Engineering (“DOTE”), Ohio Department of Transportation (“ODOT”), and whomever it may concern,

The Camp Washington Community Council is writing this letter to express our support for Bridge Forward’s alternative design for the Brent Spence Bridge (the “BSB”).

Bridge Forward’s proposal is a superior design for the BSB that reflects the national movement to minimize the amount of urban land taken up by freeways and looping on/off ramps. The City and region has a unique opportunity to regain and reconnect urban land, and it should not be squandered. ODOT has claimed that alterations to the existing 10+-year old design are not possible, when they themselves just recently made alterations that they declare now regains 10 acres of land. This new 10 acres flanks the BSB approach too closely to be nearly as usable as the reclaimed 30+ acres of land proposed by the Bridge Forward Design.

As a community that was severed by the creation of I-75, we feel that reconnecting communities torn apart by I-75’s construction should be a regional priority. This makes sense from an urban design standpoint and an economic standpoint. It will furthermore offer the opportunity to build thousands of sorely needed housing units in the central core.

We deserve a BSB that reflects what is happening now, today in Cincinnati and Covington; we deserve a bridge that reconnects what was lost and realizes the full economic development potential offered by the Bridge Forward BSB design.

We thank you for your time and consideration, and please do not hesitate to reach out to us at communitycouncil@camp-washington.org.

Sincerely,

The Camp Washington Community Council



Over-the-Rhine Community Council
P.O. Box 662
Cincinnati OH 45201

December 20, 2022

Mayor Aftab Pureval
Cincinnati City Council
Ohio Department of Transportation

Subject: Over-the-Rhine Community Council Support for Bridge Forward Vision and Measurable Priorities for the Brent Spence Corridor Project

To Whom it May Concern:

On October 28, 2022, Bridge Forward presented their current design modification recommendations for the Brent Spence Bridge Project to the Over-the-Rhine Community Council (OTRCC). Along with their design recommendations, they presented measurable priorities that they believe should be adopted for this project. Those priorities are:

- *Maximize the amount of returned land contiguous to the **City's** urban core;*
- *Maximize the amount of walkable street frontage that is conducive to street-facing development within and adjacent to the project footprint;*
- *Maximize the amount of pedestrian connection between the CBD, Queensgate, and the West End, minimizing the walking distance of each of those connections;*
- *Maximize the number of local streets that are designed according to the NACTO Urban Design Guide;*
- *Minimize the number of lane miles using high-speed directional ramps;*
- *Achieve best expenditure of public dollars measured in life-cycle return on investment.*

*This discussion prompted a motion for the OTRCC to support Bridge **Forward's** Vision, and to ask City Council to secure a seat at the table with respect to the project and to adopt their measurable priorities for The Brent Spence Corridor Project. These priorities should be adopted even after the 11/10/2022 press conference regarding limited Chamber-led design modifications. The motion was seconded and passed by the general body membership of the OTRCC.*

*Please Accept this notice as full documented support for the Bridge Forward **Coalition's** vision and proposed measurable priorities for the project.*

Respectfully,

A handwritten signature in black ink that reads "Danny Klingler". The signature is written in a cursive, flowing style.

Danny Klingler
President
Over-the-Rhine Community Council

January 26, 2023 E-Newsletter

West End Community Council

PO Box 14424

Cincinnati, Ohio 45250

Date: 10/25/2021

Notice: West End Community Council Motion to Encourage Feasibility Study of Brent Spence Bridge Design Improvements

On October 19, 2021, the West End Community Council general body reviewed and discussed the Brent Spence Bridge realignment proposal created by Ryan Laber and the improved submerged design of the current alignment created by the Bridge Forward Coalition. This discussion included information about the negative historical context and impact of the current design and the development opportunities and historical rectification of the proposed design improvements. The general body voted to encourage relevant governmental agencies to further explore the feasibility of both proposed designs improvements.

Please accept this notice as documented support from the West End Community Council to explore of the feasibility of these Brent Spence Bridge design improvements.

Additional Information:

Bridge Forward Design: <https://www.bridge-forward.org>

Ryan Laber Design: rlaber@gmail.com (available on request)

Respectfully,



Chris Griffin

President, West End Community Council

West End Community Council

PO Box 14424
Cincinnati, Ohio 45250

Date: October 28, 2022

Subject: West End Community Council Support for Bridge Forward Design Recommendations for the Brent Spence Bridge Project and Feasibility Assessments

On October 18, 2022, Steve Kenat, an architect with GBBN, presented the current Bridge Forward design-modification recommendations for the Brent Spence Bridge project to the West End Community Council. There was a discussion about the negative historical racial context of the current design and the disproportionate impact it has had on the West End community. The need for this community to be assertive in our support for these design improvements was also included in the discussion.

This discussion prompted a motion to be made for the West End Community Council to reaffirm our support for the Bridge Forward Coalition and their design recommendations. The motion was seconded and passed by the general body membership of the West End Community Council.

Please accept this notice as full documented support from the West End Community Council for the Brent Spence Bridge project design improvements proposed by the Bridge Forward Coalition and for the appropriate feasibility assessments to be conducted.

Note: Last year, on October 19, 2021, the West End Community Council general body reviewed and discussed potential design modifications to the Brent Spence Bridge project. At the meeting, a motion was passed to request relevant governmental agencies to explore the feasibility of these design improvements. Please note the West End Community Council's long-standing support for design improvements to the Brent Spence Bridge project that could lessen the highway's impact on our historic community.

Respectfully,



Chris Griffin
President, West End Community Council

January 19, 2023



PO Box 19138
Cincinnati, OH 45219

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Cody Sheets

Corresponding Secretary
Carol Gibbs

Board of Trustees
Sandi Allen
Ken Farmer
Daryl Frazier
Roger Hildebrand
Christian Huelsman
Alice Williams
Lee Wilson

On January 17, 2023, Bridge Forward presented its current design priorities for the Brent Spence Bridge Project to the Mt. Auburn Community Council. Bridge Forward's design priorities are as follows:

- Maximize the amount of returned land contiguous to the City's urban core;
- Maximize the amount of walkable street frontage that is conducive to street-facing development within and adjacent to the project footprint;
- Maximize the amount of pedestrian connection between the CBD, Queensgate, and the West End, minimizing the walking distance of each of those connections;
- Maximize the number of local streets that are designed according to the NACTO Urban Design Guide;
- Minimize the number of lane miles using high-speed directional ramps; and
- Achieve best expenditure of public dollars measured in life-cycle return on investment.

Additionally, Bridge Forward explained that, according to ODOT / KYTC, the project will be build using a "progressive design-build process." This approach allows ODOT / KYTC, the design-build team (DBT), impacted municipalities, and the local community to consider and address major project topics in a more open format. The DBT will work with ODOT / KYTC to create the basis of design, overall project requirements, and final design intent, before work substantially begins, during what is being called Phase 1A of the Project.

This discussion prompted a motion for the Mt. Auburn Community Council to support Bridge Forward's design priorities, and to ask City Council to get engaged with the project. Specifically, City Council should ensure the progressive design-build contract, to the greatest extent possible, leverages and enhances Phase 1A of the project, guaranteeing that alternative design concepts consistent with these priorities, separate from Concept I-W, are developed. We also request that Scenario Testing be done before Certified Traffic is finalized.

Please accept this notice as full documented support for Bridge Forward's design priorities for the project and as a petition for City Council to ensure this project is transformative for the urban core of Cincinnati.

A handwritten signature in black ink, appearing to read 'Stephen Gibbs', is written over a light blue horizontal line.

Brent Spence Bridge Corridor Project Expand Transit Not Highways Letters

Kyle Ley

6246 Cortelyou Avenue

Cincinnati, OH 45213
United States

Phone: (937) 238-5847

Email Address: kyle.ley@outlook.com

Date of Submission: Thu, 01/05/2023 - 08:47

Submission Letter:

I'm writing to you today in hopes that you will oppose the Brent Spence Bridge Corridor expansion project. The project won't solve the bridge's traffic and safety problems, but it will harm local communities, cost taxpayers billions of dollars, and only heightens our reliance on cars and fossil fuels.

Highway expansion projects harm already vulnerable populations. This expansion project will promulgate plumes of dust and fumes, worsening air pollution and health outcomes in nearby communities in the long term while interrupting people's everyday lives in the short term. After months of negotiation, Covington residents are still giving up over a dozen properties to the state to build this highway. And despite all of this, the project likely will not even solve the problems it is setting out to fix. Research strongly suggests that expanding highway capacity attracts more vehicles and increases traffic, the reverse effect of what KYTC's proposed plan is supposed to achieve.

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We cannot remain fully dependent on cars as transportation. As we have seen recently, the financial precariousness of car ownership places a mounting burden on American families. Investing in better public transit systems will reduce traffic congestion, reduce the cost to taxpayers while seeing a more beneficial use of their money, and improve the quality of life in neighboring communities.

I am hoping that you will not support expanding the Brent Spence highway but rather invest in our communities, our transportation system, and our health and

safety.

Matthew Lindsey

6421 Sherman Avenue

Cincinnati, OH 45230
United States

Phone: (513) 560-6919

Email Address: mlinds724@gmail.com

Date of Submission: Thu, 01/05/2023 - 03:44

Submission Letter:

I'm writing to you today in hopes that you will oppose the Brent Spence Bridge Corridor expansion project. The project won't solve the bridge's traffic and safety problems, but it will harm local communities, cost taxpayers billions of dollars, and only heightens our reliance on cars and fossil fuels.

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safety.

Analee Haas

112 East 14th Street, Apt. 2

Cincinnati, OH 45202
United States

Phone: (330) 606-7971

Email Address: aeh1331@gmail.com

Date of Submission: Thu, 01/05/2023 - 01:38

Submission Letter:

I'm writing to you today in hopes that you will oppose the Brent Spence Bridge Corridor expansion project. The project won't solve the bridge's traffic and safety problems, but it will harm local communities, cost taxpayers billions of dollars, and only heightens our reliance on cars and fossil fuels.

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safety.

Brian Cron

9724 Cooper Lane

Cincinnati, OH 45242
United States

Phone: (513) 835-5204

Email Address: cronster555@gmail.com

Date of Submission: Thu, 01/05/2023 - 00:53

Submission Letter:

I'm writing to you today in hopes that you will oppose the Brent Spence Bridge Corridor expansion project. The project won't solve the bridge's traffic and safety problems, but it will harm local communities, cost taxpayers billions of dollars, and only heightens our reliance on cars and fossil fuels.

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safety.

Ryan Dugan

8279 Sunburst Drive

West Chester, OH 45241
United States

Phone: (513) 226-1706

Email Address: rdugan@me.com

Date of Submission: Wed, 01/04/2023 - 23:34

Submission Letter:

I'm writing to you today in hopes that you will oppose the Brent Spence Bridge Corridor expansion project. The project won't solve the bridge's traffic and safety problems, but it will harm local communities, cost taxpayers billions of dollars, and only heightens our reliance on cars and fossil fuels.

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safety.

Andre Johnson

1431 Walnut Street

Cincinnati, OH 45202
United States

Phone: (513) 509-9494

Email Address: andre.johnson825@gmail.com

Date of Submission: Wed, 01/04/2023 - 21:27

Submission Letter:

I'm writing to you today in hopes that you will oppose the Brent Spence Bridge Corridor expansion project. The project won't solve the bridge's traffic and safety problems, but it will harm local communities, cost taxpayers billions of dollars, and only heightens our reliance on cars and fossil fuels.

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safety.

Allison Schaaf

7233 Timbernoll Dr

West Chester, OH 45069
United States

Phone: (513) 507-9314

Email Address: aschaaf1234@gmail.com

Date of Submission: Wed, 01/04/2023 - 21:13

Submission Letter:

I'm writing to you today in hopes that you will oppose the Brent Spence Bridge Corridor expansion project. The project won't solve the bridge's traffic and safety problems, but it will harm local communities, cost taxpayers billions of dollars, and only heightens our reliance on cars and fossil fuels.

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safety.

Tristan Conroy

6629 Roe St

Cincinnati, OH 45227
United States

Phone: (502) 435-9995

Email Address: tristan.conroy@gmail.com

Date of Submission: Wed, 01/04/2023 - 21:11

Submission Letter:

I'm writing to you today in hopes that you will oppose the Brent Spence Bridge Corridor expansion project. The project won't solve the bridge's traffic and safety problems, but it will harm local communities, cost taxpayers billions of dollars, and only heightens our reliance on cars and fossil fuels.

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safety.

Cory Aldrich

15 W 4th St - Unit 203

Cincinnati, OH 45202
United States

Phone: (937) 478-6792

Email Address: coryaldrich@gmail.com

Date of Submission: Wed, 01/04/2023 - 21:11

Submission Letter:

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safety.

Michael Gott

4247 Williamson Pl

Cincinnati, OH 45223
United States

Phone: (614) 668-9478

Email Address: mgott150@hotmail.com

Date of Submission: Wed, 01/04/2023 - 20:42

Submission Letter:

I'm writing to you today in hopes that you will oppose the Brent Spence Bridge Corridor expansion project. The project won't solve the bridge's traffic and safety problems, but it will harm local communities, cost taxpayers billions of dollars, and only heightens our reliance on cars and fossil fuels.

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safety.

Christasia Bloom

4117 Fox Run Trl

Cincinnati, OH 45255
United States

Phone: (859) 415-5362

Email Address: bloomchristasia@gmail.com

Date of Submission: Wed, 01/04/2023 - 19:20

Submission Letter:

I'm writing to you today in hopes that you will oppose the Brent Spence Bridge Corridor expansion project. The project won't solve the bridge's traffic and safety problems, but it will harm local communities, cost taxpayers billions of dollars, and only heightens our reliance on cars and fossil fuels.

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safety.

Patrick Richart

218 Woolper Avenue

Cincinnati, OH 45220
United States

Phone: (513) 460-9639

Email Address: michaelrichart@gmail.com

Date of Submission: Wed, 01/04/2023 - 19:15

Submission Letter:

I'm writing to you today in hopes that you will oppose the Brent Spence Bridge Corridor expansion project. The project won't solve the bridge's traffic and safety problems, but it will harm local communities, cost taxpayers billions of dollars, and only heightens our reliance on cars and fossil fuels.

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safety.

Luis Finke

972 Ludlow Ave

Cincinnati, OH 45220-1020
United States

Phone: (513) 608-8703

Email Address: luisfinke@gmail.com

Date of Submission: Wed, 01/04/2023 - 18:29

Submission Letter:

I'm am a Cincinnati homeowner who has relied on the bus system for most of my life. Cincinnati NEEDS better and faster public transit. Cars have only made the city more congested and difficult to get around.

Ellery Klein

37 Fells Avenue

Medford, MA 02155
United States

Phone: (617) 418-9490

Email Address: elleryeklein@gmail.com

Date of Submission: Wed, 01/04/2023 - 18:03

Submission Letter:

I'm writing to you today in hopes that you will oppose the Brent Spence Bridge Corridor expansion project. The project won't solve the bridge's traffic and safety problems, but it will harm local communities, cost taxpayers billions of dollars, and only heightens our reliance on cars and fossil fuels.

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As someone who grew up in this area, I am continually frustrated by the lack of rail travel options to and from Cincinnati, leaving me either to an exhausting drive or

an expensive flight.

I am hoping that you will not support expanding the Brent Spence highway but rather invest in our communities, our transportation system, and our health and safety.

Jordan Kohler

8518 Plainfield Road

Cincinnati, OH 45236
United States

Phone: (330) 604-0898

Email Address: jwilsonkohler@gmail.com

Date of Submission: Wed, 01/04/2023 - 17:57

Submission Letter:

I'm writing to you today in hopes that you will oppose the Brent Spence Bridge Corridor expansion project. The project won't solve the bridge's traffic and safety problems, but it will harm local communities, cost taxpayers billions of dollars, and only heightens our reliance on cars and fossil fuels.

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safety.

Garett Reinhart

331 3rd Street

Newport, KY 41071
United States

Phone: (419) 559-5782

Email Address: garettreinhart98@gmail.com

Date of Submission: Wed, 01/04/2023 - 16:04

Submission Letter:

I'm writing to you today in hopes that you will oppose the Brent Spence Bridge Corridor expansion project. The project won't solve the bridge's traffic and safety problems, but it will harm local communities, cost taxpayers billions of dollars, and only heightens our reliance on cars and fossil fuels.

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safety.

Trip Wright

1023 Hamilton Rd

Park Hills, KY 41011
United States

Phone: (615) 720-9939

Email Address: wrightt25@mail.wlu.edu

Date of Submission: Wed, 01/04/2023 - 13:32

Submission Letter:

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I am hoping that you will not support expanding the Brent Spence highway but rather invest in our communities, our transportation system, and our health and

safety.

brady boyer

1066 Altavia Ave

Park Hills, KY 41011
United States

Phone: (859) 653-3948

Email Address: brady.boyer8@gmail.com

Date of Submission: Wed, 01/04/2023 - 11:44

Submission Letter:

I'm writing to you today in hopes that you will oppose the Brent Spence Bridge Corridor expansion project. The project won't solve the bridge's traffic and safety problems, but it will harm local communities, cost taxpayers billions of dollars, and only heightens our reliance on cars and fossil fuels.

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I am hoping that you will not support expanding the Brent Spence highway but rather invest in our communities, our transportation system, and our health and

Expand Transit not Highways

safety.

Patrick LaPrade

2635 Stanton Avenue

Cincinnati, OH 45206
United States

Phone: (937) 716-8200

Email Address: laprade97@gmail.com

Date of Submission: Tue, 01/03/2023 - 22:46

Submission Letter:

I'm writing to you today in hopes that you will oppose the Brent Spence Bridge Corridor expansion project. The project won't solve the bridge's traffic and safety problems, but it will harm local communities, cost taxpayers billions of dollars, and only heightens our reliance on cars and fossil fuels.

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safety.

Patrick Ding

1601 Main Street, Apt 4

Cincinnati, OH 45202
United States

Phone: (513) 413-0850

Email Address: dingpk@gmail.com

Date of Submission: Tue, 01/03/2023 - 20:23

Submission Letter:

I'm writing to you today in hopes that you will oppose the Brent Spence Bridge Corridor expansion project. The project won't solve the bridge's traffic and safety problems, but it will harm local communities, cost taxpayers billions of dollars, and only heightens our reliance on cars and fossil fuels.

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safety.

Matt Boggs

334 West Pike Street, 2

Covington, KY 41011
United States

Phone: (937) 502-5671

Email Address: mbb871997@gmail.com

Date of Submission: Tue, 01/03/2023 - 16:36

Submission Letter:

I'm writing to you today in hopes that you will oppose the Brent Spence Bridge Corridor expansion project. The project won't solve the bridge's traffic and safety problems, but it will harm local communities, cost taxpayers billions of dollars, and only heightens our reliance on cars and fossil fuels.

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safety.

Christy Strouse

2234 Victor St

Cincinnati, OH 45219
United States

Phone: (513) 470-4184

Email Address: cfstrouse@yahoo.com

Date of Submission: Tue, 01/03/2023 - 15:33

Submission Letter:

I'm writing to you today in hopes that you will oppose the Brent Spence Bridge Corridor expansion project. The project won't solve the bridge's traffic and safety problems, but it will harm local communities, cost taxpayers billions of dollars, and only heightens our reliance on cars and fossil fuels.

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safety.

Chow-chi Huang

9035 Amy Marie Drive

West Chester, OH 45069
United States

Phone: (513) 372-0443

Email Address: chowchi_huang@yahoo.com

Date of Submission: Tue, 01/03/2023 - 12:38

Submission Letter:

I'm writing to you today in hopes that you will oppose the Brent Spence Bridge Corridor expansion project. The project won't solve the bridge's traffic and safety problems, but it will harm local communities, cost taxpayers billions of dollars, and only heightens our reliance on cars and fossil fuels.

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I am hoping that you will not support expanding the Brent Spence highway but

rather invest in our communities, our transportation system, and our health and safety.

Mitch Mohan

621 E Mehring Way

Cincinnati, OH 45202
United States

Phone: (513) 502-6057

Email Address: mitchell.mohan17@gmail.com

Date of Submission: Tue, 01/03/2023 - 12:13

Submission Letter:

I'm writing to you today in hopes that you will oppose the Brent Spence Bridge Corridor expansion project. The project won't solve the bridge's traffic and safety problems, but it will harm local communities, cost taxpayers billions of dollars, and only heightens our reliance on cars and fossil fuels.

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safety.

Melinda O'Brien

1046 Delta Ave., APT 4

Cincinnati, OH 45208
United States

Phone: (513) 240-3971

Email Address: melinda.obrien.428@gmail.com

Date of Submission: Tue, 01/03/2023 - 12:11

Submission Letter:

I'm writing to you today in hopes that you will oppose the Brent Spence Bridge Corridor expansion project. The project won't solve the bridge's traffic and safety problems, but it will harm local communities, cost taxpayers billions of dollars, and only heightens our reliance on cars and fossil fuels.

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safety.

Barbara Didrichsen

6231 Cortelyou Avenue

Cincinnati, OH 45213
United States

Phone: (513) 235-5471

Email Address: barbdid@gmail.com

Date of Submission: Tue, 01/03/2023 - 11:25

Submission Letter:

I'm writing to you today in hopes that you will oppose the Brent Spence Bridge Corridor expansion project. The project won't solve the bridge's traffic and safety problems, but it will harm local communities, cost taxpayers billions of dollars, and only heightens our reliance on cars and fossil fuels.

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safety.

Julie Uhls

405 Scott Street

Covington, KY 41011
United States

Phone: (270) 850-9053

Email Address: julesuhls@gmail.com

Date of Submission: Tue, 01/03/2023 - 10:37

Submission Letter:

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safety.

Kayla Smith

1310 Sycamore Street

Cincinnati, OH 45202
United States

Phone: (260) 463-1351

Email Address: kayla.renee.23@gmail.com

Date of Submission: Tue, 01/03/2023 - 10:20

Submission Letter:

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safety.

Shad Beard

730 Washington St Apt 132

Covington, KY 41011
United States

Phone: (937) 554-9268

Email Address: dahs1981@gmail.com

Date of Submission: Tue, 01/03/2023 - 09:53

Submission Letter:

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safety.

Luke Whalen

3918 Taft Avenue

Cincinnati, OH 45211
United States

Phone: (513) 264-8460

Email Address: luke.nukem3d@yahoo.com

Date of Submission: Tue, 01/03/2023 - 09:15

Submission Letter:

I'm writing to you today in hopes that you will oppose the Brent Spence Bridge Corridor expansion project. The project won't solve the bridge's traffic and safety problems, but it will harm local communities, cost taxpayers billions of dollars, and only heightens our reliance on cars and fossil fuels.

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safety.

Kaitlyn Brewer

3268 Midden Cir

Cincinnati, OH 45238
United States

Phone: (812) 974-1745

Email Address: kaitlynmbrewer@gmail.com

Date of Submission: Tue, 01/03/2023 - 09:06

Submission Letter:

I'm writing to you today in hopes that you will oppose the Brent Spence Bridge Corridor expansion project. The project won't solve the bridge's traffic and safety problems, but it will harm local communities, cost taxpayers billions of dollars, and only heightens our reliance on cars and fossil fuels.

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safety.

Robert Magrish

1216 Cliff Laine Drive

Cincinnati, OH 45208
United States

Phone: (513) 551-8641

Email Address: houserob61@gmail.com

Date of Submission: Mon, 01/02/2023 - 18:12

Submission Letter:

I'm writing to you today in hopes that you will oppose the Brent Spence Bridge Corridor expansion project. The project won't solve the bridge's traffic and safety problems, but it will harm local communities, cost taxpayers billions of dollars, and only heightens our reliance on cars and fossil fuels.

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safety.

Mike Renz

916 Brayton Avenue

Wyoming, OH 45215
United States

Phone: (252) 675-9738

Email Address: renzmr@gmail.com

Date of Submission: Mon, 01/02/2023 - 11:08

Submission Letter:

I'm writing to you today in hopes that you will oppose the Brent Spence Bridge Corridor expansion project. The project won't solve the bridge's traffic and safety problems, but it will harm local communities, cost taxpayers billions of dollars, and only heightens our reliance on cars and fossil fuels.

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I am hoping that you will not support expanding the Brent Spence highway but rather invest in our communities, our transportation system, and our health and

safety.

Shawn Jenkins

315 Amazon Avenue

Cincinnati, OH 45220
United States

Phone: (513) 658-5238

Email Address: sjinx1@gmail.com

Date of Submission: Sun, 01/01/2023 - 20:37

Submission Letter:

I'm writing to you today in hopes that you will oppose the Brent Spence Bridge Corridor expansion project. The project won't solve the bridge's traffic and safety problems, but it will harm local communities, cost taxpayers billions of dollars, and only heightens our reliance on cars and fossil fuels.

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safety.

Ned KALAPASEV

15 HIGHVIEW DR

FT. WRIGHT, KY 41011
United States

Phone: (859) 653-0450

Email Address: nkalapasev@gmail.com

Date of Submission: Sun, 01/01/2023 - 20:22

Submission Letter:

I'm writing to you today in hopes that you will oppose the Brent Spence Bridge Corridor expansion project. The project won't solve the bridge's traffic and safety problems, but it will harm local communities, cost taxpayers billions of dollars, and only heightens our reliance on cars and fossil fuels.

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safety.

Joe Rosemeyer

5746 Glow Ct

Cincinnati, OH 45238
United States

Phone: (513) 375-7407

Email Address: jrrosemeyer@yahoo.com

Date of Submission: Sun, 01/01/2023 - 16:47

Submission Letter:

I'm writing to you today in hopes that you will oppose the Brent Spence Bridge Corridor expansion project. The project won't solve the bridge's traffic and safety problems, but it will harm local communities, cost taxpayers billions of dollars, and only heightens our reliance on cars and fossil fuels.

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safety.

Bo Mayfield

3221 Grischy Lane

Cincinnati, OH 45208
United States

Phone: (231) 487-0834

Email Address: slimy.bibs_0o@icloud.com

Date of Submission: Thu, 12/29/2022 - 16:02

Submission Letter:

I'm writing to you today in hopes that you will oppose the Brent Spence Bridge Corridor expansion project. The project won't solve the bridge's traffic and safety problems, but it will harm local communities, cost taxpayers billions of dollars, and only heightens our reliance on cars and fossil fuels.

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safety.

Camille Boggan

3396 Miljoie Drive

Cincinnati, OH 45244
United States

Phone: (513) 799-6362

Email Address: cmadeline166@gmail.com

Date of Submission: Thu, 12/29/2022 - 12:50

Submission Letter:

I'm writing to you today in hopes that you will oppose the Brent Spence Bridge Corridor expansion project. The project won't solve the bridge's traffic and safety problems, but it will harm local communities, cost taxpayers billions of dollars, and only heightens our reliance on cars and fossil fuels.

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safety.

Brendan Ryan

731 Wakefield Drive

Cincinnati, OH 45226
United States

Phone: (574) 400-3167

Email Address: brendan.ryan3@gmail.com

Date of Submission: Fri, 12/23/2022 - 20:50

Submission Letter:

I'm writing to you today in hopes that you will oppose the Brent Spence Bridge Corridor expansion project. The project won't solve the bridge's traffic and safety problems, but it will harm local communities, cost taxpayers billions of dollars, and only heightens our reliance on cars and fossil fuels.

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safety.

Gary Cox

1215 Morning Glory Dr

Amelia, OH 45102
United States

Phone: (513) 767-5838

Email Address: gcox70@hotmail.com

Date of Submission: Fri, 12/23/2022 - 14:31

Submission Letter:

I'm writing to you today in hopes that you will oppose the Brent Spence Bridge Corridor expansion project. The project won't solve the bridge's traffic and safety problems, but it will harm local communities, cost taxpayers billions of dollars, and only heightens our reliance on cars and fossil fuels.

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safety.

Taylor Kubik

52 15th Street

Newport, KY 41071
United States

Phone: (513) 693-6012

Email Address: taylor.kubik@gmail.com

Date of Submission: Tue, 12/20/2022 - 10:08

Submission Letter:

I'm writing to you today in hopes that you will oppose the Brent Spence Bridge Corridor expansion project. The project won't solve the bridge's traffic and safety problems, but it will harm local communities, cost taxpayers billions of dollars, and only heightens our reliance on cars and fossil fuels.

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safety.

Taylor Kubik

52 15th Street

Newport, KY 41071
United States

Phone: (513) 693-6012

Email Address: taylor.kubik@gmail.com

Date of Submission: Tue, 12/20/2022 - 10:03

Submission Letter:

I'm writing to you today in hopes that you will oppose the Brent Spence Bridge Corridor expansion project. The project won't solve the bridge's traffic and safety problems, but it will harm local communities, cost taxpayers billions of dollars, and only heightens our reliance on cars and fossil fuels.

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safety.

Cody Schumacher

319 5th Avenue

Dayton, KY 41074
United States

Phone: (859) 466-1427

Email Address: cody.rschumacher@gmail.com

Date of Submission: Tue, 12/20/2022 - 09:21

Submission Letter:

I'm writing to you today in hopes that you will oppose the Brent Spence Bridge Corridor expansion project. The project won't solve the bridge's traffic and safety problems, but it will harm local communities, cost taxpayers billions of dollars, and only heightens our reliance on cars and fossil fuels.

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safety.

Corey Toon

1009 Jackson Road, Apt 3

Park Hills, KY 41011
United States

Phone: (859) 652-5043

Email Address: coreytoon27@gmail.com

Date of Submission: Tue, 12/20/2022 - 08:53

Submission Letter:

I'm writing to you today in hopes that you will oppose the Brent Spence Bridge Corridor expansion project. The project won't solve the bridge's traffic and safety problems, but it will harm local communities, cost taxpayers billions of dollars, and only heightens our reliance on cars and fossil fuels.

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I am hoping that you will not support expanding the Brent Spence highway but rather invest in our communities, our transportation system, and our health and

Expand Transit not Highways

safety.

Lauren White

982 Paradrome Street

Cincinnati, OH 45202
United States

Phone: (513) 325-7693

Email Address: laurenmariewhite@gmail.com

Date of Submission: Mon, 12/19/2022 - 19:46

Submission Letter:

I'm writing to you today in hopes that you will oppose the Brent Spence Bridge Corridor expansion project. The project won't solve the bridge's traffic and safety problems, but it will harm local communities, cost taxpayers billions of dollars, and only heightens our reliance on cars and fossil fuels.

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safety.

brian ferry

4141 Floral Ave.

Cincinnati, OH 45212
United States

Phone: (513) 289-3630

Email Address: brian.ferry@newcitycincy.org

Date of Submission: Mon, 12/19/2022 - 12:12

Submission Letter:

I'm writing to you today in hopes that you will oppose the Brent Spence Bridge Corridor expansion project. The project won't solve the bridge's traffic and safety problems, but it will harm local communities, cost taxpayers billions of dollars, and only heightens our reliance on cars and fossil fuels.

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safety.

Karen Bemmes

4467 Leslie Ave

BLUE ASH, OH 45242
United States

Phone: (513) 519-3601

Email Address: pkbemmes@fuse.net

Date of Submission: Sun, 12/18/2022 - 15:25

Submission Letter:

I'm writing to you today in hopes that you will oppose the Brent Spence Bridge Corridor expansion project. The project won't solve the bridge's traffic and safety problems, but it will harm local communities, cost taxpayers billions of dollars, and only heightens our reliance on cars and fossil fuels.

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safety.

Dorothy Bush

1729 Westwood Ave

Cincinnati, OH 45214
United States

Phone: (513) 400-8669

Email Address: dorothy.a.bush@gmail.com

Date of Submission: Sun, 12/18/2022 - 11:50

Submission Letter:

I'm writing to you today in hopes that you will oppose the Brent Spence Bridge Corridor expansion project. The project won't solve the bridge's traffic and safety problems, but it will harm local communities, cost taxpayers billions of dollars, and only heightens our reliance on cars and fossil fuels.

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safety.

Robert Otis

220 Jefferson Avenue

Cincinnati, OH 45217
United States

Phone: (616) 283-3218

Email Address: nolotis@gmail.com

Date of Submission: Sun, 12/18/2022 - 11:45

Submission Letter:

I'm writing to you today in hopes that you will oppose the Brent Spence Bridge Corridor expansion project. The project won't solve the bridge's traffic and safety problems, but it will harm local communities, cost taxpayers billions of dollars, and only heightens our reliance on cars and fossil fuels.

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safety.

Karly Otis

220 Jefferson Ave

St. Bernard, OH 45217
United States

Phone: (843) 870-6530

Email Address: karlyotis@gmail.com

Date of Submission: Sun, 12/18/2022 - 11:39

Submission Letter:

I'm writing to you today in hopes that you will oppose the Brent Spence Bridge Corridor expansion project. The project won't solve the bridge's traffic and safety problems, but it will harm local communities, cost taxpayers billions of dollars, and only heightens our reliance on cars and fossil fuels.

Highway expansion projects harm already vulnerable populations. This expansion project will promulgate plumes of dust and fumes, worsening air pollution and health outcomes in nearby communities in the long term while interrupting people's everyday lives in the short term. After months of negotiation, Covington residents are still giving up over a dozen properties to the state to build this highway. And despite all of this, the project likely will not even solve the problems it is setting out to fix. Research strongly suggests that expanding highway capacity attracts more vehicles and increases traffic, the reverse effect of what KYTC's proposed plan is supposed to achieve.

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We cannot remain fully dependent on cars as transportation. As we have seen recently, the financial precariousness of car ownership places a mounting burden on American families. Investing in better public transit systems will reduce traffic congestion, reduce the cost to taxpayers while seeing a more beneficial use of their money, and improve the quality of life in neighboring communities.

I am hoping that you will not support expanding the Brent Spence highway but rather invest in our communities, our transportation system, and our health and

safety.

Stephanie West

870 Clark St., M

Cincinnati, OH 45203
United States

Phone: (513) 208-3914

Email Address: bluedlphnsteph75@gmail.com

Date of Submission: Sat, 12/17/2022 - 20:37

Submission Letter:

I'm writing to you today in hopes that you will oppose the Brent Spence Bridge Corridor expansion project. The project won't solve the bridge's traffic and safety problems, but it will harm local communities, cost taxpayers billions of dollars, and only heightens our reliance on cars and fossil fuels.

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I am hoping that you will not support expanding the Brent Spence highway but rather invest in our communities, our transportation system, and our health and

safety.

Amy Leibrock

332 Tusculum Ave

Cincinnati, OH 45226
United States

Phone: (917) 916-8994

Email Address: aleibrock@gmail.com

Date of Submission: Sat, 12/17/2022 - 14:47

Submission Letter:

I'm writing to you today in hopes that you will oppose the Brent Spence Bridge Corridor expansion project. The project won't solve the bridge's traffic and safety problems, but it will harm local communities, cost taxpayers billions of dollars, and only heightens our reliance on cars and fossil fuels.

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I am hoping that you will not support expanding the Brent Spence highway but rather invest in our communities, our transportation system, and our health and

safety.

Jim Caskey

121 W., Ninth Street

Cincinnati, OH 45202
United States

Phone: (513) 608-0366

Email Address: jimcaskey43@gmail.com

Date of Submission: Sat, 12/17/2022 - 12:24

Submission Letter:

I'm writing to you today in hopes that you will oppose the Brent Spence Bridge Corridor expansion project. The project won't solve the bridge's traffic and safety problems, but it will harm local communities, cost taxpayers billions of dollars, and only heightens our reliance on cars and fossil fuels.

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I am hoping that you will not support expanding the Brent Spence highway but rather invest in our communities, our transportation system, and our health and

safety.

Mackenzie Kirkman

4115 Forest Avenue, 1, 1

Cincinnati, OH 45212
United States

Phone: (630) 201-3799

Email Address: mackenzie.kirkman@gmail.com

Date of Submission: Sat, 12/17/2022 - 11:32

Submission Letter:

I'm writing to you today in hopes that you will oppose the Brent Spence Bridge Corridor expansion project. The project won't solve the bridge's traffic and safety problems, but it will harm local communities, cost taxpayers billions of dollars, and only heightens our reliance on cars and fossil fuels.

Highway expansion projects harm already vulnerable populations. This expansion project will promulgate plumes of dust and fumes, worsening air pollution and health outcomes in nearby communities in the long term while interrupting people's everyday lives in the short term. After months of negotiation, Covington residents are still giving up over a dozen properties to the state to build this highway. And despite all of this, the project likely will not even solve the problems it is setting out to fix. Research strongly suggests that expanding highway capacity attracts more vehicles and increases traffic, the reverse effect of what KYTC's proposed plan is supposed to achieve.

Our region carries the burden from hundreds of trucks crossing the bridge corridor daily, but most of the freight crossing the bridge is neither made here nor delivered here. The chronic truck congestion on the bridge makes it extremely unsafe. Rather than spend billions on an un-aesthetic, unnecessary highway, Kentucky and Ohio should be investing in the livelihoods of its residents. Funding should be shifted to expanding transit options and multi-modal transportation projects while reducing non-local truck traffic. We should be reducing air pollution and greenhouse gas emissions, not creating opportunities to worsen them.

We cannot remain fully dependent on cars as transportation. As we have seen recently, the financial precariousness of car ownership places a mounting burden on American families. Investing in better public transit systems will reduce traffic congestion, reduce the cost to taxpayers while seeing a more beneficial use of their money, and improve the quality of life in neighboring communities.

I am hoping that you will not support expanding the Brent Spence highway but rather invest in our communities, our transportation system, and our health and

safety.

Ben Stockwell

4242 Florida Ave

Cincinnati, OH 45223
United States

Phone: (513) 375-3187

Email Address: stockwellbm@gmail.com

Date of Submission: Sat, 12/17/2022 - 10:11

Submission Letter:

I'm writing to you today in hopes that you will oppose the Brent Spence Bridge Corridor expansion project. The project won't solve the bridge's traffic and safety problems, but it will harm local communities, cost taxpayers billions of dollars, and only heightens our reliance on cars and fossil fuels.

Highway expansion projects harm already vulnerable populations. This expansion project will promulgate plumes of dust and fumes, worsening air pollution and health outcomes in nearby communities in the long term while interrupting people's everyday lives in the short term. After months of negotiation, Covington residents are still giving up over a dozen properties to the state to build this highway. And despite all of this, the project likely will not even solve the problems it is setting out to fix. Research strongly suggests that expanding highway capacity attracts more vehicles and increases traffic, the reverse effect of what KYTC's proposed plan is supposed to achieve.

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We cannot remain fully dependent on cars as transportation. As we have seen recently, the financial precariousness of car ownership places a mounting burden on American families. Investing in better public transit systems will reduce traffic congestion, reduce the cost to taxpayers while seeing a more beneficial use of their money, and improve the quality of life in neighboring communities.

I am hoping that you will not support expanding the Brent Spence highway but rather invest in our communities, our transportation system, and our health and

safety.

Nancy Harward

10012 Zig Zag Road

Montgomery, OH 45242
United States

Phone: (513) 793-5006

Email Address: nancy@mandnharward.com

Date of Submission: Sat, 12/17/2022 - 09:41

Submission Letter:

I'm writing in the hope that you will reconsider the Brent Spence Bridge Corridor expansion project. It's likely that the project won't solve the bridge's traffic and safety problems, but rather exacerbate those problems by increasing commercial truck traffic and heightening our reliance on cars and fossil fuels.

After months of negotiation, Covington residents are relinquishing more than a dozen properties to the state to build this highway, yet the project likely will not even solve the problems it is setting out to fix. Research strongly suggests that expanding highway capacity attracts more vehicles and increases traffic, the reverse effect of what KYTC's proposed plan is supposed to achieve. Moreover, highway expansion projects harm already vulnerable populations. This project will promulgate plumes of dust and fumes, increasing air pollution and exacerbating related health problems in nearby communities.

Our region carries the burden from hundreds of trucks crossing the bridge corridor daily, but most of the freight crossing the bridge is neither made here nor delivered here. Chronic truck congestion makes the bridge extremely unsafe. Rather than spend billions on an unaesthetic, unnecessary highway, Kentucky and Ohio should be investing in the livelihoods of its residents. Funding should be shifted to expanding transit options and multi-modal transportation projects while reducing non-local truck traffic. We should be seeking to reduce air pollution and greenhouse-gas emissions, not creating opportunities to make them worse.

We cannot remain fully dependent on cars as transportation. As we have seen recently, the financial demands of car ownership places a mounting and often unpredictable burden on American families. Investment in better public transit systems reduces traffic congestion and improves the quality of life in the communities served by such systems. Improved public transit in the Cincinnati area would be a better use of taxpayer money than an expanded Brent Spence Bridge highway system.

I hope that instead of plowing ahead with the bridge corridor expansion project,

you will consider redirecting resources toward improved public and commercial transportation systems, and support for more initiatives focusing on health, safety, and affordable housing.

Trevor LaBree

332 E Ninth St

Newport, KY 41071
United States

Phone: (859) 750-6274

Email Address: labree.trevor@yahoo.com

Date of Submission: Sat, 12/17/2022 - 09:06

Submission Letter:

I'm writing to you today in hopes that you will oppose the Brent Spence Bridge Corridor expansion project. The project won't solve the bridge's traffic and safety problems, but it will harm local communities, cost taxpayers billions of dollars, and only heightens our reliance on cars and fossil fuels.

Highway expansion projects harm already vulnerable populations. This expansion project will promulgate plumes of dust and fumes, worsening air pollution and health outcomes in nearby communities in the long term while interrupting people's everyday lives in the short term. After months of negotiation, Covington residents are still giving up over a dozen properties to the state to build this highway. And despite all of this, the project likely will not even solve the problems it is setting out to fix. Research strongly suggests that expanding highway capacity attracts more vehicles and increases traffic, the reverse effect of what KYTC's proposed plan is supposed to achieve.

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We cannot remain fully dependent on cars as transportation. As we have seen recently, the financial precariousness of car ownership places a mounting burden on American families. Investing in better public transit systems will reduce traffic congestion, reduce the cost to taxpayers while seeing a more beneficial use of their money, and improve the quality of life in neighboring communities.

I am hoping that you will not support expanding the Brent Spence highway but rather invest in our communities, our transportation system, and our health and

safety.

Kevin Shaw

634 Sycamore Street

Cincinnati, OH 45202
United States

Phone: (513) 722-6239

Email Address: kshaw229@gmail.com

Date of Submission: Fri, 12/16/2022 - 23:48

Submission Letter:
Good evening,

I'm writing to you today in hopes that you will consider the thoughtful opposition to the Brent Spence Bridge corridor expansion project. I have not been convinced by the presented evidence that this project will solve any traffic or safety problems, but the project almost certainly will cost billions of dollars and further decrease the city's ability to meaningfully expand and connect my downtown neighborhood to other nearby neighborhoods.

As a downtown resident, I want to live in a lively community, not a modified highway on-ramp. The existing interstates surrounding downtown already choke its viability to expand, create a need for parking infrastructure that decreases the vibrancy of downtown whether it's a surface lot or a large parking structure, and make it so that many drivers see downtown as only a place to speed to and through. The Brent Spence Bridge corridor expansion does not improve the safety or connectivity to and around downtown, but makes it worse, at a time when the consensus across the country is that expanding highways does not reduce congestion but does increase emissions on our most vulnerable populations.

My partner crosses this corridor daily for work and I cross it regularly as a result. She is able to time her commute and experiences minimal delays, and she would love to be less car reliant via transit. I am currently car-free and so if I want to visit her workplace on that side of the river, the transit options routinely take 3-4x the amount of time that driving would take. A minute of delay on the BSB during rush hour is getting potentially 3 billion dollars, but there is little to no attention and funding paid to the 30 minutes in waiting alone that most transit riders experience to make any trip in our region, much less one through this corridor. Expanding transit options would make it more feasible for my family to continue to reduce our car reliance and allow residents who are unable to drive for a variety of reasons to better access this corridor, with a side benefit of actually reducing the minimal congestion that exists today.

It is late 2022 and there is not a viable plan to reduce our car-dependency as a region moving forward. Car ownership is expensive for individuals, requires expensive infrastructure to maintain for our local governments, and is costly to our climate and worsening health outcomes. Our region needs to reconsider transit and bike access, recreate and reconnect communities that are less car dependent, and reduce our vehicle miles traveled in personal vehicles by taking shorter and fewer trips. Investing in public transit will help us reach climate goals, improve quality of life throughout the region, and reduce the long-term cost for taxpayers in infrastructure and climate liabilities.

I am hoping that you will not support highway expansion in the next decade, especially the Brent Spence highway corridor, and will consider investment in alternative options that will make my community more pleasant for everyone, including individuals who cannot or do not want to drive. At minimum, I am hoping that you will take a hard look at whether the proposed plan is the best path forward for our region. As part of that, please consider attempting to make one of these local trips across the river as a transit rider and reflect on whether that experience could benefit more from billions of dollars in investment than a current trip via car or truck. I believe that we can do better with these proposed funds and I hope you will work to consider those options. Thank you for your consideration.

Andrea Dale

2730 Atlantic Avenue

Cincinnati, OH 45209
United States

Phone: (513) 673-6558

Email Address: zencuppa@gmail.com

Date of Submission: Fri, 12/16/2022 - 19:06

Submission Letter:

I'm writing to you today in hopes that you will oppose the Brent Spence Bridge Corridor expansion project. The project won't solve the bridge's traffic and safety problems, but it will harm local communities, cost taxpayers billions of dollars, and only heightens our reliance on cars and fossil fuels.

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I am hoping that you will not support expanding the Brent Spence highway but rather invest in our communities, our transportation system, and our health and

safety.

Tim Rogers@fuse.net

72 Mound Avenue

Milford, OH 45150
United States

Phone: (513) 258-6992

Email Address: timrogers@fuse.net

Date of Submission: Fri, 12/16/2022 - 16:29

Submission Letter:

I'm writing to you today in hopes that you will oppose the Brent Spence Bridge Corridor expansion project. The project won't solve the bridge's traffic and safety problems, but it will harm local communities, cost taxpayers billions of dollars, and only heightens our reliance on cars and fossil fuels.

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safety.

Lauren Sprague

4981 Relleum Avenue

Cincinnati, OH 45238
United States

Phone: (513) 508-4789

Email Address: laurenlouisesprague@gmail.com

Date of Submission: Fri, 12/16/2022 - 16:20

Submission Letter:

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safety.

Olivia Niles

212 E. Liberty St.

Cincinnati, OH 45202
United States

Phone: (513) 808-8543

Email Address: livn11@yahoo.com

Date of Submission: Fri, 12/16/2022 - 15:30

Submission Letter:

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I am hoping that you will not support expanding the Brent Spence highway but rather invest in our communities, our transportation system, and our health and

safety.

Jason Matheny

3480 Evanston Ave

Cincinnati, OH 45207
United States

Phone: (513) 324-3374

Email Address: jason.r.matheny@gmail.com

Date of Submission: Fri, 12/16/2022 - 15:27

Submission Letter:

I'm writing to you today in hopes that you will oppose the Brent Spence Bridge Corridor expansion project. The project won't solve the bridge's traffic and safety problems, but it will harm local communities, cost taxpayers billions of dollars, and only heightens our reliance on cars and fossil fuels.

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I am hoping that you will not support expanding the Brent Spence highway but rather invest in our communities, our transportation system, and our health and

safety.

Travis Speice

334 Lafayette Ave

Bellevue, KY 41073
United States

Phone: (607) 760-9495

Email Address: tspeice17@gmail.com

Date of Submission: Fri, 12/16/2022 - 15:06

Submission Letter:

I'm writing to you today in hopes that you will oppose the Brent Spence Bridge Corridor expansion project. The project won't solve the bridge's traffic and safety problems, but it will harm local communities, cost taxpayers billions of dollars, and only heightens our reliance on cars and fossil fuels.

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I am hoping that you will not support expanding the Brent Spence highway but rather invest in our communities, our transportation system, and our health and

safety.

Deborah Effler

10178 Winstead Lane

Cincinnati, OH 45231
United States

Phone: (513) 967-6835

Email Address: efflerdeb@gmail.com

Date of Submission: Fri, 12/16/2022 - 14:45

Submission Letter:

I'm writing to you today in hopes that you will oppose the Brent Spence Bridge Corridor expansion project. The project won't solve the bridge's traffic and safety problems, but it will harm local communities, cost taxpayers billions of dollars, and only heightens our reliance on cars and fossil fuels.

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I am hoping that you will not support expanding the Brent Spence highway but rather invest in our communities, our transportation system, and our health and

safety.

Matthew Jent

4807 Beech St.

Cincinnati, OH 45212
United States

Phone: (513) 432-4873

Email Address: mattjent96@gmail.com

Date of Submission: Fri, 12/16/2022 - 14:45

Submission Letter:

I'm writing to you today in hopes that you will oppose the Brent Spence Bridge Corridor expansion project. The project won't solve the bridge's traffic and safety problems, but it will harm local communities, cost taxpayers billions of dollars, and only heightens our reliance on cars and fossil fuels.

Highway expansion projects harm already vulnerable populations. This expansion project will promulgate plumes of dust and fumes, worsening air pollution and health outcomes in nearby communities in the long term while interrupting people's everyday lives in the short term. After months of negotiation, Covington residents are still giving up over a dozen properties to the state to build this highway. And despite all of this, the project likely will not even solve the problems it is setting out to fix. Research strongly suggests that expanding highway capacity attracts more vehicles and increases traffic, the reverse effect of what KYTC's proposed plan is supposed to achieve.

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I am hoping that you will not support expanding the Brent Spence highway but rather invest in our communities, our transportation system, and our health and

safety.

Nazeena Alvi

7721 Glenover Dr

Cincinnati, OH 45236-2145
United States

Phone: (513) 748-0746

Email Address: nkalvi@msn.com

Date of Submission: Fri, 12/16/2022 - 14:09

Submission Letter:

I'm writing to you today in hopes that you will oppose the Brent Spence Bridge Corridor expansion project. The project won't solve the bridge's traffic and safety problems, but it will harm local communities, cost taxpayers billions of dollars, and only heightens our reliance on cars and fossil fuels.

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safety.

Shannon Healey

3604 W Diversey Ave

Chicago, IL 60647
United States

Phone: (513) 500-4215

Email Address: shannonchealey@gmail.com

Date of Submission: Fri, 12/16/2022 - 14:03

Submission Letter:

I'm writing to you today in hopes that you will oppose the Brent Spence Bridge Corridor expansion project. The project won't solve the bridge's traffic and safety problems, but it will harm local communities, cost taxpayers billions of dollars, and only heightens our reliance on cars and fossil fuels.

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safety.

Bella Amor

5745 Glengate Lane

Cincinnati, OH 45212
United States

Phone: (513) 518-3737

Email Address: bellaamor199@gmail.com

Date of Submission: Fri, 12/16/2022 - 13:48

Submission Letter:

I'm writing to you today in hopes that you will oppose the Brent Spence Bridge Corridor expansion project. The project won't solve the bridge's traffic and safety problems, but it will harm local communities, cost taxpayers billions of dollars, and only heightens our reliance on cars and fossil fuels.

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safety.

Brandon Poynter

17 David Ct

Newport, KY 41071
United States

Phone: (859) 351-6752

Email Address: redblazer1996@hotmail.com

Date of Submission: Fri, 12/16/2022 - 13:46

Submission Letter:

I'm writing to you today in hopes that you will oppose the Brent Spence Bridge Corridor expansion project. The project won't solve the bridge's traffic and safety problems, but it will harm local communities, cost taxpayers billions of dollars, and only heightens our reliance on cars and fossil fuels.

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safety.

Anita Bryant

8425 Holiday Hills Drive

Cincinnati, OH 45255
United States

Phone: (706) 892-6363

Email Address: takbryant@yahoo.com

Date of Submission: Fri, 12/16/2022 - 13:42

Submission Letter:

I'm writing to you today in hopes that you will oppose the Brent Spence Bridge Corridor expansion project. The project won't solve the bridge's traffic and safety problems, but it will harm local communities, cost taxpayers billions of dollars, and only heightens our reliance on cars and fossil fuels.

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safety.

Chelsea Phillips

2480 FAIRVIEW AVENUE, Apt 308

Cincinnati, OH 45219
United States

Phone: (513) 375-7328

Email Address: phillipsc96@gmail.com

Date of Submission: Fri, 12/16/2022 - 13:38

Submission Letter:

I'm writing to you today in hopes that you will oppose the Brent Spence Bridge Corridor expansion project. The project won't solve the bridge's traffic and safety problems, but it will harm local communities, cost taxpayers billions of dollars, and only heightens our reliance on cars and fossil fuels.

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safety.

David Borcharding

9058 Revere Run

West Chester, OH 45069
United States

Phone: (513) 257-4680

Email Address: gojiro42@gmail.com

Date of Submission: Fri, 12/16/2022 - 13:35

Submission Letter:

I'm writing to you today in hopes that you will oppose the Brent Spence Bridge Corridor expansion project. The project won't solve the bridge's traffic and safety problems, but it will harm local communities, cost taxpayers billions of dollars, and only heightens our reliance on cars and fossil fuels.

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safety.

Anne Sunyak

6529 Aspen Way

Cincinnati, OH 45224
United States

Phone: (917) 900-8904

Email Address: anne.sunyak@gmail.com

Date of Submission: Fri, 12/16/2022 - 13:34

Submission Letter:

I'm writing to you today in hopes that you will oppose the Brent Spence Bridge Corridor expansion project. The project won't solve the bridge's traffic and safety problems, but it will harm local communities, cost taxpayers billions of dollars, and only heightens our reliance on cars and fossil fuels.

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safety.

Breanna White

1943 Fairfax Avenu,Apt 2

Cincinnati, OH 45207
United States

Phone: (937) 509-1770

Email Address: breanna.n.white15@gmail.com

Date of Submission: Fri, 12/16/2022 - 13:26

Submission Letter:

I'm writing to you today in hopes that you will oppose the Brent Spence Bridge Corridor expansion project. The project won't solve the bridge's traffic and safety problems, but it will harm local communities, cost taxpayers billions of dollars, and only heightens our reliance on cars and fossil fuels.

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safety.

Marha Gronas

815 Race Street, 1

Cincinnati, OH 45202
United States

Phone: (513) 307-9954

Email Address: faucet_banyans_0m@icloud.com

Date of Submission: Fri, 12/16/2022 - 13:21

Submission Letter:

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safety.

Ryan Minnich

1863 Chase Avenue

Cincinnati, OH 45223
United States

Phone: (513) 276-6868

Email Address: minnicrs@mail.uc.edu

Date of Submission: Fri, 12/16/2022 - 13:19

Submission Letter:

I'm writing to you today in hopes that you will oppose the Brent Spence Bridge Corridor project is at its core a highway expansion project that maintains all of the same existing connections. I've been to two ODOT community outreach meetings and ODOT claims one of its goals is to reclaim land and reconnect neighborhoods, yet there are no proposed new overpasses or underpasses north of Ezzard Charles. In fact, most seem to be proposed for tear-down-rebuild. Camp Washington and the west end deserve better; they deserve a highway reduction and a street grid that gets stitched back together!

The project won't solve the bridge's traffic and safety problems, but it will harm local communities, cost taxpayers billions of dollars, and only heightens our reliance on cars and fossil fuels.

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I am hoping that you will not support expanding the Brent Spence highway but rather invest in our communities, our transportation system, and our health and safety.

Cath Rodriguez

5136 Montgomery Road

Cincinnati, OH 45212
United States

Phone: (513) 307-7950

Email Address: dthrone@protonmail.ch

Date of Submission: Fri, 12/16/2022 - 13:17

Submission Letter:

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I am hoping that you will not support expanding the Brent Spence highway but rather invest in our communities, our transportation system, and our health and

safety.

P.S. NO MORE SUBSIDIES FOR KILLER CARS IN CINCINNATI!!

Adam Wallace

350 Pleasant Hill Drive

Cincinnati, OH 45215
United States

Phone: (917) 834-6827

Email Address: adamcoreywallace@gmail.com

Date of Submission: Fri, 12/16/2022 - 13:13

Submission Letter:

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safety.

Samantha E Matthews

2 Euclid Street

Ludlow, KY 41016
United States

Phone: (513) 720-9497

Email Address: sm353609@gmail.com

Date of Submission: Fri, 12/16/2022 - 13:11

Submission Letter:

I'm writing to you today in hopes that you will oppose the Brent Spence Bridge Corridor expansion project. The project won't solve the bridge's traffic and safety problems, but it will harm local communities, cost taxpayers billions of dollars, and only heightens our reliance on cars and fossil fuels.

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safety.

Carolina de Salvo

846 Saratoga St

Newport, KY 41071
United States

Phone: (513) 923-8296

Email Address: carolinagsalvo@me.com

Date of Submission: Fri, 12/16/2022 - 13:05

Submission Letter:

I'm writing to you today in hopes that you will oppose the Brent Spence Bridge Corridor expansion project. The project won't solve the bridge's traffic and safety problems, but it will harm local communities, cost taxpayers billions of dollars, and only heightens our reliance on cars and fossil fuels.

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I am hoping that you will not support expanding the Brent Spence highway but rather invest in our communities, our transportation system, and our health and

safety.

Sheri Snider

222 Walnut St

Bellevue, KY 41073
United States

Phone: (859) 652-8045

Email Address: 08_elects.clog@icloud.com

Date of Submission: Fri, 12/16/2022 - 13:05

Submission Letter:

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I am hoping that you will not support expanding the Brent Spence highway but rather invest in our communities, our transportation system, and our health and

safety.

Kelly Blake

1701 Sycamore St, Apt 231

Cincinnati, OH 45202-8400
United States

Phone: (419) 429-9186

Email Address: kmblake1212@gmail.com

Date of Submission: Fri, 12/16/2022 - 13:00

Submission Letter:

I'm writing to you today in hopes that you will oppose the Brent Spence Bridge Corridor expansion project. The project won't solve the bridge's traffic and safety problems, but it will harm local communities, cost taxpayers billions of dollars, and only heightens our reliance on cars and fossil fuels.

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safety.

Ben Paulus

601 East Pete Rose Way

Cincinnati, OH 45202
United States

Phone: (440) 840-2005

Email Address: ben.paulus22@gmail.com

Date of Submission: Fri, 12/16/2022 - 13:00

Submission Letter:

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safety.

John Miller

4283 Dartmouth Drive

Cincinnati, OH 45217
United States

Phone: (513) 614-9609

Email Address: jonnymil@hotmail.com

Date of Submission: Fri, 12/16/2022 - 12:58

Submission Letter:

I'm writing to you today in hopes that you will oppose the Brent Spence Bridge Corridor expansion project. The project won't solve the bridge's traffic and safety problems, but it will harm local communities, cost taxpayers billions of dollars, and only heightens our reliance on cars and fossil fuels.

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safety.

William Halloran

5828 Montgomery Road

Cincinnati, OH 45212
United States

Phone: (513) 978-4812

Email Address: will.halloran01@gmail.com

Date of Submission: Thu, 12/15/2022 - 11:03

Submission Letter:

I'm writing to you today in hopes that you will oppose the Brent Spence Bridge Corridor expansion project. The project won't solve the bridge's traffic and safety problems, but it will harm local communities, cost taxpayers billions of dollars, and only heightens our reliance on cars and fossil fuels.

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safety.

Corinne Clark

5544 Hickory Ridge Lane

Cincinnati, OH 45239
United States

Phone: (513) 939-7651

Email Address: cori.clements1@gmail.com

Date of Submission: Thu, 12/15/2022 - 08:51

Submission Letter:

I'm writing to you today in hopes that you will oppose the Brent Spence Bridge Corridor expansion project. The project won't solve the bridge's traffic and safety problems, but it will harm local communities, cost taxpayers billions of dollars, and only heightens our reliance on cars and fossil fuels.

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safety.

Danny Caspi

4914 Cooper Road, 428615

Cincinnati, OH 45242
United States

Phone: (513) 657-9085

Email Address: danny.caspi@gmail.com

Date of Submission: Thu, 12/15/2022 - 02:23

Submission Letter:

I'm writing to you today in hopes that you will oppose the Brent Spence Bridge Corridor expansion project. The project won't solve the bridge's traffic and safety problems, but it will harm local communities, cost taxpayers billions of dollars, and only heightens our reliance on cars and fossil fuels.

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safety.

Allison Purdy

969 Paradrome Street Apt 202

Cincinnati, OH 45202
United States

Phone: (513) 725-5593

Email Address: allisonmpurdy@gmail.com

Date of Submission: Wed, 12/14/2022 - 22:20

Submission Letter:

I'm writing to you today in hopes that you will oppose the Brent Spence Bridge Corridor expansion project. The project won't solve the bridge's traffic and safety problems, but it will harm local communities, cost taxpayers billions of dollars, and only heightens our reliance on cars and fossil fuels.

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safety.

Hannah Benjamin

2023 Edgecliff Point

Cincinnati, OH 45206
United States

Phone: (931) 349-8476

Email Address: hannahbenj@live.com

Date of Submission: Wed, 12/14/2022 - 22:05

Submission Letter:

I'm writing to you today in hopes that you will oppose the Brent Spence Bridge Corridor expansion project. The project won't solve the bridge's traffic and safety problems, but it will harm local communities, cost taxpayers billions of dollars, and only heightens our reliance on cars and fossil fuels.

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safety.

Paul Slater

2226 Park Avenue, 3

Cincinnati, OH 45206
United States

Phone: (513) 978-3675

Email Address: pslater15@gmail.com

Date of Submission: Wed, 12/14/2022 - 21:22

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safety.

Shannon Henderson

224 East 8th Street, Apt 904

Cincinnati, OH 45202
United States

Phone: (614) 506-2841

Email Address: snhllama74@gmail.com

Date of Submission: Wed, 12/14/2022 - 21:09

Submission Letter:

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safety.

Carlie Burton

1403 Central Parkway, 200

Cincinnati, OH 45214
United States

Phone: (419) 389-7384

Email Address: shesarebel317@gmail.com

Date of Submission: Wed, 12/14/2022 - 20:32

Submission Letter:

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Lindsay Luebbering

2535 Ranchvale Drive

CINCINNATI, OH 45230
United States

Phone: (513) 319-3211

Email Address: lindsay.luebbering@gmail.com

Date of Submission: Wed, 12/14/2022 - 16:45

Submission Letter:

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safety.

Larry Busse

18 Woodlawn Avenue

Fort Mitchell, KY 41017
United States

Phone: (513) 549-4552

Email Address: ljbusse@gmail.com

Date of Submission: Wed, 12/14/2022 - 16:26

Submission Letter:

I'm writing to you today in hopes that you will oppose the Brent Spence Bridge Corridor expansion project. The project won't solve the bridge's traffic and safety problems, but it will harm local communities, cost taxpayers billions of dollars, and only heightens our reliance on cars and fossil fuels.

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Brandon Fuchs

1403 Loveland Madeira Road Apt. 8

Loveland, OH 45140
United States

Phone: (412) 735-5862

Email Address: bfuchs95@gmail.com

Date of Submission: Wed, 12/14/2022 - 14:30

Submission Letter:

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safety.

Justin Schafer

4303 Verne Avenue

Cincinnati, OH 45209
United States

Phone: (513) 378-6333

Email Address: Theschnoo@gmail.com

Date of Submission: Wed, 12/14/2022 - 14:21

Submission Letter:

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safety.

Luke Sillies

1820 Freeman Ave

Cincinnati, OH 45214
United States

Phone: (937) 750-7459

Email Address: lukesillies@gmail.com

Date of Submission: Wed, 12/14/2022 - 13:30

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I am hoping that you will not support expanding the Brent Spence highway but rather invest in our communities, our transportation system, and our health and

safety.

Andrew Baker

1934 Windhill Terrace

Cincinnati, OH 45255
United States

Phone: (513) 304-0144

Email Address: andrewbakerguy@gmail.com

Date of Submission: Wed, 12/14/2022 - 12:57

Submission Letter:

I'm writing to you today in hopes that you will oppose the Brent Spence Bridge Corridor expansion project. The project won't solve the bridge's traffic and safety problems, but it will harm local communities, cost taxpayers billions of dollars, and only heightens our reliance on cars and fossil fuels.

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safety.

Aaron Leonard

6128 Hamilton Avenue

Cincinnati, OH 45224
United States

Phone: (513) 578-4655

Email Address: aaronmileonard@gmail.com

Date of Submission: Wed, 12/14/2022 - 12:40

Submission Letter:

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safety.

Kevin Planic

2806 Woodburn Avenue Apt 401

Cincinnati, OH 45206
United States

Phone: (224) 944-1982

Email Address: kgplanic@gmail.com

Date of Submission: Wed, 12/14/2022 - 12:34

Submission Letter:

I'm writing to you today in hopes that you will oppose the Brent Spence Bridge Corridor expansion project. The project won't solve the bridge's traffic and safety problems, but it will harm local communities, cost taxpayers billions of dollars, and only heightens our reliance on cars and fossil fuels.

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safety.

Matthew Latawiec

2429 Fairview Ave Fl 1

Cincinnati, OH 45219
United States

Phone: (513) 602-1814

Email Address: latawiecmatthew@gmail.com

Date of Submission: Wed, 12/14/2022 - 11:50

Submission Letter:

I'm writing to you today in hopes that you will oppose the Brent Spence Bridge Corridor expansion project. The project won't solve the bridge's traffic and safety problems, but it will harm local communities, cost taxpayers billions of dollars, and only heightens our reliance on cars and fossil fuels.

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safety.

Patrick Burleson

1636 Philomena Ave

Cincinnati, OH 45223
United States

Phone: (513) 773-8922

Email Address: pburleson007@gmail.com

Date of Submission: Wed, 12/14/2022 - 11:43

Submission Letter:

I'm writing to you today in hopes that you will oppose the Brent Spence Bridge Corridor expansion project. The project won't solve the bridge's traffic and safety problems, but it will harm local communities, cost taxpayers billions of dollars, and only heightens our reliance on cars and fossil fuels.

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safety.

Sierra White

554 milton st

Cincinnati, OH 45202
United States

Phone: (270) 315-3924

Email Address: sierrateflcert@gmail.com

Date of Submission: Wed, 12/14/2022 - 11:12

Submission Letter:

I'm writing to you today in hopes that you will oppose the Brent Spence Bridge Corridor expansion project. The project won't solve the bridge's traffic and safety problems, but it will harm local communities, cost taxpayers billions of dollars, and only heightens our reliance on cars and fossil fuels.

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safety.

Bradley Thacker

5730 Saint Elmo Avenue

Cincinnati, OH 45224
United States

Phone: (859) 446-9845

Email Address: bradthacker31@gmail.com

Date of Submission: Wed, 12/14/2022 - 10:51

Submission Letter:

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safety.

Dylan Cameron

635 Kress Alley

Cincinnati, OH 45214
United States

Phone: (513) 288-2789

Email Address: camerondylan3@gmail.com

Date of Submission: Wed, 12/14/2022 - 10:26

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I'm writing to you today in hopes that you will oppose the Brent Spence Bridge Corridor expansion project. The project won't solve the bridge's traffic and safety problems, but it will harm local communities, cost taxpayers billions of dollars, and only heightens our reliance on cars and fossil fuels.

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safety.

Will Geisen

430 E 4th St

Newport, KY 41071
United States

Phone: (859) 663-5004

Email Address: will.geisen@gmail.com

Date of Submission: Wed, 12/14/2022 - 10:09

Submission Letter:

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safety.

Nicholas Temeles

2207 Eastern Avenue,

Covington, KY 41014
United States

Phone: (513) 903-7504

Email Address: n.temeles@gmail.com

Date of Submission: Wed, 12/14/2022 - 09:57

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Ronald Knotts

404 Reading Rd, Apt 301

Cincinnati, OH 45202
United States

Phone: (513) 225-2338

Email Address: rbrennanknotts@gmail.com

Date of Submission: Wed, 12/14/2022 - 09:41

Submission Letter:

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Anthony Isaacs

3619 Tamarack Avenue

Cincinnati, OH 45207
United States

Phone: (773) 431-9775

Email Address: isaacs.anthony@gmail.com

Date of Submission: Wed, 12/14/2022 - 09:27

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Richard Wendel

427 Delta Avenue

Cincinnati, OH 45226
United States

Phone: (513) 687-2257

Email Address: richard.wendel@hey.com

Date of Submission: Wed, 12/14/2022 - 09:15

Submission Letter:

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Cory Aldrich

15 W 4th St - Unit 203

Cincinnati, OH 45202
United States

Phone: (937) 478-6792

Email Address: coryaldrich@gmail.com

Date of Submission: Wed, 12/14/2022 - 09:10

Submission Letter:

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safety.

David Albert

1134 Maycliffe Place

Cincinnati, OH 45230
United States

Phone: (513) 761-7815

Email Address: albertdqcwp@gmail.com

Date of Submission: Wed, 12/14/2022 - 09:09

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Cory Aldrich

15 W 4th St - Unit 203

Cincinnati, OH 45140
United States

Phone: (937) 478-6792

Email Address: coryaldrich@gmail.com

Date of Submission: Wed, 12/14/2022 - 09:07

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Michael Guarente

7905 Locust Lane

Cincinnati, OH 45243
United States

Phone: (440) 225-6837

Email Address: michaelguarente@gmail.com

Date of Submission: Wed, 12/14/2022 - 09:05

Submission Letter:

I'm writing to you today in hopes that you will oppose the Brent Spence Bridge Corridor expansion project. The project won't solve the bridge's traffic and safety problems, but it will harm local communities, cost taxpayers billions of dollars, and only heightens our reliance on cars and fossil fuels.

Highway expansion projects harm already vulnerable populations. This expansion project will promulgate plumes of dust and fumes, worsening air pollution and health outcomes in nearby communities in the long term while interrupting people's everyday lives in the short term. After months of negotiation, Covington residents are still giving up over a dozen properties to the state to build this highway. And despite all of this, the project likely will not even solve the problems it is setting out to fix. Research strongly suggests that expanding highway capacity attracts more vehicles and increases traffic, the reverse effect of what KYTC's proposed plan is supposed to achieve.

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I am hoping that you will not support expanding the Brent Spence highway but rather invest in our communities, our transportation system, and our health and

safety.

Vincent Wilson

8722 Long Lane

Cincinnati, OH 45231
United States

Phone: (513) 306-1126

Email Address: vince@classyh.at

Date of Submission: Wed, 12/14/2022 - 08:32

Submission Letter:

I'm writing to you today in hopes that you will oppose the Brent Spence Bridge Corridor expansion project. The project won't solve the bridge's traffic and safety problems, but it will harm local communities, cost taxpayers billions of dollars, and only heightens our reliance on cars and fossil fuels.

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I am hoping that you will not support expanding the Brent Spence highway but rather invest in our communities, our transportation system, and our health and

safety.

Travis Gettys

915 MONROE ST

Newport, KY 41071
United States

Phone: (859) 750-0959

Email Address: wirecan@gmail.com

Date of Submission: Wed, 12/14/2022 - 08:08

Submission Letter:

I'm writing to you today in hopes that you will oppose the Brent Spence Bridge Corridor expansion project. The project won't solve the bridge's traffic and safety problems, but it will harm local communities, cost taxpayers billions of dollars, and only heightens our reliance on cars and fossil fuels.

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safety.

Daniel Streicher

407 Race Street

Cincinnati, OH 45202
United States

Phone: (859) 412-0220

Email Address: dstrike5@aol.com

Date of Submission: Wed, 12/14/2022 - 08:05

Submission Letter:

I'm writing to you today in hopes that you will oppose the Brent Spence Bridge Corridor expansion project. The project won't solve the bridge's traffic and safety problems, but it will harm local communities, cost taxpayers billions of dollars, and only heightens our reliance on cars and fossil fuels.

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safety.

Mitchell Mustain

1705 Pleasant Street

Cincinnati, OH 45202
United States

Phone: (567) 674-0588

Email Address: mitchell_mustain@yahoo.com

Date of Submission: Wed, 12/14/2022 - 08:05

Submission Letter:

I'm writing to you today in hopes that you will oppose the Brent Spence Bridge Corridor expansion project. The project won't solve the bridge's traffic and safety problems, but it will harm local communities, cost taxpayers billions of dollars, and only heightens our reliance on cars and fossil fuels.

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safety.

Evan Sasowsky

337 Thrall Street

Cincinnati, OH 45220
United States

Phone: (330) 819-0100

Email Address: evansasowsky@gmail.com

Date of Submission: Mon, 12/12/2022 - 11:56

Submission Letter:

I'm writing to you today in hopes that you will oppose the Brent Spence Bridge Corridor expansion project. The project won't solve the bridge's traffic and safety problems, but it will harm local communities, cost taxpayers billions of dollars, and only heightens our reliance on cars and fossil fuels.

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safety.

Otto Weaver

5322 Globe Avenue

Cincinnati, OH 45212
United States

Phone: (513) 279-8430

Email Address: bottoweaver@gmail.com

Date of Submission: Wed, 12/07/2022 - 17:14

Submission Letter:

I'm writing to you today in hopes that you will oppose the Brent Spence Bridge Corridor expansion project. The project won't solve the bridge's traffic and safety problems, but it will harm local communities, cost taxpayers billions of dollars, and only heightens our reliance on cars and fossil fuels.

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safety.

Justin Ogilby

3788 Clifton Avenue

Cincinnati, OH 45220
United States

Phone: (513) 545-4377

Email Address: jogilby@gmail.com

Date of Submission: Mon, 12/05/2022 - 18:54

Submission Letter:

I'm writing to you today in hopes that you will oppose the Brent Spence Bridge Corridor expansion project. The project won't solve the bridge's traffic and safety problems, but it will harm local communities, cost taxpayers billions of dollars, and only heightens our reliance on cars and fossil fuels.

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safety.

Mykala Hayes

722 E McMillan St, Apt 8

Cincinnati, OH 45206
United States

Phone: (410) 818-4589

Email Address: mykalahayes@gmail.com

Date of Submission: Mon, 12/05/2022 - 13:30

Submission Letter:

---PLEASE READ---

I'm writing to you today in hopes that you will oppose the Brent Spence Bridge Corridor expansion project. The project won't solve the bridge's traffic and safety problems, but it will harm local communities, cost taxpayers billions of dollars, and only heightens our reliance on cars and fossil fuels.

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rather invest in our communities, our transportation system, and our health and safety.

Angela Cummings

2435 Concord Street

Cincinnati, OH 45206
United States

Phone: (580) 484-2466

Email Address: angelacummings38@gmail.com

Date of Submission: Sun, 12/04/2022 - 13:03

Submission Letter:

I'm writing to you today in hopes that you will oppose the Brent Spence Bridge Corridor expansion project. The project won't solve the bridge's traffic and safety problems, but it will harm local communities, cost taxpayers billions of dollars, and only heightens our reliance on cars and fossil fuels.

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safety.

Jonathan Shepard

345 E 7th St, #1214

Cincinnati, OH 45202
United States

Phone: (513) 470-0541

Email Address: shepard.jb@gmail.com

Date of Submission: Sat, 12/03/2022 - 13:19

Submission Letter:

I'm writing to you today in hopes that you will oppose the Brent Spence Bridge Corridor expansion project. The project won't solve the bridge's traffic and safety problems, but it will harm local communities, cost taxpayers billions of dollars, and only heightens our reliance on cars and fossil fuels.

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safety.

Julie Zimmerman

1019 Redway Avenue

Cincinnati, OH 45229
United States

Phone: (513) 543-1501

Email Address: juliezimm@proton.me

Date of Submission: Sat, 12/03/2022 - 07:45

Submission Letter:

I'm writing to you today in hopes that you will oppose the Brent Spence Bridge Corridor expansion project. The project won't solve the bridge's traffic and safety problems, but it will harm local communities, cost taxpayers billions of dollars, and only heightens our reliance on cars and fossil fuels.

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safety.

Isaiah Hein

303 North Charity Street

Bethel, OH 45106
United States

Phone: (513) 310-5199

Email Address: isaiahhein55@gmail.com

Date of Submission: Fri, 12/02/2022 - 23:46

Submission Letter:

I'm writing to you today in hopes that you will oppose the Brent Spence Bridge Corridor expansion project. The project won't solve the bridge's traffic and safety problems, but it will harm local communities, cost taxpayers billions of dollars, and only heightens our reliance on cars and fossil fuels.

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safety.

Chaim Tillinghast

3232 Ferncroft Drive

Cincinnati, OH 45211
United States

Phone: (513) 301-4847

Email Address: chaimtillinghast@outlook.com

Date of Submission: Fri, 12/02/2022 - 23:33

Submission Letter:

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safety.

Elizabeth Fisher-Smith

412 Liberty Hill

Cincinnati, OH 45202
United States

Phone: (513) 258-9766

Email Address: efishsmith@hotmail.com

Date of Submission: Fri, 12/02/2022 - 15:31

Submission Letter:

I'm writing to you today in hopes that you will oppose the Brent Spence Bridge Corridor expansion project. The project won't solve the bridge's traffic and safety problems, but it will harm local communities, cost taxpayers billions of dollars, and only heightens our reliance on cars and fossil fuels.

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safety.

Stephanie Landry

6012 Mayflower Avenue

Cincinnati, OH 45237
United States

Phone: (513) 889-9176

Email Address: s.landrydesign@gmail.com

Date of Submission: Fri, 12/02/2022 - 11:57

Submission Letter:

I'm writing to you today in hopes that you will oppose the Brent Spence Bridge Corridor expansion project. The project won't solve the bridge's traffic and safety problems, but it will harm local communities, cost taxpayers billions of dollars, and only heightens our reliance on cars and fossil fuels.

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I am hoping that you will not support expanding the Brent Spence highway but rather invest in our communities, our transportation system, and our health and

safety.

Thanks for considering this request for a better system for our city.

Katilyn Kusnier

208 Mulberry St.

Cincinnati, OH 45202
United States

Phone: (419) 966-1245

Email Address: katikusnier1@gmail.com

Date of Submission: Fri, 12/02/2022 - 10:30

Submission Letter:

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I am hoping that you will not support expanding the Brent Spence highway but rather invest in our communities, our transportation system, and our health and

safety.

Timothy Menkhaus

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Cincinnati, OH 45219
United States

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Email Address: tmenkhaus@yahoo.com

Date of Submission: Fri, 12/02/2022 - 00:43

Submission Letter:

I'm writing to you today in hopes that you will oppose the Brent Spence Bridge Corridor expansion project. The project won't solve the bridge's traffic and safety problems, but it will harm local communities, cost taxpayers billions of dollars, and only heightens our reliance on cars and fossil fuels.

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safety.

Christina Lorton

7964 Burgundy Ln

Cincinnati, OH 45224
United States

Phone: (513) 617-9875

Email Address: christina_lorton@yahoo.com

Date of Submission: Thu, 12/01/2022 - 21:56

Submission Letter:

I'm writing to you today in hopes that you will oppose the Brent Spence Bridge Corridor expansion project. The project won't solve the bridge's traffic and safety problems, but it will harm local communities, cost taxpayers billions of dollars, and only heightens our reliance on cars and fossil fuels.

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safety.

Blair Traylor

528 East 3rd Street

Newport, KY 41071
United States

Phone: (859) 547-7072

Email Address: traylor.rb@gmail.com

Date of Submission: Thu, 12/01/2022 - 21:12

Submission Letter:

I'm writing to you today in hopes that you will oppose the Brent Spence Bridge Corridor expansion project. The project won't solve the bridge's traffic and safety problems, but it will harm local communities, cost taxpayers billions of dollars, and only heightens our reliance on cars and fossil fuels.

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safety.

Brian Simons

2517 Indian Mound Avenue

Cincinnati, OH 45212
United States

Phone: (423) 827-3590

Email Address: brian13simons@gmail.com

Date of Submission: Thu, 12/01/2022 - 19:57

Submission Letter:

I'm writing to you today in hopes that you will oppose the Brent Spence Bridge Corridor expansion project. The project won't solve the bridge's traffic and safety problems, but it will harm local communities, cost taxpayers billions of dollars, and only heightens our reliance on cars and fossil fuels.

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safety.

Brittany Brandenburg

4220 Virginia Avenue

Cincinnati, OH 45223
United States

Phone: (513) 906-0623

Email Address: bnbrandenburg91@gmail.com

Date of Submission: Thu, 12/01/2022 - 19:09

Submission Letter:

I'm writing to you today in hopes that you will oppose the Brent Spence Bridge Corridor expansion project. The project won't solve the bridge's traffic and safety problems, but it will harm local communities, cost taxpayers billions of dollars, and only heightens our reliance on cars and fossil fuels.

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safety.

Paul Gomez

7 Mercer Street

Cincinnati, OH 45202
United States

Phone: (513) 417-4826

Email Address: c5gatoruf@aol.com

Date of Submission: Thu, 12/01/2022 - 17:36

Submission Letter:

I'm writing to you today in hopes that you will oppose the Brent Spence Bridge Corridor expansion project. The project won't solve the bridge's traffic and safety problems, but it will harm local communities, cost taxpayers billions of dollars, and only heightens our reliance on cars and fossil fuels.

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safety.

Molly Traylor

528 E. 3rd Street

Newport, KY 41071
United States

Phone: (859) 394-3337

Email Address: molltraylor@gmail.com

Date of Submission: Thu, 12/01/2022 - 17:28

Submission Letter:

I'm writing to you today in hopes that you will oppose the Brent Spence Bridge Corridor expansion project. The project won't solve the bridge's traffic and safety problems, but it will harm local communities, cost taxpayers billions of dollars, and only heightens our reliance on cars and fossil fuels.

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safety.

Kelly Risher

4217 Fergus St

Cincinnati, OH 45223
United States

Phone: (513) 641-8745

Email Address: apoq79@gmail.com

Date of Submission: Thu, 12/01/2022 - 17:28

Submission Letter:

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safety.

Brett Hungerford

3581 Pape Avenue

Cincinnati, OH 45208
United States

Phone: (513) 658-3751

Email Address: bretthungerford@gmail.com

Date of Submission: Thu, 12/01/2022 - 16:17

Submission Letter:

I'm writing to you today in hopes that you will oppose the Brent Spence Bridge Corridor expansion project. The project won't solve the bridge's traffic and safety problems, but it will harm local communities, cost taxpayers billions of dollars, and only heightens our reliance on cars and fossil fuels.

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safety.

maria seda-reeder

1950 Washburn Street

Cincinnati, OH 45223
United States

Phone: (513) 519-5767

Email Address: msedareeder@gmail.com

Date of Submission: Thu, 12/01/2022 - 14:45

Submission Letter:

I'm writing to you today in hopes that you will oppose the Brent Spence Bridge Corridor expansion project. The project won't solve the bridge's traffic and safety problems, but it will harm local communities, cost taxpayers billions of dollars, and only heightens our reliance on cars and fossil fuels.

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safety.

Blake Dwyer

4960 Charlemar Drive

Cincinnati, OH 45227
United States

Phone: (513) 853-2237

Email Address: bdwyer31@gmail.com

Date of Submission: Thu, 12/01/2022 - 14:40

Submission Letter:

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Steven Prieto

112 E 14th St, Apt 1

Cincinnati, OH 45202
United States

Phone: (973) 626-4535

Email Address: prietoste@gmail.com

Date of Submission: Thu, 12/01/2022 - 14:26

Submission Letter:

I'm writing to you today in hopes that you will oppose the Brent Spence Bridge Corridor expansion project. The project won't solve the bridge's traffic and safety problems, but it will harm local communities, cost taxpayers billions of dollars, and only heightens our reliance on cars and fossil fuels.

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Khrystyna Melekh

1150 Vine Street

Cincinnati, OH 45202
United States

Phone: (513) 344-7250

Email Address: melech_christina@ukr.net

Date of Submission: Thu, 12/01/2022 - 14:21

Submission Letter:

I'm writing to you today in hopes that you will oppose the Brent Spence Bridge Corridor expansion project. The project won't solve the bridge's traffic and safety problems, but it will harm local communities, cost taxpayers billions of dollars, and only heightens our reliance on cars and fossil fuels.

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safety.

Rebecca Reilly

3738 Clifton Avenue

Cincinnati, OH 45220
United States

Phone: (513) 207-7624

Email Address: bstanley9@gmail.com

Date of Submission: Thu, 12/01/2022 - 13:54

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safety.

Kimberli Eddy

1616 Norcol Lane

Cincinnati, OH 45231
United States

Phone: (513) 604-9794

Email Address: advntrbabe@yahoo.com

Date of Submission: Thu, 12/01/2022 - 13:53

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Isabella Frueh

1766 Hanfield St

Cincinnati, OH 45223
United States

Phone: (513) 600-7422

Email Address: bellafrueh@gmail.com

Date of Submission: Thu, 12/01/2022 - 13:23

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Tsvetelina Georgiev

1262 Mills Pointe Ct

Batavia, OH 45103
United States

Phone: (513) 410-6873

Email Address: cvetelilio@yahoo.com

Date of Submission: Thu, 12/01/2022 - 13:15

Submission Letter:

I'm writing to you today in hopes that you will oppose the Brent Spence Bridge Corridor expansion project. The project won't solve the bridge's traffic and safety problems, but it will harm local communities, cost taxpayers billions of dollars, and only heightens our reliance on cars and fossil fuels.

Highway expansion projects harm already vulnerable populations. This expansion project will promulgate plumes of dust and fumes, worsening air pollution and health outcomes in nearby communities in the long term while interrupting people's everyday lives in the short term. After months of negotiation, Covington residents are still giving up over a dozen properties to the state to build this highway. And despite all of this, the project likely will not even solve the problems it is setting out to fix. Research strongly suggests that expanding highway capacity attracts more vehicles and increases traffic, the reverse effect of what KYTC's proposed plan is supposed to achieve.

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We cannot remain fully dependent on cars as transportation. As we have seen recently, the financial precariousness of car ownership places a mounting burden on American families. Investing in better public transit systems will reduce traffic congestion, reduce the cost to taxpayers while seeing a more beneficial use of their money, and improve the quality of life in neighboring communities.

I am hoping that you will not support expanding the Brent Spence highway but rather invest in our communities, our transportation system, and our health and

safety.

Chris Gauggel

902 Suire Ave

Cincinnati, OH 45205
United States

Phone: (513) 646-1463

Email Address: gauggel.ce@pg.com

Date of Submission: Thu, 12/01/2022 - 13:13

Submission Letter:

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safety.

Alec Enis

1919 Delaware Avenue

Cincinnati, OH 45212
United States

Phone: (513) 520-4611

Email Address: enis.alec@gmail.com

Date of Submission: Thu, 12/01/2022 - 13:09

Submission Letter:

I'm writing to you today in hopes that you will oppose the Brent Spence Bridge Corridor expansion project. The project won't solve the bridge's traffic and safety problems, but it will harm local communities, cost taxpayers billions of dollars, and only heightens our reliance on cars and fossil fuels.

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safety.

Jim Casey

1755 Wickham Pl

Cincinnati, OH 45214
United States

Phone: (937) 673-1766

Email Address: velocrazed07@yahoo.com

Date of Submission: Thu, 12/01/2022 - 13:00

Submission Letter:

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safety.

Haylee Giles

1308 Voll Rd

Cincinnati, OH 45230
United States

Phone: (513) 704-6210

Email Address: haylee282@gmail.com

Date of Submission: Thu, 12/01/2022 - 12:45

Submission Letter:

I'm writing to you today in hopes that you will oppose the Brent Spence Bridge Corridor expansion project. The project won't solve the bridge's traffic and safety problems, but it will harm local communities, cost taxpayers billions of dollars, and only heightens our reliance on cars and fossil fuels.

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safety.

Brian Goessling

832 Main St

Covington, KY 41011
United States

Phone: (859) 609-5829

Email Address: brian.goessling@gmail.com

Date of Submission: Thu, 12/01/2022 - 12:44

Submission Letter:

I'm writing to you today in hopes that you will oppose the Brent Spence Bridge Corridor expansion project. The project won't solve the bridge's traffic and safety problems, but it will harm local communities, cost taxpayers billions of dollars, and only heightens our reliance on cars and fossil fuels.

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safety.

Sierra White

554 milton st

Cincinnati, OH 45202
United States

Phone: (270) 315-3924

Email Address: sierrateflcert@gmail.com

Date of Submission: Thu, 12/01/2022 - 12:43

Submission Letter:

I'm writing to you today in hopes that you will oppose the Brent Spence Bridge Corridor expansion project. The project won't solve the bridge's traffic and safety problems, but it will harm local communities, cost taxpayers billions of dollars, and only heightens our reliance on cars and fossil fuels.

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safety.

Pat LaFleur

418 W. 9TH ST

COVINGTON, KY 41011
United States

Phone: (859) 322-7869

Email Address: PAT.F.LAFLEUR@GMAIL.COM

Date of Submission: Thu, 12/01/2022 - 12:42

Submission Letter:

I'm writing to you today in hopes that you will oppose the Brent Spence Bridge Corridor expansion project. The project won't solve the bridge's traffic and safety problems, but it will harm local communities, cost taxpayers billions of dollars, and only heightens our reliance on cars and fossil fuels.

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safety.

Jessica Laine

1026 Washington Ave

Newport, KY 41071
United States

Phone: (513) 290-3588

Email Address: Jesoverley@gmail.com

Date of Submission: Thu, 12/01/2022 - 12:27

Submission Letter:

I'm writing to you today in hopes that you will oppose the Brent Spence Bridge Corridor expansion project. The project won't solve the bridge's traffic and safety problems, but it will harm local communities, cost taxpayers billions of dollars, and only heightens our reliance on cars and fossil fuels.

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safety.

Angela Fossett

8160 Trabant Dr

Cincinnati, OH 45242
United States

Phone: (513) 290-8928

Email Address: brynnfossett@gmail.com

Date of Submission: Thu, 12/01/2022 - 12:27

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safety.

Molly Sammons

4416 Hubble Road

Cincinnati, OH 45247
United States

Phone: (513) 240-4866

Email Address: mmrumpke@gmail.com

Date of Submission: Thu, 12/01/2022 - 12:26

Submission Letter:

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Will Hack

22 S Campus Ave

Oxford, OH 45056
United States

Phone: (513) 289-1962

Email Address: hackwd@miamioh.edu

Date of Submission: Thu, 12/01/2022 - 12:16

Submission Letter:

I'm writing to you today in hopes that you will oppose the Brent Spence Bridge Corridor expansion project. The project won't solve the bridge's traffic and safety problems, but it will harm local communities, cost taxpayers billions of dollars, and only heightens our reliance on cars and fossil fuels.

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safety.

Matthew Day-Lindsey

6421 Sherman Avenue,

Cincinnati, OH 45230
United States

Phone: (513) 560-6919

Email Address: mlinds724@gmail.com

Date of Submission: Thu, 12/01/2022 - 11:59

Submission Letter:

I'm writing to you today in hopes that you will oppose the Brent Spence Bridge Corridor expansion project. The project won't solve the bridge's traffic and safety problems, but it will harm local communities, cost taxpayers billions of dollars, and only heightens our reliance on cars and fossil fuels.

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safety.

Holly Willard

515 Main Street

Covington, KY 41011
United States

Phone: (512) 966-3806

Email Address: holly14willard@gmail.com

Date of Submission: Thu, 12/01/2022 - 11:54

Submission Letter:

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Jocelyn Pettit

3818 Carlton St, Cincinnati

Cincinnati, OH 45227
United States

Phone: (513) 763-9844

Email Address: pettitjocelyn@gmail.com

Date of Submission: Thu, 12/01/2022 - 11:46

Submission Letter:

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safety.

Katie Thompson

3228 Ashwood Drive

Cincinnati, OH 45213
United States

Phone: (513) 884-1602

Email Address: katiesupermomthompson@gmail.com

Date of Submission: Thu, 12/01/2022 - 11:36

Submission Letter:

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safety.

Taylor Singleton

25 East Robbins Street

Covington, KY 41011
United States

Phone: (828) 550-7780

Email Address: tsing_97@yahoo.com

Date of Submission: Thu, 12/01/2022 - 11:35

Submission Letter:

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safety.

Melissa Soluski

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Cincinnati, OH 45209
United States

Phone: (928) 710-7845

Email Address: msoluski@gmail.com

Date of Submission: Thu, 12/01/2022 - 11:30

Submission Letter:

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safety.

Grace Hill

340 Warner Street

Cincinnati, OH 45219
United States

Phone: (513) 503-9456

Email Address: gracehill35@gmail.com

Date of Submission: Thu, 12/01/2022 - 11:29

Submission Letter:

I'm writing to you today in hopes that you will oppose the Brent Spence Bridge Corridor expansion project. The project won't solve the bridge's traffic and safety problems, but it will harm local communities, cost taxpayers billions of dollars, and only heightens our reliance on cars and fossil fuels.

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safety.

Margaret Lawlace

3025 Auten Avenue

Cincinnati, OH 45213
United States

Phone: (610) 585-8798

Email Address: mlawlace@gmail.com

Date of Submission: Thu, 12/01/2022 - 11:14

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safety.

Robert Wickens

8109 Southern Pines Drive

Maineville, OH 45039
United States

Phone: (513) 543-5833

Email Address: wickens1@aol.com

Date of Submission: Thu, 12/01/2022 - 11:12

Submission Letter:

I'm writing to you today in hopes that you will oppose the Brent Spence Bridge Corridor expansion project. The project won't solve the bridge's traffic and safety problems, but it will harm local communities, cost taxpayers billions of dollars, and only heightens our reliance on cars and fossil fuels.

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safety.

Holden Mathis

3810 Eileen Drive

Cincinnati, OH 45209
United States

Phone: (502) 619-9571

Email Address: hcm.mathis@gmail.com

Date of Submission: Thu, 12/01/2022 - 11:07

Submission Letter:

I'm writing to you today in hopes that you will oppose the Brent Spence Bridge Corridor expansion project. The project won't solve the bridge's traffic and safety problems, but it will harm local communities, cost taxpayers billions of dollars, and only heightens our reliance on cars and fossil fuels.

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Cincinnati, OH 45209
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Phone: (502) 619-9571

Email Address: hcm.mathis@gmail.com

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safety.

Albert Gustafson

409 Milton Street

Cincinnati, OH 45202
United States

Phone: (515) 720-0171

Email Address: albert.gustafson@gmail.com

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safety.

David Sherman

819 Dayton St.

Cincinnati, OH 45214
United States

Phone: (813) 270-8400

Email Address: dcs3939@gmail.com

Date of Submission: Thu, 12/01/2022 - 11:06

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David Carrero

106 Memorial Parkway

Newport, KY 41071
United States

Phone: (513) 628-2845

Email Address: carrerdm@gmail.com

Date of Submission: Thu, 12/01/2022 - 10:57

Submission Letter:

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Matthew Specter

7145 Baribill Place

Cincinnati, OH 45230
United States

Phone: (513) 808-2834

Email Address: mattspecter@gmail.com

Date of Submission: Thu, 12/01/2022 - 10:55

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Expand Transit not Highways

safety.

Agnes Kindberg

8947 Roan Lane

Cincinnati, OH 45242
United States

Phone: (513) 722-5198

Email Address: agneskindberg@gmail.com

Date of Submission: Thu, 12/01/2022 - 10:55

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Nancye Van Brunt

5027 South Ridge Drive

Cincinnati, OH 45224
United States

Phone: (513) 238-0723

Email Address: abbysoma01@gmail.com

Date of Submission: Thu, 12/01/2022 - 10:53

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Joe Cassidy

272 W. McMicken Ave

Cincinnati, OH 45214
United States

Phone: (334) 477-1860

Email Address: Jccassady@crimson.ua.edu

Date of Submission: Thu, 12/01/2022 - 10:50

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Mackenzie Green

4209 33rd Ave

Cincinnati, OH 45209
United States

Phone: (513) 218-7142

Email Address: mackenzie.green17@gmail.com

Date of Submission: Thu, 12/01/2022 - 10:49

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Aaron Horsley

7328 Maple Avenue

Cincinnati, OH 45231
United States

Phone: (859) 270-5225

Email Address: forgivenstreet@gmail.com

Date of Submission: Thu, 12/01/2022 - 10:42

Submission Letter:

I'm writing to you today in hopes that you will oppose the Brent Spence Bridge Corridor expansion project. The project won't solve the bridge's traffic and safety problems, but it will harm local communities, cost taxpayers billions of dollars, and only heightens our reliance on cars and fossil fuels.

Highway expansion projects harm already vulnerable populations. This expansion project will promulgate plumes of dust and fumes, worsening air pollution and health outcomes in nearby communities in the long term while interrupting people's everyday lives in the short term. After months of negotiation, Covington residents are still giving up over a dozen properties to the state to build this highway. And despite all of this, the project likely will not even solve the problems it is setting out to fix. Research strongly suggests that expanding highway capacity attracts more vehicles and increases traffic, the reverse effect of what KYTC's proposed plan is supposed to achieve.

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I am hoping that you will not support expanding the Brent Spence highway but rather invest in our communities, our transportation system, and our health and

safety.

Shivani Naik

14 17th St.

Newport, KY 41071
United States

Phone: (734) 731-0321

Email Address: snaik1224@gmail.com

Date of Submission: Thu, 12/01/2022 - 10:34

Submission Letter:

I'm writing to you today in hopes that you will oppose the Brent Spence Bridge Corridor expansion project. The project won't solve the bridge's traffic and safety problems, but it will harm local communities, cost taxpayers billions of dollars, and only heightens our reliance on cars and fossil fuels.

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safety.

DANIEL BOCKRATH

2621 Ridgecliff Avenue

Cincinnati, OH 45212
United States

Phone: (513) 604-0641

Email Address: DANBOCKCINCY@GMAIL.COM

Date of Submission: Thu, 12/01/2022 - 10:32

Submission Letter:

I'm writing to you today in hopes that you will oppose the Brent Spence Bridge Corridor expansion project. The project won't solve the bridge's traffic and safety problems, but it will harm local communities, cost taxpayers billions of dollars, and only heightens our reliance on cars and fossil fuels.

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safety.

Jaimey Dudley

4321 Ashland Ave

Cincinnati, OH 45212
United States

Phone: (614) 558-3090

Email Address: dudleyjl95@gmail.com

Date of Submission: Thu, 12/01/2022 - 10:30

Submission Letter:

I'm writing to you today in hopes that you will oppose the Brent Spence Bridge Corridor expansion project. The project won't solve the bridge's traffic and safety problems, but it will harm local communities, cost taxpayers billions of dollars, and only heightens our reliance on cars and fossil fuels.

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safety.

Staci Shelby

3600 Madison Road, Cincinnati

Cincinnati, OH 45209
United States

Phone: (513) 703-5880

Email Address: staci@asktcg.com

Date of Submission: Thu, 12/01/2022 - 10:24

Submission Letter:

I'm writing to you today in hopes that you will oppose the Brent Spence Bridge Corridor expansion project. The project won't solve the bridge's traffic and safety problems, but it will harm local communities, cost taxpayers billions of dollars, and only heightens our reliance on cars and fossil fuels.

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I am hoping that you will not support expanding the Brent Spence highway but rather invest in our communities, our transportation system, and our health and

safety.

Thank you for considering this email and please let me know if you have any questions. I'm a local realtor and I invest in the residential properties here locally in Cincinnati. I was born and raised here in Cincinnati and so this initiative to bring more public transportation to my city would be a great deal to myself, my investors in clients and friends. Happy holidays!

Angela Duchesnault

222 Lafayette CIR

cincinnati, OH 45220
United States

Phone: (513) 873-8193

Email Address: deshano75@yahoo.com

Date of Submission: Thu, 12/01/2022 - 10:24

Submission Letter:

I'm writing to you today in hopes that you will oppose the Brent Spence Bridge Corridor expansion project. The project won't solve the bridge's traffic and safety problems, but it will harm local communities, cost taxpayers billions of dollars, and only heightens our reliance on cars and fossil fuels.

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safety.

Clara Fasce

1506 William Howard Taft Road

Cincinnati, OH 45206
United States

Phone: (513) 882-6247

Email Address: fasceca@gmail.com

Date of Submission: Thu, 12/01/2022 - 10:23

Submission Letter:

I'm writing to you today in hopes that you will oppose the Brent Spence Bridge Corridor expansion project. The project won't solve the bridge's traffic and safety problems, but it will harm local communities, cost taxpayers billions of dollars, and only heightens our reliance on cars and fossil fuels.

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safety.

Wyatt Ross

161 Junedale Drive

Cincinnati, OH 45218
United States

Phone: (513) 461-3315

Email Address: wyatttuner@gmail.com

Date of Submission: Thu, 12/01/2022 - 10:21

Submission Letter:

I'm writing to you today in hopes that you will oppose the Brent Spence Bridge Corridor expansion project. The project won't solve the bridge's traffic and safety problems, but it will harm local communities, cost taxpayers billions of dollars, and only heightens our reliance on cars and fossil fuels.

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safety.

Ruth Anne Wolfe

3112 Gloss Ave

Cincinnati, OH 45213
United States

Phone: (513) 307-3640

Email Address: ruthannewolfe@gmail.com

Date of Submission: Thu, 12/01/2022 - 10:16

Submission Letter:

I'm writing to you today in hopes that you will oppose the Brent Spence Bridge Corridor expansion project. The project won't solve the bridge's traffic and safety problems, but it will harm local communities, cost taxpayers billions of dollars, and only heightens our reliance on cars and fossil fuels.

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safety.

Keith Rust

735 Liberty Street

Newport, KY 41071
United States

Phone: (214) 912-2648

Email Address: keithgrust@gmail.com

Date of Submission: Thu, 12/01/2022 - 10:15

Submission Letter:

I'm writing to you today in hopes that you will oppose the Brent Spence Bridge Corridor expansion project. The project won't solve the bridge's traffic and safety problems, but it will harm local communities, cost taxpayers billions of dollars, and only heightens our reliance on cars and fossil fuels.

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safety.

Raymond Pettit

3818 Carlton St

Cincinnati, OH 45227
United States

Phone: (513) 375-4499

Email Address: rwpettit@hotmail.com

Date of Submission: Thu, 12/01/2022 - 10:13

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safety.

Carrie Miller

101 West Elder Street #401

Cincinnati, OH 45202
United States

Phone: (502) 379-7108

Email Address: Carrie.cheray.miller@gmail.com

Date of Submission: Thu, 12/01/2022 - 10:13

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safety.

Faith Specter

7145 Baribill Place

Cincinnati, OH 45230
United States

Phone: (513) 903-7065

Email Address: faithspecter@gmail.com

Date of Submission: Thu, 12/01/2022 - 10:13

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safety.

Alice McIntosh

639 Dayton St

Newport, KY 41071
United States

Phone: (317) 696-5149

Email Address: mcintosh.alice@gmail.com

Date of Submission: Thu, 12/01/2022 - 10:12

Submission Letter:

I'm writing to you today in hopes that you will oppose the Brent Spence Bridge Corridor expansion project. The project won't solve the bridge's traffic and safety problems, but it will harm local communities, cost taxpayers billions of dollars, and only heightens our reliance on cars and fossil fuels.

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safety.

William Grapes

3582 Eyrich Rd

Cincinnati, OH 45248
United States

Phone: (513) 503-7368

Email Address: wjgrapes@yahoo.com

Date of Submission: Thu, 12/01/2022 - 10:12

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safety.

Alandis Johnson

1616 Mansfield Street

Cincinnati, OH 45202
United States

Phone: (480) 415-3470

Email Address: alandisj@gmail.com

Date of Submission: Thu, 12/01/2022 - 10:11

Submission Letter:

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safety.

Susan Reinhardt

3052 Taylor Ave

Cincinnati, OH 45220
United States

Phone: (513) 300-5182

Email Address: SUZI2ZOO@YAHOO.COM

Date of Submission: Thu, 12/01/2022 - 10:10

Submission Letter:

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I am hoping that you will not support expanding the Brent Spence highway but rather invest in our communities, our transportation system, and our health and

safety.

Kate Elliott

6817 Withany Avenue

Cincinnati, OH 45213
United States

Phone: (513) 919-5710

Email Address: kate.elliott7@gmail.com

Date of Submission: Thu, 12/01/2022 - 10:06

Submission Letter:

I'm writing to you today in hopes that you will oppose the Brent Spence Bridge Corridor expansion project. The project won't solve the bridge's traffic and safety problems, but it will harm local communities, cost taxpayers billions of dollars, and only heightens our reliance on cars and fossil fuels.

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safety.

Sydney Gustafson

409 Milton St

Cincinnati, OH 45202
United States

Phone: (260) 235-1239

Email Address: sydneygustafson1@gmail.com

Date of Submission: Thu, 12/01/2022 - 10:06

Submission Letter:

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safety.

Joseph Arthur

6646 Gracely Drive

Cincinnati, OH 45233
United States

Phone: (513) 240-3965

Email Address: arthurjaj13@gmail.com

Date of Submission: Thu, 12/01/2022 - 10:06

Submission Letter:

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safety.

Erin Fay

3228 Harvest Ave

Cincinnati, OH 45213-1455
United States

Phone: (513) 608-0774

Email Address: erinfay78@gmail.com

Date of Submission: Thu, 12/01/2022 - 10:04

Submission Letter:

I'm writing to you today in hopes that you will oppose the Brent Spence Bridge Corridor expansion project. The project won't solve the bridge's traffic and safety problems, but it will harm local communities, cost taxpayers billions of dollars, and only heightens our reliance on cars and fossil fuels.

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safety.

Karmae Zitzner

114 Peete Street

Cincinnati, OH 45202
United States

Phone: (951) 892-4490

Email Address: Karmaegirl@gmail.com

Date of Submission: Thu, 12/01/2022 - 10:04

Submission Letter:

I'm writing to you today in hopes that you will oppose the Brent Spence Bridge Corridor expansion project. The project won't solve the bridge's traffic and safety problems, but it will harm local communities, cost taxpayers billions of dollars, and only heightens our reliance on cars and fossil fuels.

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safety.

Algirdas Aukstuolis

3150 Auten Avenue,

Cincinnati, OH 45213
United States

Phone: (513) 609-0067

Email Address: aukstuolisaj@gmail.com

Date of Submission: Thu, 12/01/2022 - 10:04

Submission Letter:

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safety.

Rudy Gruenke

3149 Beaver Ave.

Cincinnati, OH 45213
United States

Phone: (312) 590-8994

Email Address: rudy4@gruenkes.com

Date of Submission: Thu, 12/01/2022 - 10:03

Submission Letter:

I'm writing to you today in hopes that you will oppose the Brent Spence Bridge Corridor expansion project. The project won't solve the bridge's traffic and safety problems, but it will harm local communities, cost taxpayers billions of dollars, and only heightens our reliance on cars and fossil fuels.

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safety.

Maddie Whitis

7850 Stonegate Dr, Apt. 901

Cincinnati, OH 45255
United States

Phone: (858) 638-4441

Email Address: pantry-nothing-08@icloud.com

Date of Submission: Thu, 12/01/2022 - 10:03

Submission Letter:

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safety.

David Tillman

3045 Charter Oak Rd

Edgewood, KY 41017
United States

Phone: (859) 816-0179

Email Address: david.tillman89@gmail.com

Date of Submission: Wed, 11/30/2022 - 15:47

Submission Letter:

I'm writing to you today in hopes that you will oppose the Brent Spence Bridge Corridor expansion project. The project won't solve the bridge's traffic and safety problems, but it will harm local communities, cost taxpayers billions of dollars, and only heightens our reliance on cars and fossil fuels.

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safety.

Nikhil Sekar

3680 Carpenters Green Lan

CINCINNATI, OH 45241
United States

Phone: (513) 658-9836

Email Address: nikhil.sekar@gmail.com

Date of Submission: Tue, 11/29/2022 - 13:16

Submission Letter:

I'm writing to you today in hopes that you will oppose the Brent Spence Bridge Corridor expansion project. The project won't solve the bridge's traffic and safety problems, but it will harm local communities, cost taxpayers billions of dollars, and only heightens our reliance on cars and fossil fuels.

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safety.

Lauren Hart

434 Kitty Ln

Cincinnati, OH 45238
United States

Phone: (317) 403-7008

Email Address: ldhart1331@gmail.com

Date of Submission: Mon, 11/28/2022 - 10:48

Submission Letter:

I'm writing to you today in hopes that you will oppose the Brent Spence Bridge Corridor expansion project. The project won't solve the bridge's traffic and safety problems, but it will harm local communities, cost taxpayers billions of dollars, and only heightens our reliance on cars and fossil fuels.

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safety.

B Higgins

327 W 7th St Apt 2F

Covington, KY 41011
United States

Phone: (724) 464-3928

Email Address: b.higginsky@gmail.com

Date of Submission: Sat, 11/26/2022 - 19:00

Submission Letter:

I'm writing to you today in hopes that you will oppose the Brent Spence Bridge Corridor expansion project. The project won't solve the bridge's traffic and safety problems, but it will harm local communities, cost taxpayers billions of dollars, and only heightens our reliance on cars and fossil fuels.

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safety.

Michelle Browning

1622 Peaslee Avenue

Cincinnati, OH 45224
United States

Phone: (513) 295-2274

Email Address: mcbrowning9513@gmail.com

Date of Submission: Fri, 11/25/2022 - 18:00

Submission Letter:

I'm writing to you today in hopes that you will oppose the Brent Spence Bridge Corridor expansion project. The project won't solve the bridge's traffic and safety problems, but it will harm local communities, cost taxpayers billions of dollars, and only heightens our reliance on cars and fossil fuels.

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safety.

Lauren Farquhar

3020 Gloss Ave

Cincinnati, OH 45213
United States

Phone: (316) 259-8330

Email Address: lauren.farquhar@gmail.com

Date of Submission: Wed, 11/23/2022 - 17:35

Submission Letter:

I'm writing to you today in hopes that you will oppose the Brent Spence Bridge Corridor expansion project. The project won't solve the bridge's traffic and safety problems, but it will harm local communities, cost taxpayers billions of dollars, and only heightens our reliance on cars and fossil fuels.

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safety.

Owen Green

1512 Republic Street

Cincinnati, OH 45202
United States

Phone: (513) 635-8089

Email Address: greenot@mail.uc.edu

Date of Submission: Wed, 11/23/2022 - 09:57

Submission Letter:

I'm writing to you today in hopes that you will oppose the Brent Spence Bridge Corridor expansion project. The project won't solve the bridge's traffic and safety problems, but it will harm local communities, cost taxpayers billions of dollars, and only heightens our reliance on cars and fossil fuels.

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safety.

Nate Weyand-Geise

952 John Street

Covington, KY 41016
United States

Phone: (513) 502-0613

Email Address: nweyandgeise@gmail.com

Date of Submission: Tue, 11/22/2022 - 22:16

Submission Letter:

I'm writing to you today in hopes that you will oppose the Brent Spence Bridge Corridor expansion project. The project won't solve the bridge's traffic and safety problems, but it will harm local communities, cost taxpayers billions of dollars, and only heightens our reliance on cars and fossil fuels.

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I am hoping that you will not support expanding the Brent Spence highway but rather invest in our communities, our transportation system, and our health and

safety.

Lori Rinehart

405 Broadway St Apt 701

Cincinnati, OH 45202
United States

Phone: (812) 655-5078

Email Address: loririnehart@gmail.com

Date of Submission: Tue, 11/22/2022 - 14:38

Submission Letter:

I'm writing to you today in hopes that you will oppose the Brent Spence Bridge Corridor expansion project. The project won't solve the bridge's traffic and safety problems, but it will harm local communities, cost taxpayers billions of dollars, and only heightens our reliance on cars and fossil fuels.

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safety.

Ellie Hammelrath

6839 Ken Arbore Dr

Cincinnati, OH 45236
United States

Phone: (513) 400-6121

Email Address: elliesellscincy@gmail.com

Date of Submission: Tue, 11/22/2022 - 13:42

Submission Letter:

I'm writing to you today in hopes that you will oppose the Brent Spence Bridge Corridor expansion project. The project won't solve the bridge's traffic and safety problems, but it will harm local communities, cost taxpayers billions of dollars, and only heightens our reliance on cars and fossil fuels.

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safety.

Mark Geise

7254 Longfield Drive

Cincinnati, OH 45243
United States

Phone: (760) 586-4978

Email Address: mark.w.geise@gmail.com

Date of Submission: Tue, 11/22/2022 - 12:52

Submission Letter:

I'm writing to you today in hopes that you will oppose the Brent Spence Bridge Corridor expansion project. The project won't solve the bridge's traffic and safety problems, but it will harm local communities, cost taxpayers billions of dollars, and only heightens our reliance on cars and fossil fuels.

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safety.

Max Hammersmith

4677 Summit Oak Ln

Cincinnati, OH 45248
United States

Phone: (513) 476-4064

Email Address: maxhammer77@gmail.com

Date of Submission: Tue, 11/22/2022 - 12:47

Submission Letter:

I'm writing to you today in hopes that you will oppose the Brent Spence Bridge Corridor expansion project. The project won't solve the bridge's traffic and safety problems, but it will harm local communities, cost taxpayers billions of dollars, and only heightens our reliance on cars and fossil fuels.

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safety.

Eric Geise

3204 Boudinot Avenue

Cincinnati, OH 45211
United States

Phone: (937) 750-2051

Email Address: eric.geise@gmail.com

Date of Submission: Tue, 11/22/2022 - 12:38

Submission Letter:

I'm writing to you today in hopes that you will oppose the Brent Spence Bridge Corridor expansion project. The project won't solve the bridge's traffic and safety problems, but it will harm local communities, cost taxpayers billions of dollars, and only heightens our reliance on cars and fossil fuels.

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safety.

Deanna Hillard

6723 Lebanon

Cincinnati, OH 45216
United States

Phone: (513) 372-4980

Email Address: deannahillard@gmail.com

Date of Submission: Tue, 11/22/2022 - 12:22

Submission Letter:

I'm writing to you today in hopes that you will oppose the Brent Spence Bridge Corridor expansion project. The project won't solve the bridge's traffic and safety problems, but it will harm local communities, cost taxpayers billions of dollars, and only heightens our reliance on cars and fossil fuels.

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safety.

Mary Lopez

1660 Rockhurst Lane

Cincinnati, OH 45255
United States

Phone: (515) 333-3812

Email Address: catk0917@yahoo.com

Date of Submission: Tue, 11/22/2022 - 12:04

Submission Letter:

I'm writing to you today in hopes that you will oppose the Brent Spence Bridge Corridor expansion project. The project won't solve the bridge's traffic and safety problems, but it will harm local communities, cost taxpayers billions of dollars, and only heightens our reliance on cars and fossil fuels.

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safety.

John East

5494 Honnert Dr

CINCINNATI, OH 45247
United States

Phone: (336) 705-3036

Email Address: JRE19831@GMAIL.COM

Date of Submission: Tue, 11/22/2022 - 11:51

Submission Letter:

I'm writing to you today in hopes that you will oppose the Brent Spence Bridge Corridor expansion project. The project won't solve the bridge's traffic and safety problems, but it will harm local communities, cost taxpayers billions of dollars, and only heightens our reliance on cars and fossil fuels.

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safety.

Kirby Rankin

335 Linden Avenue

Southgate, KY 41071
United States

Phone: (859) 486-5903

Email Address: Nrankin6129@yahoo.com

Date of Submission: Tue, 11/22/2022 - 11:39

Submission Letter:

I'm writing to you today in hopes that you will oppose the Brent Spence Bridge Corridor expansion project. The project won't solve the bridge's traffic and safety problems, but it will harm local communities, cost taxpayers billions of dollars, and only heightens our reliance on cars and fossil fuels.

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safety.

Bray Templeton

26 Carolina Avenue

Fort Thomas, KY 41075
United States

Phone: (859) 803-2551

Email Address: braydentempleton1@gmail.com

Date of Submission: Tue, 11/22/2022 - 11:38

Submission Letter:

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safety.

Joe Rider

30 E 42nd St

Covington, KY 41015
United States

Phone: (859) 391-6972

Email Address: jrider.wv@gmail.com

Date of Submission: Tue, 11/22/2022 - 11:32

Submission Letter:

I'm writing to you today in hopes that you will oppose the Brent Spence Bridge Corridor expansion project. The project won't solve the bridge's traffic and safety problems, but it will harm local communities, cost taxpayers billions of dollars, and only heightens our reliance on cars and fossil fuels.

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safety.

Rosa Christophel

6009 Wayside Avenue

Cincinnati, OH 45230
United States

Phone: (937) 763-9123

Email Address: rosa.christophel@gmail.com

Date of Submission: Tue, 11/22/2022 - 11:15

Submission Letter:

I'm writing to you today in hopes that you will oppose the Brent Spence Bridge Corridor expansion project. The project won't solve the bridge's traffic and safety problems, but it will harm local communities, cost taxpayers billions of dollars, and only heightens our reliance on cars and fossil fuels.

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safety.

Josh Jansen

3020 Gloss Avenue

Cincinnati, OH 45213
United States

Phone: (513) 235-9466

Email Address: jansen.jd@gmail.com

Date of Submission: Tue, 11/22/2022 - 10:51

Submission Letter:

I'm writing to you today in hopes that you will oppose the Brent Spence Bridge Corridor expansion project. The project won't solve the bridge's traffic and safety problems, but it will harm local communities, cost taxpayers billions of dollars, and only heightens our reliance on cars and fossil fuels.

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safety.

Bryan Georgilis

5518 Davies Place

Cincinnati, OH 45227
United States

Phone: (513) 560-5274

Email Address: bryangeorgilis@gmail.com

Date of Submission: Tue, 11/22/2022 - 10:48

Submission Letter:

I'm writing to you today in hopes that you will oppose the Brent Spence Bridge Corridor expansion project. The project won't solve the bridge's traffic and safety problems, but it will harm local communities, cost taxpayers billions of dollars, and only heightens our reliance on cars and fossil fuels.

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safety.

Barbara Hart

4170 Club View Drive

Cincinnati, OH 45209
United States

Phone: (971) 340-5900

Email Address: art2hart@gmail.com

Date of Submission: Tue, 11/22/2022 - 10:39

Submission Letter:

Dear Mayor,

I recently moved to Cincinnati from Portland, Oregon. I am accustomed to excellent public transportation that is frequent and accessible everywhere.

I am disappointed in the county's lack of public transportation and find it difficult to understand how to get from one area to another.

Additionally, if you build wider roadways, more vehicles will use them, further destroying our fragile air quality.

I'm writing to you today in hopes that you will oppose the Brent Spence Bridge Corridor expansion project. The project won't solve the bridge's traffic and safety problems, but it will harm local communities, cost taxpayers billions of dollars, and only heightens our reliance on cars and fossil fuels.

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I am hoping that you will not support expanding the Brent Spence highway but rather invest in our communities, our transportation system, and our health and safety.

If you are hesitant, I invite you and your team to visit Portland, Oregon. Great public transportation invites tourists to visit and explore more of what this city has to offer.

Michael wright

3955 Farrell Drive

Cincinnati, OH 45211
United States

Phone: (513) 939-9897

Email Address: michaelwr1117@gmail.com

Date of Submission: Tue, 11/22/2022 - 10:25

Submission Letter:

I'm writing to you today in hopes that you will oppose the Brent Spence Bridge Corridor expansion project. The project won't solve the bridge's traffic and safety problems, but it will harm local communities, cost taxpayers billions of dollars, and only heightens our reliance on cars and fossil fuels.

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I am hoping that you will not support expanding the Brent Spence highway but rather invest in our communities, our transportation system, and our health and

safety.

Hunter Swope

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Bellevue, KY 41073
United States

Phone: (859) 640-6711

Email Address: hunter_swope@yahoo.com

Date of Submission: Tue, 11/22/2022 - 10:21

Submission Letter:

I'm writing to you today in hopes that you will oppose the Brent Spence Bridge Corridor expansion project. The project won't solve the bridge's traffic and safety problems, but it will harm local communities, cost taxpayers billions of dollars, and only heightens our reliance on cars and fossil fuels.

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safety.

Nicholas Wright

2998 Springer Avenue

Cincinnati, OH 45208
United States

Phone: (513) 702-1918

Email Address: wright.nick0798@gmail.com

Date of Submission: Tue, 11/22/2022 - 10:20

Submission Letter:

Cincinnati was once the Athens of the West. Now we can't go anywhere without a car. Traffic, accidents, injuries, and deaths plague our streets and more roads is not the solution.

I'm writing to you today in hopes that you will oppose the Brent Spence Bridge Corridor expansion project. The project won't solve the bridge's traffic and safety problems, but it will harm local communities, cost taxpayers billions of dollars, and only heightens our reliance on cars and fossil fuels.

Highway expansion projects harm already vulnerable populations. This expansion project will promulgate plumes of dust and fumes, worsening air pollution and health outcomes in nearby communities in the long term while interrupting people's everyday lives in the short term. After months of negotiation, Covington residents are still giving up over a dozen properties to the state to build this highway. And despite all of this, the project likely will not even solve the problems it is setting out to fix. Research strongly suggests that expanding highway capacity attracts more vehicles and increases traffic, the reverse effect of what KYTC's proposed plan is supposed to achieve.

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money, and improve the quality of life in neighboring communities.

I am hoping that you will not support expanding the Brent Spence highway but rather invest in our communities, our transportation system, and our health and safety.

Tim Paff

3484 Arnold St

Cincinnati, OH 45208
United States

Phone: (513) 319-3459

Email Address: tcpaff4@yahoo.com

Date of Submission: Tue, 11/22/2022 - 10:19

Submission Letter:

I'm writing to you today in hopes that you will oppose the Brent Spence Bridge Corridor expansion project. The project won't solve the bridge's traffic and safety problems, but it will harm local communities, cost taxpayers billions of dollars, and only heightens our reliance on cars and fossil fuels.

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safety.

Mitch Klein

328 E 9th St

Newport, KY 41071
United States

Phone: (513) 309-1878

Email Address: mitchklein94@gmail.com

Date of Submission: Tue, 11/22/2022 - 10:18

Submission Letter:

I'm writing to you today in hopes that you will oppose the Brent Spence Bridge Corridor expansion project. The project won't solve the bridge's traffic and safety problems, but it will harm local communities, cost taxpayers billions of dollars, and only heightens our reliance on cars and fossil fuels.

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safety.

Wendy Silvius

2356 Park Ave, Unit 12

Cincinnati, OH 45206
United States

Phone: (513) 348-0626

Email Address: silvius4@hotmail.com

Date of Submission: Tue, 11/22/2022 - 10:14

Submission Letter:

I'm writing to you today in hopes that you will oppose the Brent Spence Bridge Corridor expansion project. The project won't solve the bridge's traffic and safety problems, but it will harm local communities, cost taxpayers billions of dollars, and only heightens our reliance on cars and fossil fuels.

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safety.

Ed Paff

1345 Cryer Ave.

Cincinnati, OH 45208
United States

Phone: (513) 252-5260

Email Address: edpaffjr@gmail.com

Date of Submission: Tue, 11/22/2022 - 10:09

Submission Letter:

I'm writing to you today in hopes that you will oppose the Brent Spence Bridge Corridor expansion project. The project won't solve the bridge's traffic and safety problems, but it will harm local communities, cost taxpayers billions of dollars, and only heightens our reliance on cars and fossil fuels.

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safety.

Cory Aldrich

15 W 4th St Unit 203

Cincinnati, OH 45202
United States

Phone: (937) 478-6792

Email Address: coryaldrich@gmail.com

Date of Submission: Tue, 11/22/2022 - 10:07

Submission Letter:

I'm writing to you today in hopes that you will oppose the Brent Spence Bridge Corridor expansion project. The project won't solve the bridge's traffic and safety problems, but it will harm local communities, cost taxpayers billions of dollars, and only heightens our reliance on cars and fossil fuels.

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safety.

Ian Maloney

7150 Hosbrook Road

Madeira, OH 45243
United States

Phone: (267) 205-9478

Email Address: ibmaloney@yahoo.com

Date of Submission: Tue, 11/22/2022 - 10:07

Submission Letter:

I'm writing to you today in hopes that you will oppose the Brent Spence Bridge Corridor expansion project. The project won't solve the bridge's traffic and safety problems, but it will harm local communities, cost taxpayers billions of dollars, and only heightens our reliance on cars and fossil fuels.

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safety.

Daniel Chimusoro

2305 Kenton Str. Apt 1

Cincinnati, OH 45206
United States

Phone: (513) 376-5374

Email Address: danielchimusoro@gmail.com

Date of Submission: Tue, 11/22/2022 - 10:06

Submission Letter:

I'm writing to you today in hopes that you will oppose the Brent Spence Bridge Corridor expansion project. The project won't solve the bridge's traffic and safety problems, but it will harm local communities, cost taxpayers billions of dollars, and only heightens our reliance on cars and fossil fuels.

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safety.

emily may

2905 Ruehlmann Pl

Cincinnati, OH 45211
United States

Phone: (513) 824-5166

Email Address: emily.may1994@gmail.com

Date of Submission: Tue, 11/22/2022 - 10:05

Submission Letter:

I'm writing to you today in hopes that you will oppose the Brent Spence Bridge Corridor expansion project. The project won't solve the bridge's traffic and safety problems, but it will harm local communities, cost taxpayers billions of dollars, and only heightens our reliance on cars and fossil fuels.

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safety.

Samuel Greenhill

501 E Southern Ave

Covington, KY 41015
United States

Phone: (859) 609-2618

Email Address: samuelcgreenhill@gmail.com

Date of Submission: Tue, 11/22/2022 - 10:03

Submission Letter:

I'm writing to you today in hopes that you will oppose the Brent Spence Bridge Corridor expansion project. The project won't solve the bridge's traffic and safety problems, but it will harm local communities, cost taxpayers billions of dollars, and only heightens our reliance on cars and fossil fuels.

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safety.

Colin Reusch

4227 Chambers Street,

Cincinnati, OH 45223
United States

Phone: (202) 486-7230

Email Address: reusch.colin@gmail.com

Date of Submission: Tue, 11/22/2022 - 08:12

Submission Letter:

I'm writing to you today in hopes that you will oppose the Brent Spence Bridge Corridor expansion project. The project won't solve the bridge's traffic and safety problems, but it will harm local communities, cost taxpayers billions of dollars, and only heightens our reliance on cars and fossil fuels.

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safety.

Mary Timmons

2737 Alberts Ct

Cincinnati, OH 45209
United States

Phone: (203) 610-1599

Email Address: mmtimmons4@gmail.com

Date of Submission: Tue, 11/22/2022 - 06:13

Submission Letter:

I'm writing to you today in hopes that you will oppose the Brent Spence Bridge Corridor expansion project. The project won't solve the bridge's traffic and safety problems, but it will harm local communities, cost taxpayers billions of dollars, and only heightens our reliance on cars and fossil fuels.

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safety.

Max Smolens

3350 Bishop Street

Cincinnati, OH 45220
United States

Phone: (919) 619-0143

Email Address: msmolens@gmail.com

Date of Submission: Tue, 11/22/2022 - 06:01

Submission Letter:

I'm writing to you today in hopes that you will oppose the Brent Spence Bridge Corridor expansion project. The project won't solve the bridge's traffic and safety problems, but it will harm local communities, cost taxpayers billions of dollars, and only heightens our reliance on cars and fossil fuels.

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safety.

Alexander Bumpus

2825 Clifton Avenue

Cincinnati, OH 45220
United States

Phone: (440) 321-8286

Email Address: bumpus.alex@gmail.com

Date of Submission: Tue, 11/22/2022 - 00:43

Submission Letter:

I'm writing to you today in hopes that you will oppose the Brent Spence Bridge Corridor expansion project. The project won't solve the bridge's traffic and safety problems, but it will harm local communities, cost taxpayers billions of dollars, and only heightens our reliance on cars and fossil fuels.

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safety.

Isaac Diskin

1322 Apjones Street

Cincinnati, OH 45223
United States

Phone: (513) 213-8151

Email Address: isaacdiskin2@gmail.com

Date of Submission: Mon, 11/21/2022 - 22:37

Submission Letter:

I'm writing to you today in hopes that you will oppose the Brent Spence Bridge Corridor expansion project. The project won't solve the bridge's traffic and safety problems, but it will harm local communities, cost taxpayers billions of dollars, and only heightens our reliance on cars and fossil fuels.

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safety.

Annie Heyob

1222 Republic Street

Cincinnati, OH 45202
United States

Phone: (513) 658-4252

Email Address: anneheyob@gmail.com

Date of Submission: Mon, 11/21/2022 - 21:35

Submission Letter:

I'm writing to you today in hopes that you will oppose the Brent Spence Bridge Corridor expansion project. The project won't solve the bridge's traffic and safety problems, but it will harm local communities, cost taxpayers billions of dollars, and only heightens our reliance on cars and fossil fuels.

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I am hoping that you will not support expanding the Brent Spence highway but rather invest in our communities, our transportation system, and our health and

safety.

Jay Leman

300 Emming Street

Cincinnati, OH 45219
United States

Phone: (224) 250-3839

Email Address: gjleman@gmail.com

Date of Submission: Mon, 11/21/2022 - 21:32

Submission Letter:

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safety.

Colin Barge

2031 Dunlap Street

Cincinnati, OH 45214
United States

Phone: (513) 869-0600

Email Address: bargecolin@gmail.com

Date of Submission: Mon, 11/21/2022 - 19:11

Submission Letter:

I'm writing to you today in hopes that you will oppose the Brent Spence Bridge Corridor expansion project. The project won't solve the bridge's traffic and safety problems, but it will harm local communities, cost taxpayers billions of dollars, and only heightens our reliance on cars and fossil fuels.

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safety.

Sarah Klein

955 Stone Ridge Ln

Lebanon, OH, OH 45036
United States

Phone: (571) 330-3634

Email Address: Sarahcaseyklein@hotmail.com

Date of Submission: Mon, 11/21/2022 - 18:53

Submission Letter:

I'm writing to you today in hopes that you will oppose the Brent Spence Bridge Corridor expansion project. The project won't solve the bridge's traffic and safety problems, but it will harm local communities, cost taxpayers billions of dollars, and only heightens our reliance on cars and fossil fuels.

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safety.

David Gould

955 Stone Ridge Lane

Lebanon, OH 45036
United States

Phone: (571) 420-5214

Email Address: David.lee.gould@gmail.com

Date of Submission: Mon, 11/21/2022 - 18:21

Submission Letter:

I'm writing to you today in hopes that you will oppose the Brent Spence Bridge Corridor expansion project. The project won't solve the bridge's traffic and safety problems, but it will harm local communities, cost taxpayers billions of dollars, and only heightens our reliance on cars and fossil fuels.

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safety.

Steven Hampton

1622 Walnut Street

Cincinnati, OH 45202
United States

Phone: (513) 484-2493

Email Address: steve@hamptonarchitects.com

Date of Submission: Mon, 11/21/2022 - 17:48

Submission Letter:

I'm writing to you today in hopes that you will oppose the Brent Spence Bridge Corridor expansion project. The project won't solve the bridge's traffic and safety problems, but it will harm local communities, cost taxpayers billions of dollars, and only heightens our reliance on cars and fossil fuels.

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safety.

Logan Price

607 Arrowhead Trail

Loveland, OH 45140
United States

Phone: (330) 749-2265

Email Address: price.logan.scott@gmail.com

Date of Submission: Mon, 11/21/2022 - 17:37

Submission Letter:

I'm writing to you today in hopes that you will oppose the Brent Spence Bridge Corridor expansion project. The project won't solve the bridge's traffic and safety problems, but it will harm local communities, cost taxpayers billions of dollars, and only heightens our reliance on cars and fossil fuels.

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safety.

Ryan Yeazell

1337 Walnut Street, Apt. C

Cincinnati, OH 45202
United States

Phone: (513) 687-9225

Email Address: ryeazell@outlook.com

Date of Submission: Mon, 11/21/2022 - 17:16

Submission Letter:

I'm writing to you today in hopes that you will oppose the Brent Spence Bridge Corridor expansion project. The project won't solve the bridge's traffic and safety problems, but it will harm local communities, cost taxpayers billions of dollars, and only heightens our reliance on cars and fossil fuels.

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safety.

Alex Browne

1342 Walnut Street

Cincinnati, OH 45202
United States

Phone: (513) 225-1864

Email Address: alexbrowne1996@yahoo.com

Date of Submission: Mon, 11/21/2022 - 15:37

Submission Letter:

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safety.

Daniel Edie

207 East 46th Street

Covington, KY 41015
United States

Phone: (859) 743-9984

Email Address: danieledie95@gmail.co

Date of Submission: Sat, 11/12/2022 - 10:17

Submission Letter:

I'm writing to you today in hopes that you will oppose the Brent Spence Bridge Corridor expansion project. The project won't solve the bridge's traffic and safety problems, but it will harm local communities, cost taxpayers billions of dollars, and only heightens our reliance on cars and fossil fuels.

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safety.

Nellie Neal

5399 Kirby Avenue,

Cincinnati, OH 45223
United States

Phone: (513) 375-1142

Email Address: nelliealicen@gmail.com

Date of Submission: Sun, 11/06/2022 - 15:36

Submission Letter:

I'm writing to you today in hopes that you will oppose the Brent Spence Bridge Corridor expansion project. The project won't solve the bridge's traffic and safety problems, but it will harm local communities, cost taxpayers billions of dollars, and only heightens our reliance on cars and fossil fuels.

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safety.

Kevin Kauffman

2729 Woodburn Avenue

Cincinnati, OH 45206
United States

Phone: (217) 418-9789

Email Address: kkauffman97@gmail.com

Date of Submission: Thu, 10/27/2022 - 23:00

Submission Letter:

I'm writing to you today in hopes that you will oppose the Brent Spence Bridge Corridor expansion project. The project won't solve the bridge's traffic and safety problems, but it will harm local communities, cost taxpayers billions of dollars, and only heightens our reliance on cars and fossil fuels.

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safety.

Nicholas Pitcher

10 Pebble Beach Ct

Fairfield, OH 45014
United States

Phone: (585) 348-1771

Email Address: jvnike@gmail.com

Date of Submission: Thu, 10/27/2022 - 12:20

Submission Letter:

I'm writing to you today in hopes that you will oppose the Brent Spence Bridge Corridor expansion project. The project won't solve the bridge's traffic and safety problems, but it will harm local communities, cost taxpayers billions of dollars, and only heightens our reliance on cars and fossil fuels.

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Augusta Pohlman

127 Findlay Street

Cincinnati, OH 45202
United States

Phone: (513) 638-8158

Email Address: acpohlman1@crimson.ua.edu

Date of Submission: Thu, 10/27/2022 - 11:18

Submission Letter:

I'm writing to you today in hopes that you will oppose the Brent Spence Bridge Corridor expansion project. The project won't solve the bridge's traffic and safety problems, but it will harm local communities, cost taxpayers billions of dollars, and only heightens our reliance on cars and fossil fuels.

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safety.

Mustafa Hilal

310 Warner Street

Cincinnati, OH 45219
United States

Phone: (614) 326-3450

Email Address: mhilalosu@gmail.com

Date of Submission: Wed, 10/26/2022 - 16:06

Submission Letter:

I'm writing to you today in hopes that you will oppose the Brent Spence Bridge Corridor expansion project. The project won't solve the bridge's traffic and safety problems, but it will harm local communities, cost taxpayers billions of dollars, and only heightens our reliance on cars and fossil fuels.

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safety.

Kawika Kahikina

3135 Pickbury Dr,

Cincinnati, OH 45211
United States

Phone: (513) 223-1899

Email Address: ITSMYDOGG@GMAIL.COM

Date of Submission: Wed, 10/26/2022 - 13:22

Submission Letter:

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safety.

Benjamin Collings

185 N Jackson St

Sabina, OH 45169
United States

Phone: (937) 728-9206

Email Address: bfcollings@gmail.com

Date of Submission: Wed, 10/26/2022 - 12:24

Submission Letter:

I'm writing to you today in hopes that you will oppose the Brent Spence Bridge Corridor expansion project. The project won't solve the bridge's traffic and safety problems, but it will harm local communities, cost taxpayers billions of dollars, and only heightens our reliance on cars and fossil fuels.

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I am hoping that you will not support expanding the Brent Spence highway but rather invest in our communities, our transportation system, and our health and

safety.

Mark Kolakowski

4229 Peace Haven

Batavia, OH 45103
United States

Phone: (859) 759-4577

Email Address: mark.kolakowski@gmail.com

Date of Submission: Mon, 10/24/2022 - 21:23

Submission Letter:

I'm writing to you today in hopes that you will oppose the Brent Spence Bridge Corridor expansion project. The project won't solve the bridge's traffic and safety problems, but it will harm local communities, cost taxpayers billions of dollars, and only heightens our reliance on cars and fossil fuels.

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I am hoping that you will not support expanding the Brent Spence highway but rather invest in our communities, our transportation system, and our health and

safety.

Kieran Phelan

5241 Rolston Avenue

Norwood, OH 45212
United States

Phone: (513) 919-7323

Email Address: phelankj@mail.uc.edu

Date of Submission: Mon, 10/24/2022 - 10:23

Submission Letter:

I'm writing to you today in hopes that you will oppose the Brent Spence Bridge Corridor expansion project. The project won't solve the bridge's traffic and safety problems, but it will harm local communities, cost taxpayers billions of dollars, and only heightens our reliance on cars and fossil fuels.

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I am hoping that you will not support expanding the Brent Spence highway but rather invest in our communities, our transportation system, and our health and

safety.

David Landis

4910 Eastern Avenue, Apt 5

Cincinnati, OH 45208
United States

Phone: (302) 430-2192

Email Address: sonnylandis8@gmail.com

Date of Submission: Mon, 10/24/2022 - 08:04

Submission Letter:

I'm writing to you today in hopes that you will oppose the Brent Spence Bridge Corridor expansion project. The project won't solve the bridge's traffic and safety problems, but it will harm local communities, cost taxpayers billions of dollars, and only heightens our reliance on cars and fossil fuels.

Highway expansion projects harm already vulnerable populations. This expansion project will promulgate plumes of dust and fumes, worsening air pollution and health outcomes in nearby communities in the long term while interrupting people's everyday lives in the short term. After months of negotiation, Covington residents are still giving up over a dozen properties to the state to build this highway. And despite all of this, the project likely will not even solve the problems it is setting out to fix. Research strongly suggests that expanding highway capacity attracts more vehicles and increases traffic, the reverse effect of what KYTC's proposed plan is supposed to achieve.

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I am hoping that you will not support expanding the Brent Spence highway but rather invest in our communities, our transportation system, and our health and

safety.

D. S. Uhlhorn

4213 Mad Anthony St.

Cincinnati, OH 45223
United States

Phone: (513) 237-2825

Email Address: dsuhlhorn@gmail.com

Date of Submission: Mon, 10/24/2022 - 01:45

Submission Letter:

I'm writing to you today in hopes that you will oppose the Brent Spence Bridge Corridor expansion project. The project won't solve the bridge's traffic and safety problems, but it will harm local communities, cost taxpayers billions of dollars, and only heightens our reliance on cars and fossil fuels.

Highway expansion projects harm already vulnerable populations. This expansion project will promulgate plumes of dust and fumes, worsening air pollution and health outcomes in nearby communities in the long term while interrupting people's everyday lives in the short term. After months of negotiation, Covington residents are still giving up over a dozen properties to the state to build this highway. And despite all of this, the project likely will not even solve the problems it is setting out to fix. Research strongly suggests that expanding highway capacity attracts more vehicles and increases traffic, the reverse effect of what KYTC's proposed plan is supposed to achieve.

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I am hoping that you will not support expanding the Brent Spence highway but rather invest in our communities, our transportation system, and our health and

safety.

Angela Bronzie

1613 Bloomingdale Avenue

Cincinnati, OH 45230
United States

Phone: (513) 238-8899

Email Address: abronzie@yahoo.com

Date of Submission: Sun, 10/23/2022 - 22:08

Submission Letter:

I'm writing to you today in hopes that you will oppose the Brent Spence Bridge Corridor expansion project. The project won't solve the bridge's traffic and safety problems, but it will harm local communities, cost taxpayers billions of dollars, and only heightens our reliance on cars and fossil fuels.

Highway expansion projects harm already vulnerable populations. This expansion project will promulgate plumes of dust and fumes, worsening air pollution and health outcomes in nearby communities in the long term while interrupting people's everyday lives in the short term. After months of negotiation, Covington residents are still giving up over a dozen properties to the state to build this highway. And despite all of this, the project likely will not even solve the problems it is setting out to fix. Research strongly suggests that expanding highway capacity attracts more vehicles and increases traffic, the reverse effect of what KYTC's proposed plan is supposed to achieve.

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I am hoping that you will not support expanding the Brent Spence highway but rather invest in our communities, our transportation system, and our health and

safety.

Peirce Johnston

156 Wentworth Ave

Cincinnati, OH 45220-1802
United States

Phone: (513) 317-4817

Email Address: johnstpw@uc.edu

Date of Submission: Sat, 10/22/2022 - 18:32

Submission Letter:

As a physician, a father, and a Cincinnati citizen, I'm writing to you today in hopes that you will oppose the Brent Spence Bridge Corridor expansion project. Cincinnati has an opportunity to become a national leader in transportation instead of blindly following current car-culture norms. Imagine a city that has all the history and charm of our wonderful city, and with the safety, civilization, and problem-solving of other livable cities. The project won't solve the bridge's traffic and safety problems, but it will harm local communities, cost taxpayers billions of dollars, and only heightens our reliance on cars and fossil fuels.

Highway expansion projects harm already vulnerable populations. This expansion project will promulgate plumes of dust and fumes, worsening air pollution and health outcomes in nearby communities in the long term while interrupting people's everyday lives in the short term. After months of negotiation, Covington residents are still giving up over a dozen properties to the state to build this highway. And despite all of this, the project likely will not even solve the problems it is setting out to fix. Research strongly suggests that expanding highway capacity attracts more vehicles and increases traffic, the reverse effect of what KYTC's proposed plan is supposed to achieve.

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money, and improve the quality of life in neighboring communities.

I am hoping that you will not support expanding the Brent Spence highway but rather invest in our communities, our transportation system, and our health and safety.

Nicholas Riegler

714 Rosewynne Court

Cleves, OH 45002
United States

Phone: (513) 578-8346

Email Address: nickr312@aol.com

Date of Submission: Fri, 10/21/2022 - 18:38

Submission Letter:

I'm writing to you today in hopes that you will oppose the Brent Spence Bridge Corridor expansion project. The project won't solve the bridge's traffic and safety problems, but it will harm local communities, cost taxpayers billions of dollars, and only heightens our reliance on cars and fossil fuels.

Highway expansion projects harm already vulnerable populations. This expansion project will promulgate plumes of dust and fumes, worsening air pollution and health outcomes in nearby communities in the long term while interrupting people's everyday lives in the short term. After months of negotiation, Covington residents are still giving up over a dozen properties to the state to build this highway. And despite all of this, the project likely will not even solve the problems it is setting out to fix. Research strongly suggests that expanding highway capacity attracts more vehicles and increases traffic, the reverse effect of what KYTC's proposed plan is supposed to achieve.

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I am hoping that you will not support expanding the Brent Spence highway but rather invest in our communities, our transportation system, and our health and

safety.

Laura Doering-Strite

747 Tweed Avenue

Cincinnati, OH 45226
United States

Phone: (321) 536-5964

Email Address: lemminpie@aol.com

Date of Submission: Fri, 10/21/2022 - 16:46

Submission Letter:

I'm writing to you today in hopes that you will oppose the Brent Spence Bridge Corridor expansion project. The project won't solve the bridge's traffic and safety problems, but it will harm local communities, cost taxpayers billions of dollars, and only heightens our reliance on cars and fossil fuels.

Highway expansion projects harm already vulnerable populations. This expansion project will promulgate plumes of dust and fumes, worsening air pollution and health outcomes in nearby communities in the long term while interrupting people's everyday lives in the short term. After months of negotiation, Covington residents are still giving up over a dozen properties to the state to build this highway. And despite all of this, the project likely will not even solve the problems it is setting out to fix. Research strongly suggests that expanding highway capacity attracts more vehicles and increases traffic, the reverse effect of what KYTC's proposed plan is supposed to achieve.

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I am hoping that you will not support expanding the Brent Spence highway but rather invest in our communities, our transportation system, and our health and

safety.

Brent Patterson

968 Hatch St

Cincinnati, OH 45202
United States

Phone: (513) 633-2562

Email Address: brpatterson@gmail.com

Date of Submission: Fri, 10/21/2022 - 13:14

Submission Letter:

I'm a proud Cincinnati resident. I'm writing to you today in hopes that you will oppose the Brent Spence Bridge Corridor expansion project. I use this bridge often and see no need for it's expansion.

The project won't solve the bridge's traffic and safety problems, but it will harm local communities, cost taxpayers billions of dollars, and only heightens our reliance on cars and fossil fuels.

Highway expansion projects harm already vulnerable populations. This expansion project will promulgate plumes of dust and fumes, worsening air pollution and health outcomes in nearby communities in the long term while interrupting people's everyday lives in the short term. After months of negotiation, Covington residents are still giving up over a dozen properties to the state to build this highway. And despite all of this, the project likely will not even solve the problems it is setting out to fix. Research strongly suggests that expanding highway capacity attracts more vehicles and increases traffic, the reverse effect of what KYTC's proposed plan is supposed to achieve.

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I am hoping that you will not support expanding the Brent Spence highway but rather invest in our communities, our transportation system, and our health and safety.

Thank you
Brent

Marita Pettit

3818 Carlton St,

Cincinnati, OH 45227
United States

Phone: (513) 375-4498

Email Address: maritapettit@gmail.com

Date of Submission: Fri, 10/21/2022 - 10:57

Submission Letter:

I'm writing to you today in hopes that you will oppose the Brent Spence Bridge Corridor expansion project. The project won't solve the bridge's traffic and safety problems, but it will harm local communities, cost taxpayers billions of dollars, and only heightens our reliance on cars and fossil fuels.

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I am hoping that you will not support expanding the Brent Spence highway but rather invest in our communities, our transportation system, and our health and

safety.

Claire Le

33 W Charlotte Ave

Cincinnati, OH 45215
United States

Phone: (330) 417-2410

Email Address: cjames96.uc@gmail.com

Date of Submission: Fri, 10/21/2022 - 08:55

Submission Letter:

I'm writing to you today in hopes that you will oppose the Brent Spence Bridge Corridor expansion project. The project won't solve the bridge's traffic and safety problems, but it will harm local communities, cost taxpayers billions of dollars, and only heightens our reliance on cars and fossil fuels.

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safety.

Larry Jebsen

2200 Victory Parkway

Cincinnati, OH 45206
United States

Phone: (513) 673-8820

Email Address: larryjebsen22@gmail.com

Date of Submission: Thu, 10/20/2022 - 17:55

Submission Letter:

I'm writing to you today in hopes that you will oppose the Brent Spence Bridge Corridor expansion project. The project won't solve the bridge's traffic and safety problems, but it will harm local communities, cost taxpayers billions of dollars, and only heightens our reliance on cars and fossil fuels.

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I am hoping that you will not support expanding the Brent Spence highway but rather invest in our communities, our transportation system, and our health and

safety.

Antonio Mazzaro

6466 Mary Ingles Hwy

Melbourne, KY 41059
United States

Phone: (859) 630-3663

Email Address: acmazzaro@gmail.com

Date of Submission: Thu, 10/20/2022 - 14:55

Submission Letter:

I'm writing to you today in hopes that you will oppose the Brent Spence Bridge Corridor expansion project. The project won't solve the bridge's traffic and safety problems, but it will harm local communities, cost taxpayers billions of dollars, and only heightens our reliance on cars and fossil fuels.

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safety.

Rachel Rekowski

2341 W. McMicken Ave.

Cincinnati, OH 45214
United States

Phone: (317) 603-4461

Email Address: rachel.reko@gmail.com

Date of Submission: Thu, 10/20/2022 - 10:37

Submission Letter:

I'm writing to you today in hopes that you will oppose the Brent Spence Bridge Corridor expansion project. The project won't solve the bridge's traffic and safety problems, but it will harm local communities, cost taxpayers billions of dollars, and only heightens our reliance on cars and fossil fuels.

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I am hoping that you will not support expanding the Brent Spence highway but rather invest in our communities, our transportation system, and our health and

safety.

Zach Crozier

36 E 13th St

Cincinnati, OH 45202
United States

Phone: (812) 801-1905

Email Address: gzcrozie15@gmail.com

Date of Submission: Thu, 10/20/2022 - 10:09

Submission Letter:

I'm writing to you today in hopes that you will oppose the Brent Spence Bridge Corridor expansion project. The project won't solve the bridge's traffic and safety problems, but it will harm local communities, cost taxpayers billions of dollars, and only heightens our reliance on cars and fossil fuels.

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I am hoping that you will not support expanding the Brent Spence highway but rather invest in our communities, our transportation system, and our health and

safety.

Callie Budrick

3436 Cascade Ct

Erlanger, KY 41018
United States

Phone: (859) 486-3704

Email Address: callie.budrick94@gmail.com

Date of Submission: Thu, 10/20/2022 - 09:39

Submission Letter:

I'm writing to you today in hopes that you will oppose the Brent Spence Bridge Corridor expansion project. The project won't solve the bridge's traffic and safety problems, but it will harm local communities, cost taxpayers billions of dollars, and only heightens our reliance on cars and fossil fuels.

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I am hoping that you will not support expanding the Brent Spence highway but rather invest in our communities, our transportation system, and our health and

safety.

Jackie Ruggiero

3518 Tiffany Ridge Lane

Cincinnati, OH 45241
United States

Phone: (310) 502-1089

Email Address: jackierugg@yahoo.com

Date of Submission: Wed, 10/19/2022 - 21:14

Submission Letter:

I'm writing to you today in hopes that you will oppose the Brent Spence Bridge Corridor expansion project. The project won't solve the bridge's traffic and safety problems, but it will harm local communities, cost taxpayers billions of dollars, and only heightens our reliance on cars and fossil fuels.

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safety.

Adam Moeller

1435 Pullan Ave, Apt 1

Cincinnati, OH 45223
United States

Phone: (513) 304-4307

Email Address: ajmoeller95@gmail.com

Date of Submission: Sun, 10/16/2022 - 13:09

Submission Letter:

I'm writing to you today in hopes that you will oppose the Brent Spence Bridge Corridor expansion project. The project won't solve the bridge's traffic and safety problems, but it will harm local communities, cost taxpayers billions of dollars, and only heightens our reliance on cars and fossil fuels.

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safety.

Alexander Spence

242 Lyness Ave, 205

Harrison, OH 45030
United States

Phone: (513) 312-9455

Email Address: alexander.m.spence@gmail.com

Date of Submission: Thu, 10/13/2022 - 18:06

Submission Letter:

I'm writing to you today in hopes that you will oppose the Brent Spence Bridge Corridor expansion project. The project won't solve the bridge's traffic and safety problems, but it will harm local communities, cost taxpayers billions of dollars, and only heightens our reliance on cars and fossil fuels.

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safety.

Grace Eliassen

1033 Scott Street

Covington, KY 41011
United States

Phone: (920) 840-4347

Email Address: graceeliasen@gmail.com

Date of Submission: Thu, 10/13/2022 - 15:22

Submission Letter:

I'm writing to you today in hopes that you will oppose the Brent Spence Bridge Corridor expansion project. The project won't solve the bridge's traffic and safety problems, but it will harm local communities, cost taxpayers billions of dollars, and only heightens our reliance on cars and fossil fuels.

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safety.

Stevan Rademann

2142 Ohio Avenue

Cincinnati, OH 45219
United States

Phone: (513) 253-7958

Email Address: stevanrademann@rocketmail.com

Date of Submission: Thu, 10/13/2022 - 14:59

Submission Letter:

I'm writing to you today in hopes that you will oppose the Brent Spence Bridge Corridor expansion project. The project won't solve the bridge's traffic and safety problems, but it will harm local communities, cost taxpayers billions of dollars, and only heightens our reliance on cars and fossil fuels.

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safety.

Harry Williams

Winnipeg, Canada

Canada, ME 54545
United States

Phone: (620) 765-4699

Email Address: harrywilliamwork4@gmail.com

Date of Submission: Thu, 10/13/2022 - 08:15

Submission Letter:

I'm writing to you today in hopes that you will oppose the Brent Spence Bridge Corridor expansion project. The project won't solve the bridge's traffic and safety problems, but it will harm local communities, cost taxpayers billions of dollars, and only heightens our reliance on cars and fossil fuels.

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Harry Williams

Winnipeg, Canada

Canada, ME 54545
United States

Phone: (620) 765-4699

Email Address: harrywilliamwork4@gmail.com

Date of Submission: Thu, 10/13/2022 - 08:15

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safety.

Charles Huber

306 Frankfort St

Augusta, KY 41002
United States

Phone: (606) 402-0467

Email Address: charlie.huber28@gmail.com

Date of Submission: Wed, 10/12/2022 - 22:53

Submission Letter:

I'm writing to you today in hopes that you will oppose the Brent Spence Bridge Corridor expansion project. The project won't solve the bridge's traffic and safety problems, but it will harm local communities, cost taxpayers billions of dollars, and only heightens our reliance on cars and fossil fuels.

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safety.

Tad Krafft

1125 Inglenook Place

Cincinnati, OH 45208
United States

Phone: (513) 518-5085

Email Address: tadkrafft@gmail.com

Date of Submission: Wed, 10/12/2022 - 12:53

Submission Letter:

I'm writing to you today in hopes that you will oppose the Brent Spence Bridge Corridor expansion project. The project won't solve the bridge's traffic and safety problems, but it will harm local communities, cost taxpayers billions of dollars, and only heightens our reliance on cars and fossil fuels.

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safety.

Joey Simmons

1404 Walnut Street

Cincinnati, OH 45202
United States

Phone: (513) 368-3451

Email Address: jsimmons@gmail.com

Date of Submission: Wed, 10/12/2022 - 11:33

Submission Letter:

I'm writing to you today in hopes that you will oppose the Brent Spence Bridge Corridor expansion project. The project won't solve the bridge's traffic and safety problems, but it will harm local communities, cost taxpayers billions of dollars, and only heightens our reliance on cars and fossil fuels.

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David Eberly

6498 Navaho Trail

Cincinnati, OH 45243
United States

Phone: (513) 460-5491

Email Address: deberly@emclawyers.com

Date of Submission: Wed, 10/12/2022 - 10:48

Submission Letter:

I'm writing to you today in hopes that you will oppose the Brent Spence Bridge Corridor expansion project. The project won't solve the bridge's traffic and safety problems, but it will harm local communities, cost taxpayers billions of dollars, and only heightens our reliance on cars and fossil fuels.

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Ian Petko- Bunney

7724 Shadowhill Way

Cincinnati, OH 45242
United States

Phone: (513) 984-9359

Email Address: ipetkobunney@cinci.rr.com

Date of Submission: Sun, 10/09/2022 - 14:13

Submission Letter:

I'm writing to you today in hopes that you will oppose the Brent Spence Bridge Corridor expansion project. The project won't solve the bridge's traffic and safety problems, but it will harm local communities, cost taxpayers billions of dollars, and only heightens our reliance on cars and fossil fuels.

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safety.

Steele DePew

2380 Ravine St, # 1

Cincinnati, OH 45219
United States

Phone: (513) 315-6020

Email Address: spdepew@gmail.com

Date of Submission: Sat, 10/08/2022 - 22:03

Submission Letter:

I'm writing to you today in hopes that you will oppose the Brent Spence Bridge Corridor expansion project. The project won't solve the bridge's traffic and safety problems, but it will harm local communities, cost taxpayers billions of dollars, and only heightens our reliance on cars and fossil fuels.

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safety.

Matthew Kennedy

543 Liberty Hl, Apt A

Cincinnati, OH 45202-6813
United States

Phone: (513) 426-6529

Email Address: kennedymm09@gmail.com

Date of Submission: Sat, 10/08/2022 - 07:39

Submission Letter:

I'm writing to you today in hopes that you will oppose the Brent Spence Bridge Corridor expansion project. The project won't solve the bridge's traffic and safety problems, but it will harm local communities, cost taxpayers billions of dollars, and only heightens our reliance on cars and fossil fuels.

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safety.

Andrew Hargis

822 Willard Street

Covington, KY 41011
United States

Phone: (859) 496-7069

Email Address: andrewhargis@me.com

Date of Submission: Fri, 10/07/2022 - 14:14

Submission Letter:

I'm writing to you today in hopes that you will oppose the Brent Spence Bridge Corridor expansion project. The project won't solve the bridge's traffic and safety problems, but it will harm local communities, cost taxpayers billions of dollars, and only heightens our reliance on cars and fossil fuels.

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safety.

Lorrie Palm

825 Mary Street

Villa Hills, KY 41017
United States

Phone: (847) 997-3888

Email Address: nokidsus@yahoo.com

Date of Submission: Fri, 10/07/2022 - 10:53

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Ryan R James

939 Paradrome St , Apt B

Cincinnati, OH 45202
United States

Phone: (513) 490-5262

Email Address: ryanrjames28@gmail.com

Date of Submission: Thu, 10/06/2022 - 21:55

Submission Letter:

I'm writing to you today in hopes that you will oppose the Brent Spence Bridge Corridor expansion project. The project won't solve the bridge's traffic and safety problems, but it will harm local communities, cost taxpayers billions of dollars, and only heightens our reliance on cars and fossil fuels.

Highway expansion projects harm already vulnerable populations. This expansion project will promulgate plumes of dust and fumes, worsening air pollution and health outcomes in nearby communities in the long term while interrupting people's everyday lives in the short term. After months of negotiation, Covington residents are still giving up over a dozen properties to the state to build this highway. And despite all of this, the project likely will not even solve the problems it is setting out to fix. Research strongly suggests that expanding highway capacity attracts more vehicles and increases traffic, the reverse effect of what KYTC's proposed plan is supposed to achieve.

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We cannot remain fully dependent on cars as transportation. As we have seen recently, the financial precariousness of car ownership places a mounting burden on American families. Investing in better public transit systems will reduce traffic congestion, reduce the cost to taxpayers while seeing a more beneficial use of their money, and improve the quality of life in neighboring communities.

I am hoping that you will not support expanding the Brent Spence highway but rather invest in our communities, our transportation system, and our health and

safety.

Troy Platt

126 South 2nd Street

Loveland, OH 45140
United States

Phone: (909) 997-3822

Email Address: tjplatt91@gmail.com

Date of Submission: Thu, 10/06/2022 - 16:53

Submission Letter:

I'm writing to you today in hopes that you will oppose the Brent Spence Bridge Corridor expansion project. The project won't solve the bridge's traffic and safety problems, but it will harm local communities, cost taxpayers billions of dollars, and only heightens our reliance on cars and fossil fuels.

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I am hoping that you will not support expanding the Brent Spence highway but rather invest in our communities, our transportation system, and our health and

safety.

Jessica Saunders

151 Seventh St W

Cincinnati, OH 45202
United States

Phone: (859) 866-9526

Email Address: eternal.flame902@gmail.com

Date of Submission: Thu, 10/06/2022 - 11:45

Submission Letter:

I'm writing to you today in hopes that you will oppose the Brent Spence Bridge Corridor expansion project. The project won't solve the bridge's traffic and safety problems, but it will harm local communities, cost taxpayers billions of dollars, and only heightens our reliance on cars and fossil fuels.

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I am hoping that you will not support expanding the Brent Spence highway but rather invest in our communities, our transportation system, and our health and

safety.

Kevin Hunt

4119 Maple dr

Cincinnati, OH 45209
United States

Phone: (614) 506-3581

Email Address: Khunt1227@gmail.com

Date of Submission: Thu, 10/06/2022 - 09:58

Submission Letter:

I'm writing to you today in hopes that you will oppose the Brent Spence Bridge Corridor expansion project. The project won't solve the bridge's traffic and safety problems, but it will harm local communities, cost taxpayers billions of dollars, and only heightens our reliance on cars and fossil fuels.

Highway expansion projects harm already vulnerable populations. This expansion project will promulgate plumes of dust and fumes, worsening air pollution and health outcomes in nearby communities in the long term while interrupting people's everyday lives in the short term. After months of negotiation, Covington residents are still giving up over a dozen properties to the state to build this highway. And despite all of this, the project likely will not even solve the problems it is setting out to fix. Research strongly suggests that expanding highway capacity attracts more vehicles and increases traffic, the reverse effect of what KYTC's proposed plan is supposed to achieve.

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I am hoping that you will not support expanding the Brent Spence highway but rather invest in our communities, our transportation system, and our health and

safety.

Danny McKelvey

1653 Marlowe Avenue

Cincinnati, OH 45224
United States

Phone: (937) 405-5857

Email Address: danny.mckelvey@cigcommunities.com

Date of Submission: Thu, 10/06/2022 - 09:58

Submission Letter:

I'm writing to you today in hopes that you will oppose the Brent Spence Bridge Corridor expansion project. The project won't solve the bridge's traffic and safety problems, but it will harm local communities, cost taxpayers billions of dollars, and only heightens our reliance on cars and fossil fuels.

Highway expansion projects harm already vulnerable populations. This expansion project will promulgate plumes of dust and fumes, worsening air pollution and health outcomes in nearby communities in the long term while interrupting people's everyday lives in the short term. After months of negotiation, Covington residents are still giving up over a dozen properties to the state to build this highway. And despite all of this, the project likely will not even solve the problems it is setting out to fix. Research strongly suggests that expanding highway capacity attracts more vehicles and increases traffic, the reverse effect of what KYTC's proposed plan is supposed to achieve.

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I am hoping that you will not support expanding the Brent Spence highway but rather invest in our communities, our transportation system, and our health and

safety.

Jack Mariani

317 Grove Avenue

Wyoming, OH 45215
United States

Phone: (513) 692-8555

Email Address: jackgmariani@gmail.com

Date of Submission: Thu, 10/06/2022 - 08:10

Submission Letter:

I'm writing to you today in hopes that you will oppose the Brent Spence Bridge Corridor expansion project. The project won't solve the bridge's traffic and safety problems, but it will harm local communities, cost taxpayers billions of dollars, and only heightens our reliance on cars and fossil fuels.

Highway expansion projects harm already vulnerable populations. This expansion project will promulgate plumes of dust and fumes, worsening air pollution and health outcomes in nearby communities in the long term while interrupting people's everyday lives in the short term. After months of negotiation, Covington residents are still giving up over a dozen properties to the state to build this highway. And despite all of this, the project likely will not even solve the problems it is setting out to fix. Research strongly suggests that expanding highway capacity attracts more vehicles and increases traffic, the reverse effect of what KYTC's proposed plan is supposed to achieve.

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I am hoping that you will not support expanding the Brent Spence highway but rather invest in our communities, our transportation system, and our health and

safety.

Celeste Williams

428 Riddle Rd, Apt 3

Cincinnati, OH 45220
United States

Phone: (216) 544-5781

Email Address: bonuses-0-wipes@icloud.com

Date of Submission: Wed, 10/05/2022 - 23:26

Submission Letter:

I'm writing to you today in hopes that you will oppose the Brent Spence Bridge Corridor expansion project. The project won't solve the bridge's traffic and safety problems, but it will harm local communities, cost taxpayers billions of dollars, and only heightens our reliance on cars and fossil fuels.

Highway expansion projects harm already vulnerable populations. This expansion project will promulgate plumes of dust and fumes, worsening air pollution and health outcomes in nearby communities in the long term while interrupting people's everyday lives in the short term. After months of negotiation, Covington residents are still giving up over a dozen properties to the state to build this highway. And despite all of this, the project likely will not even solve the problems it is setting out to fix. Research strongly suggests that expanding highway capacity attracts more vehicles and increases traffic, the reverse effect of what KYTC's proposed plan is supposed to achieve.

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I am hoping that you will not support expanding the Brent Spence highway but rather invest in our communities, our transportation system, and our health and

safety.

Cleo Bell

1639 Sycamore Street

Cincinnati, OH 45202
United States

Phone: (513) 967-9579

Email Address: cleobell@gmail.com

Date of Submission: Wed, 10/05/2022 - 23:01

Submission Letter:

It is absolutely essential that you oppose the Brent Spence Bridge Corridor expansion project. The project won't solve the bridge's traffic and safety problems, but it will harm local communities, cost taxpayers billions of dollars, and only heightens our reliance on cars and fossil fuels. More car infrastructure INCREASES traffic, and poisons neighborhoods by making them unsafe and unpleasant.

It is a historical and ongoing fact that expansion projects harm already vulnerable populations. This expansion project will promulgate plumes of dust and fumes, worsening air pollution and health outcomes in nearby communities in the long term while interrupting people's everyday lives in the short term. After months of negotiation, Covington residents are still giving up over a dozen properties to the state to build this highway. And despite all of this, the project likely will not even solve the problems it is setting out to fix. Research strongly suggests that expanding highway capacity attracts more vehicles and increases traffic, the reverse effect of what KYTC's proposed plan is supposed to achieve.

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I am hoping that you will do the right things and not support expanding the Brent Spence highway but rather invest in our communities, our transportation system, and our health and safety.

Yann Kemper

3810 Spring House Ln

Cincinnati, OH 45217-1920
United States

Phone: (513) 208-7250

Email Address: masterolafyoda@gmail.com

Date of Submission: Wed, 10/05/2022 - 22:42

Submission Letter:

I'm writing to you today in hopes that you will oppose the Brent Spence Bridge Corridor expansion project. The project won't solve the bridge's traffic and safety problems, but it will harm local communities, cost taxpayers billions of dollars, and only heightens our reliance on cars and fossil fuels.

Highway expansion projects harm already vulnerable populations. This expansion project will promulgate plumes of dust and fumes, worsening air pollution and health outcomes in nearby communities in the long term while interrupting people's everyday lives in the short term. After months of negotiation, Covington residents are still giving up over a dozen properties to the state to build this highway. And despite all of this, the project likely will not even solve the problems it is setting out to fix. Research strongly suggests that expanding highway capacity attracts more vehicles and increases traffic, the reverse effect of what KYTC's proposed plan is supposed to achieve.

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I am hoping that you will not support expanding the Brent Spence highway but rather invest in our communities, our transportation system, and our health and

safety.

Mac Fogarty

6526 Spindlewick Lane

Cincinnati, OH 45230
United States

Phone: (513) 490-7018

Email Address: mr.galaxy.s.universe.iii@gmail.com

Date of Submission: Wed, 10/05/2022 - 22:32

Submission Letter:

I'm writing to you today in hopes that you will oppose the Brent Spence Bridge Corridor expansion project. The project won't solve the bridge's traffic and safety problems, but it will harm local communities, cost taxpayers billions of dollars, and only heightens our reliance on cars and fossil fuels.

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I am hoping that you will not support expanding the Brent Spence highway but rather invest in our communities, our transportation system, and our health and

safety.

Andrew Cornuelle

392 Katiebud Dr.

Cincinnati, OH 45238
United States

Phone: (513) 607-4510

Email Address: neoakb@pm.me

Date of Submission: Wed, 10/05/2022 - 22:04

Submission Letter:

I'm writing to you today in hopes that you will oppose the Brent Spence Bridge Corridor expansion project. The project won't solve the bridge's traffic and safety problems, but it will harm local communities, cost taxpayers billions of dollars, and only heightens our reliance on cars and fossil fuels.

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I am hoping that you will not support expanding the Brent Spence highway but rather invest in our communities, our transportation system, and our health and

safety.

Willow Tuttle

2467 Fairview Avenue

Cincinnati, OH 45219
United States

Phone: (513) 675-8058

Email Address: tuttlewl@mail.uc.edu

Date of Submission: Wed, 10/05/2022 - 22:01

Submission Letter:

I'm writing to you today in hopes that you will oppose the Brent Spence Bridge Corridor expansion project. The project won't solve the bridge's traffic and safety problems, but it will harm local communities, cost taxpayers billions of dollars, and only heightens our reliance on cars and fossil fuels.

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I am hoping that you will not support expanding the Brent Spence highway but rather invest in our communities, our transportation system, and our health and

safety.

Evan Lamping

378 Meadow Lark Lane

Batesville, IN 47006
United States

Phone: (812) 663-3677

Email Address: lampingevan@gmail.com

Date of Submission: Wed, 10/05/2022 - 21:44

Submission Letter:

I'm writing to you today in hopes that you will oppose the Brent Spence Bridge Corridor expansion project. The project won't solve the bridge's traffic and safety problems, but it will harm local communities, cost taxpayers billions of dollars, and only heightens our reliance on cars and fossil fuels.

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I am hoping that you will not support expanding the Brent Spence highway but rather invest in our communities, our transportation system, and our health and

safety.

Andrew Pearson

3896 Reading Road

Cincinnati, OH 45229
United States

Phone: (513) 702-8150

Email Address: pearson92@gmail.com

Date of Submission: Wed, 10/05/2022 - 21:37

Submission Letter:

I'm writing to you today in hopes that you will oppose the Brent Spence Bridge Corridor expansion project. The project won't solve the bridge's traffic and safety problems, but it will harm local communities, cost taxpayers billions of dollars, and only heightens our reliance on cars and fossil fuels.

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I am hoping that you will not support expanding the Brent Spence highway but rather invest in our communities, our transportation system, and our health and

Expand Transit not Highways

safety.

Casey Goldman

404 Reading Road

Cincinnati, OH 45202
United States

Phone: (309) 613-8840

Email Address: caseyspeaks@hotmail.com

Date of Submission: Wed, 10/05/2022 - 20:35

Submission Letter:

I'm writing to you today in hopes that you will oppose the Brent Spence Bridge Corridor expansion project. The project won't solve the bridge's traffic and safety problems, but it will harm local communities, cost taxpayers billions of dollars, and only heightens our reliance on cars and fossil fuels.

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I am hoping that you will not support expanding the Brent Spence highway but rather invest in our communities, our transportation system, and our health and

safety.

Emily Clausen

6611 Stewart Road

Cincinnati, OH 45236
United States

Phone: (985) 290-6288

Email Address: emilyc.clausen@gmail.com

Date of Submission: Wed, 10/05/2022 - 17:10

Submission Letter:

I'm writing to you today in hopes that you will oppose the Brent Spence Bridge Corridor expansion project. The project won't solve the bridge's traffic and safety problems, but it will harm local communities, cost taxpayers billions of dollars, and only heightens our reliance on cars and fossil fuels.

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I am hoping that you will not support expanding the Brent Spence highway but rather invest in our communities, our transportation system, and our health and

safety.

Nicholas peace

3901 Oak Street

Cincinnati, OH 45236
United States

Phone: (502) 419-2746

Email Address: maplenick222@gmail.com

Date of Submission: Wed, 10/05/2022 - 16:25

Submission Letter:

I'm writing to you today in hopes that you will oppose the Brent Spence Bridge Corridor expansion project. The project won't solve the bridge's traffic and safety problems, but it will harm local communities, cost taxpayers billions of dollars, and only heightens our reliance on cars and fossil fuels.

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I am hoping that you will not support expanding the Brent Spence highway but rather invest in our communities, our transportation system, and our health and

safety.

James Glenn

3910 Wess Park Drive

Cincinnati, OH 45217
United States

Phone: (513) 608-7882

Email Address: apuducin@icloud.com

Date of Submission: Wed, 10/05/2022 - 16:16

Submission Letter:

I'm writing to you today in hopes that you will oppose the Brent Spence Bridge Corridor expansion project. The project won't solve the bridge's traffic and safety problems, but it will harm local communities, cost taxpayers billions of dollars, and only heightens our reliance on cars and fossil fuels.

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safety.

Daniel Mahoney

4321 Ashland Ave

Cincinnati, OH 45212
United States

Phone: (513) 304-8415

Email Address: danielmahoney4@yahoo.com

Date of Submission: Wed, 10/05/2022 - 09:45

Submission Letter:

I'm writing to you today in hopes that you will oppose the Brent Spence Bridge Corridor expansion project. The project won't solve the bridge's traffic and safety problems, but it will harm local communities, cost taxpayers billions of dollars, and only heightens our reliance on cars and fossil fuels.

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safety.

Marji Mendelsohn

2770 Observatory Avenue

Cincinnati, OH 45208
United States

Phone: (513) 931-1916

Email Address: mgm507@gmail.com

Date of Submission: Tue, 10/04/2022 - 23:03

Submission Letter:

I'm writing to you today in hopes that you will oppose the Brent Spence Bridge Corridor expansion project. The project won't solve the bridge's traffic and safety problems, but it will harm local communities, cost taxpayers billions of dollars, and only heightens our reliance on cars and fossil fuels.

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safety.

Daniel Streicher

407 Race Street

Cincinnati, OH 45202
United States

Phone: (859) 412-0220

Email Address: dstrike5@aol.com

Date of Submission: Tue, 10/04/2022 - 12:22

Submission Letter:

I'm writing to you today in hopes that you will oppose the Brent Spence Bridge Corridor expansion project. The project won't solve the bridge's traffic and safety problems, but it will harm local communities, cost taxpayers billions of dollars, and only heightens our reliance on cars and fossil fuels.

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safety.

Brady Barnhill

3736 White Chapel Court

Amelia, OH 45102
United States

Phone: (513) 550-1890

Email Address: bradybarnhill01@gmail.com

Date of Submission: Tue, 10/04/2022 - 10:39

Submission Letter:

I'm writing to you today in hopes that you will oppose the Brent Spence Bridge Corridor expansion project. The project won't solve the bridge's traffic and safety problems, but it will harm local communities, cost taxpayers billions of dollars, and only heightens our reliance on cars and fossil fuels.

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safety.

Mary Krippenstapel

132 Ross Avenue

Bellevue, KY 41073
United States

Phone: (513) 289-1631

Email Address: krippjo@gmail.com

Date of Submission: Tue, 10/04/2022 - 08:29

Submission Letter:

I'm writing to you today in hopes that you will oppose the Brent Spence Bridge Corridor expansion project. The project won't solve the bridge's traffic and safety problems, but it will harm local communities, cost taxpayers billions of dollars, and only heightens our reliance on cars and fossil fuels.

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safety.

Matthew Latawiec

2429 Fairview Ave Fl 1

Cincinnati, OH 45219
United States

Phone: (513) 602-1814

Email Address: latawiecmatthew@gmail.com

Date of Submission: Tue, 10/04/2022 - 05:11

Submission Letter:

I'm writing to you today in hopes that you will oppose the Brent Spence Bridge Corridor expansion project. The project won't solve the bridge's traffic and safety problems, but it will harm local communities, cost taxpayers billions of dollars, and only heightens our reliance on cars and fossil fuels.

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safety.

John Schablein

4986 Concorde Alley

Blue Ash, OH 45242
United States

Phone: (513) 265-6218

Email Address: schabljt@gmail.com

Date of Submission: Mon, 10/03/2022 - 22:12

Submission Letter:

I'm writing to you today in hopes that you will oppose the Brent Spence Bridge Corridor expansion project. The project won't solve the bridge's traffic and safety problems, but it will harm local communities, cost taxpayers billions of dollars, and only heightens our reliance on cars and fossil fuels.

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safety.

Zach Caudill

693 Woodthrush Drive

Cincinnati, OH 45244
United States

Phone: (513) 518-3833

Email Address: zcaudill117@gmail.com

Date of Submission: Mon, 10/03/2022 - 21:02

Submission Letter:

I'm writing to you today in hopes that you will oppose the Brent Spence Bridge Corridor expansion project. The project won't solve the bridge's traffic and safety problems, but it will harm local communities, cost taxpayers billions of dollars, and only heightens our reliance on cars and fossil fuels.

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safety.

Mitchell Mustain

1705 Pleasant Street

Cincinnati, OH 45202
United States

Phone: (567) 674-0588

Email Address: mitchell_mustain@yahoo.com

Date of Submission: Mon, 10/03/2022 - 16:26

Submission Letter:

I'm writing to you today in hopes that you will oppose the Brent Spence Bridge Corridor expansion project. The project won't solve the bridge's traffic and safety problems, but it will harm local communities, cost taxpayers billions of dollars, and only heightens our reliance on cars and fossil fuels.

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safety.

Daniel Koontz

32 E Lake Shore Drive

Cincinnati, OH 45237
United States

Phone: (513) 628-4530

Email Address: daniellogankoontz@gmail.com

Date of Submission: Mon, 10/03/2022 - 15:56

Submission Letter:

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safety.

Andrew Fox

2545 Dennis Street

Cincinnati, OH 45219
United States

Phone: (812) 671-3644

Email Address: andfox4511@gmail.com

Date of Submission: Mon, 10/03/2022 - 14:28

Submission Letter:

I'm writing to you today in hopes that you will oppose the Brent Spence Bridge Corridor expansion project. The project won't solve the bridge's traffic and safety problems, but it will harm local communities, cost taxpayers billions of dollars, and only heightens our reliance on cars and fossil fuels.

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safety.

Joey Slovin

3991 Ballard Avenue

Cincinnati, OH 45209
United States

Phone: (513) 504-0605

Email Address: joeyslovin@gmail.com

Date of Submission: Mon, 10/03/2022 - 14:06

Submission Letter:

I'm writing to you today in hopes that you will oppose the Brent Spence Bridge Corridor expansion project. The project won't solve the bridge's traffic and safety problems, but it will harm local communities, cost taxpayers billions of dollars, and only heightens our reliance on cars and fossil fuels.

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safety.

Ryan Minnich

1863 Chase Avenue

Cincinnati, OH 45223
United States

Phone: (513) 276-6868

Email Address: minnicrs@mail.uc.edu

Date of Submission: Mon, 10/03/2022 - 12:23

Submission Letter:

I'm writing to you today in hopes that you will oppose the Brent Spence Bridge Corridor expansion project. The project won't solve the bridge's traffic and safety problems, but it will harm local communities, cost taxpayers billions of dollars, and only heightens our reliance on cars and fossil fuels.

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safety.

Sarah Hall

1135 Cecelia Avenue

Park Hills, KY 41011
United States

Phone: (859) 443-9566

Email Address: sarah_tepe@Yahoo.com

Date of Submission: Mon, 10/03/2022 - 11:59

Submission Letter:

I'm writing to you today in hopes that you will oppose the Brent Spence Bridge Corridor expansion project. The project won't solve the bridge's traffic and safety problems, but it will harm local communities, cost taxpayers billions of dollars, and only heightens our reliance on cars and fossil fuels.

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safety.

Theodore Baker

3063 Victoria Avenue

Cincinnati, OH 45208
United States

Phone: (513) 368-4497

Email Address: theobaker2015@gmail.com

Date of Submission: Mon, 10/03/2022 - 11:56

Submission Letter:

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I am hoping that you will not support expanding the Brent Spence highway but rather invest in our communities, our transportation system, and our health and

safety.

Joshua Junker

4465 Hubble Rd

Cincinnati, OH 45247
United States

Phone: (513) 817-6550

Email Address: junkerj9@gmail.com

Date of Submission: Mon, 10/03/2022 - 11:47

Submission Letter:

I'm writing to you today in hopes that you will oppose the Brent Spence Bridge Corridor expansion project. The project won't solve the bridge's traffic and safety problems, but it will harm local communities, cost taxpayers billions of dollars, and only heightens our reliance on cars and fossil fuels.

Highway expansion projects harm already vulnerable populations. This expansion project will promulgate plumes of dust and fumes, worsening air pollution and health outcomes in nearby communities in the long term while interrupting people's everyday lives in the short term. After months of negotiation, Covington residents are still giving up over a dozen properties to the state to build this highway. And despite all of this, the project likely will not even solve the problems it is setting out to fix. Research strongly suggests that expanding highway capacity attracts more vehicles and increases traffic, the reverse effect of what KYTC's proposed plan is supposed to achieve.

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safety.

And if somehow the bridge still moves forward, any project to expand the highways should invest in rightsizing and reconnecting the 75 Corridor from the Western Hills Viaduct to the Bridge, eliminating the Spaghetti Interchange and facilitating road and pedestrian connections to west of 75 in what the historic West End was before misguided Urban Renewal and Interstate construction displaced a neighborhood of 25,000. We must do this, new bridge or not, and if it were not to happen would lay permanent the effects of urban renewal on the Urban Core of Cincinnati and lose a sense of our identity forever. We must think of the future instead of reinforcing past mistakes that ODOT with the City made from 1958-1970.

Transit in Greater Cincinnati should be considered a top investment. It is paramount for the future of our city and our region. Focusing only on building a new expensive bridge without consideration to the future of our city would be a terrible mistake.

Jon Neal

544 Terrace Avenue

Cincinnati, OH 45220
United States

Phone: (859) 907-2828

Email Address: jon.d.neal@gmail.com

Date of Submission: Mon, 10/03/2022 - 11:36

Submission Letter:

I'm writing to you today in hopes that you will oppose the Brent Spence Bridge Corridor expansion project. The project won't solve the bridge's traffic and safety problems, but it will harm local communities, cost taxpayers billions of dollars, and only heightens our reliance on cars and fossil fuels.

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I am hoping that you will not support expanding the Brent Spence highway but rather invest in our communities, our transportation system, and our health and

safety.

Rob Monsma

1435 Christy Avenue

Louisville, KY 40204
United States

Phone: (502) 794-9084

Email Address: monsma.rob@gmail.com

Date of Submission: Mon, 10/03/2022 - 11:12

Submission Letter:

I'm writing to you today in hopes that you will oppose the Brent Spence Bridge Corridor expansion project. The project won't solve the bridge's traffic and safety problems, but it will harm local communities, cost taxpayers billions of dollars, and only heightens our reliance on cars and fossil fuels.

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I am hoping that you will not support expanding the Brent Spence highway but rather invest in our communities, our transportation system, and our health and

safety.

Samuel Gaston

1869 Muskegon Drive

Cincinnati, OH 45255
United States

Phone: (513) 646-8370

Email Address: gaston45@gmail.com

Date of Submission: Mon, 10/03/2022 - 11:06

Submission Letter:

Please oppose the Brent Spence Bridge Corridor expansion project. The project will only increase our reliance on cars and fossil fuels, leave less room for residents and businesses, and will not help those who cannot afford a vehicle.

Clint Bourgeois

6265 Cortelyou Ave

Cincinnati, OH 45213
United States

Phone: (513) 404-9264

Email Address: Bourgeect@gmail.com

Date of Submission: Mon, 10/03/2022 - 10:59

Submission Letter:

I'm writing to you today in hopes that you will oppose the Brent Spence Bridge Corridor expansion project. The project won't solve the bridge's traffic and safety problems, but it will harm local communities, cost taxpayers billions of dollars, and only heightens our reliance on cars and fossil fuels.

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safety.

Nick Bauer

2138 N Fort Thomas Ave

Fort Thomas, KY 41075
United States

Phone: (614) 364-6248

Email Address: nrbauer2@gmail.com

Date of Submission: Mon, 10/03/2022 - 10:53

Submission Letter:

I'm writing to you today in hopes that you will oppose the Brent Spence Bridge Corridor expansion project. The project won't solve the bridge's traffic and safety problems, but it will harm local communities, cost taxpayers billions of dollars, and only heightens our reliance on cars and fossil fuels.

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safety.

Scott Hand

2522 May St, 1

Cincinnati, OH 45206
United States

Phone: (708) 539-5511

Email Address: scothand@gmail.com

Date of Submission: Mon, 10/03/2022 - 10:49

Submission Letter:

I'm writing to you today in hopes that you will oppose the Brent Spence Bridge Corridor expansion project. The project won't solve the bridge's traffic and safety problems, but it will harm local communities, cost taxpayers billions of dollars, and only heightens our reliance on cars and fossil fuels.

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I am hoping that you will not support expanding the Brent Spence highway but rather invest in our communities, our transportation system, and our health and

safety.

Clinton McIntosh

2622 Moorman Ave

Cincinnati, OH 45206
United States

Phone: (859) 394-3671

Email Address: clintonmcintosh91@gmail.com

Date of Submission: Mon, 10/03/2022 - 10:49

Submission Letter:

I'm writing to you today in hopes that you will oppose the Brent Spence Bridge Corridor expansion project. The project won't solve the bridge's traffic and safety problems, but it will harm local communities, cost taxpayers billions of dollars, and only heightens our reliance on cars and fossil fuels.

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safety.

David Robison

413 Ingram Road

Cincinnati, OH 45218
United States

Phone: (740) 586-9028

Email Address: robi1475@yahoo.com

Date of Submission: Mon, 10/03/2022 - 08:43

Submission Letter:

I'm writing to you today in hopes that you will oppose the Brent Spence Bridge Corridor expansion project. The project won't solve the bridge's traffic and safety problems, but it will harm local communities, cost taxpayers billions of dollars, and only heightens our reliance on cars and fossil fuels.

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safety.

Michael Fry

1626 Harbeson Ave

Cincinnati, OH 45224
United States

Phone: (513) 201-5790

Email Address: michael.fry@gmail.com

Date of Submission: Sun, 10/02/2022 - 16:23

Submission Letter:

I'm writing to you today in hopes that you will oppose the Brent Spence Bridge Corridor expansion project. The project won't solve the bridge's traffic and safety problems, but it will harm local communities, cost taxpayers billions of dollars, and only heightens our reliance on cars and fossil fuels.

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safety.

Joseph Bellante

3338 Walworth Ave

Cincinnati, OH 45226
United States

Phone: (513) 518-1114

Email Address: joejobiker@aol.com

Date of Submission: Sun, 10/02/2022 - 11:24

Submission Letter:

I'm writing to you today in hopes that you will oppose the Brent Spence Bridge Corridor expansion project. The project won't solve the bridge's traffic and safety problems, but it will harm local communities, cost taxpayers billions of dollars, and only heightens our reliance on cars and fossil fuels.

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safety.

Phillip Schaefer

2511 Langdon Farm Road

Cincinnati, OH 45212
United States

Phone: (937) 430-4084

Email Address: phillip.schaefer36@gmail.com

Date of Submission: Sat, 10/01/2022 - 14:32

Submission Letter:

I'm writing to you today in hopes that you will oppose the Brent Spence Bridge Corridor expansion project. The project won't solve the bridge's traffic and safety problems, but it will harm local communities, cost taxpayers billions of dollars, and only heightens our reliance on cars and fossil fuels.

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safety.

Peirce Johnston

156 Wentworth Ave

Cincinnati, OH 45220-1802
United States

Phone: (513) 317-4817

Email Address: johnstpw@uc.edu

Date of Submission: Sat, 10/01/2022 - 10:47

Submission Letter:

As a Cincinnati citizen, I'm writing to ask you to oppose the Brent Spence Bridge Corridor expansion project. The project won't solve the bridge's traffic and safety problems, but it will harm local communities, cost taxpayers billions of dollars, and only heightens our reliance on cars and fossil fuels.

Highway expansion projects harm already vulnerable populations. This expansion project will promulgate plumes of dust and fumes, worsening air pollution and health outcomes in nearby communities in the long term while interrupting people's everyday lives in the short term. After months of negotiation, Covington residents are still giving up over a dozen properties to the state to build this highway. And despite all of this, the project likely will not even solve the problems it is setting out to fix. Research strongly suggests that expanding highway capacity attracts more vehicles and increases traffic, the reverse effect of what KYTC's proposed plan is supposed to achieve.

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safety.

Aaron Weissman

3814 Mount Vernon Avenue

Cincinnati, OH 45209
United States

Phone: (339) 221-1265

Email Address: weissman.aaron@gmail.com

Date of Submission: Sat, 10/01/2022 - 08:54

Submission Letter:

I'm writing to you today in hopes that you will oppose the Brent Spence Bridge Corridor expansion project. The project won't solve the bridge's traffic and safety problems, but it will harm local communities, cost taxpayers billions of dollars, and only heightens our reliance on cars and fossil fuels.

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safety.

Jerry Fowler

4585 Lakeland Dr

Batavia, OH 45103
United States

Phone: (234) 716-7542

Email Address: jerryfowler88@gmail.com

Date of Submission: Fri, 09/30/2022 - 21:23

Submission Letter:

I'm writing to you today in hopes that you will oppose the Brent Spence Bridge Corridor expansion project. The project won't solve the bridge's traffic and safety problems, but it will harm local communities, cost taxpayers billions of dollars, and only heightens our reliance on cars and fossil fuels.

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safety.

Aidan Strasser

3620 Stettinius Avenue

Cincinnati, OH 45208
United States

Phone: (513) 403-1399

Email Address: aidan.strasser22@gmail.com

Date of Submission: Fri, 09/30/2022 - 15:33

Submission Letter:

I'm writing to you today in hopes that you will oppose the Brent Spence Bridge Corridor expansion project. The project won't solve the bridge's traffic and safety problems, but it will harm local communities, cost taxpayers billions of dollars, and only heightens our reliance on cars and fossil fuels.

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I am hoping that you will not support expanding the Brent Spence highway but rather invest in our communities, our transportation system, and our health and

safety.

Liam McGee

2621 Evergreen Drive

Covington, KY 41017
United States

Phone: (859) 240-3145

Email Address: ljmcgee01@gmail.com

Date of Submission: Fri, 09/30/2022 - 13:15

Submission Letter:

I'm writing to you today in hopes that you will oppose the Brent Spence Bridge Corridor expansion project. The project won't solve the bridge's traffic and safety problems, but it will harm local communities, cost taxpayers billions of dollars, and only heightens our reliance on cars and fossil fuels.

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I am hoping that you will not support expanding the Brent Spence highway but rather invest in our communities, our transportation system, and our health and

safety.

Nicholas Fischer

2904 Markbreit Avenue

Cincinnati, OH 45209
United States

Phone: (513) 679-0384

Email Address: fischer149@hotmail.com

Date of Submission: Fri, 09/30/2022 - 07:37

Submission Letter:

I'm writing to you today in hopes that you will oppose the Brent Spence Bridge Corridor expansion project. The project won't solve the bridge's traffic and safety problems, but it will harm local communities, cost taxpayers billions of dollars, and only heightens our reliance on cars and fossil fuels.

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safety.

Jon Lyons

329 W 7th street

Covington, KY 41012
United States

Phone: (502) 370-5367

Email Address: jon.lyons7793@gmail.com

Date of Submission: Fri, 09/30/2022 - 02:07

Submission Letter:

I'm writing to you today in hopes that you will oppose the Brent Spence Bridge Corridor expansion project. The project won't solve the bridge's traffic and safety problems, but it will harm local communities, cost taxpayers billions of dollars, and only heightens our reliance on cars and fossil fuels.

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safety.

Liam Silliman

4036 Ballard Woods Drive

Smithfield, KY 40068
United States

Phone: (502) 939-5426

Email Address: losilliman@gmail.com

Date of Submission: Thu, 09/29/2022 - 12:40

Submission Letter:

I'm writing to you today in hopes that you will oppose the Brent Spence Bridge Corridor expansion project. The project won't solve the bridge's traffic and safety problems, but it will harm local communities, cost taxpayers billions of dollars, and only heightens our reliance on cars and fossil fuels.

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safety.

J Dailey

138 Merlin Place

Florence, KY 41042
United States

Phone: (859) 322-8361

Email Address: daileyshop@twc.com

Date of Submission: Thu, 09/29/2022 - 07:45

Submission Letter:

I'm writing to you today in hopes that you will oppose the Brent Spence Bridge Corridor expansion project. The project won't solve the bridge's traffic and safety problems, but it will harm local communities, cost taxpayers billions of dollars, and only heightens our reliance on cars and fossil fuels.

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safety.

Michael Zaret

2312 Elstun Road

Cincinnati, OH 45230
United States

Phone: (513) 706-0612

Email Address: papayahed1@aol.com

Date of Submission: Thu, 09/29/2022 - 07:27

Submission Letter:

I'm writing to you today in hopes that you will oppose the Brent Spence Bridge Corridor expansion project. The project won't solve the bridge's traffic and safety problems, but it will harm local communities, cost taxpayers billions of dollars, and only heightens our reliance on cars and fossil fuels.

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safety.

Steven Mast

214 E 9th St

Newport, KY 41071
United States

Phone: (619) 886-9633

Email Address: knsmast@gmail.com

Date of Submission: Thu, 09/29/2022 - 05:45

Submission Letter:

I'm writing to you today in hopes that you will oppose the Brent Spence Bridge Corridor expansion project. The project won't solve the bridge's traffic and safety problems, but it will harm local communities, cost taxpayers billions of dollars, and only heightens our reliance on cars and fossil fuels.

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safety.

John Maslovar

1369 Rubyhill Ln

Florence, KY 41042
United States

Phone: (806) 401-9123

Email Address: jmaslovar@gmail.com

Date of Submission: Wed, 09/28/2022 - 19:57

Submission Letter:

I'm writing to you today in hopes that you will oppose the Brent Spence Bridge Corridor expansion project. The project won't solve the bridge's traffic and safety problems, but it will harm local communities, cost taxpayers billions of dollars, and only heightens our reliance on cars and fossil fuels.

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safety.

JULIE ZIMMERMAN

1019 Redway Ave

Cincinnati, OH 45229-1915
United States

Phone: (513) 543-1501

Email Address: julieirwinzimmerman@gmail.com

Date of Submission: Wed, 09/28/2022 - 11:02

Submission Letter:

I'm writing to ask that you oppose the Brent Spence Bridge Corridor expansion project. The project won't solve the bridge's traffic and safety problems, but it will harm local communities, cost taxpayers billions of dollars, and heighten our reliance on cars and fossil fuels.

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I am hoping that you will not support expanding the Brent Spence highway but rather invest in our communities, our transportation system, and our health and safety.

Wesley Wettengel

2517 8 Mile Road

Cincinnati, OH 45244
United States

Phone: (513) 373-0627

Email Address: wettengel@gmail.com

Date of Submission: Wed, 09/28/2022 - 08:12

Submission Letter:

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safety.

Karen Fahlgren

2517 Eight Mile Road

Cincinnati, OH 45244
United States

Phone: (513) 474-2222

Email Address: kfahlgren@yahoo.com

Date of Submission: Tue, 09/27/2022 - 21:19

Submission Letter:

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safety.

aaron earlywine

341 Cassatt St

Cincinnati, OH 45219
United States

Phone: (541) 868-7344

Email Address: ajearlywine@gmail.com

Date of Submission: Tue, 09/27/2022 - 21:16

Submission Letter:

I'm writing to you today in hopes that you will oppose the Brent Spence Bridge Corridor expansion project. The project won't solve the bridge's traffic and safety problems, but it will harm local communities, cost taxpayers billions of dollars, and only heightens our reliance on cars and fossil fuels.

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safety.

Hazel Wettengel

2517 8 Mile Road

Cincinnati, OH 45244
United States

Phone: (513) 331-4140

Email Address: hazelwettengel@gmail.com

Date of Submission: Tue, 09/27/2022 - 21:13

Submission Letter:

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Bennett Wettengel

2517 Eight Mile road

Cincinnati, OH 45244
United States

Phone: (513) 504-8334

Email Address: bennettfw@gmail.com

Date of Submission: Tue, 09/27/2022 - 21:13

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safety.

Mary Boehm

2421 Swansea Rd

Columbus, OH 43221
United States

Phone: (614) 949-1250

Email Address: monkeycraz556@gmail.com

Date of Submission: Tue, 09/27/2022 - 21:10

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safety.

Hanna Varner

620 Balbriggan Court

Cincinnati, OH 45255
United States

Phone: (513) 667-9167

Email Address: hannavarner23@gmail.com

Date of Submission: Tue, 09/27/2022 - 21:10

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safety.

Jack Tilton

9610 Fox Meadow Lane

Chardon, OH 44024
United States

Phone: (440) 975-6147

Email Address: jackdtilton@outlook.com

Date of Submission: Tue, 09/27/2022 - 21:10

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safety.

Rebekah sadler.rm@gmail.com

1733 grandle Court

Cincinnati, OH 45230
United States

Phone: (513) 307-4090

Email Address: bekahsadler17@gmail.com

Date of Submission: Tue, 09/27/2022 - 21:09

Submission Letter:

I'm writing to you today in hopes that you will oppose the Brent Spence Bridge Corridor expansion project. The project won't solve the bridge's traffic and safety problems, but it will harm local communities, cost taxpayers billions of dollars, and only heightens our reliance on cars and fossil fuels.

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safety.

Sarah Polter

3914 Avenue K,

Kearney, NE 68847
United States

Phone: (308) 440-8497

Email Address: poltersa1901@gmail.com

Date of Submission: Tue, 09/27/2022 - 18:20

Submission Letter:

I'm writing to you today in hopes that you will oppose the Brent Spence Bridge Corridor expansion project. The project won't solve the bridge's traffic and safety problems, but it will harm local communities, cost taxpayers billions of dollars, and only heightens our reliance on cars and fossil fuels.

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safety.

John Wettengel

2517 8 Mile Road

Cincinnati, OH 45244
United States

Phone: (513) 504-7552

Email Address: johnwettengel@gmail.com

Date of Submission: Tue, 09/27/2022 - 14:43

Submission Letter:

I'm writing to you today in hopes that you will oppose the Brent Spence Bridge Corridor expansion project. The project won't solve the bridge's traffic and safety problems, but it will harm local communities, cost taxpayers billions of dollars, and only heightens our reliance on cars and fossil fuels.

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safety.

Oluwatomisin Okunade

Elm Street

Cincinnati, OH 45238
United States

Phone: (513) 884-5629

Email Address: simplytomisin@yahoo.com

Date of Submission: Tue, 09/27/2022 - 12:19

Submission Letter:

I'm writing to you today in hopes that you will oppose the Brent Spence Bridge Corridor expansion project. The project won't solve the bridge's traffic and safety problems, but it will harm local communities, cost taxpayers billions of dollars, and only heightens our reliance on cars and fossil fuels.

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safety.

James McDermott

2301 Auburn Avenue

Cincinnati, OH 45219
United States

Phone: (929) 334-8574

Email Address: mcdermje@mail.uc.edu

Date of Submission: Mon, 09/26/2022 - 19:31

Submission Letter:

I'm writing to you today in hopes that you will oppose the Brent Spence Bridge Corridor expansion project. The project won't solve the bridge's traffic and safety problems, but it will harm local communities, cost taxpayers billions of dollars, and only heightens our reliance on cars and fossil fuels.

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safety.

Christopher Smith

2324 Ohio Avenue

Cincinnati, OH 45219
United States

Phone: (513) 478-9893

Email Address: courtview7@yahoo.com

Date of Submission: Mon, 09/26/2022 - 18:24

Submission Letter:

I'm writing to you today in hopes that you will oppose the Brent Spence Bridge Corridor expansion project. The project won't solve the bridge's traffic and safety problems, but it will harm local communities, cost taxpayers billions of dollars, and only heightens our reliance on cars and fossil fuels.

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safety.

marquis jones

1282 Norman Avenue

Cincinnati, OH 45231
United States

Phone: (513) 802-6359

Email Address: jones.marquis16@gmail.com

Date of Submission: Sun, 09/25/2022 - 13:31

Submission Letter:

I'm writing to you today in hopes that you will oppose the Brent Spence Bridge Corridor expansion project. The project won't solve the bridge's traffic and safety problems, but it will harm local communities, cost taxpayers billions of dollars, and only heightens our reliance on cars and fossil fuels.

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safety.

Kristina McBride

708 W Main St, Apt 323A, Apt 323A

Loveland, OH 45140
United States

Phone: (513) 257-7259

Email Address: heymaccamay@gmail.com

Date of Submission: Sat, 09/24/2022 - 14:48

Submission Letter:

I'm writing to you today in hopes that you will oppose the Brent Spence Bridge Corridor expansion project. The project won't solve the bridge's traffic and safety problems, but it will harm local communities, cost taxpayers billions of dollars, and only heightens our reliance on cars and fossil fuels.

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safety.

Nick Brophy

514 W. Course street

Cincinnati, OH 45203
United States

Phone: (859) 866-1733

Email Address: nicholas.brophy28@gmail.com

Date of Submission: Fri, 09/23/2022 - 19:10

Submission Letter:

I'm writing to you today in hopes that you will oppose the Brent Spence Bridge Corridor expansion project. The project won't solve the bridge's traffic and safety problems, but it will harm local communities, cost taxpayers billions of dollars, and only heightens our reliance on cars and fossil fuels.

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safety.

Alex Faeth

2116 Ravine St, Apt. 3

Cincinnati, OH 45214
United States

Phone: (513) 509-3239

Email Address: alex.faeth@icloud.com

Date of Submission: Fri, 09/23/2022 - 18:48

Submission Letter:

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safety.

Luke Sillies

1820 Freeman Ave

Cincinnati, OH 45214
United States

Phone: (937) 750-7449

Email Address: lukesillies@gmail.com

Date of Submission: Fri, 09/23/2022 - 17:36

Submission Letter:

I'm writing to you today in hopes that you will oppose the Brent Spence Bridge Corridor expansion project. The project won't solve the bridge's traffic and safety problems, but it will harm local communities, cost taxpayers billions of dollars, and only heightens our reliance on cars and fossil fuels.

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safety.

Gary Dangel

2318 Kemper Lane Ste 100

Cincinnati, OH 45206
United States

Phone: (513) 751-7100

Email Address: gary.dangel@gmail.com

Date of Submission: Fri, 09/23/2022 - 17:21

Submission Letter:

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safety.

Zachary Smith

4223 Delaney Street

Cincinnati, OH 45223
United States

Phone: (513) 346-0146

Email Address: Zacharysmith1.618@gmail.com

Date of Submission: Fri, 09/23/2022 - 16:19

Submission Letter:

I'm writing to you today in hopes that you will oppose the Brent Spence Bridge Corridor expansion project. The project won't solve the bridge's traffic and safety problems, but it will harm local communities, cost taxpayers billions of dollars, and only heightens our reliance on cars and fossil fuels.

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safety.

Anthony Jameson

1556 Glen Parker Avenue

Cincinnati, OH 45223
United States

Phone: (513) 508-1178

Email Address: ajamesonan@yahoo.com

Date of Submission: Fri, 09/23/2022 - 14:45

Submission Letter:

I'm writing to you today in hopes that you will oppose the Brent Spence Bridge Corridor expansion project. The project won't solve the bridge's traffic and safety problems, but it will harm local communities, cost taxpayers billions of dollars, and only heightens our reliance on cars and fossil fuels.

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safety.

Alistair Watson

801 Plum Street

Cincinnati, OH 45202
United States

Phone: (609) 865-3144

Email Address: alistair.watson@aol.com

Date of Submission: Fri, 09/23/2022 - 14:21

Submission Letter:

I'm writing to you today in hopes that you will oppose the Brent Spence Bridge Corridor expansion project. The project won't solve the bridge's traffic and safety problems, but it will harm local communities, cost taxpayers billions of dollars, and only heightens our reliance on cars and fossil fuels.

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safety.

Justin Marshall

1211 Wareham Drive

Cincinnati, OH 45202
United States

Phone: (513) 405-6387

Email Address: jmarsh5@me.com

Date of Submission: Fri, 09/23/2022 - 11:06

Submission Letter:

I'm writing to you today in hopes that you will oppose the Brent Spence Bridge Corridor expansion project. The project won't solve the bridge's traffic and safety problems, but it will harm local communities, cost taxpayers billions of dollars, and only heightens our reliance on cars and fossil fuels.

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safety.

Bob Schwartz

353 West 4th Street

Cincinnati, OH 45202
United States

Phone: (513) 324-9686

Email Address: 5chw4r7z@gmail.com

Date of Submission: Fri, 09/23/2022 - 10:52

Submission Letter:

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I am hoping that you will not support expanding the Brent Spence highway but rather invest in our communities, our transportation system, and our health and

safety.

Hannah Sparks Johnson

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Cincinnati, OH 45238
United States

Phone: (513) 807-3712

Email Address: sparkshannahe@gmail.com

Date of Submission: Fri, 09/23/2022 - 09:36

Submission Letter:

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safety.

Colin McQuinn

3009 Clifton Avenue

Cincinnati, OH 45220
United States

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Date of Submission: Fri, 09/23/2022 - 08:27

Submission Letter:

I'm writing to you today in hopes that you will oppose the Brent Spence Bridge Corridor expansion project. The project won't solve the bridge's traffic and safety problems, but it will harm local communities, cost taxpayers billions of dollars, and only heightens our reliance on cars and fossil fuels.

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safety.

Brad Fertig

3035 Gilbert Ave #10

Cincinnati, OH 45206
United States

Phone: (513) 714-2212

Email Address: hardwired513@gmail.com

Date of Submission: Fri, 09/23/2022 - 08:18

Submission Letter:

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safety.

Ryan Minnich

1863 Chase Avenue

Cincinnati, OH 45223
United States

Phone: (513) 276-6868

Email Address: minnicrs@mail.uc.edu

Date of Submission: Thu, 09/22/2022 - 21:37

Submission Letter:

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safety.

Kathleen Colley

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Cincinnati, OH 45227
United States

Phone: (513) 850-1359

Email Address: kmcolley@gmail.com

Date of Submission: Thu, 09/22/2022 - 21:15

Submission Letter:

I'm writing to you today in hopes that you will oppose the Brent Spence Bridge Corridor expansion project. The project won't solve the bridge's traffic and safety problems, but it will harm local communities, cost taxpayers billions of dollars, and only heightens our reliance on cars and fossil fuels.

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safety.

Barbara Didrichsen

6231 Cortelyou Avenue

Cincinnati, OH 45213
United States

Phone: (513) 235-5471

Email Address: barbdid@gmail.com

Date of Submission: Thu, 09/22/2022 - 20:56

Submission Letter:

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safety.

Crystal Dandridge

8318 Brigantine Ct Apt 88

West Chester, OH 45069
United States

Phone: (513) 328-4158

Email Address: cjdjam@gmail.com

Date of Submission: Thu, 09/22/2022 - 20:02

Submission Letter:

I'm writing to you today in hopes that you will oppose the Brent Spence Bridge Corridor expansion project. The project won't solve the bridge's traffic and safety problems, but it will harm local communities, cost taxpayers billions of dollars, and only heightens our reliance on cars and fossil fuels.

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safety.

Matthew Latawiec

2429 Fairview Ave Fl 1

Cincinnati, OH 45219
United States

Phone: (513) 602-1814

Email Address: latawiecmatthew@gmail.com

Date of Submission: Thu, 09/22/2022 - 18:52

Submission Letter:

I'm writing to you today in hopes that you will oppose the Brent Spence Bridge Corridor expansion project. The project won't solve the bridge's traffic and safety problems, but it will harm local communities, cost taxpayers billions of dollars, and only heightens our reliance on cars and fossil fuels.

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safety.

Justin Ogilby

3788 Clifton Ave

Cincinnati, OH 45220
United States

Phone: (513) 545-4377

Email Address: jogilby@stackct.com

Date of Submission: Thu, 09/22/2022 - 18:48

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safety.

Andrew Vielhaber

1616 Ruth Avenue,

CINCINNATI, OH 45207
United States

Phone: (440) 487-0961

Email Address: vielhaber8@gmail.com

Date of Submission: Thu, 09/22/2022 - 18:14

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Erin Wells

1220 Harrison Avenue, Apt 2B

Cincinnati, OH 45214
United States

Phone: (513) 502-7752

Email Address: ewells@powernetco.com

Date of Submission: Thu, 09/22/2022 - 18:05

Submission Letter:

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Logan Baer

330 Park Avenue

Newport, KY 41071
United States

Phone: (708) 800-7543

Email Address: lbaerpersonal@gmail.com

Date of Submission: Thu, 09/22/2022 - 17:53

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Luis Finke

972 Ludlow Ave

Cincinnati, OH 45220-1020
United States

Phone: (513) 608-8703

Email Address: luisfinke@gmail.com

Date of Submission: Thu, 09/22/2022 - 17:45

Submission Letter:

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Justin Jeffre

2144 Ohio Ave

Cincinnati, OH 45219
United States

Phone: (917) 881-9882

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Date of Submission: Thu, 09/22/2022 - 17:43

Submission Letter:

I'm writing to you today in hopes that you will oppose the Brent Spence Bridge Corridor expansion project. The project won't solve the bridge's traffic and safety problems, but it will harm local communities, cost taxpayers billions of dollars, and only heightens our reliance on cars and fossil fuels.

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Chet Closson

4223 Williamson Place

Cincinnati, OH 45223
United States

Phone: (614) 795-4149

Email Address: chet.closson@gmail.com

Date of Submission: Thu, 09/22/2022 - 17:33

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safety.

Nolan Nicaise

303 Greenup Street, Apt 22

Covington, KY 41011
United States

Phone: (859) 750-0436

Email Address: nolannicaise@gmail.com

Date of Submission: Thu, 09/22/2022 - 17:31

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safety.

JOSEPH GASTENVELD

1421 Race St, APT 1

Cincinnati, OH 45202
United States

Phone: (859) 466-2335

Email Address: joegastenveld@gmail.com

Date of Submission: Thu, 09/22/2022 - 17:30

Submission Letter:

I'm writing to you today in hopes that you will oppose the Brent Spence Bridge Corridor expansion project. The project won't solve the bridge's traffic and safety problems, but it will harm local communities, cost taxpayers billions of dollars, and only heightens our reliance on cars and fossil fuels.

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safety.

Jacob Gardner

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Cincinnati, OH 45220
United States

Phone: (330) 280-4127

Email Address: jacobgardner4@gmail.com

Date of Submission: Thu, 09/22/2022 - 17:29

Submission Letter:

I'm writing to you today in hopes that you will oppose the Brent Spence Bridge Corridor expansion project. The project won't solve the bridge's traffic and safety problems, but it will harm local communities, cost taxpayers billions of dollars, and only heightens our reliance on cars and fossil fuels.

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safety.

Grant Wagner

1632 Central Parkway, Apartment 310

Cincinnati, OH 45202
United States

Phone: (614) 947-9843

Email Address: pickledpiewagner@gmail.com

Date of Submission: Thu, 09/22/2022 - 17:24

Submission Letter:

I'm writing to you today in hopes that you will oppose the Brent Spence Bridge Corridor expansion project. The project won't solve the bridge's traffic and safety problems, but it will harm local communities, cost taxpayers billions of dollars, and only heightens our reliance on cars and fossil fuels.

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safety.

Alexander Browne

1342 Walnut Street

Cincinnati, OH 45202
United States

Phone: (513) 225-1864

Email Address: alexbrowne1996@yahoo.com

Date of Submission: Thu, 09/22/2022 - 16:59

Submission Letter:

I'm writing to you today in hopes that you will oppose the Brent Spence Bridge Corridor expansion project. The project won't solve the bridge's traffic and safety problems, but it will harm local communities, cost taxpayers billions of dollars, and only heightens our reliance on cars and fossil fuels.

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safety.

Joey Reibling

31 Dumfries Avenue

Fort Thomas, KY 41075
United States

Phone: (513) 709-6402

Email Address: jreibling@mac.com

Date of Submission: Thu, 09/22/2022 - 13:38

Submission Letter:

I'm writing to you today in hopes that you will oppose the Brent Spence Bridge Corridor expansion project. The project won't solve the bridge's traffic and safety problems, but it will harm local communities, cost taxpayers billions of dollars, and only heightens our reliance on cars and fossil fuels.

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safety.

Ana Mendoza

4773 Loretta Avenue

Cincinnati, OH 45238
United States

Phone: (806) 786-9297

Email Address: anam1899@gmail.com

Date of Submission: Thu, 09/22/2022 - 07:56

Submission Letter:

I'm writing to you today in hopes that you will oppose the Brent Spence Bridge Corridor expansion project. The project won't solve the bridge's traffic and safety problems, but it will harm local communities, cost taxpayers billions of dollars, and only heightens our reliance on cars and fossil fuels.

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safety.

Dominic Mottola

655 Eden Park Dr, 219

Cincinnati, OH 45202
United States

Phone: (567) 208-8968

Email Address: dominicmottola@gmail.com

Date of Submission: Thu, 09/22/2022 - 06:59

Submission Letter:

I'm writing to you today in hopes that you will oppose the Brent Spence Bridge Corridor expansion project. The project won't solve the bridge's traffic and safety problems, but it will harm local communities, cost taxpayers billions of dollars, and only heightens our reliance on cars and fossil fuels.

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safety.

Alexander Porr

946 Morris Street

Cincinnati, OH 45206
United States

Phone: (412) 973-0543

Email Address: aporr3@gmail.com

Date of Submission: Thu, 09/22/2022 - 00:36

Submission Letter:

I'm writing to you today in hopes that you will oppose the Brent Spence Bridge Corridor expansion project. The project won't solve the bridge's traffic and safety problems, but it will harm local communities, cost taxpayers billions of dollars, and only heightens our reliance on cars and fossil fuels.

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safety.

Noah Cable

1404 East McMillan Street

Cincinnati, OH 45206
United States

Phone: (304) 546-6455

Email Address: cablen1@nku.edu

Date of Submission: Wed, 09/21/2022 - 16:35

Submission Letter:

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safety.

Quinn Ring

6213 Beech View Circle

Cincinnati, OH 45213
United States

Phone: (513) 417-3651

Email Address: quinnring@me.com

Date of Submission: Wed, 09/21/2022 - 15:59

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safety.

charles hanson

450 Klotter Avenue

Cincinnati, OH 45214
United States

Phone: (440) 591-8834

Email Address: cjhanson42@gmail.com

Date of Submission: Wed, 09/21/2022 - 15:30

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Tyler Roth

H

Milford, OH 45150
United States

Phone: (513) 708-3607

Email Address: tylerjroth7@gmail.com

Date of Submission: Wed, 09/21/2022 - 15:22

Submission Letter:

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safety.

Jeff Dyas

435 WALLACE AVE

Covington, KY 41014
United States

Phone: (859) 444-7104

Email Address: jeffdyas@gmail.com

Date of Submission: Wed, 09/21/2022 - 15:10

Submission Letter:

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safety.

John Fening

7419 Elm St

Cincinnati, OH 45227
United States

Phone: (513) 305-1123

Email Address: johnfening5@gmail.com

Date of Submission: Wed, 09/21/2022 - 12:42

Submission Letter:

I'm writing to you today in hopes that you will oppose the Brent Spence Bridge Corridor expansion project. The project won't solve the bridge's traffic and safety problems, but it will harm local communities, cost taxpayers billions of dollars, and only heightens our reliance on cars and fossil fuels.

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safety.

Victor Minella

3266 Epworth Avenue

Cincinnati, OH 45211
United States

Phone: (513) 484-9495

Email Address: vhminella@gmail.com

Date of Submission: Wed, 09/21/2022 - 12:03

Submission Letter:

I'm writing to you today in hopes that you will oppose the Brent Spence Bridge Corridor expansion project. The project won't solve the bridge's traffic and safety problems, but it will harm local communities, cost taxpayers billions of dollars, and only heightens our reliance on cars and fossil fuels.

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safety.

Luke Brockett

202 Center Street

Bellevue, KY 41073
United States

Phone: (859) 620-9002

Email Address: luke.brockett@gmail.com

Date of Submission: Wed, 09/21/2022 - 11:39

Submission Letter:

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safety.

Sophie Shiff

1162 Rulison Avenue

Cincinnati, OH 45238
United States

Phone: (513) 490-3253

Email Address: sophie@shifty.com

Date of Submission: Tue, 09/20/2022 - 15:22

Submission Letter:

I'm writing to you today in hopes that you will oppose the Brent Spence Bridge Corridor expansion project. The project won't solve the bridge's traffic and safety problems, but it will harm local communities, cost taxpayers billions of dollars, and only heightens our reliance on cars and fossil fuels.

Highway expansion projects harm already vulnerable populations. This expansion project will promulgate plumes of dust and fumes, worsening air pollution and health outcomes in nearby communities in the long term while interrupting people's everyday lives in the short term. After months of negotiation, Covington residents are still giving up over a dozen properties to the state to build this highway. And despite all of this, the project likely will not even solve the problems it is setting out to fix. Research strongly suggests that expanding highway capacity attracts more vehicles and increases traffic, the reverse effect of what KYTC's proposed plan is supposed to achieve.

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I am hoping that you will not support expanding the Brent Spence highway but rather invest in our communities, our transportation system, and our health and

safety.

Regina Hopple

2425 Anderson Manor Court

Cincinnati, OH 45244
United States

Phone: (513) 373-9578

Email Address: ghopple@gmail.com

Date of Submission: Tue, 09/20/2022 - 14:18

Submission Letter:

I'm writing to you today in hopes that you will oppose the Brent Spence Bridge Corridor expansion project. The project won't solve the bridge's traffic and safety problems, but it will harm local communities, cost taxpayers billions of dollars, and only heightens our reliance on cars and fossil fuels.

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safety.

Lawrence Hamby

3416 Custer Avenue

Cincinnati, OH 45208
United States

Phone: (310) 600-0440

Email Address: larry.hamby@mac.com

Date of Submission: Tue, 09/20/2022 - 09:26

Submission Letter:

I'm writing to you today in hopes that you will oppose the Brent Spence Bridge Corridor expansion project. The project won't solve the bridge's traffic and safety problems, but it will harm local communities, cost taxpayers billions of dollars, and only heightens our reliance on cars and fossil fuels.

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safety.

Jake McGinty

4529 Plainville Road

Cincinnati, OH 45227
United States

Phone: (513) 444-6623

Email Address: jakemcginty4@gmail.com

Date of Submission: Mon, 09/19/2022 - 22:30

Submission Letter:

I'm writing to you today in hopes that you will oppose the Brent Spence Bridge Corridor expansion project. The project won't solve the bridge's traffic and safety problems, but it will harm local communities, cost taxpayers billions of dollars, and only heightens our reliance on cars and fossil fuels.

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safety.

monica markley

612 Liberty Street

Newport, KY 41071
United States

Phone: (928) 600-2272

Email Address: moniann67@gmail.com

Date of Submission: Mon, 09/19/2022 - 18:22

Submission Letter:

I'm writing to you today in hopes that you will oppose the Brent Spence Bridge Corridor expansion project. The project won't solve the bridge's traffic and safety problems, but it will harm local communities, cost taxpayers billions of dollars, and only heightens our reliance on cars and fossil fuels.

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safety.

Daniel Mattox

525 fortune ave apt 1,

Cincinnati, OH 45219
United States

Phone: (859) 404-9335

Email Address: elginmattox@gmail.com

Date of Submission: Mon, 09/19/2022 - 12:37

Submission Letter:

I'm writing to you today in hopes that you will oppose the Brent Spence Bridge Corridor expansion project. The project won't solve the bridge's traffic and safety problems, but it will harm local communities, cost taxpayers billions of dollars, and only heightens our reliance on cars and fossil fuels.

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safety.

Elias Lewis

671 N Bay Ct

Cincinnati, OH 45238
United States

Phone: (513) 560-8542

Email Address: eliaslewis@hotmail.com

Date of Submission: Mon, 09/19/2022 - 10:17

Submission Letter:

I'm writing to you today in hopes that you will oppose the Brent Spence Bridge Corridor expansion project. The project won't solve the bridge's traffic and safety problems, but it will harm local communities, cost taxpayers billions of dollars, and only heightens our reliance on cars and fossil fuels.

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safety.

Lilly Arthur

917 Rogers Place

Cincinnati, OH 45206
United States

Phone: (513) 288-7675

Email Address: lilliangraceful@gmail.com

Date of Submission: Sun, 09/18/2022 - 23:24

Submission Letter:

I'm writing to you today in hopes that you will oppose the Brent Spence Bridge Corridor expansion project. The project won't solve the bridge's traffic and safety problems, but it will harm local communities, cost taxpayers billions of dollars, and only heightens our reliance on cars and fossil fuels.

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I am hoping that you will not support expanding the Brent Spence highway but rather invest in our communities, our transportation system, and our health and

safety.

Matthew Corbett

917 Rogers Place

Cincinnati, OH 45206
United States

Phone: (512) 673-7861

Email Address: mattcorb419@gmail.com

Date of Submission: Sun, 09/18/2022 - 23:22

Submission Letter:

Hello! My name is Matt Corbett and I'm a resident of Walnut Hills. If you support the Brentz Spentz bridge expansion, you're living in the past.

I'm writing to you today in hopes that you will oppose the Brent Spence Bridge Corridor expansion project. The project won't solve the bridge's traffic and safety problems, but it will harm local communities, cost taxpayers billions of dollars, and only heightens our reliance on cars and fossil fuels.

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I am hoping that you will not support expanding the Brent Spence highway but rather invest in our communities, our transportation system, and our health and safety.

Best,
Matt Corbett

Madison Sadler

7910 East Bend Road

Burlington, KY 41005
United States

Phone: (859) 652-4690

Email Address: msadler1999@gmail.com

Date of Submission: Sun, 09/18/2022 - 08:13

Submission Letter:

I'm writing to you today in hopes that you will oppose the Brent Spence Bridge Corridor expansion project. The project won't solve the bridge's traffic and safety problems, but it will harm local communities, cost taxpayers billions of dollars, and only heightens our reliance on cars and fossil fuels.

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safety.

Anthony Isaacs

3619 Tamarack Avenue

Cincinnati, OH 45207
United States

Phone: (773) 431-9775

Email Address: isaacs.anthony@gmail.com

Date of Submission: Sun, 09/18/2022 - 00:00

Submission Letter:

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safety.

Connie Riley

349 E 2nd St

Newport, KY 41071
United States

Phone: (859) 814-8663

Email Address: connieriley45@gmail.com

Date of Submission: Sat, 09/17/2022 - 21:49

Submission Letter:

I'm writing to you today in hopes that you will oppose the Brent Spence Bridge Corridor expansion project. The project won't solve the bridge's traffic and safety problems, but it will harm local communities, cost taxpayers billions of dollars, and only heightens our reliance on cars and fossil fuels.

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safety.

Frank Trainor

1797 Muskegon Drive

Cincinnati, OH 45255
United States

Phone: (937) 444-9074

Email Address: hereticky@comcast.net

Date of Submission: Sat, 09/17/2022 - 20:11

Submission Letter:

I'm writing to you today in hopes that you will oppose the Brent Spence Bridge Corridor expansion project. The project won't solve the bridge's traffic and safety problems, but it will harm local communities, cost taxpayers billions of dollars, and only heightens our reliance on cars and fossil fuels.

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safety.

Rachel Vachon

9767 Acton Court

Cincinnati, OH 45241
United States

Phone: (513) 807-6401

Email Address: rvachon58@gmail.com

Date of Submission: Sat, 09/17/2022 - 11:07

Submission Letter:

I'm writing to you today in hopes that you will oppose the Brent Spence Bridge Corridor expansion project. The project won't solve the bridge's traffic and safety problems, but it will harm local communities, cost taxpayers billions of dollars, and only heightens our reliance on cars and fossil fuels.

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safety.

Joey Slovin

3991 Ballard Avenue

Cincinnati, OH 45209
United States

Phone: (513) 504-0605

Email Address: joeyslovin@gmail.com

Date of Submission: Fri, 09/16/2022 - 19:22

Submission Letter:

I'm writing to you today in hopes that you will oppose the Brent Spence Bridge Corridor expansion project. The project won't solve the bridge's traffic and safety problems, but it will harm local communities, cost taxpayers billions of dollars, and only heightens our reliance on cars and fossil fuels.

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safety.

Arnold Smith

1319 Edwards Road, Apt 1

Cincinnati, OH 45208
United States

Phone: (770) 843-0263

Email Address: ajsmith17@gmail.com

Date of Submission: Fri, 09/16/2022 - 13:23

Submission Letter:

I'm writing to you today in hopes that you will oppose the Brent Spence Bridge Corridor expansion project. The project won't solve the bridge's traffic and safety problems, but it will harm local communities, cost taxpayers billions of dollars, and only heightens our reliance on cars and fossil fuels.

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safety.

Stephen Hoerst

195 E McMillan

Cincinnati, OH 45219
United States

Phone: (513) 460-5969

Email Address: shoerst2@gmail.com

Date of Submission: Fri, 09/16/2022 - 00:12

Submission Letter:

I'm writing to you today in hopes that you will oppose the Brent Spence Bridge Corridor expansion project. The project won't solve the bridge's traffic and safety problems, but it will harm local communities, cost taxpayers billions of dollars, and only heightens our reliance on cars and fossil fuels.

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I am hoping that you will not support expanding the Brent Spence highway but rather invest in our communities, our transportation system, and our health and

safety.

Jack Martin

732 western avenue

Covington, KY 41011
United States

Phone: (859) 474-5102

Email Address: jackmartin111@gmail.com

Date of Submission: Thu, 09/15/2022 - 18:58

Submission Letter:

I'm writing to you today in hopes that you will oppose the Brent Spence Bridge Corridor expansion project. The project won't solve the bridge's traffic and safety problems, but it will harm local communities, cost taxpayers billions of dollars, and only heightens our reliance on cars and fossil fuels.

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safety.

Jacob Sheridan

534 Liberty Hill Lane

Cincinnati, OH 45202
United States

Phone: (574) 225-0513

Email Address: jwaynesheridan@gmail.com

Date of Submission: Thu, 09/15/2022 - 11:49

Submission Letter:

I'm writing to you today in hopes that you will oppose the Brent Spence Bridge Corridor expansion project. The project won't solve the bridge's traffic and safety problems, but it will harm local communities, cost taxpayers billions of dollars, and only heightens our reliance on cars and fossil fuels.

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safety.

Joe Bredestege

1162 Rulison Avenue

Cincinnati, OH 45238
United States

Phone: (513) 373-9531

Email Address: jackoweese@gmail.com

Date of Submission: Wed, 09/14/2022 - 13:04

Submission Letter:

I'm writing to you today in hopes that you will oppose the Brent Spence Bridge Corridor expansion project. The project won't solve the bridge's traffic and safety problems, but it will harm local communities, cost taxpayers billions of dollars, and only heightens our reliance on cars and fossil fuels.

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safety.

Theodore Baker

3063 Victoria Ave

Cincinnati, OH 45208-1505
United States

Phone: (513) 368-4497

Email Address: theobaker2015@gmail.com

Date of Submission: Wed, 09/14/2022 - 11:41

Submission Letter:

I'm writing to you today in hopes that you will oppose the Brent Spence Bridge Corridor expansion project. The project won't solve the bridge's traffic and safety problems, but it will harm local communities, cost taxpayers billions of dollars, and only heightens our reliance on cars and fossil fuels.

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safety.

Deborah Ballard

15 Marquette Avenue

Fort Mitchell, KY 41017
United States

Phone: (859) 905-9474

Email Address: deborah.stamm@gmail.com

Date of Submission: Tue, 09/13/2022 - 22:15

Submission Letter:

I'm writing to you today in hopes that you will oppose the Brent Spence Bridge Corridor expansion project. The project won't solve the bridge's traffic and safety problems, but it will harm local communities, cost taxpayers billions of dollars, and only heightens our reliance on cars and fossil fuels.

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safety.

Charlie Schwartz

2896 Victoria Avenue

Cincinnati, OH 45208
United States

Phone: (419) 377-9181

Email Address: c.schwartz.1983@gmail.com

Date of Submission: Tue, 09/13/2022 - 14:08

Submission Letter:

I'm writing to you today in hopes that you will oppose the Brent Spence Bridge Corridor expansion project. The project won't solve the bridge's traffic and safety problems, but it will harm local communities, cost taxpayers billions of dollars, and only heightens our reliance on cars and fossil fuels.

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safety.

Mark Pritt

1313 Mimosa Lane

Cincinnati, OH 45238
United States

Phone: (614) 572-9076

Email Address: pritt.13@osu.edu

Date of Submission: Tue, 09/13/2022 - 09:01

Submission Letter:

I'm writing to you today in hopes that you will oppose the Brent Spence Bridge Corridor expansion project. The project won't solve the bridge's traffic and safety problems, but it will harm local communities, cost taxpayers billions of dollars, and only heightens our reliance on cars and fossil fuels.

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safety.

CYNTHIA HAYNAM

9122 Nottingham Way

Mason, OH 45040
United States

Phone: (513) 515-8051

Email Address: garridcr@hotmail.com

Date of Submission: Mon, 09/12/2022 - 21:16

Submission Letter:

I'm writing to you today in hopes that you will oppose the Brent Spence Bridge Corridor expansion project. The project won't solve the bridge's traffic and safety problems, but it will harm local communities, cost taxpayers billions of dollars, and only heightens our reliance on cars and fossil fuels.

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safety.

Joyce Hammonds

854 E Laguna Ct

Villa Hills, KY 41017
United States

Phone: (859) 380-0392

Email Address: joycech1@outlook.com

Date of Submission: Mon, 09/12/2022 - 21:10

Submission Letter:

I'm writing to you today in hopes that you will oppose the Brent Spence Bridge Corridor expansion project. The project won't solve the bridge's traffic and safety problems, but it will harm local communities, cost taxpayers billions of dollars, and only heightens our reliance on cars and fossil fuels.

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safety.

John Morris

3145 Troy Ave

Cincinnati, OH 45213
United States

Phone: (317) 441-7052

Email Address: johnharoldmorris@gmail.com

Date of Submission: Mon, 09/12/2022 - 14:49

Submission Letter:

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safety.

Kathy Cunningham

6271 Cortelyou Ave

Cincinnati, OH 45213
United States

Phone: (513) 393-9828

Email Address: kbike26@gmail.com

Date of Submission: Sun, 09/11/2022 - 09:12

Submission Letter:

I'm writing to you today in hopes that you will oppose the Brent Spence Bridge Corridor expansion project. The project won't solve the bridge's traffic and safety problems, but it will harm local communities, cost taxpayers billions of dollars, and only heightens our reliance on cars and fossil fuels.

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safety.

Laura McDonald

1027 Apple Blossom Drive

Florence, KY 41042
United States

Phone: (859) 907-0742

Email Address: laurad1123@gmail.com

Date of Submission: Sun, 09/11/2022 - 07:10

Submission Letter:

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safety.

Eric Hammer

1408 Republic Street

Cincinnati, OH 45202
United States

Phone: (513) 745-9052

Email Address: ehammer1408@gmail.com

Date of Submission: Sat, 09/10/2022 - 21:56

Submission Letter:

I'm writing to you today in hopes that you will oppose the Brent Spence Bridge Corridor expansion project. The project won't solve the bridge's traffic and safety problems, but it will harm local communities, cost taxpayers billions of dollars, and only heightens our reliance on cars and fossil fuels.

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safety.

Ryan Keifer

2828 WERK RD

CINCINNATI, OH 45211
United States

Phone: (646) 689-2121

Email Address: rwkeifer@gmail.com

Date of Submission: Sat, 09/10/2022 - 14:24

Submission Letter:

I'm writing to you today in hopes that you will oppose the Brent Spence Bridge Corridor expansion project. The project won't solve the bridge's traffic and safety problems, but it will harm local communities, cost taxpayers billions of dollars, and only heightens our reliance on cars and fossil fuels.

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safety.

Jess Summers

209 W. pike st, 2E

Covington, KY 41011
United States

Phone: (513) 646-5552

Email Address: hello@sayyestojess.com

Date of Submission: Fri, 09/09/2022 - 20:44

Submission Letter:

I'm writing to you today in hopes that you will oppose the Brent Spence Bridge Corridor expansion project. The project won't solve the bridge's traffic and safety problems, but it will harm local communities, cost taxpayers billions of dollars, and only heightens our reliance on cars and fossil fuels.

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safety.

Ciara Flotka

8581 Winthrop Cor

Florence, KY 41042
United States

Phone: (989) 423-7846

Email Address: cfran11@me.com

Date of Submission: Fri, 09/09/2022 - 18:44

Submission Letter:

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safety.

Charissa Hamilton

122 W 34th St

Covington, KY 41015
United States

Phone: (859) 279-3957

Email Address: Charissah427@gmail.com

Date of Submission: Fri, 09/09/2022 - 14:53

Submission Letter:

I'm writing to you today in hopes that you will oppose the Brent Spence Bridge Corridor expansion project. The project won't solve the bridge's traffic and safety problems, but it will harm local communities, cost taxpayers billions of dollars, and only heightens our reliance on cars and fossil fuels.

Highway expansion projects harm already vulnerable populations. This expansion project will promulgate plumes of dust and fumes, worsening air pollution and health outcomes in nearby communities in the long term while interrupting people's everyday lives in the short term. After months of negotiation, Covington residents are still giving up over a dozen properties to the state to build this highway. And despite all of this, the project likely will not even solve the problems it is setting out to fix. Research strongly suggests that expanding highway capacity attracts more vehicles and increases traffic, the reverse effect of what KYTC's proposed plan is supposed to achieve.

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We cannot remain fully dependent on cars as transportation. As we have seen recently, the financial precariousness of car ownership places a mounting burden on American families. Investing in better public transit systems will reduce traffic congestion, reduce the cost to taxpayers while seeing a more beneficial use of their money, and improve the quality of life in neighboring communities.

I am hoping that you will not support expanding the Brent Spence highway but rather invest in our communities, our transportation system, and our health and

safety.

Hannah Toton

2213 Feldman Avenue

Cincinnati, OH 45212
United States

Phone: (440) 396-7287

Email Address: ht_twilite@yahoo.com

Date of Submission: Fri, 09/09/2022 - 14:11

Submission Letter:

I'm writing to you today in hopes that you will oppose the Brent Spence Bridge Corridor expansion project. The project won't solve the bridge's traffic and safety problems, but it will harm local communities, cost taxpayers billions of dollars, and only heightens our reliance on cars and fossil fuels.

Highway expansion projects harm already vulnerable populations. This expansion project will promulgate plumes of dust and fumes, worsening air pollution and health outcomes in nearby communities in the long term while interrupting people's everyday lives in the short term. After months of negotiation, Covington residents are still giving up over a dozen properties to the state to build this highway. And despite all of this, the project likely will not even solve the problems it is setting out to fix. Research strongly suggests that expanding highway capacity attracts more vehicles and increases traffic, the reverse effect of what KYTC's proposed plan is supposed to achieve.

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I am hoping that you will not support expanding the Brent Spence highway but rather invest in our communities, our transportation system, and our health and

safety.

Adam Graham

557 Tusculum Avenue

Cincinnati, OH 45226
United States

Phone: (513) 706-0368

Email Address: gradamt@gmail.com

Date of Submission: Fri, 09/09/2022 - 09:27

Submission Letter:

I'm writing to you today in hopes that you will oppose the Brent Spence Bridge Corridor expansion project. The project won't solve the bridge's traffic and safety problems, but it will harm local communities, cost taxpayers billions of dollars, and only heightens our reliance on cars and fossil fuels.

Highway expansion projects harm already vulnerable populations. This expansion project will promulgate plumes of dust and fumes, worsening air pollution and health outcomes in nearby communities in the long term while interrupting people's everyday lives in the short term. After months of negotiation, Covington residents are still giving up over a dozen properties to the state to build this highway. And despite all of this, the project likely will not even solve the problems it is setting out to fix. Research strongly suggests that expanding highway capacity attracts more vehicles and increases traffic, the reverse effect of what KYTC's proposed plan is supposed to achieve.

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I am hoping that you will not support expanding the Brent Spence highway but rather invest in our communities, our transportation system, and our health and

safety.

Michele Meininger

144 Burdsall Ave

Fort Mitchell, KY 41017
United States

Phone: (859) 620-1818

Email Address: mmeininger525@yahoo.com

Date of Submission: Fri, 09/09/2022 - 08:58

Submission Letter:

I'm writing to you today in hopes that you will oppose the Brent Spence Bridge Corridor expansion project. The project won't solve the bridge's traffic and safety problems, but it will harm local communities, cost taxpayers billions of dollars, and only heightens our reliance on cars and fossil fuels.

Highway expansion projects harm already vulnerable populations. This expansion project will promulgate plumes of dust and fumes, worsening air pollution and health outcomes in nearby communities in the long term while interrupting people's everyday lives in the short term. After months of negotiation, Covington residents are still giving up over a dozen properties to the state to build this highway. And despite all of this, the project likely will not even solve the problems it is setting out to fix. Research strongly suggests that expanding highway capacity attracts more vehicles and increases traffic, the reverse effect of what KYTC's proposed plan is supposed to achieve.

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I am hoping that you will not support expanding the Brent Spence highway but rather invest in our communities, our transportation system, and our health and

safety.

David Donnelly

4262 Langland Street

Cincinnati, OH 45223
United States

Phone: (260) 348-6102

Email Address: donnellyd11@gmail.com

Date of Submission: Thu, 09/08/2022 - 23:48

Submission Letter:

I'm writing to you today in hopes that you will oppose the Brent Spence Bridge Corridor expansion project. The project won't solve the bridge's traffic and safety problems, but it will harm local communities, cost taxpayers billions of dollars, and only heightens our reliance on cars and fossil fuels.

Highway expansion projects harm already vulnerable populations. This expansion project will promulgate plumes of dust and fumes, worsening air pollution and health outcomes in nearby communities in the long term while interrupting people's everyday lives in the short term. After months of negotiation, Covington residents are still giving up over a dozen properties to the state to build this highway. And despite all of this, the project likely will not even solve the problems it is setting out to fix. Research strongly suggests that expanding highway capacity attracts more vehicles and increases traffic, the reverse effect of what KYTC's proposed plan is supposed to achieve.

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I am hoping that you will not support expanding the Brent Spence highway but rather invest in our communities, our transportation system, and our health and

safety.

Keena Cole

3167 Place Street

Erlanger, KY 41018
United States

Phone: (859) 781-3554

Email Address: kcole69@gmail.com

Date of Submission: Wed, 09/07/2022 - 21:31

Submission Letter:

I'm writing to you today in hopes that you will oppose the Brent Spence Bridge Corridor expansion project. The project won't solve the bridge's traffic and safety problems, but it will harm local communities, cost taxpayers billions of dollars, and only heightens our reliance on cars and fossil fuels.

Highway expansion projects harm already vulnerable populations. This expansion project will promulgate plumes of dust and fumes, worsening air pollution and health outcomes in nearby communities in the long term while interrupting people's everyday lives in the short term. After months of negotiation, Covington residents are still giving up over a dozen properties to the state to build this highway. And despite all of this, the project likely will not even solve the problems it is setting out to fix. Research strongly suggests that expanding highway capacity attracts more vehicles and increases traffic, the reverse effect of what KYTC's proposed plan is supposed to achieve.

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I am hoping that you will not support expanding the Brent Spence highway but rather invest in our communities, our transportation system, and our health and

safety.

Andrew Smith

3918 Elsmere Ave

Cincinnati, OH 45212
United States

Phone: (513) 238-9752

Email Address: smittyxu@yahoo.com

Date of Submission: Wed, 09/07/2022 - 20:21

Submission Letter:

I'm writing to you today in hopes that you will oppose the Brent Spence Bridge Corridor expansion project. The project won't solve the bridge's traffic and safety problems, but it will harm local communities, cost taxpayers billions of dollars, and only heightens our reliance on cars and fossil fuels.

Highway expansion projects harm already vulnerable populations. This expansion project will promulgate plumes of dust and fumes, worsening air pollution and health outcomes in nearby communities in the long term while interrupting people's everyday lives in the short term. After months of negotiation, Covington residents are still giving up over a dozen properties to the state to build this highway. And despite all of this, the project likely will not even solve the problems it is setting out to fix. Research strongly suggests that expanding highway capacity attracts more vehicles and increases traffic, the reverse effect of what KYTC's proposed plan is supposed to achieve.

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We cannot remain fully dependent on cars as transportation. As we have seen recently, the financial precariousness of car ownership places a mounting burden on American families. Investing in better public transit systems will reduce traffic congestion, reduce the cost to taxpayers while seeing a more beneficial use of their money, and improve the quality of life in neighboring communities.

I am hoping that you will not support expanding the Brent Spence highway but rather invest in our communities, our transportation system, and our health and

safety.

Jody Robinson

309 POPLAR ST

BELLEVUE, KY 41073-1108
United States

Phone: (513) 646-3525

Email Address: jodylrobinson41073@gmail.com

Date of Submission: Wed, 09/07/2022 - 18:43

Submission Letter:

I'm writing to you today asking you to oppose the current plans for the Brent Spence Bridge Corridor. The emphasis is on its function as a US transportation corridor that is outweighing our local communities health and vibrancy. The project won't solve the I-71/75 traffic and safety problems, but will harm local communities, cost taxpayers billions of dollars to construct, maintain, and deal with the health impacts, and heightens our reliance on cars and fossil fuels. Once it is constructed the damage will be permanent suggesting we have not learned from past design mistakes.

Highway expansion projects harm already vulnerable populations, are expensive to build, and maintain. This expansion project will promulgate plumes of dust and fumes, worsening air pollution and health outcomes in nearby communities in the long term while interrupting people's everyday lives in the short term. After months of negotiation, Covington residents are still giving up over a dozen properties to the state to build this highway. And despite all of this, the project likely will not even solve the problems it is setting out to fix. Research strongly suggests that expanding highway capacity attracts more vehicles and increases traffic, the reverse effect of what KYTC's proposed plan is supposed to achieve.

Our region carries the burden from hundreds of trucks crossing the bridge corridor daily, but most of the freight crossing the bridge is neither made here nor delivered here, so why are they prioritize over our community's wellbeing? The chronic truck congestion on the bridge makes it extremely unsafe. Rather than spend billions on an un-aesthetic, unnecessary highway, Kentucky and Ohio should be investing in the livelihoods of its residents. Funding should be shifted to expanding transit options and multi-modal transportation projects while reducing non-local truck traffic. Pass through trucks should use I-275 for multiple reasons and we know it can work because there have been a number of instances over the years where that was their only option due to construction. We should be reducing air pollution and greenhouse gas emissions, not creating opportunities to worsen them.

We cannot remain fully dependent on cars as transportation. As we have seen recently, the financial precariousness of car ownership places a mounting burden on American families. Investing in better public transit systems will reduce traffic congestion, reduce the cost to taxpayers while seeing a more beneficial use of their money, and improve the quality of life in neighboring communities.

I am hoping that you will not support expanding the Brent Spence highway but rather invest in our communities, our transportation system, and our health and safety.

Rebecca Arwood

3010 Frazier Street,

Covington, KY 41015
United States

Phone: (859) 628-7423

Email Address: BECKYARWOOD@GMAIL.COM

Date of Submission: Wed, 09/07/2022 - 13:08

Submission Letter:

I'm writing to you today in hopes that you will oppose the Brent Spence Bridge Corridor expansion project. The project won't solve the bridge's traffic and safety problems, but it will harm local communities, cost taxpayers billions of dollars, and only heightens our reliance on cars and fossil fuels.

Highway expansion projects harm already vulnerable populations. This expansion project will promulgate plumes of dust and fumes, worsening air pollution and health outcomes in nearby communities in the long term while interrupting people's everyday lives in the short term. After months of negotiation, Covington residents are still giving up over a dozen properties to the state to build this highway. And despite all of this, the project likely will not even solve the problems it is setting out to fix. Research strongly suggests that expanding highway capacity attracts more vehicles and increases traffic, the reverse effect of what KYTC's proposed plan is supposed to achieve.

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safety.

Karen Blocher

498 Wellesley Avenue

Cincinnati, OH 45224
United States

Phone: (513) 510-6125

Email Address: karenlee.blocher@gmail.com

Date of Submission: Wed, 09/07/2022 - 08:19

Submission Letter:

I'm writing to you today in hopes that you will oppose the Brent Spence Bridge Corridor expansion project. The project won't solve the bridge's traffic and safety problems, but it will harm local communities, cost taxpayers billions of dollars, and only heightens our reliance on cars and fossil fuels.

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safety.

Jose Munoz

918 Monroe Street

Newport, KY 41071
United States

Phone: (787) 519-9184

Email Address: munozbaez@gmail.com

Date of Submission: Tue, 09/06/2022 - 19:47

Submission Letter:

I'm writing to you today in hopes that you will oppose the Brent Spence Bridge Corridor expansion project. The project won't solve the bridge's traffic and safety problems, but it will harm local communities, cost taxpayers billions of dollars, and only heightens our reliance on cars and fossil fuels.

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safety.

Ryan Minnich

1863 Chase Avenue

Cincinnati, OH 45223
United States

Phone: (513) 276-6868

Email Address: minnicrs@mail.uc.edu

Date of Submission: Tue, 09/06/2022 - 19:06

Submission Letter:

I'm writing to you today in hopes that you will oppose the Brent Spence Bridge Corridor expansion project. The project won't solve the bridge's traffic and safety problems, but it will harm local communities, cost taxpayers billions of dollars, and only heightens our reliance on cars and fossil fuels.

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I am hoping that you will not support expanding the Brent Spence highway but rather invest in our communities, our transportation system, and our health and

safety.

Consider instead freight tolls using I-71 and I-75 inside the 275 loop. Consider removing Ft Washington Way in downtown Cincinnati so I-71 traffic must cross the River and use 471 to connect to I-75. Why does an interstate highway interchange make sense inside the center of a city's business district.

The highways inside city limits, inside the urban core actually need to be "right sized" and reduced.

Dominic Mottola

655 Eden Park Dr, 219

Cincinnati, OH 45202
United States

Phone: (567) 208-8968

Email Address: dominicmottola@gmail.com

Date of Submission: Tue, 09/06/2022 - 16:46

Submission Letter:

I'm writing to you today in hopes that you will oppose the Brent Spence Bridge Corridor expansion project. The project won't solve the bridge's traffic and safety problems, but it will harm local communities, cost taxpayers billions of dollars, and only heightens our reliance on cars and fossil fuels.

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I am hoping that you will not support expanding the Brent Spence highway but rather invest in our communities, our transportation system, and our health and

safety.

Jordan Wilson

8518 Plainfield Road

Cincinnati, OH 45236
United States

Phone: (330) 604-0898

Email Address: jwilson1193@gmail.com

Date of Submission: Tue, 09/06/2022 - 16:40

Submission Letter:

I'm writing to you today in hopes that you will oppose the Brent Spence Bridge Corridor expansion project. The project won't solve the bridge's traffic and safety problems, but it will harm local communities, cost taxpayers billions of dollars, and only heightens our reliance on cars and fossil fuels.

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safety.

Amina Sankoh

1713 Riverbirch Drive

Columbus, OH 43229
United States

Phone: (614) 707-2322

Email Address: amina.sankoh@gmail.com

Date of Submission: Tue, 09/06/2022 - 15:28

Submission Letter:

I'm writing to you today in hopes that you will oppose the Brent Spence Bridge Corridor expansion project. The project won't solve the bridge's traffic and safety problems, but it will harm local communities, cost taxpayers billions of dollars, and only heightens our reliance on cars and fossil fuels.

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I am hoping that you will not support expanding the Brent Spence highway but rather invest in our communities, our transportation system, and our health and

safety.

David Woerner

2530 Ridgeland Place,

Cincinnati, OH 45212
United States

Phone: (513) 301-6994

Email Address: woerneda@gmail.com

Date of Submission: Tue, 09/06/2022 - 14:55

Submission Letter:

I'm writing to you today in hopes that you will oppose the Brent Spence Bridge Corridor expansion project. The project won't solve the bridge's traffic and safety problems, but it will harm local communities, cost taxpayers billions of dollars, and only heightens our reliance on cars and fossil fuels.

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safety.

Nolan Nicaise

303 Greenup Street, Apt 22

Covington, KY 41011
United States

Phone: (859) 750-0436

Email Address: nolannicaise@gmail.com

Date of Submission: Tue, 09/06/2022 - 14:14

Submission Letter:

I'm writing to you today in hopes that you will oppose the Brent Spence Bridge Corridor expansion project. The project won't solve the bridge's traffic and safety problems, but it will harm local communities, cost taxpayers billions of dollars, and only heightens our reliance on cars and fossil fuels.

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I am hoping that you will not support expanding the Brent Spence highway but rather invest in our communities, our transportation system, and our health and

safety.

Tom Blumena

731 Philadelphia Street

Covington, KY 41011
United States

Phone: (513) 404-8443

Email Address: blumes33@gmail.com

Date of Submission: Mon, 09/05/2022 - 17:39

Submission Letter:

I'm writing to you today in hopes that you will oppose the Brent Spence Bridge Corridor expansion project. The project won't solve the bridge's traffic and safety problems, but it will harm local communities, cost taxpayers billions of dollars, and only heightens our reliance on cars and fossil fuels.

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safety.

Mary Leonard

918 Monroe St.

Newport, KY 41071
United States

Phone: (787) 383-8764

Email Address: maremicro@gmail.com

Date of Submission: Mon, 09/05/2022 - 16:33

Submission Letter:

I'm writing to you today in hopes that you will oppose the Brent Spence Bridge Corridor expansion project. The project won't solve the bridge's traffic and safety problems, but it will harm local communities, cost taxpayers billions of dollars, and only heightens our reliance on cars and fossil fuels.

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safety.

Bradley Detro

1439 Walnut Street

Cincinnati, OH 45202
United States

Phone: (859) 486-8475

Email Address: bcdetr01@gmail.com

Date of Submission: Mon, 09/05/2022 - 16:25

Submission Letter:

I'm writing to you today in hopes that you will oppose the Brent Spence Bridge Corridor expansion project. The project won't solve the bridge's traffic and safety problems, but it will harm local communities, cost taxpayers billions of dollars, and only heightens our reliance on cars and fossil fuels.

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safety.

Lauren DeMarks

105 W Fourth St

Cincinnati, OH 45202
United States

Phone: (513) 410-3247

Email Address: kicksterlpd@gmail.com

Date of Submission: Mon, 09/05/2022 - 13:47

Submission Letter:

I'm writing to you today in hopes that you will oppose the Brent Spence Bridge Corridor expansion project. The project won't solve the bridge's traffic and safety problems, but it will harm local communities, cost taxpayers billions of dollars, and only heightens our reliance on cars and fossil fuels.

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safety.

Kevin Nielson

223 Center Street

Bellevue, KY 41073
United States

Phone: (816) 262-1046

Email Address: kevin24nielson@gmail.com

Date of Submission: Mon, 09/05/2022 - 12:23

Submission Letter:

I'm writing to you today in hopes that you will oppose the Brent Spence Bridge Corridor expansion project. The project won't solve the bridge's traffic and safety problems, but it will harm local communities, cost taxpayers billions of dollars, and only heightens our reliance on cars and fossil fuels.

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safety.

Steve Horner

665 Westerly Drive

Crescent Springs, KY 41017
United States

Phone: (859) 760-6332

Email Address: shorner0615@twc.com

Date of Submission: Mon, 09/05/2022 - 09:08

Submission Letter:

I'm writing to you today in hopes that you will oppose the Brent Spence Bridge Corridor expansion project. The project won't solve the bridge's traffic and safety problems, but it will harm local communities, cost taxpayers billions of dollars, and only heightens our reliance on cars and fossil fuels.

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safety.

Henry Brown

1754 Jester Street

Cincinnati, OH 45223
United States

Phone: (202) 674-2102

Email Address: hab43@case.edu

Date of Submission: Mon, 09/05/2022 - 04:30

Submission Letter:

I'm writing to you today in hopes that you will oppose the Brent Spence Bridge Corridor expansion project. The project won't solve the bridge's traffic and safety problems, but it will harm local communities, cost taxpayers billions of dollars, and only heightens our reliance on cars and fossil fuels.

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safety.

Kelly McCullough

1819 Holman Street

Covington, KY 41014
United States

Phone: (865) 247-3453

Email Address: kebattles@gmail.com

Date of Submission: Sun, 09/04/2022 - 19:13

Submission Letter:

I'm writing to you today in hopes that you will oppose the Brent Spence Bridge Corridor expansion project. The project won't solve the bridge's traffic and safety problems, but it will harm local communities, cost taxpayers billions of dollars, and only heightens our reliance on cars and fossil fuels.

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safety.

Andrew Smith

3918 Elsmere Ave

Cincinnati, OH 45212
United States

Phone: (513) 238-9752

Email Address: smittyxu@yahoo.com

Date of Submission: Sun, 09/04/2022 - 06:34

Submission Letter:

I'm writing to you today in hopes that you will oppose the Brent Spence Bridge Corridor expansion project. The project won't solve the bridge's traffic and safety problems, but it will harm local communities, cost taxpayers billions of dollars, and only heightens our reliance on cars and fossil fuels.

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safety.

Ned Kalapasev

15 Highview Drive

Ft. Wright, KY 41011
United States

Phone: (859) 653-0450

Email Address: nkalapasev@gmail.com

Date of Submission: Sat, 09/03/2022 - 21:45

Submission Letter:

I'm writing to you today in hopes that you will oppose the Brent Spence Bridge Corridor expansion project. The project won't solve the bridge's traffic and safety problems, but it will harm local communities, cost taxpayers billions of dollars, and only heightens our reliance on cars and fossil fuels.

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safety.

Megan Lamkin

1542 Spring Lawn Avenue

Cincinnati, OH 45223
United States

Phone: (555) 555-5555

Email Address: meganlamkin@gmail.com

Date of Submission: Sat, 09/03/2022 - 20:22

Submission Letter:

I'm writing to you today in hopes that you will oppose the Brent Spence Bridge Corridor expansion project. The project won't solve the bridge's traffic and safety problems, but it will harm local communities, cost taxpayers billions of dollars, and only heightens our reliance on cars and fossil fuels.

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safety.

Becky Johnson

2561, Villa Lane

Cincinnati, OH 45208
United States

Phone: (513) 441-6893

Email Address: ms.beckyjohnson@yahoo.com

Date of Submission: Sat, 09/03/2022 - 20:20

Submission Letter:

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safety.

Sharon Roush

7731 Bridgewater Lane

Hamilton, OH 45011
United States

Phone: (513) 907-7651

Email Address: sroush1367@gmail.com

Date of Submission: Sat, 09/03/2022 - 12:59

Submission Letter:

I'm writing to you today in hopes that you will oppose the Brent Spence Bridge Corridor expansion project. The project won't solve the bridge's traffic and safety problems, but it will harm local communities, cost taxpayers billions of dollars, and only heightens our reliance on cars and fossil fuels.

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safety.

Jor Bredestege

1162 Rulison Avenue

Cincinnati, OH 45238
United States

Phone: (513) 373-9531

Email Address: jackoweese@gmail.com

Date of Submission: Fri, 09/02/2022 - 15:22

Submission Letter:

I'm writing to you today in hopes that you will oppose the Brent Spence Bridge Corridor expansion project. The project won't solve the bridge's traffic and safety problems, but it will harm local communities, cost taxpayers billions of dollars, and only heightens our reliance on cars and fossil fuels.

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safety.

James McDermott

2301 Auburn Avenue

Cincinnati, OH 45219
United States

Phone: (929) 334-8574

Email Address: mcdermje@mail.uc.edu

Date of Submission: Fri, 09/02/2022 - 11:01

Submission Letter:

I'm writing to you today in hopes that you will oppose the Brent Spence Bridge Corridor expansion project. The project won't solve the bridge's traffic and safety problems, but it will harm local communities, cost taxpayers billions of dollars, and only heightens our reliance on cars and fossil fuels.

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safety.

John Pieper

615 Lake Knoll Court

Erlanger, KY 41018
United States

Phone: (859) 815-9422

Email Address: jpeeps1@gmail.com

Date of Submission: Fri, 09/02/2022 - 09:57

Submission Letter:

I'm writing to you today in hopes that you will oppose the Brent Spence Bridge Corridor expansion project. The project won't solve the bridge's traffic and safety problems, but it will harm local communities, cost taxpayers billions of dollars, and only heightens our reliance on cars and fossil fuels.

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I am hoping that you will not support expanding the Brent Spence highway but rather invest in our communities, our transportation system, and our health and

safety.

Janet Neidhard

3896 Reading Road

Cincinnati, OH 45229
United States

Phone: (513) 616-3361

Email Address: jsneidhard@gmail.com

Date of Submission: Fri, 09/02/2022 - 07:18

Submission Letter:

I'm writing to you today in hopes that you will oppose the Brent Spence Bridge Corridor expansion project. In addition to the explanations below, I emphasize my strong support for expanding public transit. I have credibility in suggesting this As a lifelong user of Cincinnati Metro, The project won't solve the bridge's traffic and safety problems, but it will harm local communities, cost taxpayers billions of dollars, and only heightens our reliance on cars and fossil fuels.

Highway expansion projects harm already vulnerable populations. This expansion project will promulgate plumes of dust and fumes, worsening air pollution and health outcomes in nearby communities in the long term while interrupting people's everyday lives in the short term. After months of negotiation, Covington residents are still giving up over a dozen properties to the state to build this highway. And despite all of this, the project likely will not even solve the problems it is setting out to fix. Research strongly suggests that expanding highway capacity attracts more vehicles and increases traffic, the reverse effect of what KYTC's proposed plan is supposed to achieve.

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I am hoping that you will not support expanding the Brent Spence highway but rather invest in our communities, our transportation system, and our health and safety.

Barbara Wiedeman

705 Ervin Terr

Dayton, KY 41074
United States

Phone: (513) 264-5191

Email Address: barb.wiedeman@gmail.com

Date of Submission: Thu, 09/01/2022 - 17:18

Submission Letter:

I'm writing to you today in hopes that you will oppose the Brent Spence Bridge Corridor expansion project. The project won't solve the bridge's traffic and safety problems, but it will harm local communities, cost taxpayers billions of dollars, and only heightens our reliance on cars and fossil fuels.

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safety.

Annie Knecht

614 Garrard Street

Covington, KY 41011
United States

Phone: (513) 290-4762

Email Address: annieknecht@gmail.com

Date of Submission: Thu, 09/01/2022 - 15:06

Submission Letter:

I'm writing to you today in hopes that you will oppose the Brent Spence Bridge Corridor expansion project. The project won't solve the bridge's traffic and safety problems, but it will harm local communities, cost taxpayers billions of dollars, and only heightens our reliance on cars and fossil fuels.

Highway expansion projects harm already vulnerable populations. This expansion project will promulgate plumes of dust and fumes, worsening air pollution and health outcomes in nearby communities in the long term while interrupting people's everyday lives in the short term. After months of negotiation, Covington residents are still giving up over a dozen properties to the state to build this highway. And despite all of this, the project likely will not even solve the problems it is setting out to fix. Research strongly suggests that expanding highway capacity attracts more vehicles and increases traffic, the reverse effect of what KYTC's proposed plan is supposed to achieve.

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I am hoping that you will not support expanding the Brent Spence highway but rather invest in our communities, our transportation system, and our health and

safety.

Cristina Storer

834 Willard Street

Covington, KY 41011
United States

Phone: (787) 902-3467

Email Address: stolich@hotmail.com

Date of Submission: Thu, 09/01/2022 - 06:48

Submission Letter:

I'm writing to you today in hopes that you will oppose the Brent Spence Bridge Corridor expansion project. The project won't solve the bridge's traffic and safety problems, but it will harm local communities, cost taxpayers billions of dollars, and only heightens our reliance on cars and fossil fuels.

Highway expansion projects harm already vulnerable populations. This expansion project will promulgate plumes of dust and fumes, worsening air pollution and health outcomes in nearby communities in the long term while interrupting people's everyday lives in the short term. After months of negotiation, Covington residents are still giving up over a dozen properties to the state to build this highway. And despite all of this, the project likely will not even solve the problems it is setting out to fix. Research strongly suggests that expanding highway capacity attracts more vehicles and increases traffic, the reverse effect of what KYTC's proposed plan is supposed to achieve.

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safety.

John Saxton

721 Dalton Street

Covington, KY 41011
United States

Phone: (724) 344-4034

Email Address: saxton32@hotmail.com

Date of Submission: Wed, 08/31/2022 - 23:28

Submission Letter:

I'm writing to you today in hopes that you will oppose the Brent Spence Bridge Corridor expansion project. The project won't solve the bridge's traffic and safety problems, but it will harm local communities, cost taxpayers billions of dollars, and only heightens our reliance on cars and fossil fuels.

Highway expansion projects harm already vulnerable populations. This expansion project will promulgate plumes of dust and fumes, worsening air pollution and health outcomes in nearby communities in the long term while interrupting people's everyday lives in the short term. After months of negotiation, Covington residents are still giving up over a dozen properties to the state to build this highway. And despite all of this, the project likely will not even solve the problems it is setting out to fix. Research strongly suggests that expanding highway capacity attracts more vehicles and increases traffic, the reverse effect of what KYTC's proposed plan is supposed to achieve.

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safety.

Tom Blumena

731 Philadelphia Street

Covington, KY 41011
United States

Phone: (513) 404-8443

Email Address: blumes33@gmail.com

Date of Submission: Wed, 08/31/2022 - 20:53

Submission Letter:

I'm writing to you today in hopes that you will oppose the Brent Spence Bridge Corridor expansion project. The project won't solve the bridge's traffic and safety problems, but it will harm local communities, cost taxpayers billions of dollars, and only heightens our reliance on cars and fossil fuels.

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safety.

Olivia Gobble

311 Pershing Avenue

Covington, KY 41011
United States

Phone: (217) 415-5790

Email Address: omgobble8@gmail.com

Date of Submission: Wed, 08/31/2022 - 16:42

Submission Letter:

I'm writing to you today in hopes that you will oppose the Brent Spence Bridge Corridor expansion project. The project won't solve the bridge's traffic and safety problems, but it will harm local communities, cost taxpayers billions of dollars, and only heightens our reliance on cars and fossil fuels.

Highway expansion projects harm already vulnerable populations. This expansion project will promulgate plumes of dust and fumes, worsening air pollution and health outcomes in nearby communities in the long term while interrupting people's everyday lives in the short term. After months of negotiation, Covington residents are still giving up over a dozen properties to the state to build this highway. And despite all of this, the project likely will not even solve the problems it is setting out to fix. Research strongly suggests that expanding highway capacity attracts more vehicles and increases traffic, the reverse effect of what KYTC's proposed plan is supposed to achieve.

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safety.

Josh Hatton

934 York St.

Covington, KY 41011
United States

Phone: (859) 338-9208

Email Address: jhatton81@me.com

Date of Submission: Wed, 08/31/2022 - 14:47

Submission Letter:

I'm writing to you today in hopes that you will oppose the Brent Spence Bridge Corridor expansion project. The project won't solve the bridge's traffic and safety problems, but it will harm local communities, cost taxpayers billions of dollars, and only heightens our reliance on cars and fossil fuels.

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safety.

Jeremy miller

717 Philadelphia Street,

Covington, KY 41011
United States

Phone: (504) 289-6439

Email Address: jmillspaysbills@gmail.com

Date of Submission: Wed, 08/31/2022 - 14:46

Submission Letter:

I'm writing to you today in hopes that you will oppose the Brent Spence Bridge Corridor expansion project. The project won't solve the bridge's traffic and safety problems, but it will harm local communities, cost taxpayers billions of dollars, and only heightens our reliance on cars and fossil fuels.

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safety.

Sean Buckley

607 Main Street, apartment 2

Covington, KY 41011
United States

Phone: (859) 446-1784

Email Address: sbuckley2015@gmail.com

Date of Submission: Wed, 08/31/2022 - 14:24

Submission Letter:

I'm writing to you today in hopes that you will oppose the Brent Spence Bridge Corridor expansion project. The project won't solve the bridge's traffic and safety problems, but it will harm local communities, cost taxpayers billions of dollars, and only heightens our reliance on cars and fossil fuels.

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safety.

Sharon Payton

513 W 8th Street

Covington, KY 41011
United States

Phone: (859) 512-6628

Email Address: sharonpayton@outlook.com

Date of Submission: Wed, 08/31/2022 - 13:54

Submission Letter:

I'm writing to you today in hopes that you will oppose the Brent Spence Bridge Corridor expansion project. The project won't solve the bridge's traffic and safety problems, but it will harm local communities, cost taxpayers billions of dollars, and only heightens our reliance on cars and fossil fuels.

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safety.

Leslie Blair

209 Fairfield Ave

Bellevue, KY 41073
United States

Phone: (859) 835-0598

Email Address: leslieblaur@yahoo.com

Date of Submission: Wed, 08/31/2022 - 13:22

Submission Letter:

I'm writing to you today in hopes that you will oppose the Brent Spence Bridge Corridor expansion project. The project won't solve the bridge's traffic and safety problems, but it will harm local communities, cost taxpayers billions of dollars, and only heightens our reliance on cars and fossil fuels.

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safety.

Katie Eelman

842 Philadelphia Street

Covington, KY 41011
United States

Phone: (215) 206-2744

Email Address: kateeelman@gmail.com

Date of Submission: Wed, 08/31/2022 - 13:11

Submission Letter:

I'm writing to you today in hopes that you will oppose the Brent Spence Bridge Corridor expansion project. The project won't solve the bridge's traffic and safety problems, but it will harm local communities, cost taxpayers billions of dollars, and only heightens our reliance on cars and fossil fuels.

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safety.

Steve Viel

510 W 7th Street

Covington, KY 41011
United States

Phone: (513) 281-1837

Email Address: eastwhsteve@yahoo.com

Date of Submission: Wed, 08/31/2022 - 13:06

Submission Letter:

I'm writing to you today in hopes that you will oppose the Brent Spence Bridge Corridor expansion project. The project won't solve the bridge's traffic and safety problems, but it will harm local communities, cost taxpayers billions of dollars, and only heightens our reliance on cars and fossil fuels.

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safety.

jason colonel

651 Dalton Street

Covington, KY 41011
United States

Phone: (859) 322-2969

Email Address: jason.colonel@gmail.com

Date of Submission: Wed, 08/31/2022 - 12:34

Submission Letter:

I'm writing to you today in hopes that you will oppose the Brent Spence Bridge Corridor expansion project. The project won't solve the bridge's traffic and safety problems, but it will harm local communities, cost taxpayers billions of dollars, and only heightens our reliance on cars and fossil fuels.

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safety.

Kila Hanrahan

206 E 2nd St. #B

Covington, KY 41011
United States

Phone: (859) 409-0009

Email Address: kila.hanrahan@gmail.com

Date of Submission: Wed, 08/31/2022 - 12:17

Submission Letter:

I'm writing to you today in hopes that you will oppose the Brent Spence Bridge Corridor expansion project. The project won't solve the bridge's traffic and safety problems, but it will harm local communities, cost taxpayers billions of dollars, and only heightens our reliance on cars and fossil fuels.

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safety.

Barbara Didrichsen

6231 Cortelyou Avenue

Cincinnati, OH 45213
United States

Phone: (513) 235-5471

Email Address: barbdid@gmail.com

Date of Submission: Wed, 08/31/2022 - 12:04

Submission Letter:

I'm writing to you today in hopes that you will oppose the Brent Spence Bridge Corridor expansion project. The project won't solve the bridge's traffic and safety problems, but it will harm local communities, cost taxpayers billions of dollars, and only heightens our reliance on cars and fossil fuels.

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I am hoping that you will not support expanding the Brent Spence highway but rather invest in our communities, our transportation system, and our health and

safety.

Justin Herald

713 Delmar Place

Covington, KY 41014
United States

Phone: (513) 338-4652

Email Address: jrherald@me.com

Date of Submission: Wed, 08/31/2022 - 11:57

Submission Letter:

I'm writing to you today in hopes that you will oppose the Brent Spence Bridge Corridor expansion project. The project won't solve the bridge's traffic and safety problems, but it will harm local communities, cost taxpayers billions of dollars, and only heightens our reliance on cars and fossil fuels.

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safety.

Christ Wyatt

3453 Kleybolte Avenue

Cincinnati, OH 45226
United States

Phone: (513) 382-1345

Email Address: christophernealwyatt@gmail.com

Date of Submission: Wed, 08/31/2022 - 10:46

Submission Letter:

I'm writing to you today in hopes that you will oppose the Brent Spence Bridge Corridor expansion project. The project won't solve the bridge's traffic and safety problems, but it will harm local communities, cost taxpayers billions of dollars, and only heightens our reliance on cars and fossil fuels.

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safety.

John Riffle

1508 Race Street, # 5R

Cincinnati, OH 45202
United States

Phone: (603) 801-5386

Email Address: mealier_revenue.0r@icloud.com

Date of Submission: Wed, 08/31/2022 - 10:39

Submission Letter:

I'm writing to you today in hopes that you will oppose the Brent Spence Bridge Corridor expansion project. The project won't solve the bridge's traffic and safety problems, but it will harm local communities, cost taxpayers billions of dollars, and only heightens our reliance on cars and fossil fuels.

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safety.

Joe Rosemeyer

5746 Glow Ct

Cincinnati, OH 45238
United States

Phone: (513) 375-7407

Email Address: jrrosemeyer@yahoo.com

Date of Submission: Wed, 08/31/2022 - 08:17

Submission Letter:

I'm writing to you today in hopes that you will oppose the Brent Spence Bridge Corridor expansion project. The project won't solve the bridge's traffic and safety problems, but it will harm local communities, cost taxpayers billions of dollars, and only heightens our reliance on cars and fossil fuels.

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safety.

Stephen Gastright

117 Wallace Avenue

Covington, KY 41014
United States

Phone: (215) 917-0132

Email Address: sgastright@gmail.com

Date of Submission: Wed, 08/31/2022 - 05:18

Submission Letter:

I'm writing to you today in hopes that you will oppose the Brent Spence Bridge Corridor expansion project. The project won't solve the bridge's traffic and safety problems, but it will harm local communities, cost taxpayers billions of dollars, and only heightens our reliance on cars and fossil fuels.

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safety.

Alex Browne

1342 Walnut Street

Cincinnati, OH 45202
United States

Phone: (513) 225-1864

Email Address: alexbrowne1996@yahoo.com

Date of Submission: Tue, 08/30/2022 - 23:47

Submission Letter:

I'm writing to you today in hopes that you will oppose the Brent Spence Bridge Corridor expansion project. The project won't solve the bridge's traffic and safety problems, but it will harm local communities, cost taxpayers billions of dollars, and only heightens our reliance on cars and fossil fuels.

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safety.

Sarah Thomas

1546 Knowlton Street

Cincinnati, OH 45223
United States

Phone: (514) 253-3480

Email Address: sthomas5191@gmail.com

Date of Submission: Tue, 08/30/2022 - 21:46

Submission Letter:

I'm writing to you today in hopes that you will oppose the Brent Spence Bridge Corridor expansion project. The project won't solve the bridge's traffic and safety problems, but it will harm local communities, cost taxpayers billions of dollars, and only heightens our reliance on cars and fossil fuels.

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safety.

Crystal Dandridge

11111 Springfield Pike

Cincinnati, OH 45246
United States

Phone: (513) 328-4158

Email Address: cjdjam@gmail.com

Date of Submission: Tue, 08/30/2022 - 21:43

Submission Letter:

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safety.

Justin Ogilby

3788 Clifton Avenue

Cincinnati, OH 45220
United States

Phone: (513) 545-4377

Email Address: jogilby@gmail.com

Date of Submission: Tue, 08/30/2022 - 21:27

Submission Letter:

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safety.

Joseph Bellante

3338 Walworth Ave

Cincinnati, OH 45226
United States

Phone: (513) 518-1114

Email Address: joejobiker@aol.com

Date of Submission: Tue, 08/30/2022 - 21:19

Submission Letter:

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Ryan Crane

1 Annwood Lane

Cincinnati, OH 45206
United States

Phone: (715) 577-7388

Email Address: racranemd@gmail.com

Date of Submission: Tue, 08/30/2022 - 20:16

Submission Letter:

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safety.

Matt Butler

900 Edgehill Rd

Covington, KY 41011
United States

Phone: (859) 203-8206

Email Address: matt.mb64@gmail.com

Date of Submission: Tue, 08/30/2022 - 20:02

Submission Letter:

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safety.

Andrew Norton

4029 Hamilton Ave

Cincinnati, OH 45223
United States

Phone: (513) 910-5067

Email Address: nortonaj2@gmail.com

Date of Submission: Tue, 08/30/2022 - 19:58

Submission Letter:

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safety.

Andrew Vielhaber

1616 Ruth Avenue,

CINCINNATI, OH 45207
United States

Phone: (440) 487-0961

Email Address: vielhaber8@gmail.com

Date of Submission: Tue, 08/30/2022 - 19:23

Submission Letter:

I'm writing to you today in hopes that you will oppose the Brent Spence Bridge Corridor expansion project. The project won't solve the bridge's traffic and safety problems, but it will harm local communities, cost taxpayers billions of dollars, and only heightens our reliance on cars and fossil fuels.

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safety.

Ryan Cook

983 , Windsor Avenue

Cincinnati, OH 45206
United States

Phone: (937) 707-7757

Email Address: Rmcook13@gmail.com

Date of Submission: Tue, 08/30/2022 - 18:35

Submission Letter:

I'm writing to you today in hopes that you will oppose the Brent Spence Bridge Corridor expansion project. The project won't solve the bridge's traffic and safety problems, but it will harm local communities, cost taxpayers billions of dollars, and only heightens our reliance on cars and fossil fuels.

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safety.

Jo Krippenstapel

132 Ross Ave

Bellevue, KY 41073
United States

Phone: (513) 289-1631

Email Address: krippji@gmail.com

Date of Submission: Tue, 08/30/2022 - 18:21

Submission Letter:

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I am hoping that you will not support expanding the Brent Spence highway but rather invest in our communities, our transportation system, and our health and

safety.

Charles Schwartz

2896 Victoria Avenue

Cincinnati, OH 45208
United States

Phone: (419) 377-9181

Email Address: c.schwartz.1983@gmail.com

Date of Submission: Tue, 08/30/2022 - 17:59

Submission Letter:

I'm writing to you today in hopes that you will oppose the Brent Spence Bridge Corridor expansion project. The project won't solve the bridge's traffic and safety problems, but it will harm local communities, cost taxpayers billions of dollars, and only heightens our reliance on cars and fossil fuels.

Highway expansion projects harm already vulnerable populations. This expansion project will promulgate plumes of dust and fumes, worsening air pollution and health outcomes in nearby communities in the long term while interrupting people's everyday lives in the short term. After months of negotiation, Covington residents are still giving up over a dozen properties to the state to build this highway. And despite all of this, the project likely will not even solve the problems it is setting out to fix. Research strongly suggests that expanding highway capacity attracts more vehicles and increases traffic, the reverse effect of what KYTC's proposed plan is supposed to achieve.

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safety.

Shad Beard

730 Washington St Apt 132

Covington, KY 41011
United States

Phone: (937) 554-9268

Email Address: dahs1981@gmail.com

Date of Submission: Tue, 08/30/2022 - 17:49

Submission Letter:

I'm writing to you today in hopes that you will oppose the Brent Spence Bridge Corridor expansion project. The project won't solve the bridge's traffic and safety problems, but it will harm local communities, cost taxpayers billions of dollars, and only heightens our reliance on cars and fossil fuels.

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Shad Beard

730 Washington Street Apt 132

Covington, KY 41011
United States

Phone: (937) 554-9268

Email Address: dahs1981@gmail.com

Date of Submission: Tue, 08/30/2022 - 17:47

Submission Letter:

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I am hoping that you will not support expanding the Brent Spence highway but rather invest in our communities, our transportation system, and our health and

safety.

Jackson Thurnquist

121 East Freedom Way

Cincinnati, OH 45202
United States

Phone: (513) 543-7621

Email Address: jackson.thurnquist@gmail.com

Date of Submission: Tue, 08/30/2022 - 17:35

Submission Letter:

I'm writing to you today in hopes that you will oppose the Brent Spence Bridge Corridor expansion project. The project won't solve the bridge's traffic and safety problems, but it will harm local communities, cost taxpayers billions of dollars, and only heightens our reliance on cars and fossil fuels.

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safety.

Kerry Devery

6600 Buckingham Place

Cincinnati, OH 45227
United States

Phone: (614) 506-6372

Email Address: kerry.devery@gmail.com

Date of Submission: Tue, 08/30/2022 - 13:28

Submission Letter:

I'm writing to you today in hopes that you will oppose the Brent Spence Bridge Corridor expansion project. The project won't solve the bridge's traffic and safety problems, but it will harm local communities, cost taxpayers billions of dollars, and only heightens our reliance on cars and fossil fuels.

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safety.

**Brent Spence Bridge Corridor Project
Coalition for Transit and Sustainable Development
Correspondence**



COALITION FOR TRANSIT AND SUSTAINABLE DEVELOPMENT
GREATER CINCINNATI

Concerns over Brent Spence Corridor Project's Compliance with Civil Rights and Environmental Justice Regulations

Division Administrator Laura Leffler
Federal Highway Administration
Ohio Division
200 North High Street
Room 328
Columbus, Ohio 43215

614-280-6896
Laurie.leffler@dot.gov

Letter to Federal Highway Administration regarding Brent Spence Corridor

The undersigned individuals and organizations write to express our concern that issuing a Finding of No Significant Impact (FONSI) or other environmental approval for the Brent Spence Corridor Project would violate the National Environmental Policy Act, 42 U.S.C. § 4321, *et seq.*, Title VI of the Civil Rights Act of 1964, 42 U.S.C. §§ 2000d to 2000d-7, the U.S. Department of Transportation's (USDOT) implementing regulations, 49 C.F.R. pt. 21 and the Ohio Department of Transportation's (ODOT) own Title VI/Nondiscrimination Plan and assurances submitted to the federal government, as well as other state and federal civil rights and environmental justice requirements. The Federal Highway Administration (FHWA) should not provide an environmental approval for this project until the civil rights and environmental justice issues presented herein are properly analyzed, addressed, and resolved.

Under Title VI of the Civil Rights Act of 1964 (Title VI), “[n]o person in the United States, on the ground of race, color, or national origin, shall be excluded from participation in, be denied the benefits of, or be subjected to discrimination under any program or activity receiving federal financial assistance.” ODOT is a recipient of federal financial assistance and submits annual Nondiscrimination Statements and Assurances required by 49 C.F.R §21.7, to U.S. Department of Transportation, as a condition of its eligibility for federal financial assistance.

In addition, Executive Order 12898, “Federal Actions to Address Environmental Justice in Minority Populations and Low-Income Populations” 59 F.R. 7626, provides:

“To the greatest extent practicable and permitted by law, . . . **each Federal agency shall make achieving environmental justice part of its mission by identifying and addressing, as appropriate, disproportionately high and adverse human health or environmental effects of its programs, policies, and activities on minority populations and low-income populations in the United States . . .**”

Moreover, where prior discriminatory practice or usage has tended to subject individuals to discrimination under any program or activity to which Title VI applies, the applicant or recipient, in this case ODOT, “must take affirmative action to remove or overcome the effects of the prior discriminatory practice or usage.” 49 C.F.R. § 21.5(b)(7). Both ODOT and the Kentucky Transportation Cabinet (KYTC) acknowledge their requirement to comply with Title VI in their application materials.

The President of the United States has acknowledged the discriminatory history and ongoing discriminatory effects of the federal highway program:

[t]he creation of the Interstate Highway System, funded and constructed by the Federal Government and State governments in the 20th century, disproportionately burdened many historically Black and low-income neighborhoods in many American cities. Many urban interstate highways were deliberately built to pass through Black neighborhoods, often requiring the destruction of housing and other local institutions. To this day, many Black

neighborhoods are disconnected from access to high-quality housing, jobs, public transit, and other resources.[\[1\]](#)

The use of highway construction and “Urban renewal” programs to demolish and segregate Black and Hispanic/Latinx neighborhoods and communities is part of a longstanding pattern of federal, state, and local government use of policy and allocation of public resources to impose segregation and benefit white communities at the expense of Black, Hispanic/Latinx and other communities of color.[\[2\]](#)

“During the 20th century, Federal, State, and local governments systematically implemented racially discriminatory housing policies that contributed to segregated neighborhoods and inhibited equal opportunity and the chance to build wealth for Black, Latino, Asian American and Pacific Islander, and Native American families, and other underserved communities. Ongoing legacies of residential segregation and discrimination remain ever-present in our society. These include a racial gap in homeownership; a persistent undervaluation of properties owned by families of color; a disproportionate burden of pollution and exposure to the impacts of climate change in communities of color...”[\[3\]](#)

Construction of the original I-75 project through a predominantly Black community concluded in 1963 and created ongoing disproportionate negative impacts on low-income communities and communities of color. The ODOT application for federal funding under the Multimodal Project Discretionary Grant program shows that the entire project impact area in the state of Ohio is made up of areas designated as Areas of Persistent Poverty, Historically Disadvantaged Communities, or both. Of note, the West End neighborhood is designated as both a Historically Disadvantaged Community and an Area of Persistent Poverty, and it was this neighborhood that was most severely impacted by the razing of properties during the initial construction of the interstate in the City of Cincinnati.

ODOT is required to take “affirmative action to remove or overcome the effects of the prior discriminatory practice” of constructing an interstate highway through a predominately Black community. We believe ODOT should

investigate, through formal technical feasibility studies, narrowing the right of way and reconnecting city streets to reduce impact of the interstate highway through this neighborhood. This would facilitate the long-term rehabilitation of this community and bring the project in alignment with stated USDOT objectives of reconnecting communities that have been adversely impacted by prior infrastructure projects.

NEPA regulations require that an agency preparing an impact statement must “rigorously explore and objectively evaluate all reasonable alternatives which were eliminated from detailed study, and for alternatives which were eliminated from detailed study, briefly discuss the reasons for them having been eliminated” 40 C.F.R. § 1502.14(a). The stated purpose of this highway expansion project is to reduce congestion along the Brent Spence Corridor. Neither ODOT nor OKI discuss the use of tolling or congestion pricing in a no-build scenario in their consideration of alternatives to this project. While Kentucky state law prohibits the use of tolling to finance an expansion project of this type (“a development agreement or financial plan”), no regulation exists which would prohibit the use of tolling for congestion relief in a no-build scenario. Use of tolling as a financing mechanism was used in a similar project in Louisville, and the presence of tolling resulted in a significant decrease in traffic across a previously un-tolled river crossing. Evidence in the field of urban planning, including direct experience in the state of Kentucky, supports the use of congestion pricing or tolling as a “reasonable alternative” to highway widening for congestion relief, and no consideration of this alternative has been made in the development of the BSCP. The Federal Highway Administration Office of Operations promotes congestion pricing as a “way of harnessing the power of the market to reduce the waste associated with traffic congestion.”[\[4\]](#) Arguments related to the political feasibility of congestion pricing should not be considered in the NEPA process, since the purpose of the environmental impact study is to allow the public to make informed decisions about the use of significant amounts of state and federal money – nearly three billion dollars in this case. If a reasonable alternative, such as congestion tolling on the existing infrastructure, is not rigorously explored and objectively evaluated, the public is unable to make an informed decision. ODOT’s NEPA submissions are therefore deficient on this basis.

The BSCP is currently advancing under an outdated Finding of No Significant Impact from FHWA, with a re-evaluation of this decision due to be completed in early 2023. The initial FONSI made no mention of issues related to air quality, and the adverse impact of highway expansion on air quality for minority and low-income communities directly adjacent to the Brent Spence Corridor has not been addressed in either the 2015 or 2018 environmental updates. ODOT applications reference an Air Quality Analysis from 2011, which states that the project would decrease emissions when compared to a no-build scenario. However, in *1000 Friends of Wisconsin v. United States Department of Transportation*, USDOT apparently acknowledged that expanding highway access “will result in new vehicle trips and additional vehicle miles traveled and therefore cause some increase in harmful emissions.”^[5] Elsewhere, the decision states that USDOT has “acknowledged that induced travel will lead to higher emissions and somewhat lower air quality” and that “the impact statement [for the proposed widening of Wisconsin Highway 23] acknowledges that increased travel will lead to higher emissions.” ODOT and OKI estimate that the Brent Spence Corridor will carry 227,900 vehicles per day eighteen years from now, compared with around 165,000 vehicles per day under current conditions.^[6] In a 2015 interview, ODOT’s Brent Spence Corridor Project manager at the time stated that “the way Cincinnati is laid out, the more lanes you build on [Interstate] 75, the more traffic you draw . . . We could continue to build lanes on 75 but they would fill because of the nature of the traffic network in the region.”^[7] It is not clear how USDOT or ODOT reconcile these statements about air quality and induced travel with the referenced 2011 Air Quality Analysis, and there is no way of knowing whether any supposed reduction in emissions from decreased congestion after highway expansion is superior to the decrease that would be provided by congestion pricing because ODOT has not performed the relevant analysis.

The American Lung Association has noted the disparate impact of air pollution on low-income communities and communities of color, stating that

Many studies have looked at differences in the impact of air pollution on premature death. Recent studies have looked at the mortality in the Medicaid population and found that those who live in predominately black or African

American communities suffered greater risk of premature death from particle pollution than those who live in communities that are predominately white.[\[8\]](#)

A 2010 study commissioned by the City of Cincinnati noted that the adverse impact of air pollution related to traffic and construction along the I-75 corridor predominantly affects communities to the north and east, due to the prevailing wind patterns in the region. [\[9\]](#) The West End community is situated directly to the north and east of the project area. The American Lung Association's 2022 State of the Air Report named Cincinnati as the 12th most polluted metropolitan area for annual particle pollution.[\[10\]](#) The FHWA should not provide a FONSI or other environmental approval unless ODOT and KYTC satisfactorily address concerns related to air quality, as air quality issues exist at the intersection of environmental and racial justice concerns. As noted above, ODOT and KYTC did not rigorously explore the alternative of congestion pricing, and the FHWA Division of Operations notes that

Three cities have implemented congestion pricing on a broad scale and have realized significant environmental benefits. Through cordon pricing in its central business district, London reduced emissions of particulate matter and nitrogen oxides by 12 percent and fossil fuel consumption and carbon dioxide emissions by 20 percent. Singapore's congestion pricing scheme prevents the emission of an estimated 175,000 lb of carbon dioxide each day; and Stockholm's congestion pricing system has led to a 10 to 14 percent drop in carbon dioxide emissions in its central area.[\[11\]](#)

FHWA is required to identify and address these concerns about air quality and environmental justice, including exploring reasonable alternatives to highway expansion, under Executive Order 12898.

FHWA should not issue further environmental approval for this project unless ODOT and KYTC substantively address the civil rights and environmental justice concerns outlined in this letter. Steps to address these concerns would include:

- Formal technical consideration of design alternatives that take affirmative action towards remedying the ongoing disparate negative

impacts of interstate highway construction through predominantly Black and low-income communities

- “Rigorous exploration and objective evaluation” of the use of congestion pricing or tolling as a “reasonable alternative” to highway expansion for congestion relief
- Analysis and mitigation of the adverse environmental impact of expanding interstate highway capacity through the cities of Cincinnati and Covington, which include but are not limited to concerns about air quality

Providing environmental approval for this project without doing so would violate Title VI of the Civil Rights Act of 1964, the NEPA process, its implementing regulations, and other federal civil rights and environmental justice requirements, including Executive Order 12898. We believe that the outdated Finding of No Significant Impact for this project should be rescinded, and no new environmental determination regarding the project should be issued, until these concerns are addressed.

We sincerely thank you for your attention to these concerns.

Respectfully submitted,

Ryan A. Crane, MD

Matt Butler
President
Devou Good Foundation

Reid Yearwood
Executive Director
Esperanza Latino Center of NKY

Joe Girandola
President
Art Academy of Cincinnati

Joe Humpert
President
Queen City Bike

Brewster Rhoads
Principal
Brewster Rhoads & Associates

Jody Robinson
President
Flourish Ventures

Sheryl Rajbhandari
Founder & Executive Director
Heartfelt Tidbits

Charles J. Kelly, MD, FAAP
Pediatric Care of KY

Beth Johnson
Executive Director
Cincinnati Preservation Association

Rachel Hastings
Executive Director
Price Hill Will

Rev. Dr. Johannon R. Tate BS, MA, DMIN
Pastor
Jerriel Missionary Baptist Church

Tanner Yess
Co-Executive Director
Groundwork Ohio River Valley

Rev. Peter D'Angio
Rector
Trinity Episcopal Church

Nolan Nicaise
Commissioner
Covington, KY

David Hastings
Executive Director
Housing Opportunities of Northern Kentucky (HONK)

Margy Waller
Founder
Art on the Streets

Cal Cullen
Co-Founder and Executive Director
Wave Pool Corporation

Jim Guthrie
Principal
Hub + Weber Architects

Mimi Rook
President
Camp Washington Community Council

Ericka Copeland
State Director
Sierra Club Ohio

As Individuals:

Barbara Didrichsen

Matthew Corbett

Chow-chi Huang

cc: Attorney Dennis M Grzezinski

[1] President Joseph A. Biden, “*Memorandum on Redressing Our Nation’s and the Federal Government’s*

History of Discriminatory Housing Practices and Policies”, January 26, 2021. Available at:

<https://www.whitehouse.gov/briefing-room/presidential-actions/2021/01/26/memorandum-on-redressingour-nations-and-the-federal-governments-history-of-discriminatory-housing-practices-and-policies/>

[2] In addition to other forms, discrimination based on national origin includes discrimination against persons with limited English proficiency (LEP) under Title VI. See, *Improving Access to Services for People with Limited English Proficiency*, 65 Fed. Reg. 159 (Aug. 16, 2000); see also *Coldwell v. Dep’t of Health & Human Servs.*, 558 F.3d 1112 (9th Cir. 2009); *United States v. Maricopa Cnty.*, 915 F. Supp. 2d 1073 (D. Ariz. 2012); *Jones v. Gusman*, 296 F.R.D. 416 (E.D. La. June 6, 2013); and *Lau v. Nichols*, 414 U.S. 563 (1974). See also, DOT Guidance on LEP, 70 Fed. Reg. 7408, 74095 (Wednesday, December 14, 2005).

[3] January 26, 2021 Memorandum.

[4] “Welcome to the FHWA Congestion Pricing Website.” Federal Highway Administration Office of Operations.

<https://ops.fhwa.dot.gov/congestionpricing/>

[5] *1000 Friends of Wisconsin v. U.S. Department of Transportation, et al.*. US District Court – Eastern District of Wisconsin. Case No 11-C-0545.

<https://1kfriends.org/wp-content/uploads/2015/05/decision-2015-05-22-1.pdf>

[6] “Brent Spence and ‘companion’ bridge could see 40% jump in traffic.” by Patricia Gallagher Newberry. Cincinnati Enquirer. August 29, 2022.

<https://www.cincinnati.com/story/news/traffic/2022/08/29/how-much-traffic-will-brent-spence-companion-bridge/65407695007/>

[7] "What will happen on the new Brent Spence when more traffic comes to I-75?" by Chris Wetterich. Cincinnati Business Courier. bizjournals.com/cincinnati/blog/2015/03/what-will-happen-on-the-new-brent-spence-when-more.html

[8] "Disparities in the Impact of Air Pollution." American Lung Association. <https://www.lung.org/clean-air/outdoors/who-is-at-risk/disparities> Last updated November 17, 2022. Accessed December 3, 2022.

[9] "Interstate 75 Focus Area Study Health Impact Assessment." City of Cincinnati, December 2010.

<https://www.cincinnati-oh.gov/sites/health/assets/File/I75FocusAreaHIA.pdf>
Accessed December 3, 2020

[10] "State of the Air 2022". American Lung Association. <https://www.lung.org/research/sota/city-rankings/msas/cincinnati-wilmington-maysville-oh-ky-in#pmann> Accessed December 3, 2022.

[11] "Congestion Pricing: Environmental Benefits." Federal Highway Administration Division of Operations. https://ops.fhwa.dot.gov/congestionpricing/resources/enviro_benefits.htm
Accessed December 6, 2022

From: C Elifrits <elifritsc@nku.edu>
Sent: Monday, February 6, 2023 12:59:58 PM
To: Leffler, Laurie (FHWA) <Laurie.Leffler@dot.gov>
Cc: C Elifrits <elifritsc@nku.edu>
Subject: Devou Good Foundation letter dated 25 January 2023

CAUTION: This email originated from outside of the Department of Transportation (DOT). Do not click on links or open attachments unless you recognize the sender and know the content is safe.

MS Laura Leffler, Division Administrator
Federal Highway Administration, Ohio Division
200 North High Street, Room 328
Columbus, Ohio 43215
Laurie.leffler@dot.gov

RE: Devou Good Foundation letter dated 25 January 2023, signed by Ryan A. Crane, MD, *et al.*, regarding delay in progress of Bent Spence Bridge Corridor Project

Dear Division Administrator Leffler,

I have read the subject letter and heard interviews of Mr. Matt Butler regarding the Devou Good Foundation plea to pause work on the Brent Spence Bridge Corridor Project while more studies are conducted. Further, I have followed the progress on this long overdue project since moving to Highland Heights, Kentucky, during July 2002 to take the position of Director of Pre-Engineering and Outreach at Northern Kentucky University from which I am now retired.

Suffice it to say that the letter writers and we **all know very well** that all the issues presented in the referenced letter **have been carefully and sufficiently studied** and the resulting conclusions used in the project plans. I have attended technical and public meetings during the last 20 years where these issues were discussed, and vast amounts of study data were shared across agencies and various public groups. It is abundantly clear that the issues raised in the referenced letter have been thoroughly studied by numerous highly qualified research and consulting groups and agencies. It is equally clear that these issues have been more than suitably integrated into the plans as we see the plans today.

Hence, I ask that you carefully but firmly respond to this group that their concerns and issues have, indeed, been studied and that outcomes of such studies have been integrated into the current plans. Further, that no more study time and expense are warranted. I strongly encourage that you take all necessary steps to move this project to construction with all due haste and absolutely no delay.

Thank you for your consideration of my thoughts and for your work to move this project to its current situation.
The
immense need for this completed project is self-apparent and grows daily.

Sincerely,

/s/ C. Dale Elifrits

C. Dale Elifrits, Ph.D., F SME, R.M. of SME, M ASCE, Sr. Emeritus M AEG, M ASEE, M NSTA, M Sigma Xi
Director of Pre-Engineering, Visiting Prof. Geology, retired, Northern Kentucky University
Professor Emeritus, Geological Engineering, Missouri Univ. of Science and Technology
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February 14, 2023

Laura Leffler
Ohio Division Administrator
Federal Highway Administration
200 North High Street, Room 328
Columbus, Ohio 43215

Dear Ms. Leffler:

On January 25, 2023, Tri-State Trails joined in a letter from the Coalition for Sustainable Transit and Development, along with 20 other signatories, to the Ohio Division Administrator of the Federal Highway Administration. The letter raised a number of issues related to the environmental impact of the Brent Spence Bridge Corridor Project, and specifically questioned whether the project complied with the applicable environmental statutes. The letter called for the Federal Highway Administration to rescind the enabling environmental designation of the project, effectively pausing it, while a range of further analysis and alternatives are explored.

While promoting environmental sustainability, increasing transportation equity, and promoting public health are among the pillars of Tri-State Trails mission, the Board has concluded that the focus of the Coalition's activities regarding the Brent Spence Bridge Corridor Project do not sufficiently align with our core purpose to merit our participation. Accordingly, we are ending our affiliation with the Coalition and we have asked to have Tri-State Trails removed as a signatory to the letter.

We remain committed to our mission of making active transportation an accessible and safe option for pedestrians, bicyclists, and individuals who use mobility devices. The Brent Spence Bridge Corridor Project may be the largest infrastructure investment that Cincinnati and Northern Kentucky sees in the 21st century, impacting local roadways from Dixie Highway in Fort Mitchell to the Western Hills Viaduct in Cincinnati. This presents a once-in-a-generation opportunity to not only improve vehicular congestion and throughput, but to also mitigate the impact of the interstate highway on the

adjacent communities. We welcome the opportunity to engage with the City of Cincinnati, the City of Covington, ODOT, KYTC, community groups, and residents to ensure that safe and comfortable active transportation infrastructure are included in the local roadways impacted by the Brent Spence Bridge Corridor Project.

Respectfully,

A handwritten signature in black ink, appearing to read "Wade Johnston". The signature is fluid and cursive, with a large loop at the end.

Wade Johnston, AICP
Executive Director
Tri-State Trails

From: Long, Timothy (FHWA)
Sent: Wednesday, March 15, 2023 3:20 PM
To: matt@devougood.com
Cc: dennisglaw@gmail.com; Stone, Scott (FHWA); Garczewski, Ron (FHWA); Burroughs, Mary (FHWA); Lopez, Dina (FHWA); Johnson, Adam (FHWA); Leffler, Laurie (FHWA); Hoyle, Jim (FHWA); Diop, Mour (FHWA); Ballantyne, John (FHWA); Toni, Melissa (FHWA); Morales, Silvio (FHWA); Baughman, Pamela (FHWA); Bromley, Xiola (FHWA); Jeter, Todd (FHWA); Borres, Boday (FHWA)
Subject: RE: Brent Spence Corridor Project

Good afternoon Mr. Butler,

Thank you for your interest in the Brent Spence Bridge Project. We received the letter you sent on behalf of the Coalition for Transit and Sustainable Development. We will investigate the concerns that were discussed in the letter and ensure they are considered prior to making a NEPA determination. A response to these comments will be included in the Supplemental EA. FHWA is committed to full compliance with the requirements of Title VI of the Civil Rights Act of 1964, NEPA and all Executive Orders.

Respectfully,

Tim Long
Planning, Environment and Realty Team Leader
Federal Highway Administration
Ohio Division Office
614-280-6879

Appendix J:
Alternative Concepts Letters and Responses





June 17, 2022

Governor Mike DeWine
Riffe Center
77 S High St, 30th Floor
Columbus, OH 43215

Dear Governor DeWine,

We understand that ODOT is pursuing a performance-based design-build procurement contract for the Brent Spence Bridge Corridor Project. We are thrilled that this project is now gaining momentum, as a result of the recent Infrastructure Bill. However, we also recognize the current solution is rooted in the preliminary configuration developed in 2012-2013.

While we appreciate the reduction in the right-of-way footprint in that configuration, we also understand that the design-build process can accommodate alternative technical concepts to bring additional creativity and achieve better value. Specifically, we believe that improved connectivity, which was admittedly not a priority in 2012, to the west side of I-75 is now more critical to the future of Cincinnati.

We believe a solution that not only reduces the right-of-way footprint, but may include a street and freeway configuration similar to ODOT's celebrated Fort Washington Way project may be a better option. It would enhance future connectivity to the west with safer streets, allow the central business district to expand east of I-75, simplify high speed on and off ramp movements, and create new economic growth.

GBBN is a committed collaborator working closely with the Greater Cincinnati Chamber of Commerce and Bridge Forward Cincinnati to promote the potential of improved connectivity and land reclamation resulting from this project. We see Queensgate and the West End as critical to Cincinnati's future growth. As local business, community, and political leaders, we urge ODOT to include criteria for the design/build process that is responsive to current goals and needs of the local community.

We request that the design/builders for the project be proactively invited to provide creative alternative concepts responsive to community goals during the procurement process. We have confidence that with clear criteria, this level of innovation can be achieved while breaking ground in late 2023.

We pledge our support to assist ODOT and Cincinnati in shaping the project procurement process to all parties' best interests.

Respectfully,
GBBN Architects, Inc.

A handwritten signature in blue ink that reads "Matthew J Schottelkotte".

Matthew J Schottelkotte AIA LEED AP
President / CEO

A handwritten signature in blue ink that reads "Steven T. Kenat".

Steven T. Kenat AIA LEED AP
Director of Community Development

Cc: ODOT Director, Jack Marchbanks PhD, jack.marchbanks@dot.oh.gov
Mayor Aftab Pureval, mayor.aftab@cincinnati-oh.gov



OHIO DEPARTMENT OF TRANSPORTATION
Mike DeWine, Governor Jack Marchbanks, Ph.D., Director

1980 W. Broad Street, Columbus, OH 43223
614-466-7170
transportation.ohio.gov

June 28, 2022

Mr. Matthew J. Schottelkotte
President/CEO
GBBN Architects, Inc.
332 East 8th St.
Cincinnati, OH 45202

Dear Mr. Schottelkotte,

Thank you for your letter of June 17, 2022, to Governor DeWine and copying me, referencing the Brent Spence Bridge Corridor Project. We, too, are excited with the momentum gained for this project and hope to have financing finalized later this year that will allow ODOT and our partner, the Kentucky Transportation Cabinet (KYTC), to begin construction in 2024.

ODOT and KYTC are planning to deliver most of the project via a design-build procurement, most likely a two-step, value-based procurement. This will include a prescriptive scope of work based on the environmentally approved alternative. We do plan to allow the design-build teams to submit alternative technical concepts as part of the procurement phase. We have utilized this process on numerous contracts managed by ODOT in the past and believe they allow for creativity to achieve the prescribed scope of work and meet goals established by the project sponsors.

The scope of work will include addressing the primary goals identified to address the transportation needs of the project. As part of this process, we plan to continue to make community goals consistent with residential/commercial growth, as well as aesthetics and reducing overall environmental impacts, a primary focus for this project. In addition, in partnership with the communities in both Ohio and Kentucky, this process will include evaluating connectivity within the central business districts and further footprint minimization.

Please feel free to contact our Project Manager, Stefan Spinosa, ODOT District 8 Capital Programs Administrator, at 513-933-6639; Stefan.spinosa@dot.ohio.gov if you wish to discuss the project further.

Respectfully,

Handwritten signature of Jack Marchbanks in green ink.

Jack Marchbanks, Ph.D.
Director

20 June 2022

Governor Mike DeWine
Riffe Center
77 S High St, 30th Floor
Columbus, OH 43215

Dear Governor DeWine,

We understand that ODOT is pursuing a performance-based design-build procurement contract for the Brent Spence Bridge Corridor Project. We are very pleased that this project is now gaining momentum.

While we appreciate the reduction in the right-of-way footprint in the 2012-13 design configuration, we also understand that the design-build process can accommodate alternative technical concepts to bring additional creativity and achieve better value. We believe that improved connectivity, which was admittedly not a priority in 2012, to the west side of I-75 is now more critical to the future of Cincinnati.

We believe a solution that not only reduces the right-of-way footprint but may include a street and freeway configuration similar to ODOT's For Washington Way project may be a better option, by enhancing future connectivity to the west, allowing the central business district to expand east of I-75, simplifying high speed on and off-ramp movements, and **creating a new economic engine for Cincinnati's future.**

As local businesses and community we expect flexibility from ODOT in designing the procurement process and contracts to be responsive to the goals and needs of the local community. Our company, Fern Exposition Services, LLC, is a stakeholder in this effort because we have been headquartered within a mile or two of the bridge site since our founding over 110 years ago. Our headquarters and Eastern Distribution Center, remain in Cincinnati's Queensgate neighborhood just off the bridge. This area needs revitalization and enhanced efficiency that can come from the right design.

We request that the designer/contracting community be proactively invited to provide creative alternative concepts responsive to community goals during the procurement process. We have confidence that with clear criteria, this level of innovation can be achieved while breaking ground in late 2023.

I would be happy to speak with a member of your team or ODOT to share our thoughts on the area and why a creative long-term approach matters to our business.

Respectfully,



Aaron Bludworth
CEO
Fern

Cc: ODOT Director, Jack Marchbanks PhD, jack.marchbanks@dot.oh.gov
Mayor Aftab Pureval, mayor.aftab@cincinnati-oh.gov



OHIO DEPARTMENT OF TRANSPORTATION
Mike DeWine, Governor

Jack Marchbanks, Ph.D., Director

1980 W. Broad Street, Columbus, OH 43223
614-466-7170
transportation.ohio.gov

June 28, 2022

Aaron Bludworth
CEO
Fern
645 Linn Street
Cincinnati, OH 45203

Dear Mr. Bludworth,

Thank you for copying me on your letter of June 20, 2022 to Governor DeWine referencing the Brent Spence Bridge Corridor Project. We agree with you that we are gaining momentum on the project and hope to have financing finalized later this year that will allow ODOT and our partner the Kentucky Transportation Cabinet (KYTC) to begin construction in 2024.

ODOT and KYTC currently plan to deliver most of the project via a design-build procurement. This will include a prescriptive scope of work based on the environmentally approved alternative. We do plan to allow the design-build team to submit alternative technical concepts as part of the procurement phase. We have utilized this process on numerous contracts managed by ODOT in the past and believe they allow for creativity to achieve the prescribed scope of work and meet goals established by the project sponsors.

We will continue to work with the project's stakeholders, including the City of Cincinnati, local communities, and local businesses in developing the scope of work for the project. This will include evaluating connectivity within the central business districts of Cincinnati. We plan to hold public meetings in August 2022 to provide updates on the project including the plan to deliver the project and scope of work we will be including in the contract.

Please feel free to contact our Project Manager, Stefan Spinosa, ODOT District 8 Capital Programs Administrator at 513-933-6639; Stefan.spinosa@dot.ohio.gov if you wish to discuss the project further.

Respectfully,

A handwritten signature in green ink that reads "Jack Marchbanks".

Jack Marchbanks, Ph.D.
Director



June 17th, 2022

Governor Mike DeWine
Riffe Center
77 S High St, 30th Floor
Columbus, OH 43215

Dear Governor DeWine,

I am thrilled that the Brent Spence Bridge likely moving forward as a result of the Infrastructure Bill and your leadership. ODOT is pursuing a performance-based design-build procurement contract for the Brent Spence Bridge Corridor Project. Many of us have been part of the design and construction of a new home, development, commercial space etc. As you have likely experienced, a thoughtful and engaged owner working alongside a team of designers, engineers, and contractors is the formula for a winning project.

The current solution is rooted in the preliminary configuration developed in 2012-2013. As our community needs have changed, so should the project. Attached a concept that was recently brought forward simply to highlight examples of items we value as a community beyond traffic management and geometry. We should add priorities as follows:

- Job Growth and economic expansion
- Land Recapture (30 acres)
- Establishing community connections to Queensgate (75 acres) much like the redesigned Fort Washington Way reconnected the Banks
- Reducing the environmental impacts, especially sights and sounds from regional through traffic (trucks)
- Aesthetics matter. This is the front door to our city and you only get one chance to make a first impression.

Before us is a once in a 100-year decision about a physical and operational framework that defines the relationship of the region toward its infrastructure. A properly framed and creative delivery approach can enable the region to achieve best value for its future without risking funding or significantly delaying the project.

Governor, I would ask that ODOT provides for progressive design build process heavily utilizing local community leadership. A revised solution will take more work, but it can be done within budget and schedule. As owners, we are ready to be engaged and committed to ensure the best possible outcome. Feel free to reach out for more information at 859-753-2156 or gfischer@fischerhomes.com.

Respectfully,

A handwritten signature in black ink, appearing to read 'G. Fischer'.

Greg Fischer

Cc: ODOT Director, Jack Marchbanks PhD, jack.marchbanks@dot.oh.gov
Mayor Aftab Pureval, mayor.aftab@cincinnati-oh.gov

CORPORATE - CINCINNATI
3940 Olympic Boulevard, Suite 400
Erlanger, KY 41018
office 859.341.4709
fax 859.344.5900



2013 ODOT Proposal



Reconnecting Westway



CORPORATE - CINCINNATI

3940 Olympic Boulevard, Suite 400

Erlanger, KY 41018

office 859.341.4709

fax 859.344.5900



OHIO DEPARTMENT OF TRANSPORTATION
Mike DeWine, Governor

Jack Marchbanks, Ph.D., Director

1980 W. Broad Street, Columbus, OH 43223
614-466-7170
transportation.ohio.gov

June 28, 2022

Greg Fischer
Fischer Homes
3940 Olympic Boulevard, Suite 400
Erlanger, KY 41018

Dear Mr. Fischer,

Thank you for your letter of June 17, 2022, to Governor DeWine and copying me, referencing the Brent Spence Bridge Corridor Project. We, too, are excited with the momentum gained for this project and hope to have financing finalized later this year that will allow ODOT and our partner, the Kentucky Transportation Cabinet (KYTC), to begin construction in 2024.

ODOT and KYTC are planning to deliver most of the project via a design-build procurement, most likely a two-step, value-based procurement. This will include a prescriptive scope of work based on the environmentally approved alternative. We do plan to allow the design-build teams to submit alternative technical concepts as part of the procurement phase. We have utilized this process on numerous contracts managed by ODOT in the past and believe they allow for creativity to achieve the prescribed scope of work and meet goals established by the project sponsors.

We are also evaluating legal requirements, risks, and opportunities associated with the progressive design-build process. We will continue to work with our stakeholders, including the city of Cincinnati, local communities and local businesses, to develop the scope of work for the project to be included regardless of the type of design-build delivery ultimately chosen.

The scope of work will include addressing the primary goals identified to address the transportation needs of the project. As part of this process, we plan to continue to make community goals consistent with residential/commercial growth, as well as aesthetics and reducing overall environmental impacts, a primary focus for this project. In addition, in partnership with the communities in both Ohio and Kentucky, this process will include evaluating connectivity within the central business districts and further footprint minimization.

Please feel free to contact our Project Manager, Stefan Spinosa, ODOT District 8 Capital Programs Administrator, at 513-933-6639; Stefan.spinosa@dot.ohio.gov if you wish to discuss the project further.

Respectfully,

A handwritten signature in green ink that reads "Jack Marchbanks".

Jack Marchbanks, Ph.D.
Director



July 15, 2022

Dr. Jack Marchbanks
Ohio Department of Transportation
1980 West Broad St.
Columbus, OH 43223

Dear Dr. Marchbanks,

CINCINNATI
3777 West Fork Road
Cincinnati, OH 45247
513.661.3100

CINCINNATI | KENWOOD
7755 Montgomery Road
Suite 180
Cincinnati, OH 45236
513.389.2761

CLEVELAND | AKRON
7000 Fitzwater Road
Suite 110
Brecksville, OH 44141
440.877.9990

COLUMBUS
50 West Broad Street
Suite 1130
Columbus, OH 43215
614.365.9103

DAYTON
40 North Main Street
Suite 2110
Dayton, OH 45423
937.461.3790

We understand that ODOT is pursuing a performance-based design-build procurement contract for the Brent Spence Bridge Corridor Project. We are thrilled that this project is now gaining momentum, as a result of the recent Infrastructure bill. However, we also recognize the current solution is rooted in the preliminary configuration developed in 2012-2013.

While we appreciate the reduction in the right-of-way footprint in that configuration, we also understand that the design-build process can accommodate alternative technical concepts to bring additional creativity and achieve better value. Specifically, we believe that improved connectivity, which was admittedly not a priority in 2012, to the west side of I-75 is now more critical to the future of Cincinnati.

We believe a solution that not only reduces the right-of-way footprint, but may include a street and freeway configuration similar to ODOT's celebrated For Washington Way project may be a better option, by enhancing future connectivity to the west, allowing the central business district to expand east of I-75, simplifying high speed on and off ramp movements, and creating a new economic engine for Cincinnati's future. Because as one of Ohio's leading cities, Cincinnati's success is ultimately Ohio's success.

Johnson Investment Counsel, headquartered in Cincinnati with offices throughout the State of Ohio, is committed to the best solution to maximize value for the City of Cincinnati while keeping the Brent Spence Bridge Project moving forward. We are growing and our 155 employees will benefit from the reduced congestion and enhanced regional connectivity.

We request that the designer/contracting community be proactively invited to provide creative alternative concepts responsive to community goals during the procurement process. We have confidence that with clear criteria, this level of innovation can be achieved while breaking ground in late 2023.

We pledge our support to assist ODOT and Cincinnati in shaping the project procurement process to all parties' best interests.

Respectfully,

A handwritten signature in blue ink, appearing to read "J. Jackman", is written over a light blue horizontal line.

Jason Jackman, President, Johnson Investment Counsel, Inc.

Cc: Governor of Ohio Mike DeWine

Mayor Aftab Pureval, mayor.aftab@cincinnati-oh.gov

- WEALTH MANAGEMENT
- FAMILY OFFICE SERVICES
- TRUST COMPANY
- ASSET MANAGEMENT

JOHNSONINV.COM



July 25, 2022

Mr. Jason Jackman
President
Johnson Investment Counsel, Inc.
3777 West Fork Road
Cincinnati, OH 45247

Dear Mr. Jackman,

Thank you for your letter of July 15, 2022, to Director Marchbanks, referencing the Brent Spence Bridge Corridor Project. We, too, are excited with the momentum gained for this project and hope to have financing finalized later this year that will allow ODOT and our partner, the Kentucky Transportation Cabinet (KYTC), to begin construction in 2024.

ODOT and KYTC are planning to deliver most of the project via a design-build procurement, most likely a two-step, value-based procurement. This will include a prescriptive scope of work based on the environmentally approved alternative. We do plan to allow the design-build teams to submit alternative technical concepts as part of the procurement phase. We have utilized this process on numerous contracts managed by ODOT in the past and believe they allow for creativity to achieve the prescribed scope of work and meet goals established by the project sponsors.

The scope of work will include addressing the primary goals identified to address the transportation needs of the project. As part of this process, we plan to continue to make community goals consistent with residential/commercial growth, as well as aesthetics and reducing overall environmental impacts, a primary focus for this project. In addition, in partnership with the communities in both Ohio and Kentucky, this process will include evaluating connectivity within the central business districts and providing further opportunities for developable area.

Please feel free to contact our Project Manager, Stefan Spinosa, ODOT District 8 Capital Programs Administrator, at 513-933-6639; Stefan.spinosa@dot.ohio.gov if you wish to discuss the project further.

Respectfully,

A handwritten signature in black ink that reads "Tammy K. Campbell".

Tammy K. Campbell
District 8 Deputy Director



Messer Construction Co.
643 W Court Street
Cincinnati, OH 45203
(513) 242-1541 Phone
(513) 381-1806 Fax
www.messer.com

June 22, 2022

Governor Mike DeWine
Riffe Center
77 S High St, 30th Floor
Columbus, OH 43215

Dear Governor DeWine,

We understand that ODOT is pursuing a design-build procurement for the Brent Spence Bridge Corridor Project. We are thrilled that this project is now gaining momentum as a result of the recent Infrastructure bill. However, we also recognize the current solution is rooted in the preliminary configuration developed in 2012-2013, and may not be the optimal solution for our community today.

Messer's corporate office is directly adjacent to the northbound ramp from 9th St. to I-75. As a community leader and a neighbor to the project, we believe a more consolidated configuration for the "spaghetti bowl" between downtown Cincinnati and Queensgate would reclaim significant real estate for redevelopment and enhancement for downtown and the adjacent neighborhoods of Queensgate and West End.

While we appreciate the reduction in the right-of-way footprint in the current planned configuration, we also understand that the design-build process can accommodate alternative technical concepts to bring additional creativity and achieve better value. Specifically, we believe that improved connectivity, which was admittedly not a priority in 2012, to the west side of I-75 is now critical to the future of Cincinnati.

We believe a solution that not only reduces the right-of-way footprint, but may include a street and freeway configuration similar to ODOT's celebrated For Washington Way project may be a better option: enhancing future connectivity to the west, allowing the central business district to expand on the east side of I-75, simplifying high speed on and off ramp movements, and creating a new economic engine for Cincinnati's future. As one of Ohio's leading cities, Cincinnati's success is ultimately Ohio's success.

As local business, community, and political leaders, we urge ODOT to include criteria for evaluating proposals that prioritize land reclamation. These criteria would encourage design build teams to bring creative ideas for consolidating the interstate and ramp systems, and offer significant opportunity for economic redevelopment of reclaimed land.

We have confidence that with clear criteria, this level of innovation can be achieved while also maintaining the current schedule of breaking ground in late 2023. While Messer is not an infrastructure contractor, we are committed to assisting ODOT and the City of Cincinnati in any way we can in this process.

Sincerely,

Messer Construction Co.
Tim Steigerwald
President & CEO

Cc: ODOT Director, Jack Marchbanks PhD, jack.marchbanks@dot.state.oh.us
Mayor Aftab Pureval, mayor.aftab@cincinnati-oh.gov



OHIO DEPARTMENT OF TRANSPORTATION
Mike DeWine, Governor Jack Marchbanks, Ph.D., Director

1980 W. Broad Street, Columbus, OH 43223
614-466-7170
transportation.ohio.gov

June 28, 2022

Mr. Tim Steigerwald
President & CEO
Messer Construction Co.
643 W. Court Street
Cincinnati, OH 45203

Dear Mr. Steigerwald,

Thank you for your letter of June 22, 2022, to Governor DeWine and copying me, referencing the Brent Spence Bridge Corridor Project. We, too, are excited with the momentum gained for this project and hope to have financing finalized later this year that will allow ODOT and our partner, the Kentucky Transportation Cabinet (KYTC), to begin construction in 2024.

ODOT and KYTC are planning to deliver most of the project via a design-build procurement, most likely a two-step, value-based procurement. This will include a prescriptive scope of work based on the environmentally approved alternative. We do plan to allow the design-build teams to submit alternative technical concepts as part of the procurement phase. We have utilized this process on numerous contracts managed by ODOT in the past and believe they allow for creativity to achieve the prescribed scope of work and meet goals established by the project sponsors.

The scope of work will include addressing the primary goals identified to address the transportation needs of the project. As part of this process, we plan to continue to make community goals consistent with residential/commercial growth, as well as aesthetics and reducing overall environmental impacts, a primary focus for this project. In addition, in partnership with the communities in both Ohio and Kentucky, this process will include evaluating connectivity within the central business districts and further footprint minimization.

Please feel free to contact our Project Manager, Stefan Spinosa, ODOT District 8 Capital Programs Administrator, at 513-933-6639; Stefan.spinosa@dot.ohio.gov if you wish to discuss the project further.

Respectfully,

A handwritten signature in green ink that reads "Jack Marchbanks".

Jack Marchbanks, Ph.D.
Director

Appendix K:
Bridge Forward Coalition



Brent Spence Bridge Corridor Project Position Paper

Redesign of the Brent Spence Bridge Project

A working position paper

December 14, 2021

Prepared by: Ryan Laber, P.E., in collaboration with the Bridge-Forward Cincinnati Coalition

This paper advocates that the Brent Spence Bridge replacement project, as it currently exists, be redesigned in way that would benefit the Cincinnati region both economically and socially. This paper both describes the specific goals of the proposed redesign and provides strong evidence that a redesign is achievable within reasonable technical, financial, and temporal constraints. This paper was developed in collaboration with Bridge-Forward Cincinnati, a grassroots and 501(c)(3) organization advocating for a specific redesign of the Brent Spence Bridge project.

Executive Summary

For over a decade, the Brent Spence Bridge Replacement/Rehabilitation Project (BSB Project) has languished as an unfunded, top, national infrastructure priority. With the recent passage of the bipartisan Infrastructure Investment and Jobs Act however, it seems likely that federal funding for the project may soon be secured. Unfortunately, as it is currently designed, the BSB Project misses a major opportunity to deliver significant economic development benefits to the region, beyond the project's core scope of reducing highway congestion.

Since replacement options for the bridge were initially explored in 2003, Cincinnati and Covington have experienced an explosion of investment in their urban cores, with Cincinnati's urban core, specifically, benefiting from \$1.3 billion of investment from 2003 to 2018.¹ Cincinnati's Over-the-Rhine neighborhood has been central in this reinvestment story, and the collective pride and energy that its renaissance has created in the region has been tremendous.

For the economic health of the entire region, fostering continued urban-core growth and development must be a top priority. One barrier to the region's long-term, urban-core growth, however, is the fact that the City of Cincinnati's urban core is landlocked. Due to its historic designation, Over-the-Rhine can only support a limited amount of growth, and Cincinnati's CBD is constrained by the topography, the Ohio River, and I-75. Also, in Covington, antiquated roadway infrastructure to and from key interstate junctions is not encouraging the highest and best possible use of urban land.

The Bridge-Forward Cincinnati Coalition, therefore, proposes that the current plans for the BSB Project be redesigned and/or realigned in a way that would bring significant economic development to the cities of Cincinnati and Covington, and to the entire Greater Cincinnati region. Specifically, Bridge-Forward Cincinnati proposes that the current plans for the BSB Project be redesigned in a way that would:

- 1) Expand the landlocked urban core of Cincinnati
- 2) Improve connectivity between Covington and Cincinnati, and upgrade local streets, especially in Covington
- 3) Elevate the highest and best use for land in both Cincinnati and Covington
- 4) Attract talent to the region by providing for the development of dynamic, diverse, live-work-play neighborhoods
- 5) Maximize the BSB Project's return on investment

If the BSB Project were to be redesigned in a way that optimizes highway location and minimizes the footprint of highway access ramps, the resulting configuration could return more than one hundred developable acres of land – almost entirely free of existing landowners and historic buildings – to the landlocked CBD and West End neighborhoods of Cincinnati. In this area, after removal of the existing highway infrastructure, local leadership could pursue an ambitious development agenda centered around economic development and modern, equitable housing. On the south side of the river, local road network upgrades could transform the development potential of Covington's CBD too. The resulting, expanded urban core of Greater Cincinnati would significantly boost the region's economic strength, attractiveness, and social and civil wellbeing.

This paper offers specific design strategies for the BSB Project, as well as three alternative design concepts that represent how the goals established above might be realized. These three alternative concepts are shown in Figures 1, 2, and 3 on the next page.

¹ <https://www.bizjournals.com/cincinnati/news/2018/05/18/3cdc-shifting-its-mission-in-downtown-cincinnati.html>

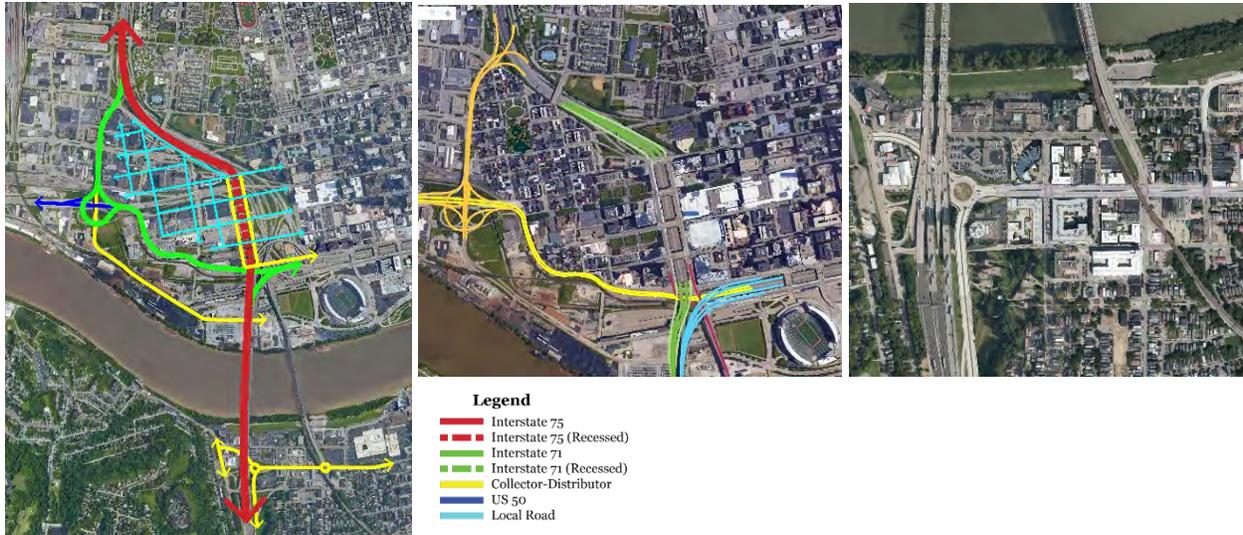


Figure 1: Bridge-Forward Cincinnati's original concept, designed by Brian Boland

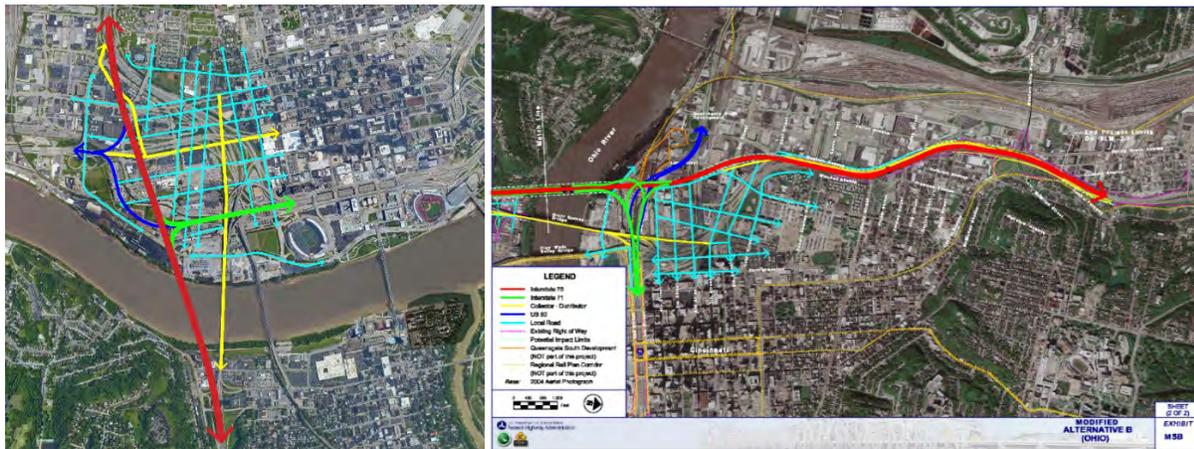


Figure 2: the *Modified Alternative B* concept, based on the *Alternative B* concept from the 2009 Conceptual Alternatives Study.



Figure 3: the *Modified Concept 85* design, based on *Concept 85* (also known as *Innovation 85*) from the 2012 HNTB Practical Design / Value Engineering Workshop (PD/VE Workshop)

Each of these three alternative design concepts are discussed in more detail in the *Alternative redesign concepts* section of this paper. Here, it must be noted that all three alternative concepts significantly expand the developable, contiguous urban core of Cincinnati, as shown in Figure 4, below.

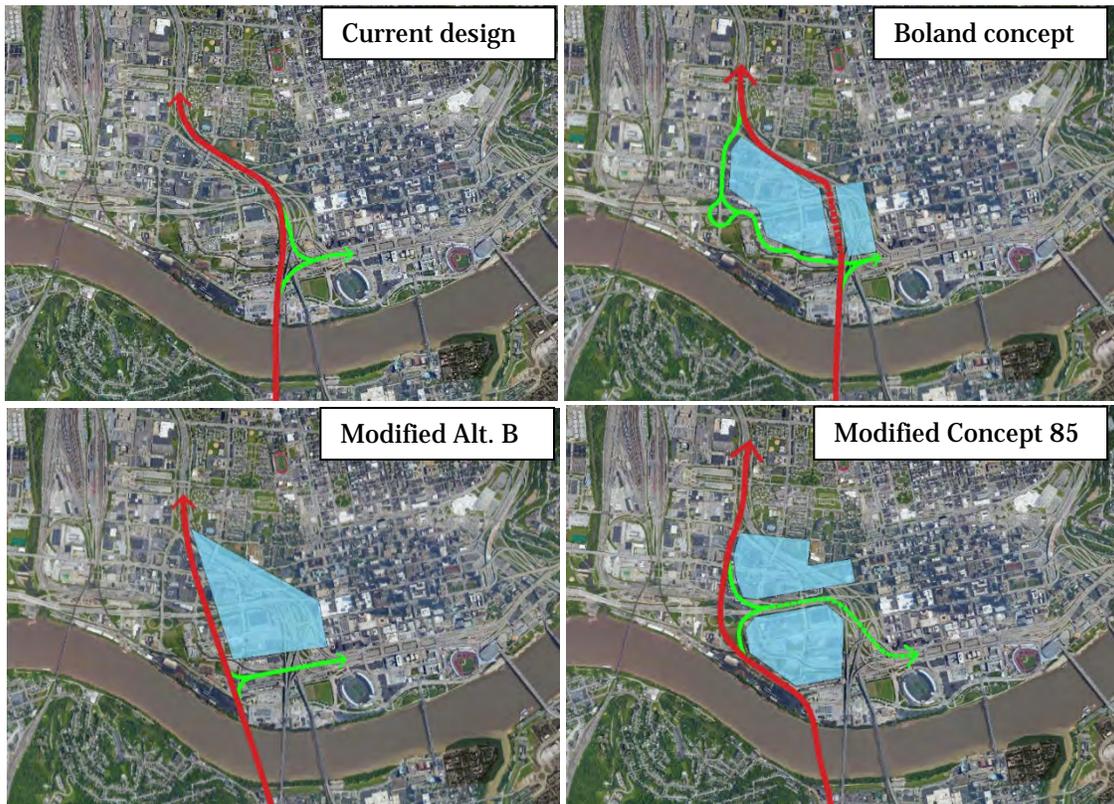


Figure 4: From left to right, top to bottom: Current preferred alternative design; Bridge-Forward’s original concept, designed by Brian Boland; Modified Alternative B concept; Modified Concept 85. Blue polygons represent the new areas that could be made available for modern, urban development.

To provide context to the tremendous opportunity presented by a BSB Project redesign, Figure 5 shows that the new area created by the Modified Alternative B concept, for instance, would be roughly equal in size to the area of Over-the-Rhine bound by Liberty Street, Central Parkway, and Sycamore Street. Both areas shown are approximately 135 acres. All alternative concepts presented are estimated to provide between 130 and 135 acres of newly developable land.



Figure 5: Scale of potential urban core expansion.

In addition to the long-term benefits of an expanded urban core, the Modified Alternative B and Modified Concept 85 designs would also benefit the region during the construction phase of the project. Because the Modified Alternative B and Modified Concept 85 both shift the alignment of I-75 to the west, in these two scenarios, the existing BSB infrastructure could remain in service for much longer during the project's years-long construction process. This would significantly reduce interstate closures and congestion during construction, and this may even reduce the cost of the project overall.², page B6

Fortunately, there currently exists a unique set of circumstances that, when considered together, presents a window of opportunity for the realization of a BSB Project redesign. First, the Mayor of Covington and the entire Covington Board of Commissioners are opposed to the project as it is currently envisioned because of what they call “fatal flaws” in the design. These flaws include the proposed bridge’s extremely wide footprint in Covington, the fact that access to Covington will not be improved by the project, and the negative impacts of the construction process on Covington businesses, mostly due to years of expected traffic congestion on local streets.³ Therefore, some pressure to reassess the bridge already exists, and common cause can be found between Covington leadership and redesign advocates at large.

Second, the BSB Project still lacks sufficient funding to substantially move forward. Therefore, time is still available to achieve a redesign. ODOT must apply for funding through grant programs established in the bipartisan, federal Infrastructure Investment and Jobs Act.⁴ Through these grant programs, the federal government can be expected to contribute around 80% of the total cost of the BSB Project. Therefore, it can be expected that each state will need to contribute about 10% of the total project cost. At a total cost of \$3.5 billion, the contribution that Kentucky will need to make is about \$350 million—no small amount.⁵ In fact, the annual appropriation made by the Commonwealth of Kentucky to its state highway fund – the State Supported Construction Program – is about \$350 million.⁶ Securing funding from Kentucky is likely to be a limiting factor in terms of moving the BSB Project forward.

Third, because the FONSI for the BSB Project was secured in 2012 – nearly a decade ago – the project’s Environmental Assessment (EA) will likely need to be re-evaluated before the project can substantially move forward. According to NEPA rules, re-evaluations are needed if there are “significant new circumstances or information relevant to environmental concerns” or relevant to the subject project more broadly.⁷ Therefore, if local leadership and the local community were to support the concept of a redesign, the re-evaluation process would need to react to this change in circumstances. Fortunately, requesting a substantial re-evaluation of the EA, or potentially requesting a new Environmental Impact Statement (EIS) and Record of Decision (ROD) will not condemn the project to years in regulatory limbo. The bipartisan Infrastructure Investment and Jobs Act codifies an executive order first issued by President Trump called *One Federal Decision* (OFD), which instructs all federal agencies to provide environmental approval/denial decisions within two years to all major projects requiring NEPA clearance.⁸ This is a

² <https://tinyurl.com/2012HNTBPDVEWS>

³ <https://www.cincinnati.com/story/opinion/2021/03/22/opinion-brent-spence-expansion-plan-contains-fatal-flaws/4699000001/>

⁴ <https://www.cincinnati.com/story/news/politics/2021/11/09/infrastructure-bill-brent-spence-bridge-passenger-rail-streetcar/6342314001/>

⁵ <https://www.bizjournals.com/cincinnati/news/2021/08/11/game-changer-infrastructure-bill-could-pay-for-hal.html>

⁶ [https://transportation.ky.gov/BudgetFiscalManagement/Documents/General%20Assembly%20House%20Bill%20193%20\(2021\).pdf](https://transportation.ky.gov/BudgetFiscalManagement/Documents/General%20Assembly%20House%20Bill%20193%20(2021).pdf)

⁷ <https://www.law.cornell.edu/cfr/text/40/1502.9>

⁸ <https://www.congress.gov/bill/117th-congress/house-bill/3684/text>

major improvement to the status quo in regards to permitting timelines, and this policy development makes a BSB Project redesign all the more attainable within a reasonable timeframe.⁹

Finally, the bipartisan Infrastructure Investment and Jobs Act provides an allocation of \$1 billion to a new program called the Reconnecting Communities Pilot Program. In a press release, the White House explained the motivation for the program: “Too often, past transportation investments divided communities ... or left out the people most in need of affordable transportation options. In particular, significant portions of the interstate highway system were built through Black neighborhoods.”¹⁰ The Reconnecting Communities Pilot Program will provide competitive planning grants of up to \$2 million aimed at restoring community connectivity.⁸ Grants can cover feasibility studies, traffic analyses, alternative roadway design, economic impact studies, environmental impact studies, and public engagement. The existing BSB and associated access ramps in Cincinnati certainly form an infrastructure barrier between the CBD and Queensgate. And, when it was constructed, the BSB Corridor and the City of Cincinnati’s accompanying Queensgate urban renewal project forced 25,737 people – the vast majority of whom were Black – from their homes.¹¹ This pilot program is certainly intended for contexts like I-75 in Cincinnati. And, regarding the competitive nature of the Reconnecting Communities grant program, the BSB Project is in an enviable position. Since the BSB Project is likely to proceed with regular surface transportation funding anyway, if a \$2 million Reconnecting Communities grant were leveraged in a redesign effort to increase community connectivity, the BSB Project, because of its huge size, could potentially be one of the highest-profile successes of the pilot program in the country.

The fact that the BSB Project will likely soon secure federal funding is positive and exciting for the entire region. However, as it is currently designed, the BSB Project essentially replicates the original bridge’s alignment, which was designed in the 1950s and implemented in the 1960s. Unfortunately, this alignment prioritizes highway infrastructure over neighborhoods, residents, and economic development. With a total investment cost of nearly \$3.5 billion, and with built infrastructure that will remain in service for at least 50 years, the BSB replacement should reflect today’s priorities, not the priorities of the 1950s and 1960s. If presented with the choice, Greater Cincinnatians would enthusiastically support right-sized highway infrastructure and an expanded, thriving, and dynamic urban core. Bold and visionary leadership is needed to ensure that the design of the BSB Project – one of the most consequential infrastructure projects in the country – both reflects the values of the community it serves and delivers the economic development benefits to the community that such infrastructure is intended to support.

⁹ <https://thehill.com/opinion/finance/574370-the-most-important-part-of-the-infrastructure-bill-is-little-noticed>

¹⁰ <https://www.whitehouse.gov/briefing-room/statements-releases/2021/08/02/updated-fact-sheet-bipartisan-infrastructure-investment-and-jobs-act/>

¹¹ <https://www.cincinnati.com/citywiseblog/lost-city-kenyon-barr-queensgate/>

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History of the Lower West End

The construction of the Brent Spence Bridge corridor and the accompanying Queensgate industrial area in the early 1960's eliminated a dense urban neighborhood, systematically forced thousands of people from their homes, and destroyed the "cultural epicenter of Cincinnati's Black community."¹² The City of Cincinnati's "Kenyon Barr" urban redevelopment scheme, which later become known as "Queensgate I," aimed at creating an industrial district anchored by the new highway, the river, and the railroads. At the outset of the project, residents of the lower West End neighborhood received letters from the City stating that their buildings had been purchased by the City, and that they would need to find somewhere else to live. In all, 25,737 residents – 25,155 of whom were non-white – were displaced, and "137 food stores, 118 bars and restaurants, 86 barber shops and beauty parlors, 80 churches and missions, 24 dry cleaners, and 6 funeral homes" were eliminated.¹¹ This forced relocation eliminated the wealth of thousands of families, caused an immeasurable psychological toll, and destroyed a beautiful neighborhood and the community that called it home.¹³ Local historian Anne Delano Steinert, who in 2017 launched an exhibit called "Finding Kenyon Barr: Exploring Photography of Cincinnati's Lost West End" reported that, during interviews for the project, some former Kenyon Barr residents were, to this day, moved to tears when reflecting on the destruction of their former neighborhood.¹⁴



Figure 6: Photos of Cincinnati's West End, before "urban renewal" (top) and today (bottom) (Taken from https://www.uc.edu/cdc/urban_database/historical.html#maps and Google Earth, respectively)

¹² <https://www.citybeat.com/news/article/20981774/echoes-of-a-lost-west-end>

¹³ <https://www.cincinnati.com/citywiseblog/finding-kenyon-barr-documents-wholesale-demolition-lower-west-end/>

¹⁴ <https://www.graypape.com/project/>

History of the BSB Project's design development

The currently proposed design of the BSB Project was developed over approximately seven years, from 2006 through 2012. Generally, the development of the design followed ODOT's five-phase Project Development Process:

- 1) Planning
- 2) Preliminary Engineering
- 3) Environmental Engineering
- 4) Final Engineering / ROW
- 5) Construction ¹⁵

In the Planning phase, in May of 2006, a Purpose and Needs Statement was completed, identifying the project goals as follows:

- improve traffic flow and level of service (LOS),
- improve safety,
- correct geometric deficiencies, and
- maintain links in key mobility, trade, and national defense transportation corridors.^{16, page 31}

Notably absent is any sort of goal pertaining to the impact of the highway infrastructure itself on the communities through which it passes.

In September of 2006, a Planning Study Report was completed, which considered 25 alternatives for the BSB Project.^{17, page 48} Of these 25 alternatives, eight alternatives – Alternatives 1, 2, 6, 7, 8, 9, 10, and 15 – included a new bridge in a “Queensgate alignment,” with I-75 essentially traveling in a straight line between its current location at West 5th Street in Covington and its current location at Ezzard Charles Drive in Cincinnati. No Alternative considered a recessed highway with a configuration like that of Fort Washington Way.

The Planning Study Report concluded by dismissing 19 of the 25 alternatives considered, since they were not perceived to have met the project goals stated above.^{17, page 51} Six alternatives were advanced for further study, two of which – Alternatives 1 and 2 – included a new bridge in a “Queensgate alignment.” A few of the subgoals in which the eliminated “Queensgate alignment” alternatives scored poorly included: “congestion mitigation, separation of regional & local traffic, community, environmental justice, construction cost, and constructability.”¹⁸ Any changes to existing land uses caused by potential BSB Project designs were automatically considered to be negative. The full Alternatives Comparison Matrix accompanying the Planning Study Report should be studied to understand the scoring of each Alternative.¹⁹

In April of 2009, a Conceptual Alternatives Study was completed, in which “Queensgate alignment” Alternatives 1 and 2 from the Planning Study Report were reclassified as Alternative A and Alternative B respectively. Alternative A was eliminated due to an impact on the Longworth Hall building, a registered historic building.^{20, page 28} Alternative B, shown in Figure 7 – the only alternative with a bridge in a “Queensgate alignment” given serious consideration – was eliminated due to “adverse impacts to

¹⁵ <https://www.transportation.ohio.gov/wps/portal/gov/odot/working/pdp/pdp-phases-paths>

¹⁶ <https://www.brentspencebridgecorridor.com/uploads/pdfs/P&N%20Report%20May%202006.pdf>

¹⁷ [https://www.brentspencebridgecorridor.com/uploads/pdfs/Planning Study Report September 2006.pdf](https://www.brentspencebridgecorridor.com/uploads/pdfs/Planning%20Study%20Report%20September%202006.pdf)

¹⁸ [https://www.brentspencebridgecorridor.com/uploads/pdfs/Evaluation Criteria MatrixFeb2006.pdf](https://www.brentspencebridgecorridor.com/uploads/pdfs/Evaluation%20Criteria%20Matrix%20Feb%202006.pdf)

¹⁹ [https://www.brentspencebridgecorridor.com/uploads/pdfs/Criteria Matrix Description 17Feb2006.pdf](https://www.brentspencebridgecorridor.com/uploads/pdfs/Criteria%20Matrix%20Description%2017Feb%202006.pdf)

²⁰ [https://www.brentspencebridgecorridor.com/uploads/pdfs/BSBC ConceptualAlternativeStudy.pdf](https://www.brentspencebridgecorridor.com/uploads/pdfs/BSBC%20ConceptualAlternativeStudy.pdf)

communities, residences, businesses, hazardous material sites, and utilities” as well as “overall complexity, constructability risk, and cost.”²⁰, page 163 Indeed, Alternative B would have had impacts on areas of West Covington and Queensgate, even prompting the City of Covington to issue a letter of opposition to Alternative B on October 8, 2008, and prompting the City of Cincinnati to issue a report in entitled “Queensgate Area Issues, Considerations and Recommendations for Implementation of the Brent Spence Bridge Project” in September, 2008. However, as the names suggest, Alternative B in the Conceptual Alternatives Study has significant differences from the *Modified Alternative B* proposed in this paper. Alternative B included the maintenance of the existing Brent Spence Bridge and nearly all existing ramps on the Ohio side of the bridge. Alternative B also included the addition of an entirely new highway bridge and ramp network—effectively doubling the land area in Queensgate required for the highway system. The Modified Alternative B proposed in this paper reduces the total land area dedicated to highway infrastructure. The Modified Alternative B design is discussed more in the *Alternative redesign concepts* section of this paper.

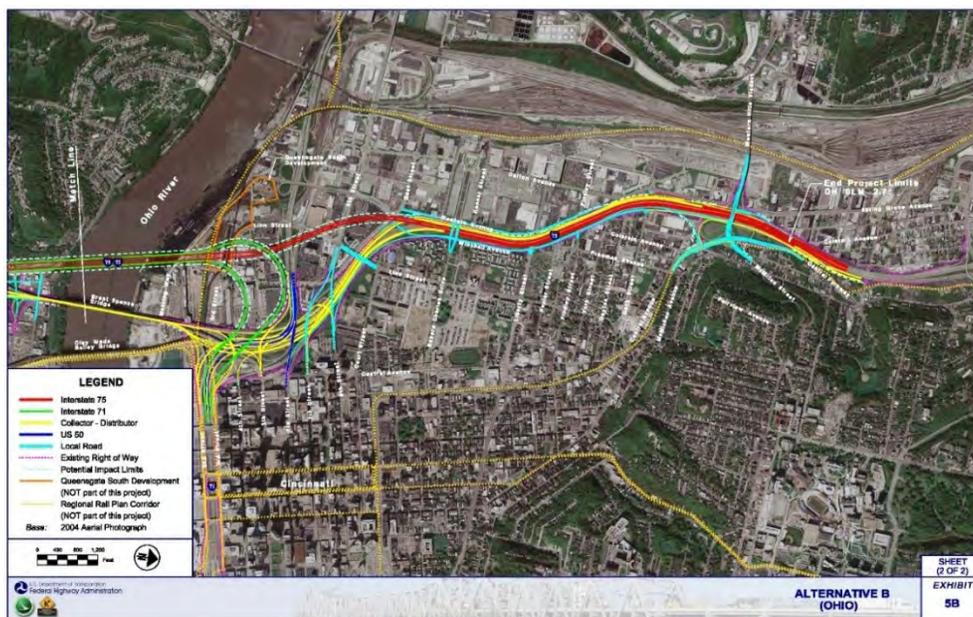


Figure 7: Alternative B from the 2009 Conceptual Alternatives Study

From the Conceptual Alternatives Study completed in 2009, Alternatives C, D, and E were advanced. Alternatives C and D were subsequently combined to form Alternative I, which was then compared against Alternative E. In May of 2011, Alternative I was selected as the preferred alternative, and a Preferred Alternative Verification Report (PAVR) was completed.²¹ Alternative I is shown in Figure 8 on the next page.

Alternative I, as much as possible, replicates the alignment of the existing Brent Spence Bridge. In Alternative I, the existing Brent Spence Bridge is maintained, and adjacent to the west, a new, double-decker, six-lane bridge is to be constructed. Of the 16 total interstate lanes crossing the Ohio River proposed in Alternative I, six are dedicated exclusively to local traffic. According to project documents, these six lanes serve local roads between and including West 9th Street in Covington and West 9th Street in Cincinnati. Therefore, Alternative I is designed to accommodate a very large number of local trips all having origins or destinations within the urban cores of Cincinnati and Covington. Figure 9, also on the next page, illustrates the details of Alternative I’s proposed lanes and their functions.

²¹ <https://www.brentspencebridgecorridor.com/uploads/pdfs/PAVR - May 2011.pdf>

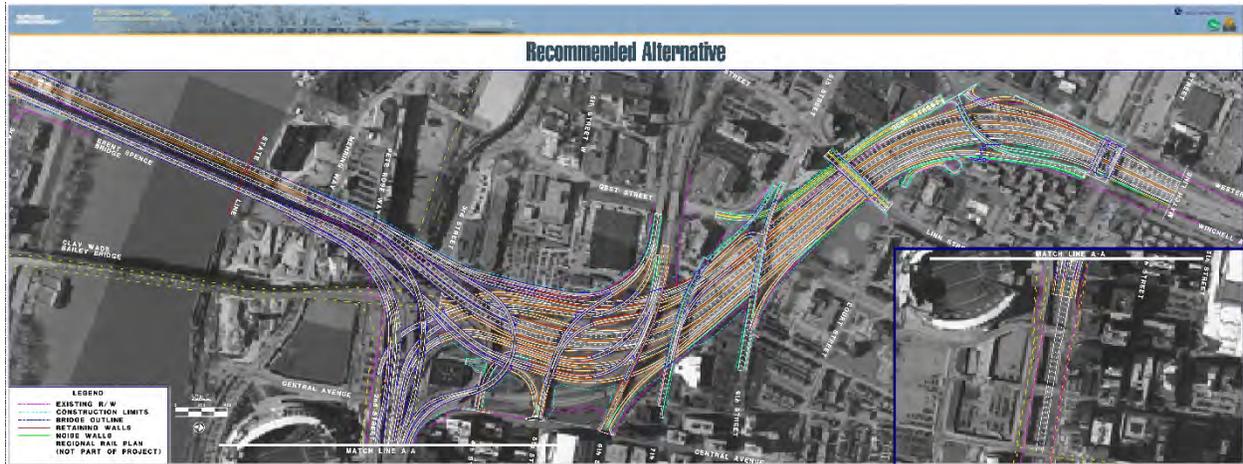


Figure 8: Alternative I – the Preferred Alternative – from the 2011 Preferred Alternative Verification Report (PAVR)

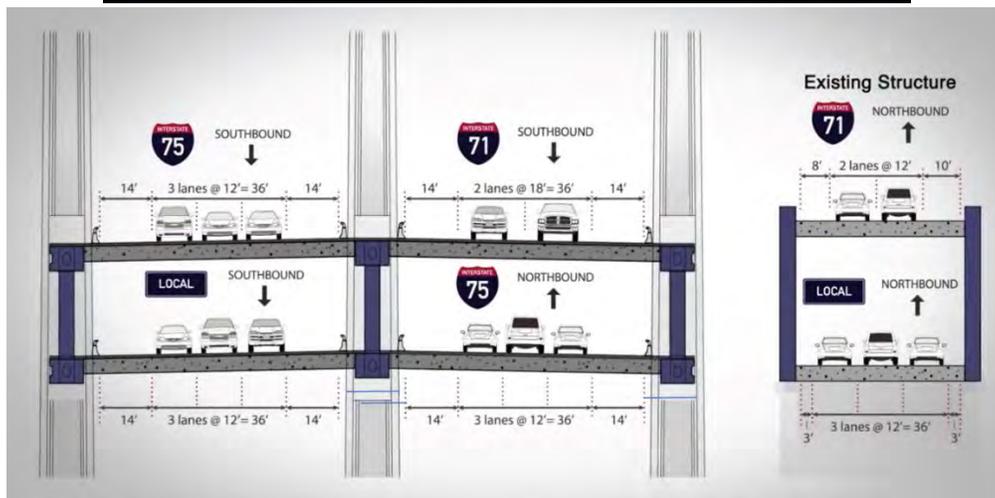


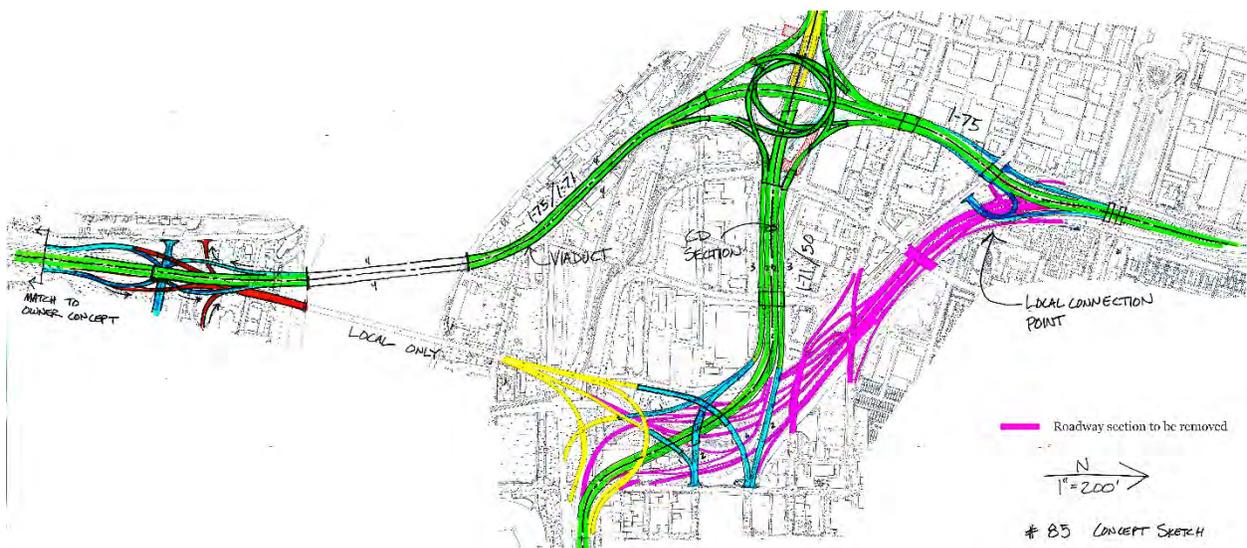
Figure 9: Alternative I design details

Since Alternative I was selected, it has advanced through the Environmental Engineering phase of ODOT's PDP and has won nearly all required approvals for implementation. In accordance with the National Environmental Policy Act (NEPA) at the time, an Environmental Assessment (EA) was completed in March of 2012. Additionally, the FHWA issued a Finding of No Significant Impact (FONSI) for Alternative I in August, 2012.²²

Despite its overcoming these regulatory hurdles, shortly after Alternative I was selected as the preferred alternative, it was met with skepticism. In October, 2012, the engineering firm HNTB hosted a three-day "Practical Design / Value Engineering Workshop" (PD/VE Workshop) for the BSB Project. The purpose of this PD/VE Workshop was to generate improvements and alternatives to Alternative I, in order to increase the project's overall expected technical performance, decrease cost, and minimize the risks posed by construction. Accordingly, the PD/VE Workshop took a "blank slate" approach toward identifying design goals and alternatives. A few of the design strategies that the PD/VE Workshop recommended include:

- Reduce connectivity at the "spaghetti bowl" area of access ramps on the Ohio side of the river
- Keep local traffic on the existing bridge and thru traffic on the new bridge
- Take advantage of highway design criteria reductions for local traffic on the existing bridge, since regional traffic can be separated and routed through the new bridge
- Separate local traffic and regional traffic to provide opportunity for dynamic tolling of regional traffic only²

One of the most innovative design alternatives to emerge from the PD/VE Workshop was *Concept 85* (also called *Innovation 85*) which "shifted I-75 to the west along the Freemont Avenue corridor ... [via a new, skewed,] single-level bridge west of the existing BSB."² Sketches of Concept 85 are shown in Figure 10 on this page and the next.



²² <https://www.brentspencebridgecorridor.com/uploads/pdfs/FONSI.pdf>

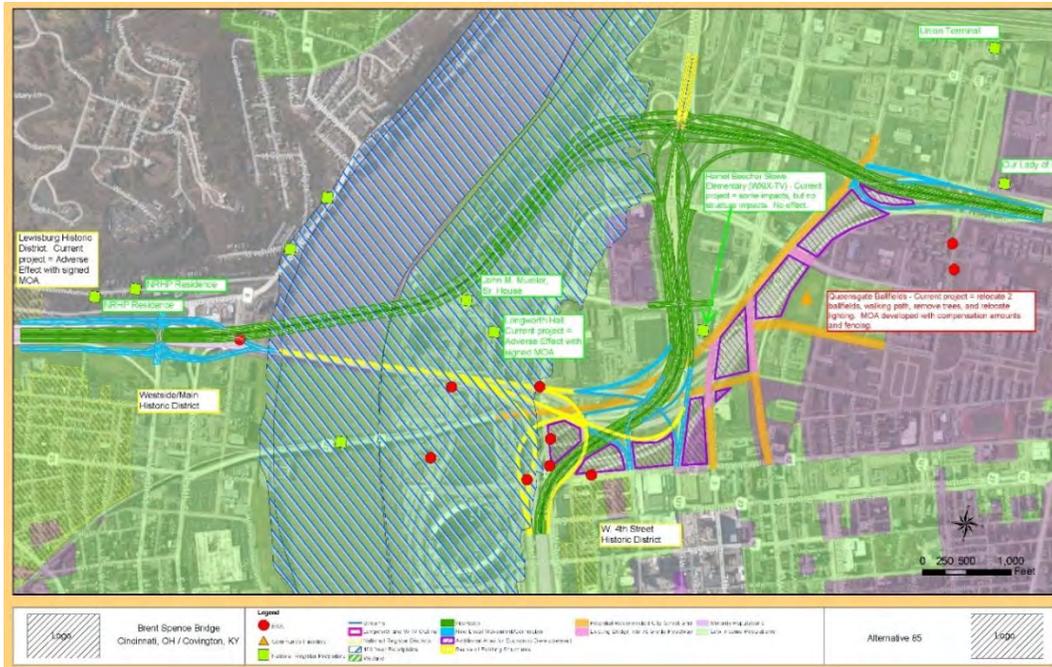


Figure 10: Concept 85 (taken from the October, 2012, HNTB PD/VE Workshop documents)

Concept 85, with one small modification, is presented in the *Alternative redesign concepts* section of this paper as *Modified Concept 85*. Benefits of both the original Concept 85 and the Modified Concept 85, including improvements in safety, cost, and construction risk relative to Alternative I, are detailed in that section as well. Ultimately, Concept 85 was not advanced further, likely because the HNTB team did not feel they had the license to pursue a project requiring relatively more right-of-way acquisition.

Since the HNTB PD/VE Workshop in 2012, a “blank slate” approach to the project has not been taken. Modest modifications to Alternative I have been developed and studied, specifically in 2015 with the development of the *Whiz Bang* concept and in 2019 during the *Performance-Based Design Workshop*. The two modifications most preferred by project engineers – Concept M and Concept W – are still being developed by HNTB today.^{23, 24, 25} However, Concepts M and W are simply modest modifications to the existing preferred alternative – Alternative I – and do not significantly address any of the additional goals or evaluation criteria being promoted by this paper and the Bridge-Forward Cincinnati Coalition.²⁶

Presently, ODOT and KYTC are continuing to advance the existing preferred alternative. Indeed, on April 29, 2021, ODOT and KYTC amended an existing Bi-State Agreement with a Fifth Supplemental Agreement (No. 20541-E) which, among other things, established the goal of preparing the BSB Project for funding opportunities, i.e., grant applications.²⁷ More information on current project status is discussed in the *Current project status and the window of opportunity for project realignment* section of this paper.

²³<https://www.brentspencebridgecorridor.com/uploads/pdfs/Whiz%20Bang%20Concepts%202%20and%204%20Prel%20Traffic%20Anal%20Memo.pdf>

²⁴<https://www.brentspencebridgecorridor.com/uploads/pdfs/Performance-Based%20Design%20Workshop%20Minutes.pdf>

²⁵<https://www.brentspencebridgecorridor.com/uploads/pdfs/Analysis%20of%20Design%20Concepts.pdf>

²⁶<https://www.brentspencebridgecorridor.com/uploads/pdfs/BSB%20Traffic%20and%20Concept%20Analysis.pdf>

²⁷https://www.brentspencebridgecorridor.com/uploads/pdfs/2021-11-4%20Brent_Spence_Summary%20Report.pdf

Issues with the current design

On April 8th, 2021, the entire five-member Covington Board of Commissioners, including Covington Mayor Joe Meyer, penned an op-ed in the Cincinnati Enquirer opposing the BSB Project as it is currently proposed.³ As Mayor Meyer put it recently, “If you gave them all the money in the world, [the BSB Project] would not be ready to start construction tomorrow.”²⁸ Mayor Meyer and the Commissioners’ three chief concerns are 1) the planned size of the highway infrastructure in Covington – the width of the bridge is planned to expand from 42 feet to 128 feet of pavement on the south side of the river – 2) the fact that “Covington as a destination is treated as an afterthought,” and 3) the negative impacts of the construction process on Covington businesses, mostly due to years of expected traffic congestion on local streets.³

At a townhall meeting on March 31st, Mayor Meyer circulated a list of “Covington’s minimum requirements” for the BSB Project, which included the following:

- “Reduce the size of the proposed BSB,
- Mitigate the impact of [traffic] diversion [during construction by] providing resources to upgrade Covington’s infrastructure, [and]
- Protect and compensate Covington’s businesses that will be negatively impacted...”

And, in an acknowledgement of the severity of Covington’s concerns, when asked about these concerns this summer, Kentucky Governor Andy Beshear welcomed the federal government’s help in finding a potential compromise design.²⁹

Bridge-Forward Cincinnati believes that the issues raised by Mayor Meyer and the Covington Commissioners are serious and legitimate. In many ways, it seems that Covington is trying to avoid the construction of an infrastructure barrier like the one currently formed by I-75 in downtown Cincinnati. Indeed, because the current preferred alternative for the BSB Project essentially replicates the existing, approximately 60-acre highway footprint in Cincinnati known as the “spaghetti bowl,” major issues with the preferred alternative exist on the Ohio side of the river too:

- I-75 and associated ramps and interchanges will form a physical and visual barrier between Cincinnati CBD and Queensgate.
- There will be inadequate vehicle, bicycle, and pedestrian connections between the Cincinnati CBD and Queensgate.
- I-75’s design and location will constrain the ability of Cincinnati’s urban core to further develop and expand.
- Too much land is devoted to transportation in proportion to available land.

Figure 11 on the next page shows the magnitude of the proposed interstate footprint, all within the region’s valuable, limited, urban core.

²⁸ <https://www.cincinnati.com/story/news/2021/08/05/brent-spence-bridge-infrastructure-bill-could-pave-way-project/5469479001/>

²⁹ <https://www.bizjournals.com/cincinnati/news/2021/07/21/beshear-on-brent-spence.html>

Finally, and perhaps most importantly, the current preferred alternative misses the opportunity to fundamentally improve the urban fabric of Covington and Cincinnati by leveraging this once-in-a-generation infrastructure investment. The narrow set of goals listed below, which was established for the project in the 2006 Purpose and Needs Statement, is insufficient to steer a design that meets the region's needs.

- Improve traffic flow and level of service,
- Improve safety,
- Correct geometric deficiencies, and
- Maintain links in key mobility, trade, and national defense transportation corridors.^{16, page 31}

Notably absent from this list is any sort of goal pertaining to the impact of the highway infrastructure itself on the communities through which it passes. As a result, alternative design options for the project were developed, evaluated, and refined, all with a paramount focus being given to the traffic goals defined above. Any changes to existing land uses caused by potential BSB Project designs were automatically considered to be negative. Eventually, a final, preferred alternative – the current BSB Project design – was selected. KYTC, ODOT, and consultants found an effective solution to the traffic congestion and safety issues raised in the Purpose and Needs Statement, but they were not asked to find – and therefore did not arrive at – a solution that leverages this massive investment to advance the additional needs of the region.

The Bridge-Forward Cincinnati Coalition argues that the goals of the BSB project should be expanded. As John Norquist, former Mayor of Milwaukee, once pointed out, “moving vehicular traffic is obviously a necessary function [of roadways], but by making it the only goal, cities lose out on the economic potential created by the crowds of people that bring life to a city.”³⁰ The goals presented in the next section of this paper showcase the opportunities available to the region in a redesigned BSB Project.



Figure 11: A rendering of the current design for the BSB Project (Provided by ODOT)

³⁰ <https://www.strongtowns.org/journal/2018/6/25/2-reasons-why-the-american-approach-to-congestion-is-totally-wrong>

Goals for a redesign

This paper and the Bridge-Forward Cincinnati Coalition propose that the current plans for the BSB Project be redesigned and/or realigned in a way that would bring significant economic development to the cities of Cincinnati and Covington and to the entire Greater Cincinnati region. Specifically, Bridge-Forward Cincinnati proposes that the current plans for the BSB Project be redesigned in a way that would:

- 1) Expand the landlocked urban core of Cincinnati
- 2) Improve connectivity between Covington and Cincinnati, and upgrade local streets, particularly in Covington
- 3) Elevate the highest and best use for land in both Cincinnati and Covington
- 4) Attract talent to the region by providing for the development of dynamic, diverse, live-work-play neighborhoods
- 5) Maximize the BSB Project's return on investment

The motivations for each of these five goals are discussed in detail below:

1. Expand the landlocked urban core of Cincinnati

The Ohio-Kentucky-Indiana Regional Council of Governments (OKI) anticipates that “between 2015 and 2050, the combined population of [the region’s] eight counties is expected to grow ... 11 percent from 2 million to 2.3 million.”³¹ OKI also notes that “Hamilton County has been one of the fastest growing counties in the region, as people have returned to the urban core. This trend is expected to continue through 2040 and, possibly, beyond.”

Indeed, over the next 50 years – a time period that could be considered the minimum design life of a new BSB – growth and change will occur in the region at large scale. As shown in Figure 12, below, by looking back 50 years one can imagine the magnitude of growth and development that could occur in the region over the next 50 years.



Figure 12: The I-75/I-71 junction with KY-18, in 1964 (left) and today (right)
(Taken from <https://facesandplaces.kentonlibrary.org/viewimage.php?i=di128139> and Google Earth, respectively)

Unfortunately, right now the Cincinnati region has a supply shortage of urban real estate and urban developable land for those businesses, families, and individuals who desire it. Due to its historic nature, Over-the-Rhine can only support a limited amount of population growth, and Cincinnati's CBD – the region's urban center – is constrained by the river, topography, and I-75. Rather than letting its urban growth be constrained, Greater Cincinnati should prioritize the expansion of its urban core, by prioritizing a BSB redesign. Although other regional centers like Oakley and Montgomery are experiencing high levels

³¹ <https://2050.oki.org/population/>

of relatively dense development, in an 18-hour city like Cincinnati, there is no substitute for the urban offerings that downtown-adjacent neighborhoods can provide. Certainly, over the life of the new bridge, the region will want to accommodate those businesses and people who desire an urban setting, lest they chose to locate in a different region.

The potential alternative concepts for a redesigned BSB Project, presented as examples in this paper, therefore, all significantly expand the urban core, as shown in Figure 4, redisplayed below.

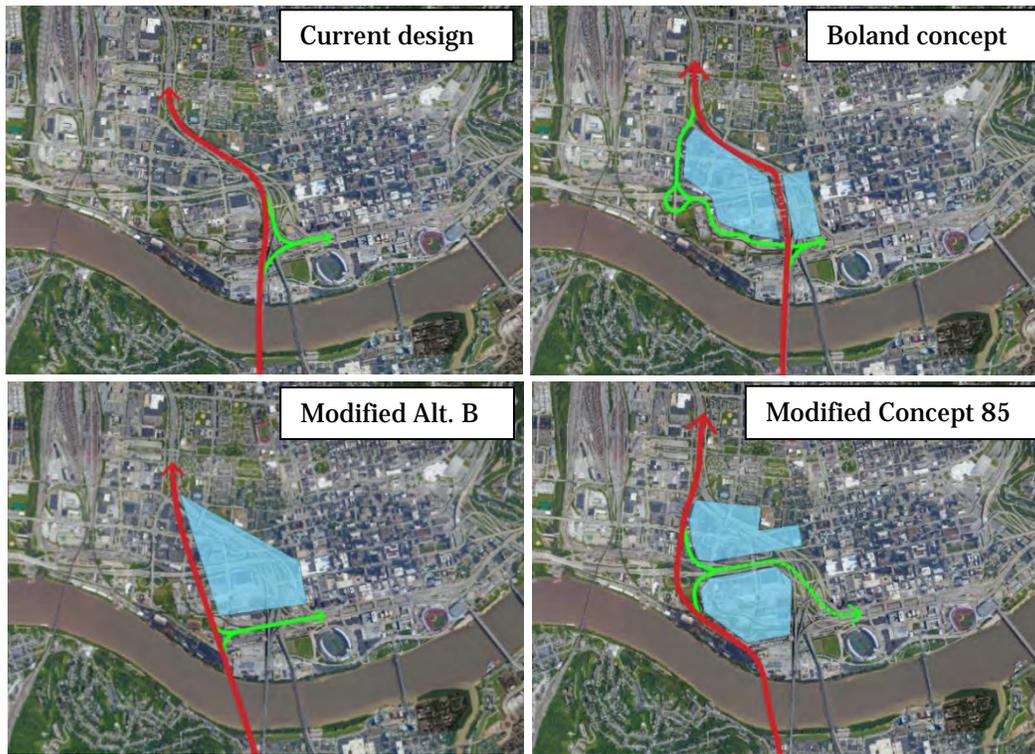


Figure 4 (redisplayed): From left to right, top to bottom: Current preferred alternative design; Bridge-Forward original concept, designed by Brian Boland; Modified Alternative B concept; Modified Concept 85. Blue polygons represent the new areas that could be made available for modern, urban development.

To provide context to the tremendous opportunity presented by a BSB Project redesign, Figure 5 shows that the new area created by the Modified Alternative B concept, for instance, would be roughly equal in size to the area of Over-the-Rhine bound by Liberty Street, Central Parkway, and Sycamore Street. Both areas shown are approximately 135 acres.



Figure 5 (redisplayed): Scale of potential urban core expansion.

These designs are discussed in further detail in the *Alternative redesign concepts* section of this paper.

If the BSB Project were to be redesigned and/or realigned in a way that optimizes highway alignment and minimizes the footprint of access ramps, the resulting configuration could return more than one hundred developable acres of land – almost entirely free of existing landowners and historic buildings – to the landlocked CBD and West End neighborhoods of Cincinnati. In this area, after removal of the existing highway infrastructure, local leadership could pursue an ambitious development agenda centered around economic development and modern, equitable housing. The resulting, expanded urban core of Cincinnati would significantly boost the region's economic strength, attractiveness, and social and civil wellbeing.

2. Improve connectivity between Covington and Cincinnati, and upgrade local streets, especially in Covington

As discussed in the *Issues with the current design* section of this paper, Covington leadership has raised serious and legitimate concerns with the current project design. Generally, these concerns are related to the large footprint of the proposed highway, poor connectivity between Covington and Cincinnati, and negative impacts on Covington businesses during the BSB Project's construction. By pursuing a BSB Project redesign however, these concerns can be addressed.

Currently, often the easiest way to make local trips between west Covington and the urban core of Cincinnati is to take the interstate bridge. By adding wide collector-distributor roads along both sides of I-75/I-71 in Covington, and by dedicating six of 16 planned interstate lanes to local traffic, the current preferred alternative exacerbates this situation. Unfortunately, this approach necessitates complicated junctions and congestion-inducing signalized intersections in Covington, none of which are conducive to urban, street-facing development.

Instead, Bridge-Forward Cincinnati believes that engineers should take a comprehensive, network-based approach to connectivity between Covington and Cincinnati. The existing Brent Spence Bridge and the Clay Wade Bailey Bridge are assets that could be repositioned in restructured street grids on both sides of the river. Drivers, bicyclists, and pedestrians could be given many, easy options for crossing the river. As a result, Covington's roadway system would be better connected to Cincinnati's, and Covington's roadway system as a whole would be made more resilient in the face of major congestion events on the replacement bridge.

Furthermore, currently in Covington, antiquated roadway infrastructure providing access to I-75 / I-71 is not encouraging the highest and best possible use of urban land. A BSB Project redesign could ensure that interstate junctions and important arterial streets providing access to the interstate are optimized for desired future conditions. For instance, streets such as West 4th Street, West 5th Street, and Crescent Avenue could receive significant performance and safety improvements, especially for bicyclists and pedestrians. These improvements could be similar to the 12th St./Martin Luther King Jr. Boulevard corridor improvements already completed in Covington and the Liberty Street improvements more recently completed in Cincinnati.

Bridge-Forward Cincinnati's original concept, designed by Brian Boland, illustrates a potential street grid in Covington that could improve traffic flow, elevate land values, and provide for better connectivity to Cincinnati, without dependency on the interstate. Figure 13 on the next page shows the Brian Boland's concept in Covington.



Figure 13: A concept for a better local road network in Covington, included in Bridge-Forward Cincinnati's original concept, designed by Brian Boland

3. Elevate the highest and best use for land in both Cincinnati and Covington

The Queensgate area in Cincinnati that would be impacted by a realignment – mostly bound by Freeman Avenue, I-75, and the Ohio River – is not a tremendous contributor to Cincinnati's economic health. The area, designed and developed in the 1950s & 1960s, is currently characterized by obsolete warehouses, Class C real estate, storage yards, and small automobile-oriented commercial businesses. It is structured around eight-lane wide, 800-foot long “superblocks” that are completely inappropriate for urban development.^{32, 33} However, with a BSB Project redesign/realignment, the new area could be restructured and repositioned to become a regional economic engine. The existing urban street grid of Cincinnati could be extended into the new area, creating miles of safe, inviting local streets with developable frontage.

Many businesses today favor locating in downtowns, and local leadership could prioritize commercial development that supports both Greater Cincinnati's strong entrepreneurship community and Greater Cincinnati's leading industries of biohealth, business and professional services, advanced manufacturing, and technology.^{34, 35} The area could become a “dense enclave that merges the innovation and employment potential of ... high-growth firms and tech and creative start-ups, [all in a] well-designed, amenity-rich residential and commercial environment. [The area could] facilitate the creation and commercialization of new ideas ... by leveraging ... [its] intrinsic qualities of ... proximity, density, authenticity, and vibrancy. Given the proximity of [the area created] to [existing] low-income neighborhoods and the large number of sub-baccalaureate jobs [that the area created could provide], its development [would also be] ... a tool to help connect disadvantaged populations to employment and educational opportunities.”³⁶

“Asset managers and capital markets are increasingly [searching] for opportunities to invest in urban development. Investment strategies are focused on ‘winning’ cities and ‘growing’ neighborhoods with mandates to fund new asset types such as urban retail, the private-rental residential sector, and hotel-type projects that deliver above market risk-adjusted returns by capitalizing on the location of dense urban areas. Capital structures and financial products are increasingly tailored to the funding requirements of

³² https://www.uc.edu/cdc/urban_database/historical/Poster_VI.pdf

³³ <https://dougalleninstitute.org/archives/10094/>

³⁴ <https://smartgrowthamerica.org/resources/core-values-why-american-companies-are-moving-downtown/>

³⁵ <https://redicincinnati.com/>

³⁶ <https://www.brookings.edu/innovation-districts/>

large-scale developments, particularly the need for significant upfront investment in urban infrastructure, the incremental increase in land values, and the inclusion of affordable housing.”³⁷

Indeed, in cities from the size of Boulder, Colorado, to Brooklyn, New York, affordable and market-rate housing developments coincide to create the most dynamic, diverse, and healthy neighborhoods in the country.³⁸ In addition to high-end, market-rate housing, a plentiful supply of middle-income housing near to major employment centers, like downtowns, is crucial to the success of healthy major cities.³⁹ In this area, too, a district housing and development policy designed to provide a mix of affordable and market-rate, modern housing could be established. The Queensgate area being discussed is currently designated by HUD as an Opportunity Zone, which will help spur investment in real estate in any newly developable land created.⁴⁰ Additionally, a land value capture scheme that supports the entire Queensgate and West End neighborhoods – much like the TIF districts extending north-south from the CBD to Over-the-Rhine – could possibly be established. Federal and state funding tools such as the low-income housing tax credit (LIHTC) program and the Ohio Department of Development’s Transformational Mixed Use Development program could also be leveraged during the area’s development.

It should be expected that the full, mature development of this area would take decades to realize. However, given the amount of downtown, urban development that Cincinnati has experienced in recent years, and given Greater Cincinnati region’s dearth of shovel-ready commercial development sites, there is strong reason to believe that businesses and developers would be eager to invest in a newly expanded CBD.

4. Attract talent to the region by providing for the development of dynamic, diverse, live-work-play neighborhoods

Attracting and retaining human capital is *the key* to economic development and regional competitiveness.⁴¹ In a “winner take all” environment in this arena, if the Greater Cincinnati community is to enjoy the same quality of life that we’ve enjoyed since the Brent Spence Bridge was originally constructed, it is imperative that our region keep pace with competitors like Nashville, Charlotte, Columbus, – and yes – Oslo, Hiroshima, and Brisbane as well.^{42, 43}

Currently, one of the best assets for attracting new talent to Greater Cincinnati is the region’s urban core.⁴⁴ Greater Cincinnati unfortunately does not have a competitive advantage in the areas of population diversity, favorable climate, or outdoor attractions, but Greater Cincinnati is affordable, and Greater Cincinnati has great urban-core neighborhoods, anchored by the City of Cincinnati’s downtown and Over-the-Rhine. In fact, the urban core is a top selling point to new Cincinnati-bound corporate recruits, and the collective pride and energy that the renaissance of Over-the-Rhine has created in the region is undeniable.^{45, 46} Investments made in center-city development and 3CDC, by Kroger, Procter & Gamble,

³⁷ <https://aecom.com/without-limits/article/integrated-urban-development-investment/>

³⁸ <https://www.dailycamera.com/2020/07/05/a-vision-for-change-boulders-north-broadway-comes-of-age-with-affordable-housing-mix-of-uses/>

³⁹ <https://youtu.be/4tCEHgkJyxM>

⁴⁰ <https://opportunityzones.hud.gov/resources/map>

⁴¹ <https://www.kearney.com/global-cities/2019>

⁴² <https://www.theatlantic.com/business/archive/2017/04/richard-florida-winner-take-all-new-urban-crisis/522630/>

⁴³ https://unhabitat.org/sites/default/files/2020/10/global_urban_competitiveness_report_2019-2020_the_world_300_years_of_transformation_into_city.pdf

⁴⁴ <https://www.soapboxmedia.com/features/the-case-for-cities.aspx>

⁴⁵ <https://cincinnati.pglocations.com/>

⁴⁶ <https://www.westernsouthern.com/careers/home-office-careers/working-in-cincinnati>

and others, have undoubtedly paid off.⁴⁷ Cities like Salt Lake City, Charlotte, and Indianapolis simply cannot compete with Cincinnati in this arena.

Over the next 50 years however, a single, magnificent, downtown-adjacent neighborhood will not be enough to sustain Cincinnati's competitiveness. Our region must embrace the fact that interest in downtown, urban living is at historical highs. Indeed, studies have shown that young people "favor living in central urban neighborhoods significantly more than previous generations did at the same stages in life" and that "Millennials are 21% more likely to buy their first homes near city centers than Generation X."^{48, 49} Over the next 50 years, growth in demand for urban amenities, urban living, and urban real estate is likely to outpace growth in demand for non-urban real estate. One can look to Atlanta, Nashville, and Austin – cities that have experienced sustained urban development pressure – to see that this trend is durable. One can also look to peer cities like Cleveland, which just announced a major project to develop a dense, 100-acre area of its urban core, to realize that the prioritization of urban development is essential to remain competitive.⁵⁰

Made possible by a BSB Project redesign/realignment, a larger urban core could provide more vibrant social spaces, support new entertainment amenities, and significantly strengthen the City's cultural institutions. Cincinnati would be given a pathway to compete with peer cities for talent that wants an urban experience, and as a result, Cincinnati would ensure its relevance in an increasingly competitive landscape.

5. Maximize the BSB Project's return on investment

Although a redesigned BSB Project may have a cost premium, by accepting this cost increase, and by achieving the goals set forth here, the project's overall return on investment will be maximized. From a transportation perspective alone, by achieving the first goal of expanding the urban core, a redesigned BSB would be able to serve more people and more businesses, thereby increasing the overall impact of each dollar spent. From a more holistic perspective, a redesign would also be a boon to local governments fiscally, and to the regional economy overall. The Bridge-Forward Cincinnati Coalition believes that a redesigned BSB Project could deliver so many spillover benefits to the region that overall quality of life could be increased. Regardless of how many layers of benefits one wants to consider, with billions of dollars being invested in the project, the region should receive a greater return than simply decreased congestion on the interstate during rush hour.

⁴⁷ <https://www.politico.com/magazine/story/2016/06/what-works-cincinnati-ohio-over-the-rhine-crime-neighborhood-turnaround-city-urban-revitalization-213969/>

⁴⁸ <https://www.bloomberg.com/news/articles/2019-05-28/u-s-millennials-really-do-prefer-cities>

⁴⁹ <https://www.bloomberg.com/news/articles/2018-11-15/millennial-homebuyers-look-to-cities-more-than-gen-x>

⁵⁰ <https://www.cleveland.com/realestate-news/2021/09/cavs-owner-dan-gilberts-company-announces-vision-for-thousands-of-homes-development-along-cuyahoga-river-near-downtown-cleveland.html>

Recommended design strategies

The design approach used in the development of the BSB Project should be modified so that it includes, from the start, strategies and evaluation criteria aimed at accomplishing the *Goals for a redesign* established previously in this paper.

Specifically, the Bridge-Forward Cincinnati Coalition recommends that the following design strategies be used in any redesign/realignment effort:

- Minimize the footprint of the highway
- Recess the highway wherever possible
- Use the interstate primarily as an efficient processor of regional, through traffic
- Provide a network of safe, multi-modal streets for local traffic
- Use only modern, progressive engineering practices

Additionally, the Bridge-Forward Cincinnati Coalition recommends that the following design goals be used as evaluation criteria in any future alternatives analysis and/or comparison of alternatives:

- Maximize the amount of contiguous land in Cincinnati's urban core
- Maximize the street frontage that is walkable and conducive to urban, street-facing development

If highway engineers are tasked with designing alternatives using, from the start, the holistic set of goals and evaluation criteria established here – as opposed to the limited set of goals established in the 2006 Purpose and Needs Statement – they will be empowered to better weigh trade-offs between community and traffic-efficiency priorities. They will also be able to defensibly advance alternatives that deliver the desires of the community. A few of the design strategies and evaluation criteria are discussed below.

First, it is a common, contemporary practice to design urban interstates to be recessed and to have as small a footprint as possible. As will be discussed in the *Precedent projects* section of this paper, this is precisely the design approach that was used during the development of two successful Ohio interstate improvement projects—the Fort Washington Way project in Cincinnati and the I-70/I-71 Columbus Crossroads/Downtown Ramp Up project in Columbus. The BSB Project should be no different. See Figure 14 at the end of this section for an illustration of this design approach, created by the design firm MKSK for the I-70/I-71 interstate upgrades in Columbus.

Second, the I-75 highway design should prioritize regional traffic, and not be designed to accommodate – to the extent the current BSB Project design does – traffic making short, intra-urban core trips. The new bridge should not be designed to accommodate people making trips from 4th Street in Covington to 2nd Street in Cincinnati. One driver of the proposed, expansive interstate footprint in the current preferred alternative is the fact that six of the planned 16 total interstate lanes are dedicated to local traffic, with origins or destinations between West 9th Street in Cincinnati and West 9th Street in Covington. Because of interstate highway geometric design requirements, such as minimum turning radii, the use of the interstate for local trips is far less efficient in terms of land area required. And, as detailed in the *Precedent projects* section of this paper, the utilization of high-speed, free-flowing interstate highways to carry local traffic in urban core areas is precisely what led to the type of highway infrastructure many cities are now attempting to remove. A better approach would be to generally use the new interstate bridge to efficiently process regional traffic, while providing local traffic with a well-designed network of separate, at-grade infrastructure intended not only to process vehicles but to also support both local businesses and other modes of transportation.

Finally, the BSB Project should be designed with only modern, progressive engineering practices. The current preferred alternative for the BSB Project was first developed in 2006 and is therefore most likely not based on current state-of-the-industry thinking. Design input data should be based on a hybrid of econometric and network-based models “built to consider individual trips that shift to other corridors;

occur at a different time of day; involve a different mode of transportation; or disappear due to telecommuting or a shifted trip.”⁵¹ This will be especially critical for achieving the goal of improving connectivity between Covington and Cincinnati, and upgrading local streets, particularly in Covington. Whenever possible, exceptions should be obtained from outdated guidelines such as the Manual on Uniform Traffic Control Devices (MUTCD). Because of its unique requirements, in no instance should the design of the BSB be a “cut and paste” job.

With engineers and planners focused on achieving community goals from the start, shrinking and recessing the highway, upgrading and encouraging the use of local roads, and utilizing state-of-the-industry design practices, an engineering/technical design that achieves all critical goals, including community goals, is possible.

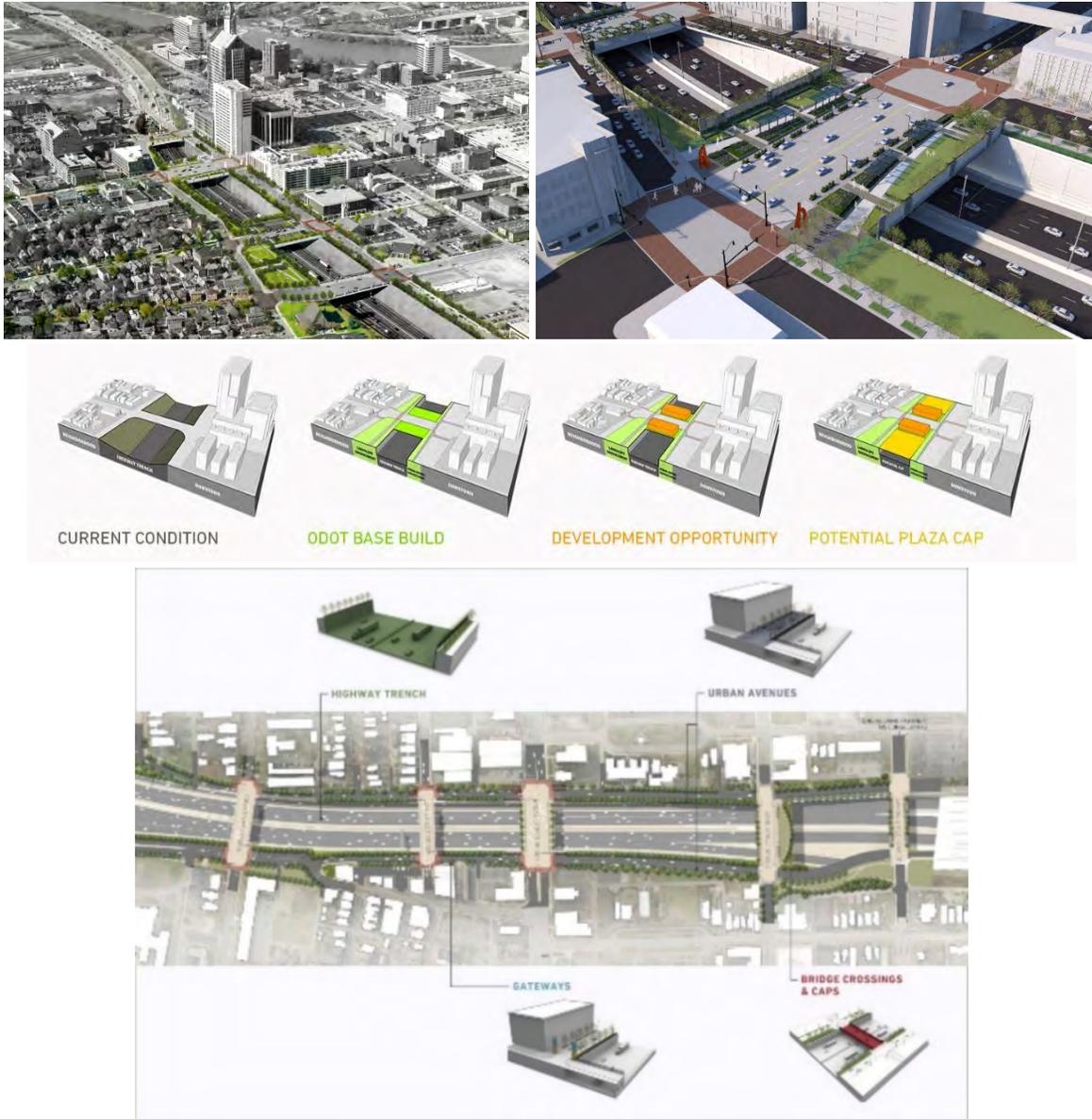


Figure 14: Design principles used in Columbus' urban interstate projects (Taken from <https://www.mkskstudios.com/projects/i70-i71-innerbelt>)

⁵¹ <https://t4america.org/2020/12/07/four-recommendations-to-undo-the-damage-of-urban-renewal/>

Alternative redesign concepts

In order to receive federal funding, the Federal Highway Administration (FHWA) requires that major projects achieve a minimum “level of service (LOS)” as defined in engineering manuals. The FHWA also requires that major projects be designed using a codified project development process. This project development process involves technical engineering design work and environmental assessment and permitting, all of which is done by highway engineers and other qualified professionals and is beyond the scope of this position paper. However, the Bridge-Forward Cincinnati Coalition is elevating three alternative design concepts that illustrate how the goals established in this paper could be realized:

- 1) Bridge-Forward Cincinnati’s original concept, designed by Brian Boland
- 2) Modified Alternative B, based on Alternative B from the 2009 Conceptual Alternatives Study
- 3) Modified Concept 85, based on Concept 85 from the 2012 HNTB PD/VE Workshop

Each of these three concepts are discussed further below:

1) Bridge-Forward Cincinnati concept by Brian Boland

Bridge-Forward Cincinnati’s original concept, designed by Brian Boland, proposes maintaining the existing BSB and I-75 rights-of-way, but with modifications to the physical form of the interstate. Specifically, Brian Boland’s concept utilizes a minimized interstate footprint and a depressed roadbed, like that of Fort Washington Way, for I-75 through the CBD and Queensgate neighborhoods. Brian Boland’s concept also presents the idea of shifting certain turning movements between I-75, I-71, and US 50 to the west, specifically to the area that is already dedicated right-of-way and that is currently being used for the junction between US 50 and Freeman Avenue. Finally, Brian Boland’s concept also provides a potential configuration for improved boulevards and traffic flow in Covington. See Figures 15, 16, and 17 for Bridge-Forward Cincinnati’s original concept, designed by Brian Boland.

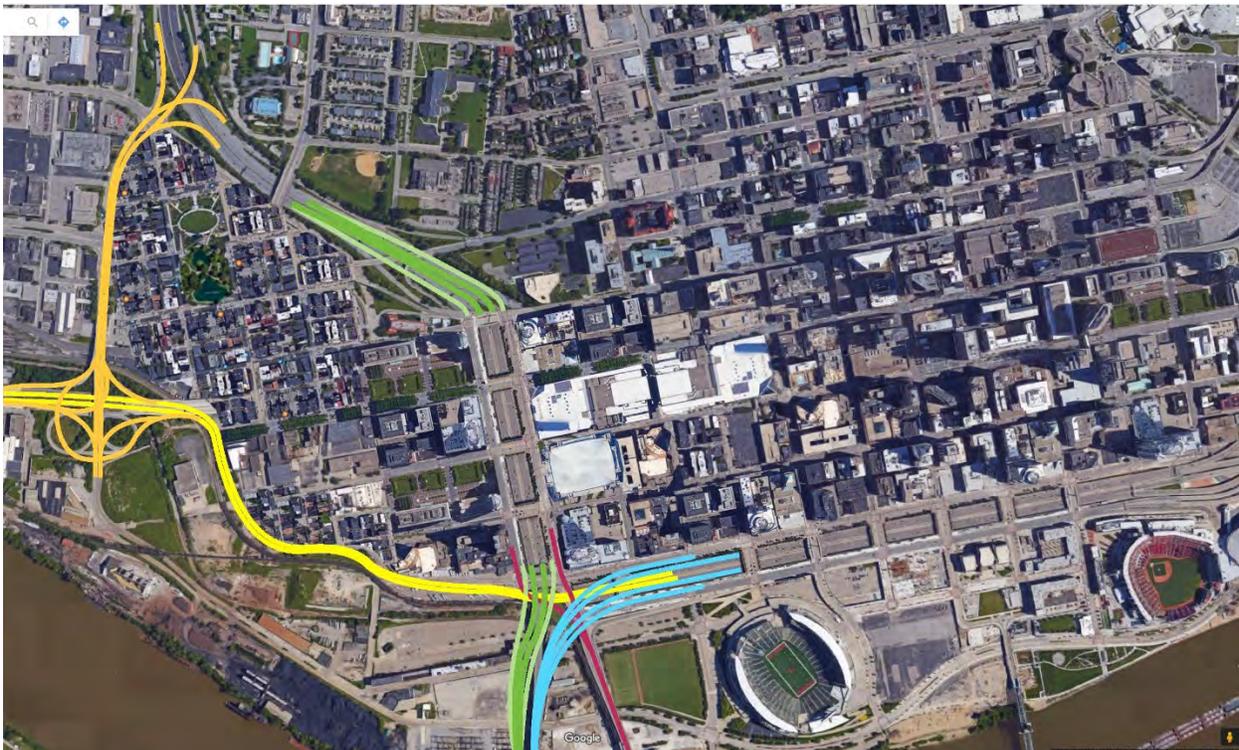


Figure 15: Bridge-Forward Cincinnati’s original concept, designed by Brian Boland, on the Ohio side of the river



Figure 16: Before and after renderings of Brian Boland's concept (Produced by Jeffery Tompkins of Meticulous Design + Architecture (MD+A); available at <https://bridge-forward.org/>)



Figure 17: Brian Boland's concept, on the Kentucky side of the river

2) Modified Alternative B

Next, the *Modified Alternative B* concept presented here is based on the *Alternative B* analyzed in the 2009 Conceptual Alternatives Study. Alternative B took an approach that directed regional traffic across a new, Queensgate-aligned bridge and that directed local traffic mainly over the existing Brent Spence Bridge, which was to remain.^{20, page 160} Conceptually, this seems like an effective method for achieving all of the technical needs of the project, and it is worth revisiting. The weakness of the original Alternative B was that it maintained the entirety of the existing, extensive ramping system on the Ohio side of the existing Brent Spence Bridge. If this extensive ramping system was scaled back and right-sized, putting local traffic on local streets, the resulting *Modified Alternative B* may be a compelling option for a realignment. Figures 18 and 19 on the next page show the Modified Alternative B concept.

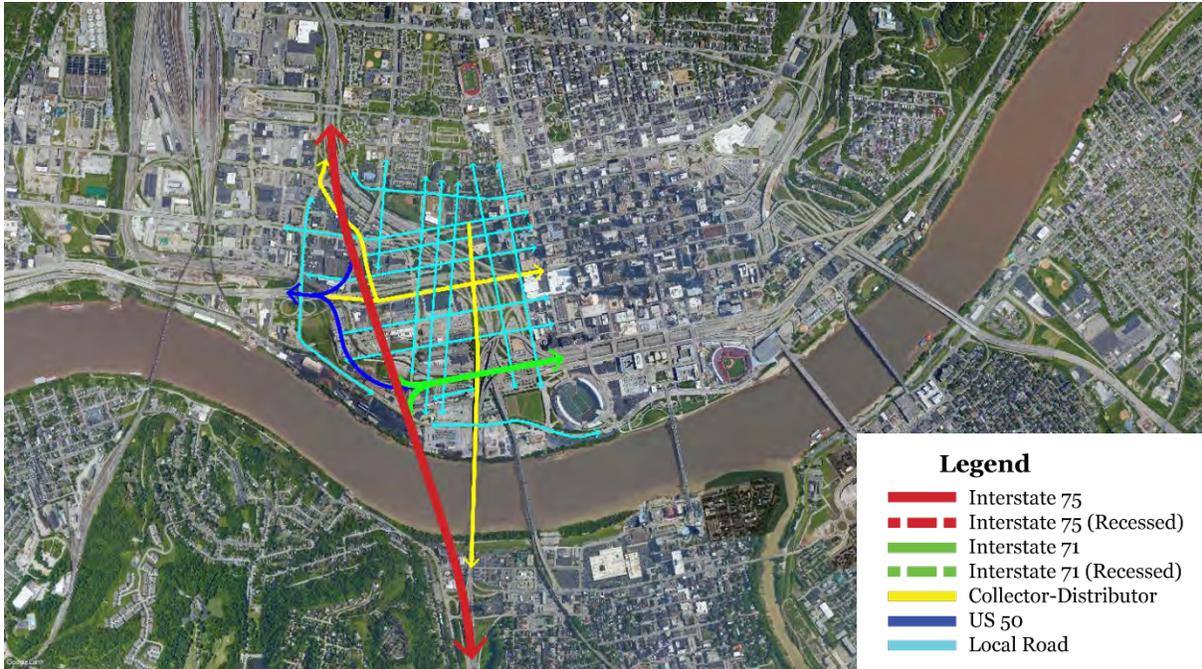


Figure 18: Modified Concept B

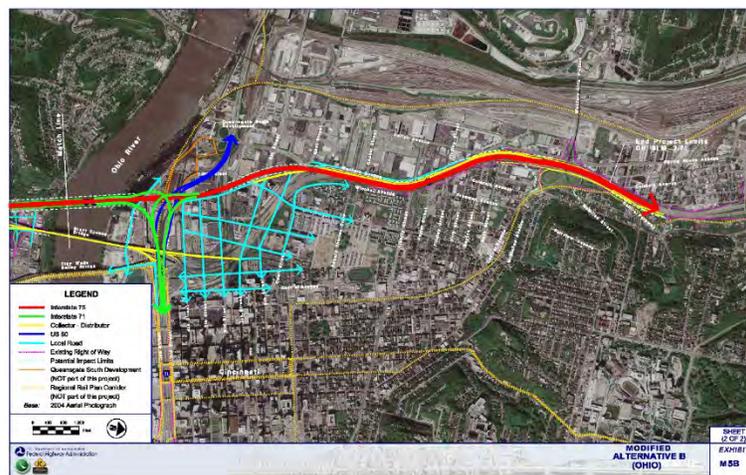
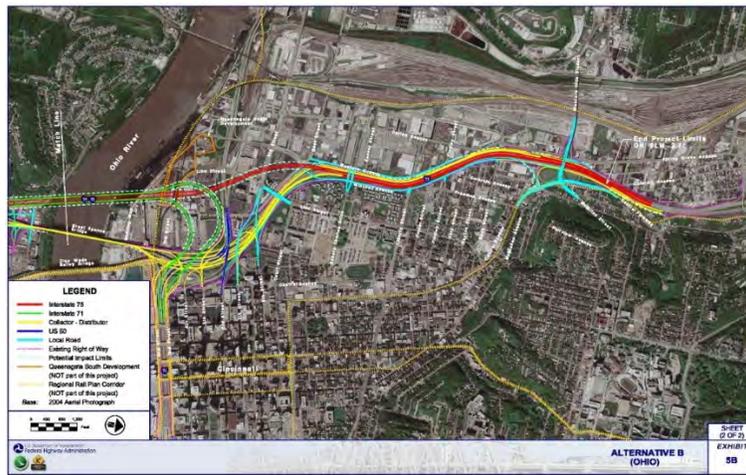


Figure 19: Alternative B from the 2009 Conceptual Alternatives Study (above) and Modified Alt. B (below)

3) Modified Concept 85

The last of the three alternative concepts presented here is the *Modified Concept 85*, which is based on *Concept 85* (also known as *Innovation 85*) from the 2012 HNTB PD/VE Workshop. The purpose of the PD/VE Workshop – held after the preferred alternative was selected – was to generally increase the project’s overall expected technical performance, decrease cost, and minimize construction risk. Concept 85, being a product of the PD/VE Workshop, can therefore be thought of as a rigorous, technical concept that has significant advantages over the preferred alternative. Concept 85 was actually developed using Alternative B as a starting point, but Concept 85 took a significant departure from Alternative B in that I-75 in Concept 85 follows the existing Freeman Avenue right-of-way through Queensgate, thereby reducing required land acquisitions. According to workshop documentation, Concept 85 has the following distinct advantages:

- Provides potential phased construction opportunity with I-71 and I-75 corridors developed separately
- I-75 corridor and bridge crossing would be constructed offline, lessening the construction impact to drivers, businesses, civic events
- Significantly reduces new infrastructure within congested construction area immediately downtown, and reduces construction cost as a result
- The north-south corridor on the Ohio side of the river would carry fewer lanes, making it a less divisive element of the downtown infrastructure
- Interstate traffic would be separate from local traffic, thereby reducing the design criteria for some portion of the project and reducing footprint and significantly reducing construction costs.
- Improves geometrics and improves safety
- Separates complicated/confusing system movements; will be easier for drivers to navigate ^{2, page B6}

This paper advocates for a Concept 85 in which the east-west connection between US 50 and Fort Washington Way is recessed, according to the design strategies established in the *Recommended design strategies* section of this paper. Concept 85, with this small change, is referred to here as *Modified Concept 85*. Figure 20, below, illustrates the Modified Concept 85, and Figure 10, redisplayed on the next page shows Concept 85 images from the 2012 PD/VE Workshop.

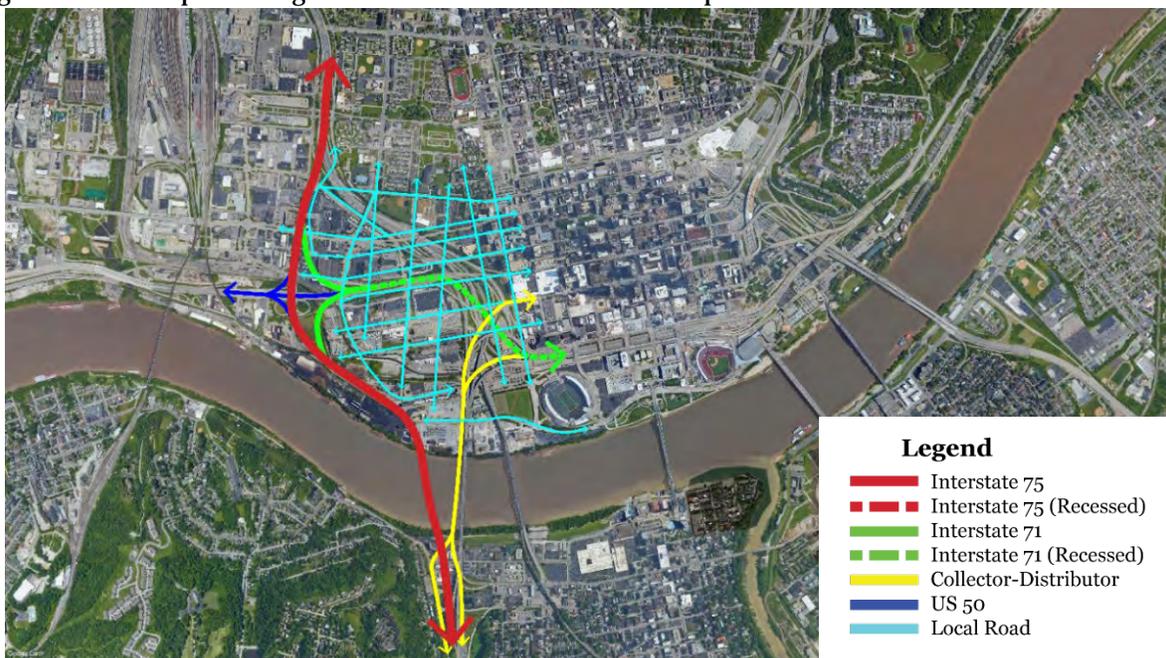


Figure 20: Modified Concept 85

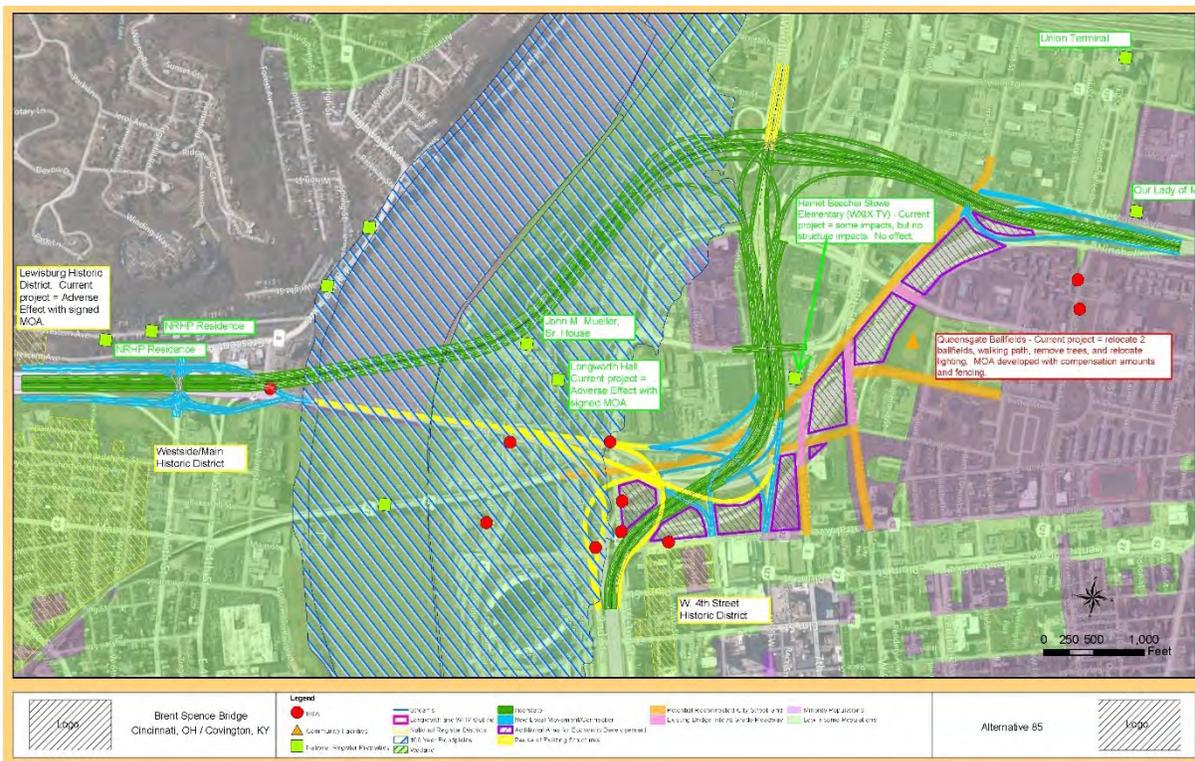
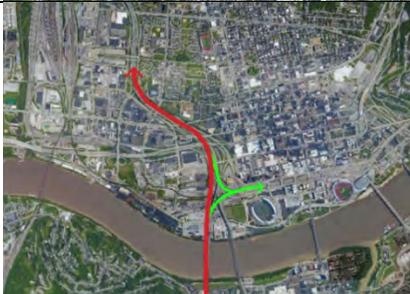


Figure 10 (redisplayed): Concept 85 (taken from the October, 2012, HNTB PD/VE Workshop documents)

A full comparison of the current preferred alternative and the three new alternative concepts presented is beyond the scope of this position paper. However, Table 1, below, enumerates, for each alternative, the estimated new land area made contiguous with Cincinnati's urban core in all four alternatives.

Table 1: Estimated new land area made contiguous with Cincinnati's urban core

	Alternative	Land area
	<p><i>Over-the-Rhine bound by Liberty, Central, and Sycamore, shown for reference</i></p>	<p>135 acres</p>
	<p>Alternative I—the current design</p>	<p>0 acres</p>
	<p>Bridge-Forward Cincinnati's original concept, designed by Brian Boland</p>	<p>130 acres</p>
	<p>Modified Alternative B</p>	<p>135 acres</p>
	<p>Modified Concept 85</p>	<p>135 acres</p>

Current project status and the existing window of opportunity

The BSB Project is currently being advanced by a Bi-State Management Team (BSMT) formed by ODOT and KYTC. According to the governing Memorandum of Understanding (MOU) between ODOT and KYTC – most recently amended and signed by Governors Mike DeWine and Andy Beshear in April, 2021 – the following work has been completed to date:

- “[The BSMT has spent] \$18.2M during the preliminary engineering and environmental engineering phases to determine the Preferred Alternative.
- “As to ODOT’s acquisition of property in anticipation of the Project, ODOT has identified 79 parcels to acquire for right of way. Acquisition is complete for 60 parcels. ODOT is in negotiation or appraisal for 4 parcels. There are 5 railroad parcels that will be acquired via agreement at time of construction. ODOT is purchasing 10 parcels from the City of Cincinnati. As to KYTC’s acquisition of property in anticipation of the Project, no activity has been undertaken.”⁵²

Also, according to the MOU and the Project Summary Report published in November, 2021, the following is a list of current, ongoing work:

- “The BSMT has begun work on the procurement of professional services to assist in the preparation and processing of applications through FHWA with respect to funding, but the work is not complete... The BSMT will [continue] to prepare the BSB companion bridge project for future funding opportunities.
- “The BSMT has begun work on developing an updated Initial Financing Plan, but the work is not complete.”⁵²
- “Two sections of the BSB project in Ohio are moving forward in design with consultant teams expected to proceed in October 2021:
 - HAM-75-1.05 Linn to Findlay (PID 113361) with construction anticipated for 2025.
 - The HAM-75-1.05 project scope includes an update to the environmental documents ... including ... community engagement. This project also includes the reestablishment of the BSB Advisory Committee and Aesthetic Committee.
 - HAM-75-1.95 Findlay to north of the WHV (PID 114161) with construction anticipated for 2028.
- “MSD is developing a new stormwater model for the BSB corridor.
- “Concepts W and M for crossing the Ohio River are undergoing further analysis and comparison to Alternative I. This includes value engineering considerations to reduce costs and address local concerns that have been raised about the project. While each utilize the existing BSB and a new companion bridge to the west of the existing BSB, the use of each bridge and level of the double decked structures by interstate and local connections vary. These alternatives also vary in the potential for reuse of existing highway and ramp facilities.
- “ODOT continues to coordinate with the city/county for environmental issues related to the BSB project, most notably historic structures.”²⁷

Fortunately, there currently exists a unique set of circumstances that, when considered together, presents a window of opportunity for the realization of a redesign of the BSB Project. First, as discussed in the *Issues with the current design* section, Covington leadership is demanding a better design, with a smaller footprint. Second, although a significant amount of funding will be made available through the bipartisan Infrastructure Investment and Jobs Act, the BSB Project is still not yet fully funded, and therefore time is still available to achieve a redesign. Third, the BSB Project’s NEPA clearance will need to be re-evaluated before the project can substantially move forward, and this re-evaluation should include stakeholder input. And finally, funding will be made available through the bipartisan Infrastructure Investment and Jobs Act for precisely the type of planning, feasibility, and cost-benefit analysis (CBA) study that will be

⁵² <https://www.brentspencebridgecorridor.com/uploads/pdfs/MOU%20Agreements.pdf>

needed to begin the process of a project redesign. The topics of funding gaps and permitting requirements are each discussed below. The topic of a planning, feasibility, and CBA study is discussed in the *Implementation plan* section of this paper.

Funding

The BSB Project still lacks sufficient funding to substantially move forward, and therefore time is still available to achieve a redesign. ODOT must apply for federal funding through grant programs established in the bipartisan, federal Infrastructure Investment and Jobs Act.⁴ Through these grant programs, the federal government can be expected to contribute around 80% of the total cost of the BSB Project. Therefore, it can be expected that each state will need to contribute about 10% of the total project cost. At a total cost of \$3.5 billion, the contribution that Kentucky will need to make is about \$350 million—no small amount.⁵ In fact, the annual appropriation made by the Commonwealth of Kentucky to its state highway fund – the State Supported Construction Program – is about \$350 million.⁶ Securing funding from Kentucky is likely to be a limiting factor in terms of moving the BSB Project forward.

Permitting

According to guidance from the FHWA, because the NEPA-mandated EA for the BSB Project was completed in 2012, the BSB Project EA will likely need to be re-evaluated before the project can substantially move forward.^{53, 54} According to NEPA rules, re-evaluations are needed if there are “significant new circumstances or information relevant to environmental concerns” or relevant to the subject project more broadly.⁷ Within the scope of the original EA was an evaluation of “project purpose and need, [the] alternatives development process, [the] impacts of each alternative, mitigation measures, and public involvement & agency coordination.”^{55, page ES-1} Therefore, if local leadership and/or the local community were to support a redesign of the project, the EA re-evaluation process would likely need to react to this change in circumstances. Additionally, assumptions made in the original EA regarding air quality conformity, highway widening, and rail investment have been raised by some as a topic that will need to be re-evaluated as well.

Fortunately, requesting a substantial re-evaluation of the EA, or potentially requesting a new Environmental Impact Statement (EIS) and Record of Decision (ROD) will not condemn the project to years in regulatory limbo. The bipartisan Infrastructure Investment and Jobs Act codifies an executive order first issued by President Trump called *One Federal Decision* (OFD), which instructs all federal agencies to provide environmental approval/denial decisions within two years to all major projects requiring NEPA clearance.⁸ This is a major improvement to status quo in regards to permitting timelines, and this policy development makes a BSB Project redesign all the more attainable within a reasonable timeframe.⁹

⁵³ https://www.environment.fhwa.dot.gov/legislation/nepa/Reevaluation_guidance_08142019.aspx

⁵⁴ <https://www.fhwa.dot.gov/resourcecenter/teams/environment/vol5iss2.pdf>

⁵⁵ <https://www.brentspencebridgecorridor.com/uploads/pdfs/Brent%20Spence%20Bridge%20Environmental%20Assessment%20March%202012.pdf>

Challenges and reasons for optimism

The primary challenges facing a BSB Project redesign fall into two categories: right-of-way acquisition and schedule. Although these are two significant challenges, there is reason to be optimistic that these challenges can be sufficiently addressed and overcome.

Right-of-way acquisition

Acquiring the land necessary to build a realigned BSB Project will be a major challenge that will need to be further investigated and further quantified. Of the alternative concepts presented here, the Modified Alternative B would require the most right-of-way acquisition. Bridge-Forward Cincinnati's original concept would require the least. Concept 85 from the 2012 HNTB PD/VE Workshop was based on the original Alternative B from 2009, but altered to, among other things, reduce the amount of right-of-way acquisition required.²

Although outdated, a few historical assessments may be useful at this stage in order to understand the overall scope of potentially required right-of-way acquisition. Notably, the historical assessments discussed here represent the worst-case scenario for required right-of-way acquisition, because they specifically investigated Alternative B, which, again, requires the most land acquisition of all three alternative concepts presented.

During preparation of the BSB Project's 2007 Conceptual Alternatives Study, "a Relocation Assistance Program Conceptual Survey (January 2007) and Conceptual Stage Relocation Report (February 2007) were completed for Ohio and Kentucky, respectively. [These studies] identified potential displacements and relocations resulting from the conceptual alternatives being considered at the time."^{20, page 86} The results are useful because they include the current BSB Project design – Alternative I – and they include Alternative B which can be used as a reasonable stand-in for the Modified Alternative B concept and for any "Queensgate alignment" concept under consideration. It must be noted, however, that the original Alternative B did not envision a low-impact scheme of access ramps, like that envisioned in the Modified Alternative B concept, and therefore it did not envision a westward extension of the urban core either. The major findings of the Conceptual Alternatives Study are shown in Table 2 below.

Table 2: Comparison of estimated displacements

Alt.	Est. No. of Residential Units Displaced		Est. No. of Persons Displaced from Residential Units		Est. No. of Businesses Displaced		Est. No. of Employees Displaced from Businesses		Est. Property Value Loss (million \$)		Est. Real Estate Costs (million \$)	
	OH	KY	OH	KY	OH	KY	OH	KY	OH	KY	OH	KY
B	5	38	10 - 36	65 - 260	26	8	1,791 - 1,831	121 - 158	40.1	16.0	46.5	18.4
I	5	11	10 - 36	13 - 52	31	4	203 - 237	90 - 115	11.9	2.1	13.8	2.5

This table is adopted from Tables 21, 22, and 43, and §4.2.8.1 of the Conceptual Alternatives Study (April 2009). Alternative B represents any "Queensgate alignment" including Modified Alternative B. Alternative I represents the current BSB Project design. Estimates for Alternative I (the Preferred Alternative) are the average of estimates provided for Alternatives C and D, of which Alternative I is a composite.

Indeed, there are serious implication of building a "Queensgate alignment" bridge. Potential residential displacements in Covington are of particular importance; the design of I-75 in Covington would need to be adjusted to minimize impacts. Fortunately, there is reason to be optimistic about potential displacements in Cincinnati. In 2009, KYTC and ODOT sent a survey to all of the businesses within the limits of the

conceptual alternatives, and “the majority of businesses that responded indicated they would not be impacted.”^{20, page 93} Also, the largest employer in Queensgate, United Parcel Service (UPS), which at the time of the study had 919 employees, is likely to remain in the region due to the nature of its sorting and delivery function. Finally, at the time of the study, the second-largest impacted employer in Queensgate was Butternut Breads, which has since closed. Shelterhouse, a homeless shelter, now occupies the site of the Butternut Breads facility.⁵⁶

For those businesses that would need to relocate due to the project, regional economic development organizations like REDI and Tri-ED could be used to guarantee a new location meeting those businesses’ needs. For instance, impacted warehouse-based businesses in Queensgate could be guaranteed superior sites at CVG’s modern logistics hub in Boone County, Kentucky. These businesses’ relocation expenses could be subsidized, and similar special accommodations could be provided for residents displaced by the project too.

Schedule

A second major challenge associated the redesigning of the BSB Project will be gaining support to seemingly pause forward momentum on the project, while simultaneously spending resources on preliminary design work that has already been substantially advanced. Indeed, multiple U.S. Presidents, U.S. senators, and other political leaders have all positioned themselves as strong advocates of a BSB Project. The “U.S. Chamber of Commerce, the Greater Cincinnati USA Regional Chamber, the Northern Kentucky Chamber, the Ohio and Kentucky state chambers,” and the Northern Kentucky AFL-CIO Labor Council all launched a campaign earlier this year to support the project moving forward.^{57, 58} Additionally, \$18.2M has been spent on “preliminary engineering and environmental engineering phases to determine the Preferred Alternative.”⁵² As former Kentucky Governor Steve Beshear said in December 2014, “the cost of the bridge project increases \$7 million a month due to inflation and added project costs. That’s about \$233,000 a day, \$84 million a year.”⁵⁹

However, most Greater Cincinnatians do not yet realize that a superior, holistically-considered alternative to the current design is possible. The Bridge-Forward Cincinnati Coalition has found that, as it presents the idea of a redesigned BSB Project, its audiences, to a person, are supportive of the idea. The economic benefits of a redesigned BSB Project are clear. However, the case for a redesign can be compelling even for those who would favor proceeding with the current design simply to provide relief as soon as possible to the daily congestion on I-71/I-75. Engineers at HNTB anticipated that Concept 85 would lessen lane closures and construction impacts to drivers, because a realigned new bridge would allow the existing BSB to remain in service for potentially years longer during the construction process.² Finally, as discussed in the previous *Funding* and *Permitting* subsections, the BSB Project is not yet ready to be awarded to a design-build contractor. Therefore, the investing of alternative concepts will not necessarily slow down the overall, current project schedule.

Because the true implications of redesigning the BSB Project have yet to be determined, a formal planning, feasibility, and cost-benefits analysis study should be completed. Only then should a final commitment to the ultimately selected alignment be made.

⁵⁶ <https://www.wlwt.com/article/drop-inn-center-to-move-to-former-bread-factory-in-queensgate/3537392>

⁵⁷ https://www.bizjournals.com/cincinnati/news/2021/02/25/business-groups-launch-new-campaign-pushing-brent.html?ana=e_cinci_bn_breakingnews_breakingnews

⁵⁸ <https://www.cincinnati.com/story/opinion/2021/03/05/opinion-no-more-temporary-fixes-brent-spence-bridge/6918705002/>

⁵⁹ <https://buildournewbridgenow.com/reality-vs-myth/>

Implementation plan: recommended first steps

The first steps required to achieve a BSB Project redesign are:

1. identify a redesign champion, and build a support coalition
2. adopt a vision & goals statement, and establish evaluation criteria for alternatives
3. complete a formal planning, feasibility, and cost-benefit analysis (CBA) study.

If a redesign champion – such as the City of Cincinnati or the Cincinnati USA Regional Chamber – were to accomplish at least the first two steps listed above, organizations with the power to pursue a full redesign – ODOT, KYTC, and FHWA – would likely take notice and begin working with the champion to consider a redesign.

If sold on the potential benefits of a redesign, many local stakeholders can be counted as likely supporters. Importantly, as different groups with different perspectives join the coalition supporting a redesign, the insights driving a redesign, and therefore the case for a redesign, will only get stronger. First, the West End Community Council has already officially voiced support for the studying of a project redesign. Next, the region's major urban developers are likely to support the effort. A similar proposal for an interstate redesign project in Syracuse, New York, received the public support of 21 downtown developers, builders, and investors in a recent op-ed published there.⁶⁰ As discussed previously, Covington leadership is very likely to support a redesign, as long as it meets the demands they have established. And, although the local business community is currently advocating for immediate investment in the BSB Project, it may support the investigation of holistically-considered alternatives, especially if the alternatives promise to deliver significant economic benefits.⁶¹ As will be discussed below, proponents of the new, bipartisan Reconnecting Communities program can be counted as supporters. Finally and most importantly, all Greater Cincinnatians who have experienced joy and pride thanks to the region's urban renaissance over the last 20 years can also be counted as likely supporters of a project redesign.

Next, the creation of a vision & goals statement will be critical in ensuring that design decisions are “driven by a long-term community investment strategy rather than by the short-term pressure of spending federal allocations.”⁶² Unlike the Purpose and Needs Report that was completed in 2006, the redesign's vision & goals statement should be holistic and explicit in identifying economic goals and neighborhood goals, in addition to transportation goals. The vision & goals statement can be brief and high-level, but it should make clear the priorities that the community wants from the BSB Project. Ideally, the vision & goals statement will be the product of an entire coalition of redesign supporters. Critically, the vision & goals statement will need to be accompanied by a set of evaluation criteria like those suggested in the *Recommended design strategies* section of this paper. This set of evaluation criteria will empower highway engineers to produce alternatives that meet the community's needs.

The vision & goals statement and evaluation criteria will ultimately act as the basis for a formal, high-level planning, feasibility, and CBA study. During the course of this study, planners, engineers, and economists will first develop high-level alternative concepts for a bridge redesign. These concepts will then be put through an iterative process of assessing feasibility and cost-benefit, and refining design, until a few distinct, durable alternatives emerge. Unlike the alternatives developed in 2006, these alternatives will be built with the community's priorities in mind from the start.

⁶⁰ <https://www.syracuse.com/opinion/2021/10/downtown-developers-build-on-our-momentum-by-choosing-i-81-community-grid-guest-opinion.html>

⁶¹ https://www.bizjournals.com/cincinnati/news/2021/02/25/business-groups-launch-new-campaign-pushing-brent.html?ana=e_cinci_bn_breakingnews_breakingnews

⁶² <https://www.itdp.org/event/urban-highways/>

Once a few alternatives emerge, they will need to be formally analyzed for feasibility and cost-benefit in at least the following areas:

- Transportation/technical
 - Safety
 - Access
 - Level of service
 - Impact on existing infrastructure, such as utilities
 - Constructability
- Environmental justice
 - Impact on existing businesses, residents, and public facilities
 - Willingness of existing businesses and residents to relocate
 - Opportunities created to advance priorities within the new developable area
- Economic
 - Cost premium
 - Real estate market demand for new developable land
 - Fiscal impact
- Policy/legal
 - Right-of-way acquisition required
 - Policy, legal, and financial tools available for the administering of development in any new developable area created

This study may be procured within the framework of the existing BSMT, or the contract for this study could be held by a third party, such as the City of Cincinnati. Regardless, it will be crucial that a good firm is selected to lead the study. The firm should have a specialty in transit-oriented development, highway design, master plans involving highways, urban policy and economics, and/or adaptive re-use of infrastructure. The firm should also demonstrate progressive thinking regarding design standards, transportation network concepts, engineering specifications, and metrics to measure success. Because questions of highway design and urban development will be integrated, it is likely that a consortium of firms will be most qualified to carry out the study. Any consortium of firms bidding for the project will need to demonstrate past experience working together. A few representative firms that could potentially participate in this assignment would be AECOM, Arup, Buro Happold, HR&A Advisors, and WSP. The firm's work would not have to adhere to all guidelines governing the ODOT Project Development Process (PDP), but the work should be able to contribute to the potential future reworking of steps in the PDP process. A good example of a similar study recently completed is the Chicagoland Chamber of Commerce's *One Central Station: Mobility, Tourism, & Economic Development Implications* study.^{63, 64} Finally, during the course of the study, it would be helpful if a task force of local officials and knowledgeable individuals were assembled to assist the study team in rapidly answering questions, providing connections, and reducing the need for the making of assumptions.

With popular support and a completed planning, feasibility, and CBA study in-hand, a serious, informed conversation about further pursuing a redesign can then take place. Until then, the possibility of a redesign should not be foreclosed.

Fortunately, funding can likely be secured for the type of planning, feasibility, and CBA study needed to realize a BSB Project redesign. First, a local government could use leftover American Rescue Plan Act (ARPA) funds from 2021, or a future 2022-allocated ARPA funds, to support a planning study for a BSB

⁶³ <https://www.chicagolandchamber.org/chamber-news/chicagoland-chamber-of-commerce-study-projects-120-billion-in-new-tax-revenues-other-public-benefits-of-one-central/>

⁶⁴ <https://media.chicagolandchamber.org/wp-content/uploads/2020/04/ONECentralImpact4.16.19.pdf>

Project redesign.^{65, 66} Also, both ODOT and KYTC have a record of funding feasibility studies for highway projects that were originated from advocacy groups: The Cincinnati Eastern Bypass project – a project initially advocated for and long championed by home builders – was able to receive funding for studies from both states.^{67, 68} Additionally, as it has done for so many other critical efforts in Greater Cincinnati, the local business community may be willing to support and invest in this vision for the region.

Reconnecting Communities: a potential source of funding for a redesign planning study

Finally, there is another funding source that may be a perfect match for this type of study effort. The bipartisan Infrastructure Investment and Jobs Act provided an allocation of \$1 billion to a new program called the Reconnecting Communities Pilot Program. In a press release, the White House explained the motivation for the program: “Too often, past transportation investments divided communities ... or left out the people most in need of affordable transportation options. In particular, significant portions of the interstate highway system were built through Black neighborhoods.”¹⁰

The existing BSB and associated access ramps in Cincinnati certainly form an infrastructure barrier between the CBD and Queensgate. And, when it was constructed, the BSB Corridor and the City of Cincinnati’s accompanying Queensgate urban renewal project forced 25,737 people – the vast majority of whom were Black – from their homes.¹¹ This pilot program is certainly intended for contexts like I-75 in Cincinnati.

The Reconnecting Communities Pilot Program will provide competitive planning grants of up to \$2 million aimed at restoring community connectivity.⁸ Grants can cover feasibility studies, traffic analyses, alternative roadway design, economic impact studies, environmental impact studies, and public engagement. Notably, grants can be awarded to states, local governments, metropolitan planning organizations (MPOs), or non-profits.

With regards to the competitive nature of the Reconnecting Communities grant program, the BSB Project is in an enviable position. Since the BSB Project is likely to proceed with regular surface transportation funding anyway, if a \$2 million Reconnecting Communities grant were leveraged in a redesign effort to increase community connectivity, the BSB Project, because of its huge size, could potentially be one of the highest-profile successes of the pilot program in the country.

⁶⁵ <https://thehill.com/homenews/senate/566607-cornyn-cuts-a-deal-with-white-house-on-covid-19-money-for-infrastructure?rl=1>

⁶⁶ <https://www.cincinnati.com/story/news/politics/2021/05/19/american-rescue-plan-cincinnati-city-council-approves-spending/5167148001/>

⁶⁷ <https://www.cincinnati.com/story/news/2019/04/09/eastern-bypass-ohio-complete-study-end-2019/3352678002/>

⁶⁸ https://2050.oki.org/wp-content/uploads/2020/04/ODOT-2019-Report-to-Legislature_w_attach-Final.pdf

Precedent projects

Proponents of a BSB Project redesign in Cincinnati can point to celebrated, highway right-sizing, precedent projects both in Ohio and around the country. As these precedent projects demonstrate, highway infrastructure can successfully be altered to advance neighborhood priorities and create new development opportunities. In fact, Cincinnati's own Fort Washington Way project is often held up as a successful example of highway right-sizing and contextualizing.

Ohio precedents

The original Fort Washington Way (FTWW) in Cincinnati was constructed in the early 1960s.⁶⁹ By the early 1990s however, it was clear to community leaders that the highway was creating a barrier to riverfront development.⁷⁰ Thanks to a partnership between activists and local leadership, a simple pavement resurfacing project on FTWW was leveraged as an opportunity to restructure the entire FTWW segment of highway through downtown. Project issues, goals, and evaluation criteria were decided upon and adopted by local leadership and project engineers, ensuring that the final selected alternative design would meet the community's priorities.⁷⁰ Alternative concepts were compared against one another, with the current configuration of FTWW being selected. This highway project created the conditions for what is now known as *The Banks* in downtown Cincinnati. See Figure 21 below for images of FTWW before and after its restructuring.



Figure 21: Fort Washington Way in 1973 and 2019

(Taken from: <https://dianerehm.org/2016/05/24/how-highway-construction-has-affected-listeners-lives> and “Rosamar / Shutterstock.com” respectively)

Partially inspired by the success of Cincinnati's FTWW project, local leadership in Columbus, Ohio, also successfully pursued a major retrofit of Columbus's downtown highway infrastructure in a project called *Columbus Crossroads/Downtown Ramp Up*.^{71, 72, 73} Much like Cincinnati is home to the junction of two major interstates at the BSB, downtown Columbus is home to the junction of two major interstates too—I-70 and I-71. When it came time to upgrade these interstates' levels of service and safety through downtown Columbus, local leadership leveraged the opportunity to ensure both that the urban fabric of the city was improved and that new development opportunities were created. Refer to Figure 14 on page 23 for design principles currently driving these projects in Columbus.

⁶⁹ <https://www.oki.org/studies-plans/fort-washington-way/>

⁷⁰ <https://www.oki.org/departments/transportation/pdf/FTWW/FTWW%20Evaluation%20of%20Alternatives%20Report.pdf>

⁷¹ <https://www.columbus.gov/publicservice/streets/project-information/I-70-and-I-71-Corridor/>

⁷² <https://www.morpc.org/columbuscrossroads/>

⁷³ <https://www.transportation.ohio.gov/wps/portal/gov/odot/projects/mega-projects/mega-projects/downtown-ramp-up>

National precedents

Nationally, perhaps the most commonly known highway removal and contextualization projects are San Francisco's removal of the Embarcadero freeway near its downtown and Boston's burying of I-93 in the Central Artery/Tunnel Project, also known as the "Big Dig". However, highway right-sizing projects have been successfully completed in peer cities of Cincinnati, including Milwaukee, Rochester, and Chattanooga. In fact, there is a growing highway right-sizing movement in the country, led and organized by local, grassroots groups and supported by national organizations like the Congress for New Urbanism (CNU), Third Way, and Transportation for America.^{74, 75}

In Milwaukee, a mile-long spur highway called the Park East Freeway was removed in 2002. "A master urban design plan and form-based code were prepared to shape pedestrian-scaled development and reinforce the area's original form and character."⁷⁶ "For an investment of \$25 million in federal money used to remove the freeway, plus tax increment financing to clean up the land and re-establish a street grid, the corridor has attracted \$886 million in investment, with another \$300 million on the way... The county reported that the project had more than \$2 billion of 'economic impact' as of April, 2019. The 24-acre corridor has become a magnet for corporate headquarters, the latest being The American Family Insurance Company, which announced in December 2018 a plan to rehabilitate the historic Mandel building and bring in 250 jobs from the suburbs—while creating 150 more jobs downtown."⁷⁷

In Chattanooga, "a variety of positive economic benefits have come from the replacement of an old highway with an attractive boulevard. The visual and economic aura of the downtown area has been revitalized, and population has consistently grown in recent years, by 30% since 1990. The boulevard conversion attracted millions of dollars of investment in the area, as the riverfront is becoming one of the city's premier addresses. And, a variety of new developments have been constructed, all concentrated around the boulevard."⁷⁸

In Rochester, thanks to a TIGER grant, a one-mile stretch of the urban I-490 freeway was buried.⁷⁹ "In all, the new neighborhood on and around the former expressway will include 534 housing units, more than half subsidized or below market rate, and 152,000 square feet of new commercial space, including services and amenities such as a day care center and restaurants, reflecting the city's priority for an inclusive neighborhood with affordable homes and needed services. The largest project located on the new parcels will be the Neighborhood of Play, an expansion of the city's popular Strong National Museum of Play that will include 236 apartments, a 120-suite hotel, retail, and a parking garage. Seeing '\$229 million in economic development from \$22 million in public investment is a real coup,' said Anne DaSilva Tella, Rochester's assistant commissioner of the Department of Neighborhood and Business Development."⁷⁶

Other similar projects have been completed in Madrid, Montreal, New York City, Oakland, Paris, Portland, Providence, Seattle, and Seoul, with more planned in Detroit and Dallas.⁸⁰

⁷⁴ <https://www.cnu.org/our-projects/highways-boulevards/resources>

⁷⁵ <https://www.thirdway.org/memo/community-replenishment-undoing-the-damage-of-urban-renewal>

⁷⁶ <https://www.lincolnst.edu/publications/articles/2020-03-deconstruction-ahead-urban-highway-removal-changing-cities>

⁷⁷ <https://www.cnu.org/publicsquare/2020/01/22/park-east-transformative>

⁷⁸ <https://www.cnu.org/highways-boulevards/model-cities/chattanooga>

⁷⁹ <https://www.nytimes.com/interactive/2021/05/27/climate/us-cities-highway-removal.html>

⁸⁰ <https://www.cnu.org/our-projects/highways-boulevards/completed-h2b-projects>

Conclusion

The prospect of redesigning the BSB Project is daunting, but the benefits of significantly expanding the landlocked urban core and concretely advancing the economic interests of the region would be transformational. With a well-developed strategy for district infrastructure, urban design, equity, housing affordability, economic development, land disposition, and project financing & implementation, the increased cost of the BSB Project's redesign would be a worthy investment. A unique opportunity exists now to explore the feasibility of a redesign, and a coalition of stakeholders will support such an exploration. Projects like this have been successfully completed before in other communities and here in Cincinnati. Greater Cincinnati leadership should seize this great, once-in-a-generation opportunity to fundamentally strengthen the economic health and overall competitiveness of the region, through a BSB Project redesign.

**Brent Spence Bridge Corridor Project
Response to Public Comment
Position Paper**



**Ohio Department of Transportation Response to Public Comment:
*Working Position Paper: Redesign of the Brent Spence Bridge Project***

EXECUTIVE SUMMARY

A group of citizens prepared a detailed public comment (*Working Position Paper: Redesign of the Brent Spence Bridge Project by Ryan Laber, P.E.*) in response to the Brent Spence Bridge Corridor (BSBC) project being undertaken by the Ohio Department of Transportation (ODOT) and the Kentucky Transportation Cabinet (KYTC).

The project team carefully reviewed the suggestions from this position paper and provided a detailed response, which includes technical and engineering analyses. This summary outlines the many project priorities where there is agreement and offers the technical rationale for why the primary focus of the position paper – lowering I-75 and creating a street grid system in Downtown Cincinnati – is unsafe for the driving public and impractical to construct.

Discussion

I. Public engagement is the cornerstone of everything we do

The Ohio Department of Transportation (ODOT), Kentucky Transportation Cabinet (KYTC), and full leadership team of the Brent Spence Bridge Corridor (BSBC) project, is fully committed to robust public involvement to gain insights and receive feedback as part of the project development process.

ODOT and KYTC have engaged in thorough public involvement since the inception of the project and have worked closely with local partners to incorporate changes to the project. These adjustments have already significantly reduced the impact to local municipalities while investing in underserved populations on both sides of the river.

II. The Brent Spence Bridge Corridor project provides an opportunity to address shared community priorities, including:

- Reconnecting communities through multi-modal and pedestrian-friendly improvements
- Returning public land for local use
- Separating local and through traffic to improve safety and support better access to the Covington and Cincinnati business districts
- Honoring local requests to separate highway runoff from the combined sewer system
- Investing in much-needed quality of life amenities in urban communities within the corridor
- Providing aesthetic features that establish a more dynamic sense of place in urban neighborhoods
- Completing and investing in necessary bike, pedestrian, and roadway connections in both Cincinnati and Covington that will enhance connections between the east and west sides of the freeway and will accommodate the recent RAISE and INFRA investments in the City of Cincinnati
- Continuing public engagement as project plans are refined

The project team continues to work hand-in-hand with local stakeholders to identify every opportunity to reduce the footprint of the project and invest in changes that support and connect communities throughout the corridor.

III. Discussion regarding the ideas in the position paper

Two of the concepts presented were previously evaluated and dismissed during the study phase of the BSBC project. The first was a modification to an alternative (Alternative B) studied in 2009, which resulted in negative impacts to communities, residences, businesses, regulated materials sites, and utilities – impacts that were substantially higher than others under consideration. In addition, this option had a more complex design, higher cost, and posed a more significant constructability risk when compared to other alternatives.

The position paper also presented a modification to another alternative (Concept 85), which was originally developed during a Value Engineering Workshop in 2012. This alternative was removed from consideration because of negative impacts to parks and/or historic resources, the City of Cincinnati's opposition to reducing east-west connectivity and alternatives that shift I-75 to the west, impacts to the United Parcel Service (UPS) in Ohio, and engineering concerns about the angle of the new companion bridge across the Ohio River.

The third concept involved “depressing” I-75 with parallel frontage roads, similar to the layout for I-71 east of the project area (known locally as Fort Washington Way). This concept also relocates I-71 and U.S. 50 by carrying them across the BSB corridor and creating a new connection with I-75 to the north.

This concept would be unsafe for the driving public and impractical to construct for the following reasons:

- Lowering I-75 to the extent proposed in public comments would require the grade from the Central Business District to the bridges crossing the Ohio River to be as much as 8%, which is unsafe and impractical. For context, the current uphill grade for traffic moving southbound on I-71/75 from the Ohio River to Dixie Highway is 5%.
- Lowering I-75 and creating a street grid system that would eliminate the option for traffic to move continuously from the interstate to the local network system and replace it with a series of signalized intersections is not feasible. This solution, as proposed, does not meet the objectives for the project; specifically, it does not address congestion and does not address the crashes we see as a result of congestion.
- Providing a street grid design over I-75 would create a larger footprint, reducing the amount of land that could be reclaimed for local use.
- Eliminating the proposed local connections in favor of a street grid concept would bring longer travel times and increased congestion on local streets.
- Changing connections to I-71/U.S. 50 as proposed in other concepts does not take into account historic resources and impacts to local communities. This preliminary idea by proposers would need further vetting as a standalone transportation project outside of the BSBC effort.

Each of these points is discussed in more detail in the full report that follows.

IV. Well-reasoned analysis that reflects local input will move our region forward

The Brent Spence Bridge Corridor project presents the opportunity to upgrade aging infrastructure and invest in underserved populations that have suffered from disinvestment for decades.

The passage of the Bipartisan Infrastructure Law has created a once-in-a-generation opportunity to improve the quality of life for the millions of Americans who use the federal highway system to travel between Ohio and Kentucky and beyond.

The BSBC project will:

➤ Establish more multi-modal connections

In Cincinnati, the project currently includes sidewalk and shared use path expansions and improvements across the following east-west connectors: 6th St., 7th St., 8th/9th St., Linn St., Freeman Ave. (including pedestrian bridge to W. Court St.), Ezzard Charles Dr., W. Liberty St., Findlay St., Bank St., Harrison Ave., and Winchell Ave. from 6th St. to Ezzard Charles. In Covington, improvements are planned on 5th St., 9th St., Pike St., and 12th St.

In addition, the project team continues to work with the Cincinnati and Covington to collaborate on reducing the project's footprint and improving multi-modal connections.

➤ Reduce flooding impacts and address infrastructure inequities in low-income communities

ODOT is coordinating with the Metropolitan Sewer District to build storm sewers that will separate combined sewers and/or provide retention to mitigate adverse effects on low-income communities that are disproportionately impacted by flooding that occurs during extreme rain events, including basement flooding in a vulnerable Health Improvement Zone in the Mill Creek Valley where existing combined sewer systems can flood the creek with sewage.

KYTC has signed a Memorandum of Understanding with the City of Covington to also separate interstate runoff from combined sewers that create flooding issues for disadvantaged communities in the Willow Run watershed.

➤ Engage the public

- Upcoming one-on-one meetings with directly-affected communities are being scheduled now
- Additional neighborhood meetings will be held later this year
- Two public hearings will be held in 2023
- BrentSpenceBridgeCorridor.com provides project information and an opportunity to submit feedback; members of the public who wish to submit a comment and/or sign up for periodic project updates can visit the project website [here](#).
- In addition, the team has met with a [Project Advisory Committee](#), which represents constituents throughout the project area to keep them apprised of the project's progress and receive feedback. An [Aesthetics Committee](#) – a subcommittee of this group – is in place to provide input on the design and appearance of the new bridge and corridor.

The Brent Spence Bridge Corridor Project is ready to go. The project team is prepared to break ground in late 2023 and looks forward to working with all members of the public who have an interest in this transformative project. For more information about the Brent Spence Bridge Corridor project, please visit the project website at [BrentSpenceBridgeCorridor.com](#).



Response to Public Comment

Working Position Paper: Redesign of the Brent Spence Bridge Project

TO: KYTC and ODOT

FROM: HNTB Corporation

DATE: August 31, 2022

RE: Brent Spence Bridge Corridor Project | ODOT PID 89068 | KYTC Project Item No. 6-17

Introduction

The purpose of this memorandum is to provide a response to three alternative design concepts for the Brent Spence Bridge (BSB) Corridor Project as presented in the *Working Position Paper: Redesign of the Brent Spence Bridge Project (Position Paper)* prepared by Ryan Laber, P.E. that includes concepts presented by the Bridge-Forward Cincinnati Coalition (December 21, 2022). The concepts in the *Position Paper* were first presented to the Ohio Department of Transportation (ODOT) on January 7, 2022. The full *Position Paper* was provided to the Kentucky Transportation Cabinet (KYTC) and ODOT via email on January 17, 2022. The stated goal of the *Position Paper* was to develop concepts for redesigning the project to promote economic development in the region.

Concepts Previously Studied and Dismissed

Two concepts presented in the *Position Paper* were variations of concepts previously evaluated and dismissed during the study phase of the BSB Corridor Project, as discussed in the following sections.

Modified Alternative B

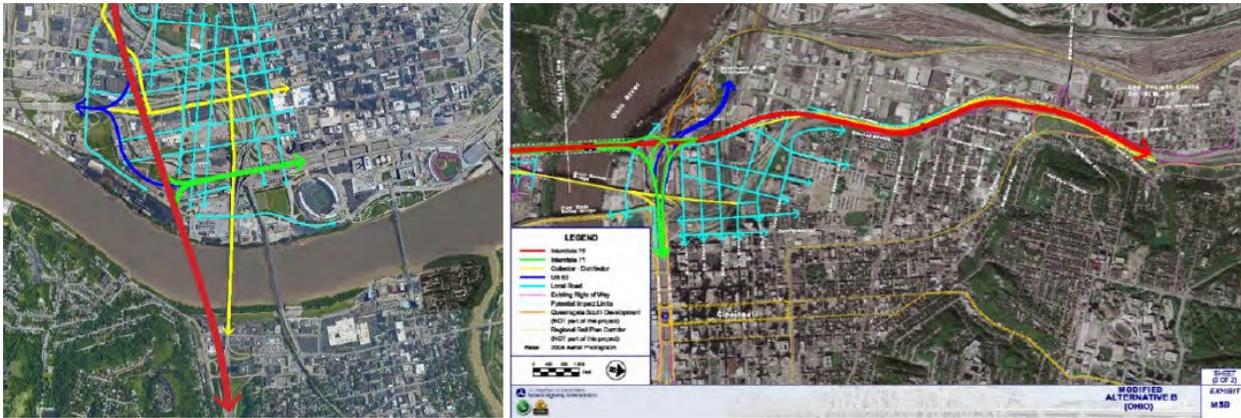
The *Position Paper* presents a modification to Alternative B from the 2009 *Conceptual Alternatives Study (CAS)* prepared for the BSB Corridor Project (see Figure 1). The CAS concluded that Alternative B resulted in adverse impacts to communities, residences, businesses, regulated materials sites, and utilities, which were substantially higher than other alternatives under consideration. In addition, Alternative B had substantially greater overall complexity, constructability risk, and cost when compared to other alternatives. Finally, the concept was strongly opposed by both the City of Cincinnati, Ohio and the City of Covington, Kentucky. Therefore, it was removed from further consideration.

Alternative B was developed based on extensive traffic operational analyses that established the traffic movements required to maintain local connectivity and acceptable traffic flow through the project area. In addition, ramp layouts – including high speed interstate to interstate connections between I-71 and I-75 – were established based on extensive geometric analysis. This alternative was developed to minimize impacts, therefore there are limited (if any) opportunities to “scale back and right-size”¹ Alternative B further such that the overriding negative impacts that led to its dismissal would be reversed.

¹ *Working Position Paper: Redesign of the Brent Spence Bridge Project*, page 25.



Figure 1: Modified Alternative B



Source: Working Position Paper: Redesign of the Brent Spence Bridge Project, page 3.

Figure 2: Modified Concept 85



Source: Working Position Paper: Redesign of the Brent Spence Bridge Project, page 3.

Modified Concept 85

The *Position Paper* also presents a modification to Concept 85, which was developed during a Value Engineering Workshop for the BSB Corridor Project in October 2012 (see Figure 2). The *Position Paper* states that “Concept 85 was not advanced further, likely because the [project] team did not feel they had the license to pursue a project requiring relatively more right-of-way acquisition².” However, the project team evaluated Concept 85 and removed it from further study due to a number of factors, including:

- Additional impacts to parks and/or historic resources;

² Working Position Paper: Redesign of the Brent Spence Bridge Project, page 13.



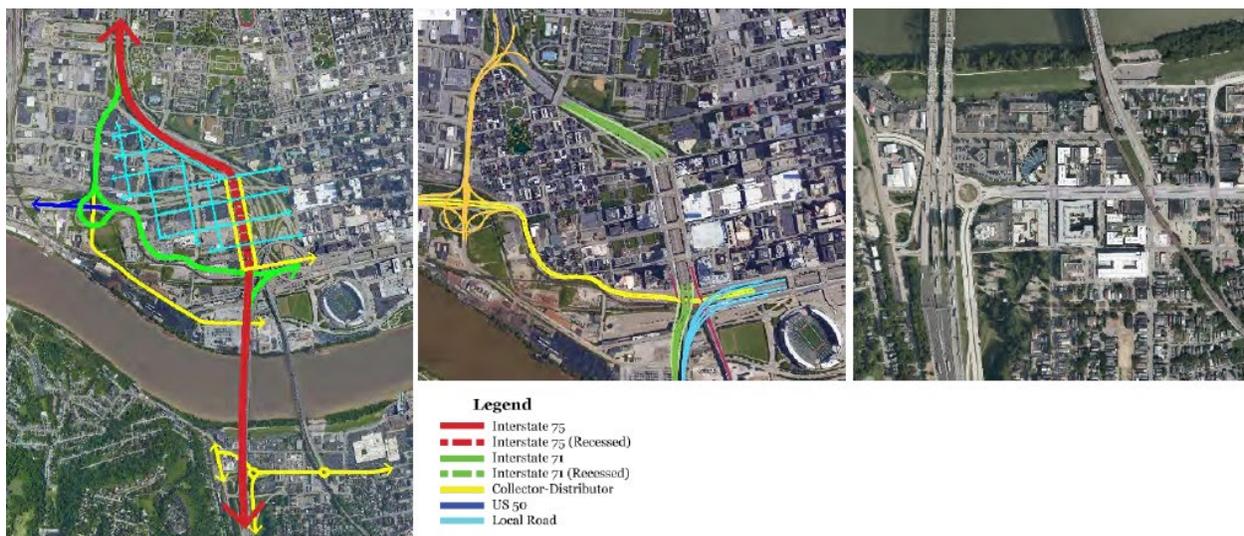
- City of Cincinnati opposition to reducing east-west connectivity and alternatives that shift I-75 to the west³;
- Impacts to United Parcel Service (UPS) in Ohio; and
- Engineering concerns related to a new bridge that crosses the Ohio river on a skew.

Given the above, the modifications to Concept 85 presented in the *Position Paper* – recessing the east-west connection between US 50 and Fort Washington Way – would not address the factors for which it was removed from further study.

Bridge-Forward Cincinnati Concept by Brian Boland

The final concept presented in the *Position Paper* was originally developed by Brian Boland (the Boland Concept). The Boland Concept (see Figure 3) depresses I-75 with parallel frontage roads similar to the layout of I-71 just east of the project area, which is locally known as Fort Washington Way. The Boland Concept also relocates I-71 and US 50 by carrying them across the BSB corridor and creating a new connection with I-75 to the north via Freeman Avenue. Finally, it reconfigures local roadways in Covington, Kentucky. The following sections discuss the geometric and traffic operational feasibility of the Boland Concept.

Figure 3: The Boland Concept



Source: *Working Position Paper: Redesign of the Brent Spence Bridge Project*, page 3.

³ The *Position Paper* acknowledges this opposition and suggests the proposed modified design addresses these concerns, and the value engineering workshop also noted the positive nature of some of these refinements. ODOT and the City of Cincinnati are working collaboratively to implement the BSB Corridor Project. While the City has requested refinements to the current design to better meet their needs – and in many cases, the design has been refined to accommodate these requests – they have not expressed a desire to incorporate whole-scale changes comparable to those presented with Modified Concept 85.



Depressed I-75

Although ODOT supports the idea of depressing I-75 through downtown Cincinnati, the concept simply is not feasible for this specific location due to the following limitations:

- Any bridge over the Ohio River must meet U.S. Coast Guard clearance requirements, which means the bottom of the bridge will need to be 55 feet above the flowline of the Ohio River.
- Once the interstate passes over the Ohio River, it cannot descend directly into downtown Cincinnati. First, it must stay elevated to cross active CSX rail lines between Pete Rose Avenue and 3rd Street.
- Any design must accommodate a complicated system of mainline and ramp movements to provide local access and continuity along I-71, I-75, and US 50. Weaving these ramps through the project area requires a complex tiered bridge system with the highest tier rising over 50 feet above the ground (see Attachment 1).

To meet these geometric constraints, the preferred alternative for the BSB Corridor Project currently descends toward downtown Cincinnati at a 4- to 5.5-percent grade. Depressing the roadway any further as presented in the Boland Concept would require substantially steeper roadway grades (up to 7.7 percent). For reference, ODOT's *Location and Design Manual, Volume 1* lists the maximum grade for urban interstates in similar terrain as 6 percent. Furthermore, given the complicated geometry, any changes to the mainline profile would cascade throughout the BSB interchange area, resulting in prohibitively steep grades along other ramps and C-D roads. Introducing grades of this magnitude would require design exceptions and present traffic operational and safety concerns, particularly considering the high volumes of heavy truck traffic traveling through the corridor. This would be contrary to the project's approved purpose and need, which includes improving traffic flow and safety and correcting geometric deficiencies through a key regional and national transportation corridor.

The geometric constraints in the corridor also affect local cross streets in the Boland Concept. Because the I-75 mainline and access ramps cannot be depressed further, local Cincinnati cross streets must be raised substantially to pass over I-75. In addition, 6th Street must be raised substantially to connect to eastbound US 50. These changes would require grades ranging from 5 to 9 percent on 4th Street, 5th Street, and 6th Street⁴ (see Figure 4 and Attachment 2). ODOT's *Location and Design Manual, Volume 1* lists the maximum grade for urban arterial roadways as 7 percent. However, the manual states that maximum design grades should be used infrequently, rather than a value to be used in most cases. Grades of this magnitude are not only undesirable for vehicular traffic, but they also present substantial mobility and accessibility concerns for cyclists and pedestrians. The Americans with Disabilities Act (ADA) requires facilities that accommodate pedestrians to be constructed with grades of 5 percent or less.

⁴ Potential exists to reduce the grades on 6th Street with more extensive engineering study and refinement. However, the constraints on 4th and 5th Street would remain.



When compared to the preferred alternative for the BSB Corridor Project, elevating the downtown Cincinnati cross streets over I-75 would substantially increase the project footprint on the west side of I-75 to tie into the existing elevations near 4th Street. This additional footprint would result in new impacts to existing businesses and potential developable land. For example, the Boland Concept would create steep slopes that would negatively affect potential development opportunities on 2.5 to 3.0 acres of land east of I-75 that is currently slated to be transferred to the City of Cincinnati once the project is completed.

Parallel Frontage Roads

The Boland Concept includes parallel frontage roads that intersect the local street network in a grid configuration (see Figure 3). However, the *Position Paper* did not include a traffic analysis to determine the number of lanes required for the frontage road system to operate. A high-level screening of traffic operations was conducted using available traffic data to determine the minimum number of lanes required to provide acceptable levels of service⁵ for the frontage road intersections. Based on the screening, the frontage roads would require three to four lanes for through traffic with additional left and right turn lanes at each intersection to eliminate any adverse operations on the freeway system. The local cross streets would also require additional lanes for turning traffic. The minimum lanes required for the parallel frontage roads are depicted schematically in Figure 4.

The BSB Corridor Project must serve both long-distance interstate traffic and local trips. When compared to the preferred alternative for the project, the Boland Concept eliminates a C-D system that serves local movements. As a result, traffic traveling between Covington and downtown Cincinnati would be required to utilize other congested cross-river routes and the signalized frontage road intersections, resulting in longer travel times and increased congestion on local streets.

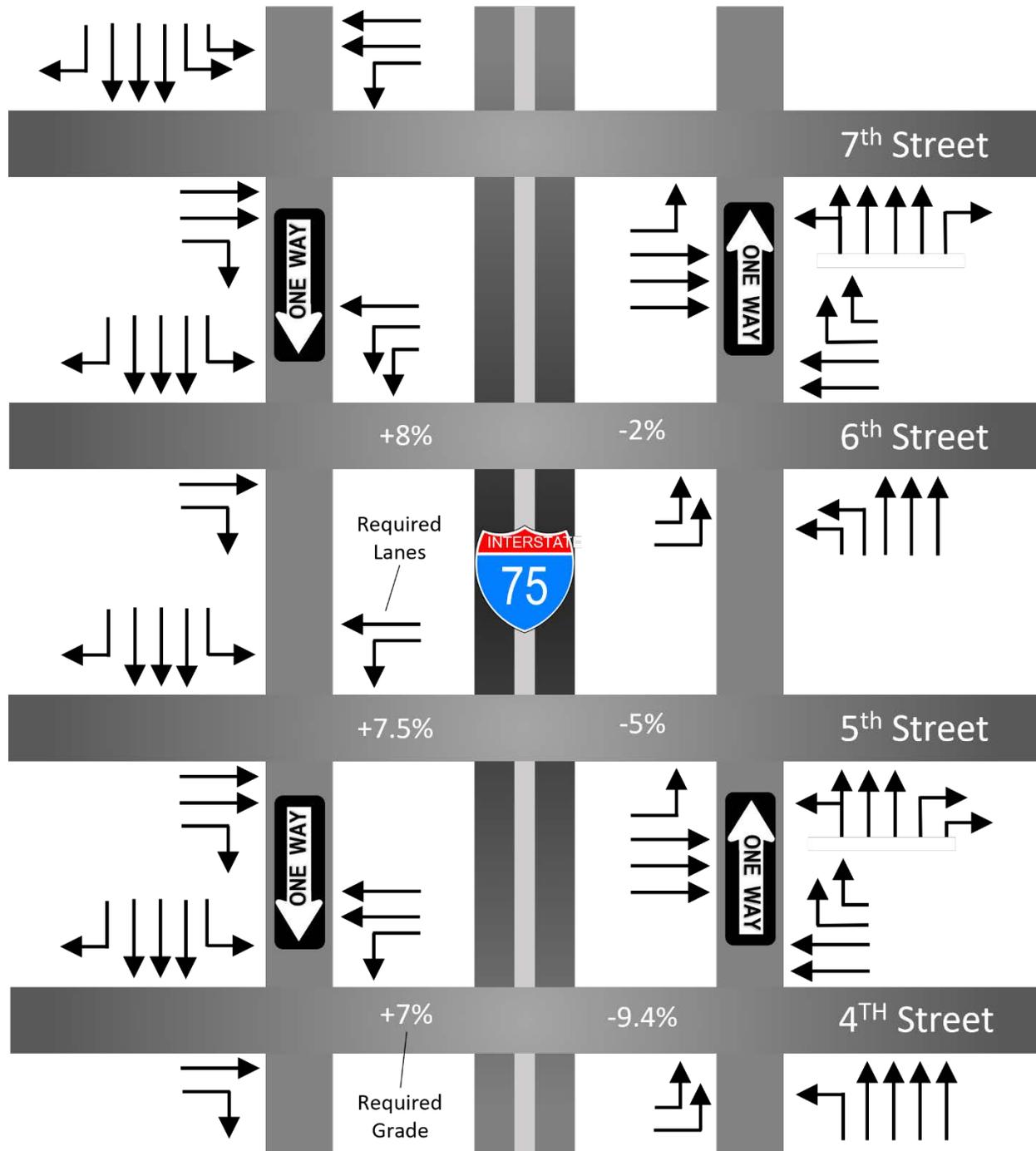
The *Position Paper* states that the preferred alternative for the BSB Corridor Project would form a barrier and provide “inadequate vehicle, bicycle, and pedestrian connections between the Cincinnati [Central Business District] CBD and Queensgate⁶.” However, the preferred alternative maintains or improves local connectivity for both vehicular and pedestrian/bicycle traffic. A system of C-D roads and ramps provide connections between the interstate highways and 4th Street, 5th Street, 6th Street, 7th Street and other streets in Queensgate, the Riverfront, and the CBD. The C-D roads also provide north-south interconnectivity between local streets. Furthermore, the east-west connectivity of US 50, 6th Street, 7th Street, and other local roads is maintained. Attachment 3 includes a color-coded map illustrating the interstate and local roadway connections in the Queensgate, the Riverfront, and downtown Cincinnati areas. In addition, the preferred alternative maintains existing sidewalk connections in downtown Cincinnati and adds new pedestrian and bicycle shared use paths parallel to and across I-75.

⁵ Level of Service (LOS) is a way of describing the amount of traffic congestion on a roadway by “grading” it on a letter scale from A (best) to F (worst). LOS A represents near ideal traffic flow, while LOS F represents a breakdown of traffic flow. LOS D is considered acceptable urban intersections such as those in downtown Cincinnati.

⁶ *Working Position Paper: Redesign of the Brent Spence Bridge Project*, page 14.



Figure 4: Schematic Depiction of Boland Concept at Frontage Road Intersections





I-71 and US 50

The purpose and need for the BSB Corridor Project did not identify any specific needs related to the I-71 or US 50 corridors beyond maintaining connections to these existing corridors. As such, the project scope addresses transportation needs along the I-75 corridor while maintaining existing connections to I-71 and US 50 in Cincinnati. Furthermore, the continuity of US 50 east and west of I-75 must be maintained (see Attachment 3). Relocating and realigning I-71 and US 50 west of I-75 would need to be investigated as part of a separate regional project with an approved purpose and need specific to that undertaking. That project would need to address a number of issues, including but not limited to: historic opposition to moving US 50 due to potential impacts to Longworth Hall; impacts to businesses and access in Queensgate; impacts to regional travel connections; and impacts to traffic operations along I-71, I-75, US 50, Freeman Avenue, and other local routes.

Covington

The Boland Concept proposes accommodating local cross-river trips via the local street grid in Covington, Kentucky. To better accommodate these trips, it proposes reworking the street grid in Covington, including converting 4th Street to a two-way roadway, removing 5th Street, and installing roundabouts. The *Position Paper* maintains that the Boland Concept addresses concerns Covington officials have expressed about the project in the past. However, Covington officials have consistently expressed concerns about traffic traveling through their local street network to other Ohio River bridges during construction of the BSB Corridor Project. The Boland Concept would exacerbate these concerns by requiring all traffic with local origins and destinations to travel through the street network in Covington and cross the Ohio River on other local bridges. KYTC recognizes Covington's concerns about increased traffic on the local street network and is working cooperatively with the City of Covington to address them as they implement the preferred alternative for the BSB Corridor Project. To that end, KYTC and the City signed a Memorandum of Understanding on June 15, 2022 to further evaluate traffic impacts and to implement context sensitive design principles in conjunction with the BSB Corridor Project.

Furthermore, under the existing conditions, incidents on the BSB force traffic onto the local street network, often overburdening the system⁷. The construction of a new companion bridge and C-D system introduces additional resiliency into the local and regional transportation network by providing additional options for maintaining cross-river traffic if an incident or future construction or maintenance activities occur, options that would not be available with the Boland Concept.

Finally, the extensive changes to the Covington street grid that are proposed in the Boland Concept are beyond the purpose and need for the BSB Corridor Project, which focuses on the I-71/I-75 interstate corridor. Any network-wide changes to the street grid in Covington would need to be investigated as part of one or more separate projects with an approved purpose and need specific to that undertaking that is developed in consideration of the City's long-term goals.

⁷ <https://local12.com/news/local/trucks-causing-traffic-problems-despite-order-to-stay-on-interstates-after-bridge-closure-cincinnati>



Conclusion

The concepts presented in the *Working Position Paper: Redesign of the Brent Spence Bridge Project (Position Paper)* prepared by Ryan Laber, P.E. that includes concepts presented by the Bridge-Forward Cincinnati Coalition are not recommended for further consideration. Variations of two concepts presented in the *Position Paper* – Modified Alternative B and Modified Concept 85 – were previously evaluated and eliminated from consideration during the BSB Corridor Project preliminary development activities. The modifications proposed in the *Position Paper* would not address the factors for which these concepts were removed from further study. The final concept – the Boland Concept – was not found to be geometrically feasible and would result in a greater project footprint than the current preferred alternative.

KYTC and ODOT share many of the goals articulated by the Bridge-Forward Coalition and summarized in the *Position Paper*, including:

- Minimizing the footprint of the highway;
- Using the interstate primarily as an efficient processor of regional, through traffic;
- Providing a network of safe, multi-modal streets for local traffic; and
- Using only modern, progressive engineering practices.⁸

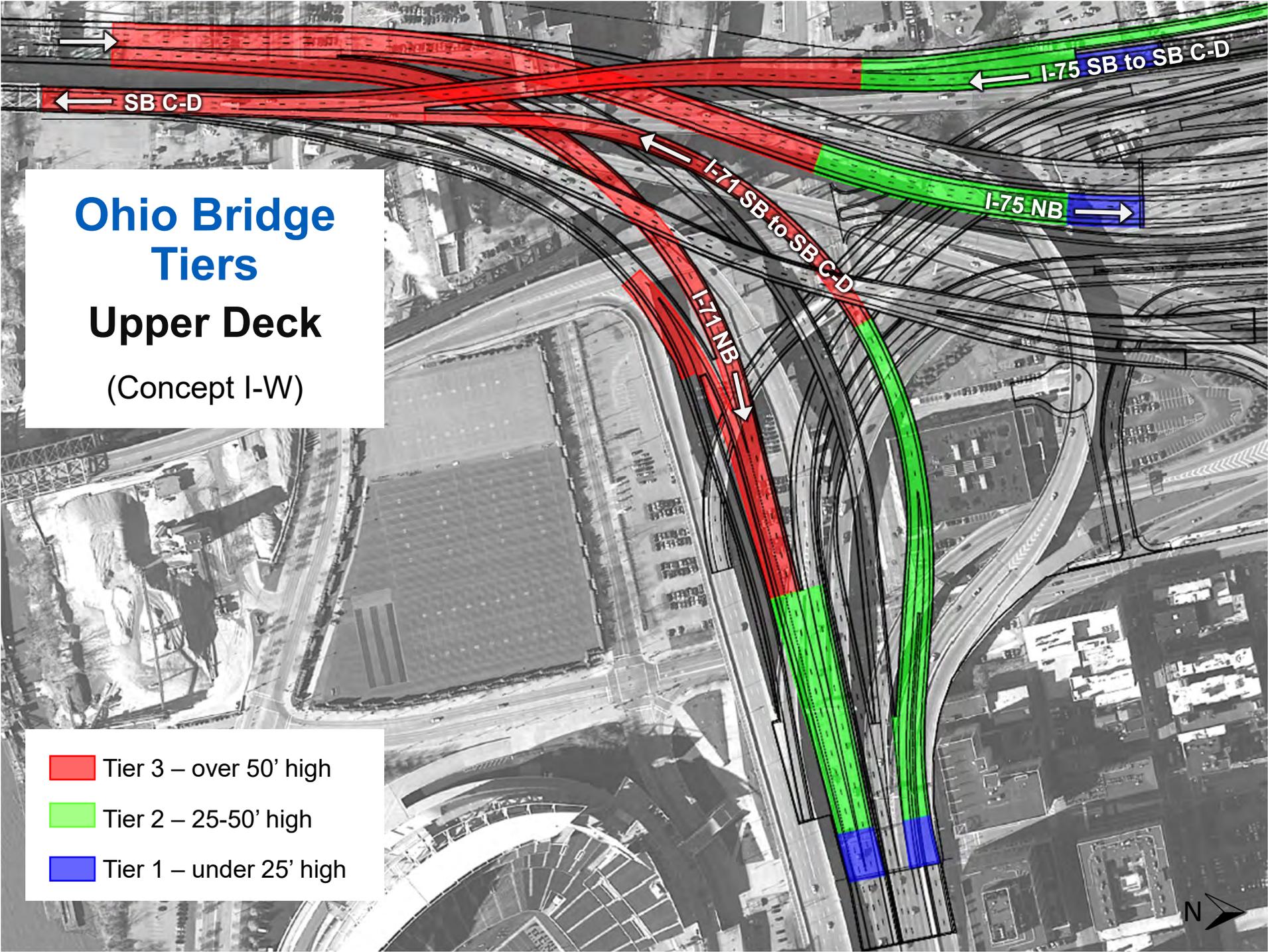
To accomplish those goals, KYTC and ODOT have developed a set of refinements to the preferred alternative since the approval of the project's Environmental Assessment (EA) and Finding of No Significant Impact (FONSI) in 2012. These refinements reconfigured the river crossing to use the existing BSB for local traffic and a new double decker companion bridge to the west for through (interstate) traffic. In addition, performance-based design principles have been incorporated into the project's design, substantially reducing the project's footprint and associated impacts. Multi-modal facilities have been incorporated into the project, and KYTC and ODOT are continuing to coordinate the project with the cities of Cincinnati and Covington to address local concerns while further reducing the highway's footprint and impacts to the communities in the project area.

While the specific concepts presented in the *Position Paper* are not feasible and, as such, will not be considered in the project's Supplemental Environmental Assessment, KYTC and ODOT will continue to incorporate the overriding principles championed by the Bridge-Forward Coalition as the BSB Corridor Project moves toward implementation.

⁸ *Working Position Paper: Redesign of the Brent Spence Bridge Project*, page 22.



Attachment 1: BSB Corridor Project Tiered Bridge System



**Ohio Bridge
Tiers**
Upper Deck
(Concept I-W)

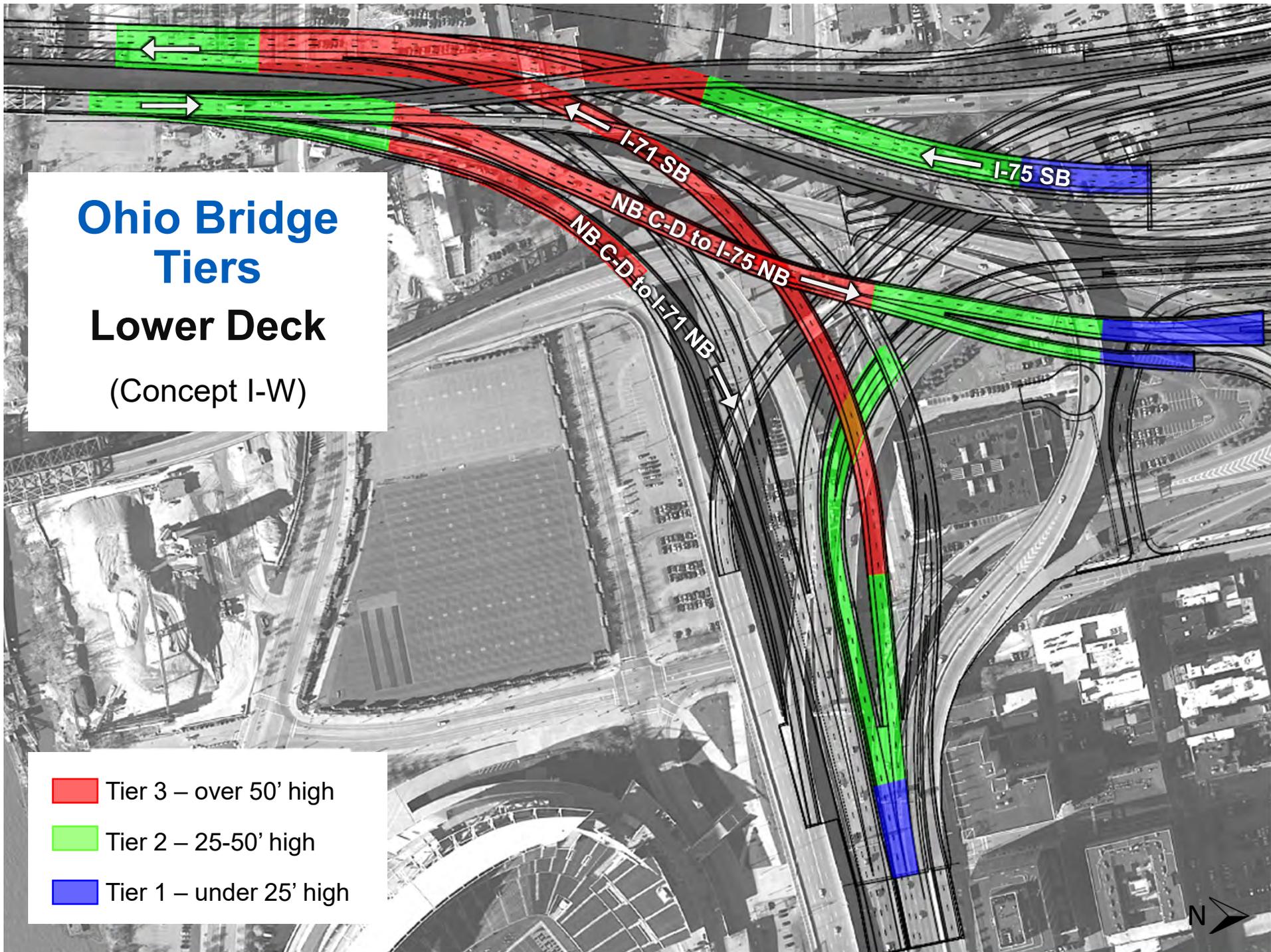
- Tier 3 – over 50' high
- Tier 2 – 25-50' high
- Tier 1 – under 25' high

Ohio Bridge Tiers

Lower Deck

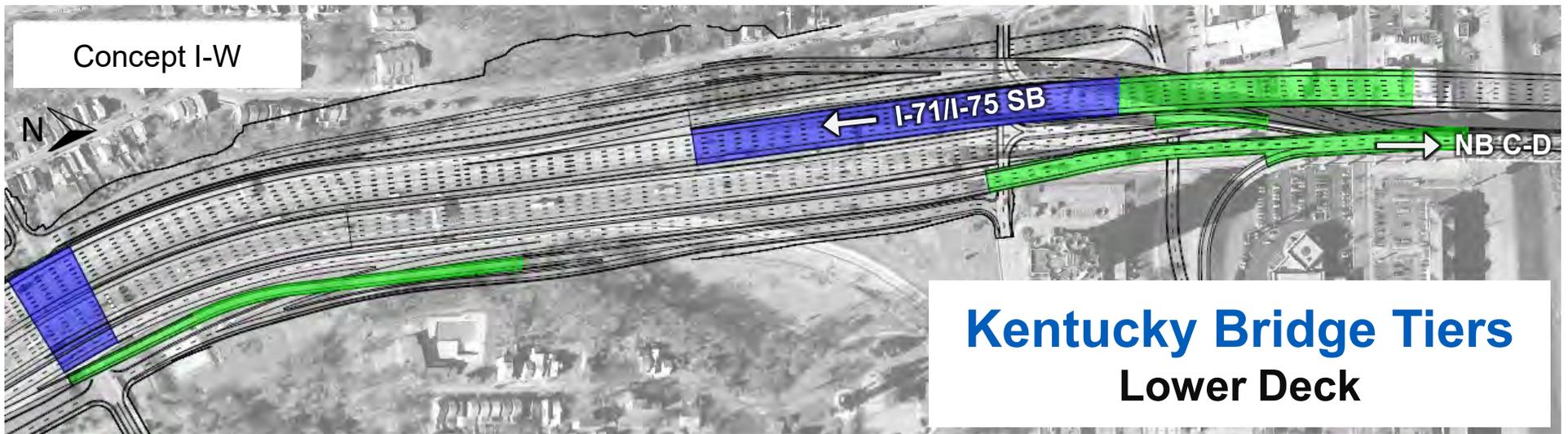
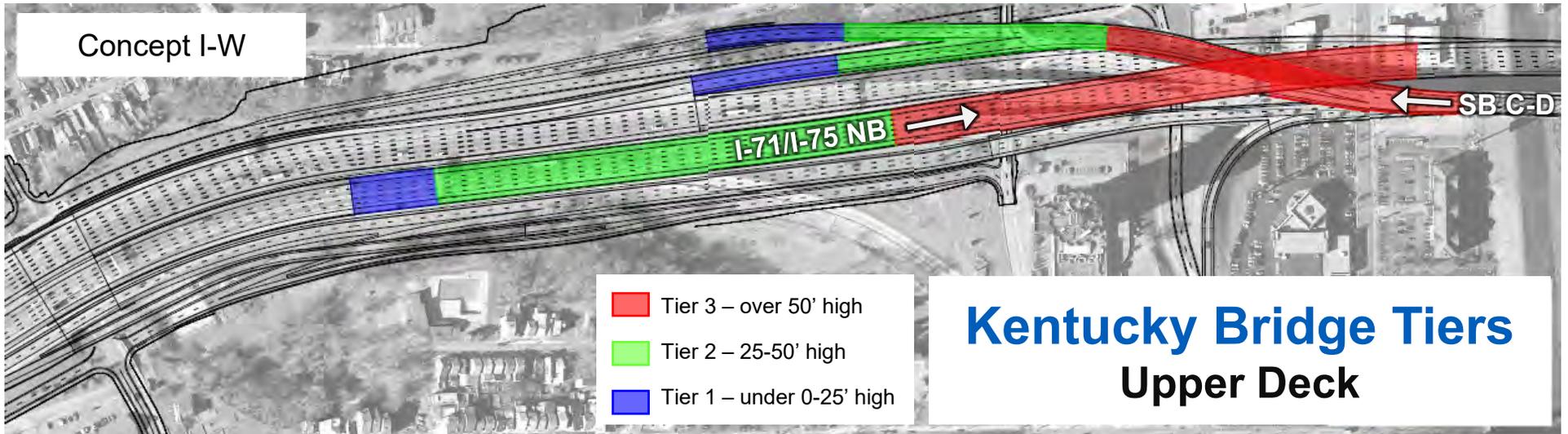
(Concept I-W)

-  Tier 3 – over 50' high
-  Tier 2 – 25-50' high
-  Tier 1 – under 25' high





Project Overview





Attachment 2: BSB Corridor Project Cross Sectional Views

*BSB - CONCEPT I-W
XS CUTS IN PLAN VIEW*

CENTERLINE XS

6TH ST

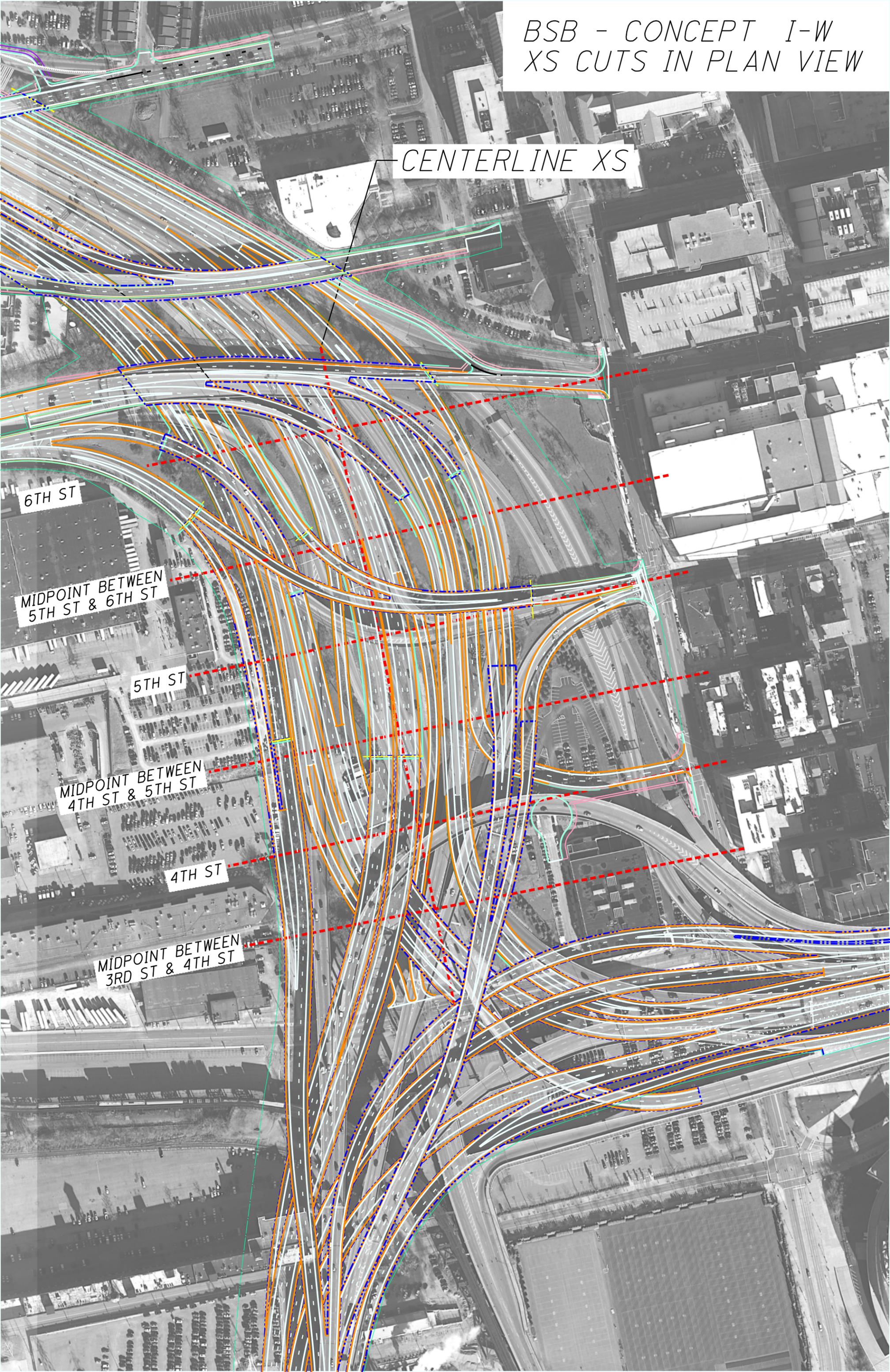
MIDPOINT BETWEEN
5TH ST & 6TH ST

5TH ST

MIDPOINT BETWEEN
4TH ST & 5TH ST

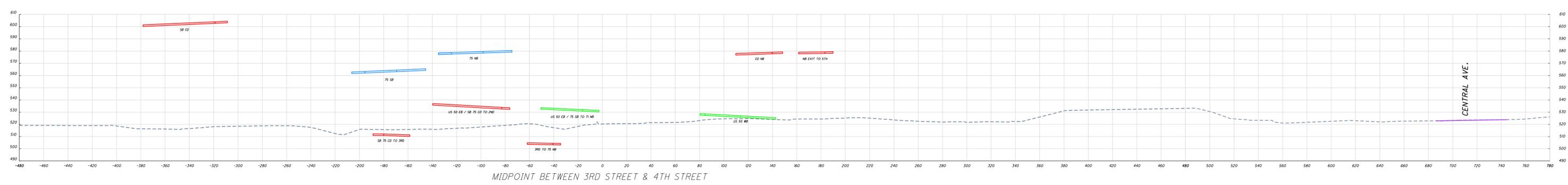
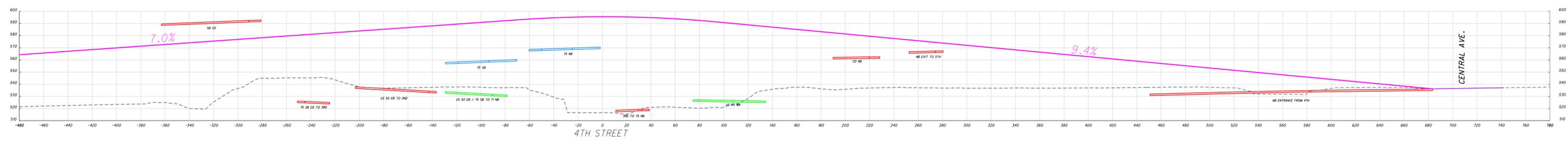
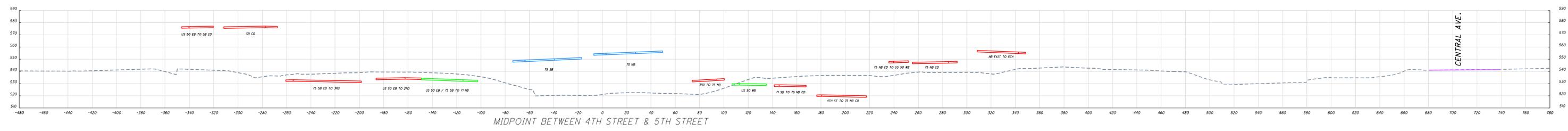
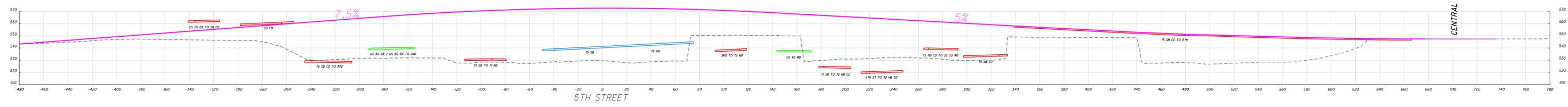
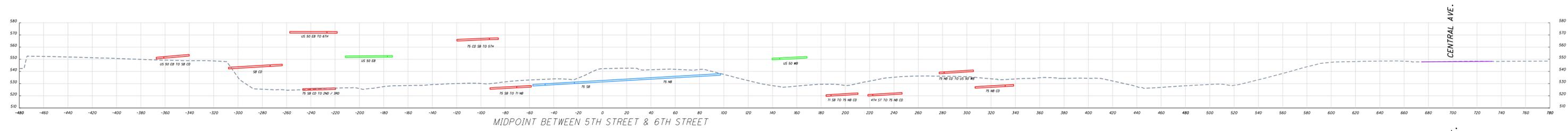
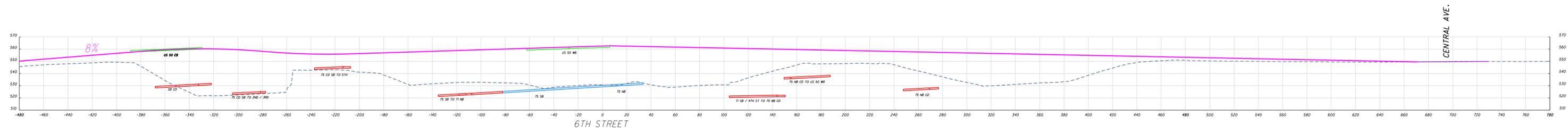
4TH ST

MIDPOINT BETWEEN
3RD ST & 4TH ST



- INTERSTATE
- CD / RAMPS
- US 50
- LOCAL ROADS

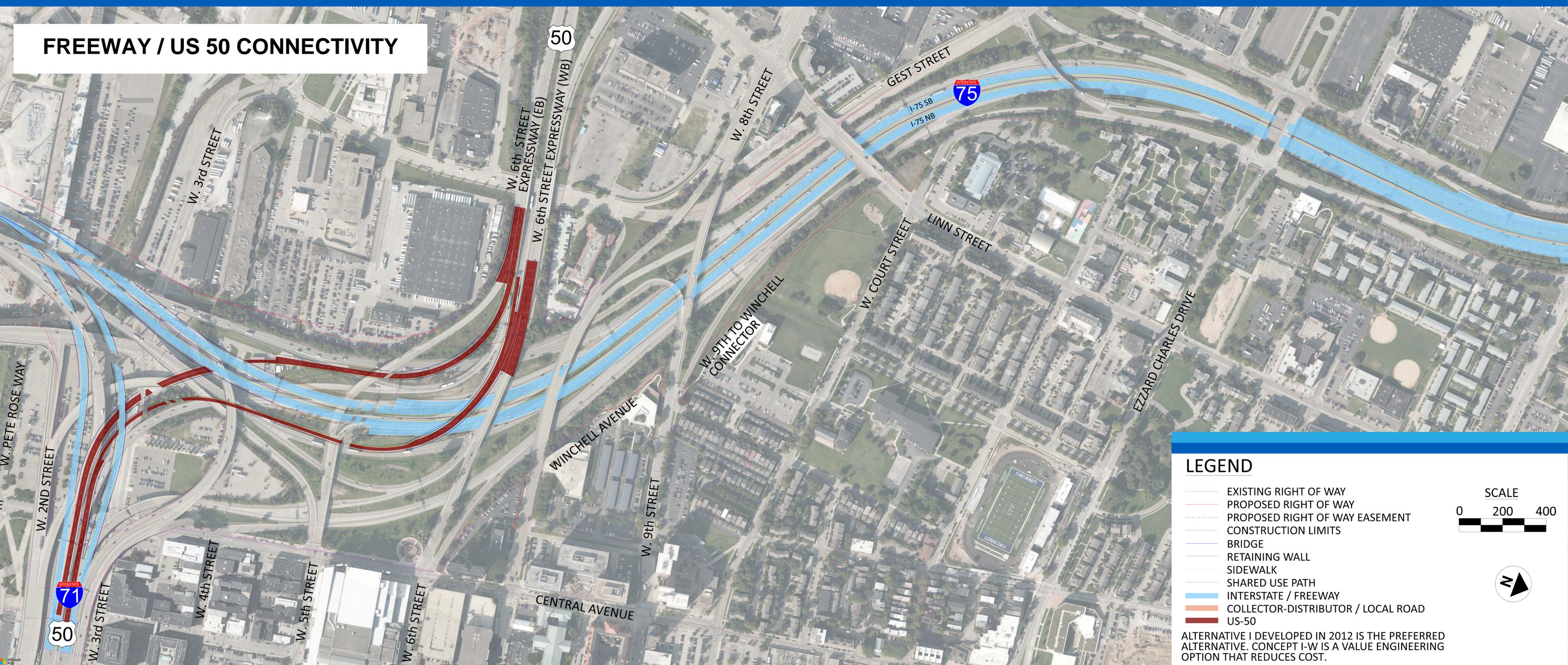
BSB CONCEPT I-W CROSS SECTIONS 4TH ST TO 6TH ST CINCINNATI





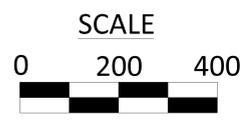
Attachment 3: BSB Corridor Project Interstate and Local Continuity

FREEWAY / US 50 CONNECTIVITY



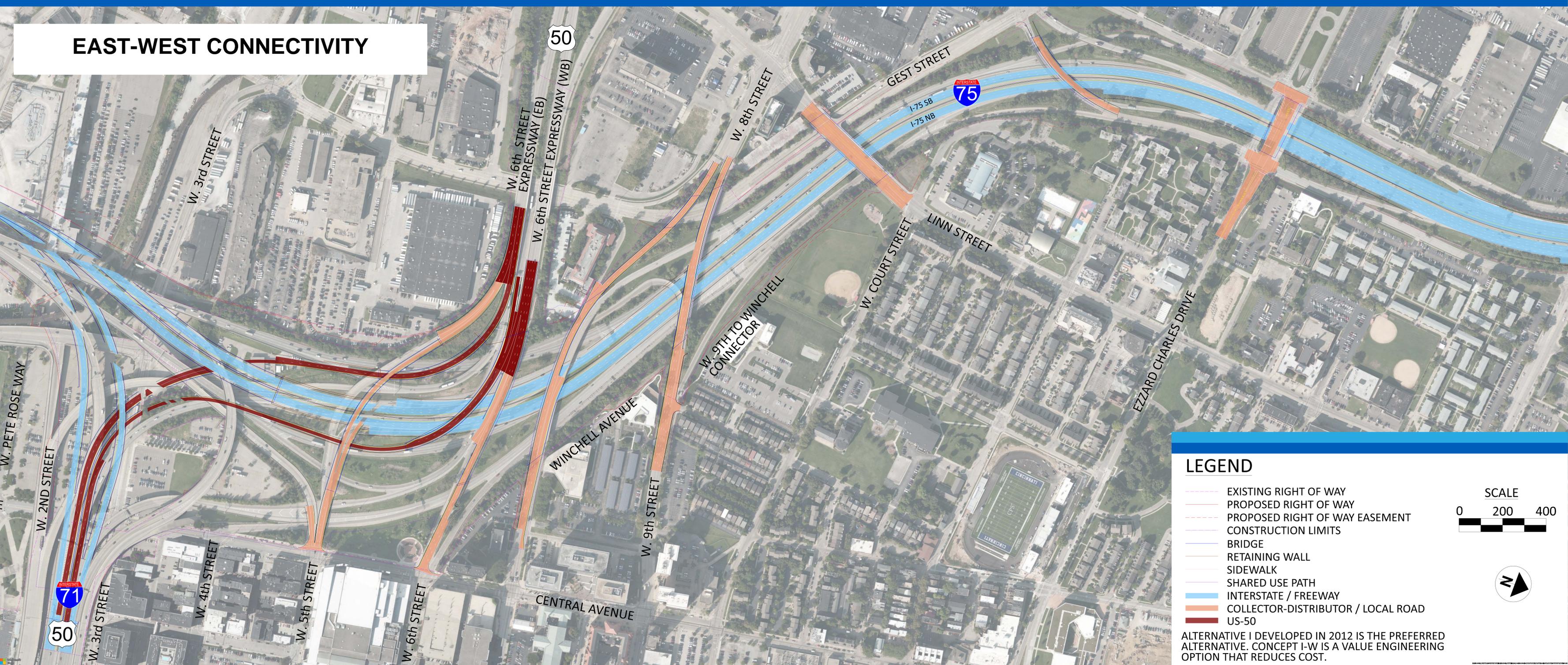
LEGEND

-  EXISTING RIGHT OF WAY
-  PROPOSED RIGHT OF WAY
-  PROPOSED RIGHT OF WAY EASEMENT
-  CONSTRUCTION LIMITS
-  BRIDGE
-  RETAINING WALL
-  SIDEWALK
-  SHARED USE PATH
-  INTERSTATE / FREEWAY
-  COLLECTOR-DISTRIBUTOR / LOCAL ROAD
-  US-50



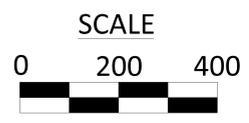
ALTERNATIVE I DEVELOPED IN 2012 IS THE PREFERRED ALTERNATIVE. CONCEPT I-W IS A VALUE ENGINEERING OPTION THAT REDUCES COST.

EAST-WEST CONNECTIVITY



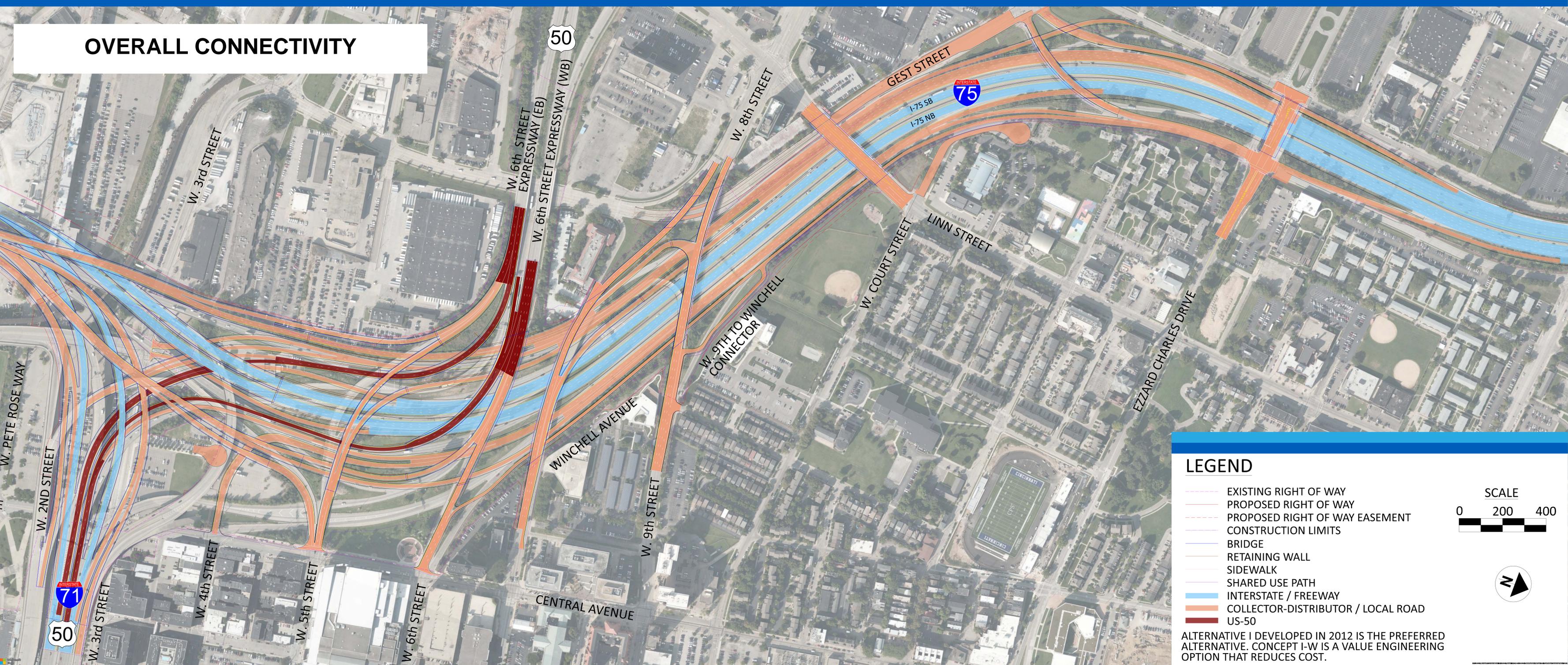
LEGEND

-  EXISTING RIGHT OF WAY
-  PROPOSED RIGHT OF WAY
-  PROPOSED RIGHT OF WAY EASEMENT
-  CONSTRUCTION LIMITS
-  BRIDGE
-  RETAINING WALL
-  SIDEWALK
-  SHARED USE PATH
-  INTERSTATE / FREEWAY
-  COLLECTOR-DISTRIBUTOR / LOCAL ROAD
-  US-50



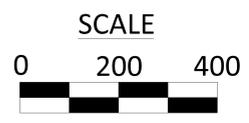
ALTERNATIVE I DEVELOPED IN 2012 IS THE PREFERRED ALTERNATIVE. CONCEPT I-W IS A VALUE ENGINEERING OPTION THAT REDUCES COST.

OVERALL CONNECTIVITY



LEGEND

-  EXISTING RIGHT OF WAY
-  PROPOSED RIGHT OF WAY
-  PROPOSED RIGHT OF WAY EASEMENT
-  CONSTRUCTION LIMITS
-  BRIDGE
-  RETAINING WALL
-  SIDEWALK
-  SHARED USE PATH
-  INTERSTATE / FREEWAY
-  COLLECTOR-DISTRIBUTOR / LOCAL ROAD
-  US-50



ALTERNATIVE I DEVELOPED IN 2012 IS THE PREFERRED ALTERNATIVE. CONCEPT I-W IS A VALUE ENGINEERING OPTION THAT REDUCES COST.

**Brent Spence Bridge Corridor Project
Response to Public Comment
Concept to Reconfigure the US 50 Connection**

Response to Public Comment Concept to Reconfigure the US 50 Connection

TO: KYTC and ODOT

FROM: HNTB Corporation

DATE: August 29, 2023

RE: Brent Spence Bridge Corridor Project | ODOT PID 89068 | KYTC Project Item No. 6-17

Introduction

The Bi-State Management Team (BSMT), which is comprised of the Kentucky Transportation Cabinet (KYTC) and the Ohio Department of Transportation (ODOT) met with representatives and proponents of Bridge Forward on five occasions in 2023 as part of the community outreach program for the Brent Spence Bridge Corridor. These meetings also included participation by the City of Cincinnati, Hamilton County, and the Federal Highway Administration (FHWA). During those meetings, overall project goals and constraints were shared and discussed, including the goal of minimizing the project footprint while still meeting the overall transportation goals. Refer to Attachment 1 for the list of design-build contract objectives that the BSMT shared with Bridge Forward. After the third meeting in March 2023, Bridge Forward prepared and presented an alternative concept for connecting US 50 to I-75, I-71, the collector-distributor (C-D) system, and the local road system. In April 2023, Bridge Forward prepared a revised version of the same concept and presented it to the Cincinnati City Council on May 3, 2023. Bridge Forward has continued to update and prepare concept revisions over the past few months. In 2023, proponents of Bridge Forward hired an engineering firm to refine the Bridge Forward concepts and submitted the latest version on June 26, 2023. Below is the BSMT's initial high-level review and comments on the latest version; the June 26, 2023 submittal. See attachment 2 for a layout of both the latest Bridge Forward concept and Refined Alternative I (Concept I-W).

US 50 Connection (Bridge Forward June 26 Concept)

The Bridge Forward Concepts approach the design of the I-75 ramps to and from downtown Cincinnati (referred to hereafter as the “interchange”) differently from Refined Alternative I (Concept I-W) in terms of the overpass and underpass relationships. The Bridge Forward approach is to stack the movements with US 50 moved to the lowest level of the interchange. The stated goal of Bridge Forward is to develop a concept similar to the trenched approach of Fort Washington Way.

The majority of the differences between the Bridge Forward concept and Refined Alternative I (Concept I-W) occur between 7th St and the Ohio River therefore this response focuses on that area. This

response includes a very high-level conceptual review of the Bridge Forward concept by the BSMT focused in five areas:

- 1) Alignment and profile
- 2) Access
- 3) Footprint
- 4) Costs
- 5) Traffic

Alignment and Profile

The Bridge Forward Concept approaches the interchange design differently from Concept I-W by stacking the interchange and moving US 50 to the lowest level of the interchange. This arrangement presents some geometric issues as described below and shown in cross sections in Attachment 3:

- 1) The proposed northbound and southbound local streets at 7th and 8th Streets are 50 feet (west side) and 30 feet (east side) above the existing ground. This places them at the same level as the top of the adjoining buildings.
- 2) The 7th Street crossing over I-75 is 40 feet higher than the existing crossing and results in a 9.5% grade coming up from Gest Street and a 6.5% grade coming up from Central Avenue.
- 3) Both the northbound and southbound local streets are 30 feet to 40 feet higher than the existing ground and Gest and Central Avenue which results in higher grades on the east/west connecting roadways and a potential physical and visual barrier between downtown and the Queensgate neighborhood. This elevation difference is not reflected on the renderings presented by Bridge Forward.
- 4) US 50 as currently proposed in the June 26, 2023 Bridge Forward concept requires a tunnel under mainline I-75. Depending on the final tunnel length, life safety access and an ancillary structure may be necessary, impacting costs, footprint, constructability, and grades and elevations in the area of the tunnel.
- 5) Both the northbound and southbound local streets are at over 9% grade from 3rd to 5th Street.
- 6) The Bridge Forward concept crosses directly over a major Duke Energy gas valve which; due to the high impacts and costs associated with relocation; has been identified as a utility to be avoided.
- 7) The Bridge Forward concept impacts +/- 4 additional acres south of 2nd Street near the Bengals practice facility and game day parking, which are not included in the current project impacts or the construction cost comparison.

In the Bridge Forward Concept, none of the locations listed above meet the project design criteria set during the performance-based design process and could result in operational and safety issues. Correcting the geometry at these locations would require creating more distance between the constraint points and/or less elevation change by revising the levels of the various roadway elements. In both cases, that would require a larger footprint and would impact the additional contiguous developable acreage made available in the Bridge Forward Concept.

Access

In reviewing access to the interstate and C-D system, the Bridge Forward Concept was compared to the access provided in Concept I-W. Most of the major differences were revised with the June 26, 2023 submittal:

- 1) The 3rd Street ramp to northbound I-75 and the C-D roadway system is moved to directly across from the Clay Wade Bailey Bridge.
- 2) The eastbound US 50 to 2nd Street access is moved from the current location to utilizing the local road system from 6th Street to 2nd Street. This would require driving through 4 or 5 additional traffic signals before reaching 2nd Street from EB US 50 or southbound I-75,
- 3) Accessing the 5th Street and Central Avenue from SB I-75 in the Bridge Forward concept requires exiting at 7th St and utilizing the SB arterial road and passing through 3 signals. In the I-W concept the same intersection is accessed through 7th Street and Central Avenue and passes through only one signalized intersection.
- 4) The southbound I-75 off ramp to 3rd Street requires passing through an additional signal.

These revisions all require further analysis of the operational impacts to the local road system and could require more lanes on the local roads, and/or degraded operations on the local road system. Additional impacts would occur at each of the intersections to account for truck traffic and specifically truck turning traffic. This would impact pedestrian crossings and the amount of contiguous developable land being made available. A high level Highway Capacity Software analysis was performed as part of this review and is included in the Traffic section of this response.

Footprint

The stated goal of Bridge Forward has been to reduce the Brent Spence Bridge Corridor Project footprint by 30 acres, allowing for redevelopment opportunities along the west side of the Cincinnati Central Business District. The BSMT and local government agencies have a similar goal of minimizing project footprint and maximizing contiguous developable land.

The Bridge Forward Concept appears to identify approximately 23.7 acres of potentially available land (although the text provided by Bridge Forward states 30 acres). Concept I-W creates approximately

12.9 acres of additional land. However, the most desirable development sites in both plans is the contiguous area west of Central Avenue between 3rd Street and 6th Street. Areas identified north of 6th Street and west of Central Avenue have some constraints and potentially are not as developable. Attachment 4 provides a plan view of the potentially contiguous developable area. The color coding differentiates the contiguous developable land south of 6th Street and east of I-75 from the area north of 6th Street.

As shown in Bridge Forward's Concept, about 5.82 acres of land is shown in various areas north of 6th Street and east of I-75. In this same area Concept I-W shows 2.24 acres available. For a consistent comparison the area at Gest and 9th Street in the Concept is not included in either concept. The intersection layout in the Concept would have operational issues as shown and correcting those could result in much less; if any; acreage available.

In looking at truly contiguous developable land the focus of this review is in the area bound by I-75 to the west, Central Avenue to the east and between 6th Street and 3rd Street. Based on feedback from the City of Cincinnati in the fall of 2022, the BSMT revised Concept I-W to make approximately 9.5 acres of land available west of Central Avenue between 3rd Street and 6th Street. The Bridge Forward Concept creates another 5.5 acres for a total of approximately 15.7 acres. See Attachment 4 for a layout of this area.

The additional developable land shown in the Bridge Forward Concept is a best-case scenario. Cross sections were developed based on the Bridge Forward Concept with the appropriate lane and shoulder widths (See Attachment 3). Based on the cross sections developed by the BSMT for the Bridge Forward Concept, it appears that the amount of developable land available between 6th and 3rd Streets is closer to 15.0 acres. Also, the cross sections show the new arterial/local road 30 to 40 feet higher than Central Avenue and the surrounding land both east and west of the I-75 corridor. This would result in either a visual and/or physical barrier between downtown Cincinnati and the Queensgate neighborhood. This is also not reflected in the renderings provided by Bridge Forward and presented at numerous events.

Costs

A high-level construction cost comparison between the Bridge Forward Concept and Concept I-W was prepared by developing overall lane miles on local streets, US 50, C-D roads, and I-75 for each of the concepts. Costs were then applied to determine an overall cost comparison. See the tables in Attachment 5 for more detail. For consistency the costs applied to the lane miles are those utilized to develop the overall conceptual level costs for the BSB Corridor Project. Where the new arterial road was on significant fill or on structure some additional costs were added to the base lane mile costs to account for the additional materials and construction constraints.

The overall lane miles between the two concepts is almost identical. The differences are in the lane mile splits of local roads and C-D roads. The concept of stacking the freeway system as proposed in the Bridge Forward concept results in more construction complexity and increased construction costs over the more traditional approach in Concept I-W. This results in the June 26, 2023 Bridge Forward concepts lane mile based construction cost being over \$100,000,000 more than Concept I-W. Not considered in the

high-level cost comparison was constructability and maintenance of traffic. Stacking a freeway system as Bridge Forward proposes will result in a more complex construction and maintenance of traffic process (as compared to Concept I-W). Based on previous projects these complexities could further increase construction costs by 20% to 40%, resulting in a cost increase on the order of \$120,000,000-\$140,000,000. A detailed analysis would be required to provide an accurate estimate.

Operation and maintenance costs were also compared. Due to the stacking of the roadways the yearly operations and maintenance costs for the Bridge Forward Concept are higher than those for Concept I-W. The operation and maintenance costs for the Bridge Forward Concept (based on the tunnel alone) are estimated at approximately \$1 million per year, while the operation and maintenance costs for Concept I-W are estimated at \$160,000 per year. The additional local streets included in the Bridge Forward concept; with a significant amount on structure; significantly adds to the City's inventory and increases maintenance costs.

Traffic

A high-level analysis of the traffic operations was performed as well. This analysis was done by manually rerouting the Certified Traffic for Concept I-W and analyzing each intersection using HCS software. HCS is an intersection focused software and does not analyze the entire system. To perform a proper, detailed analysis the traffic data would be rerouted using OKI's travel-demand model and analyzed using software that evaluates the whole traffic system.

The high-level analysis showed significant queues on the local street network which could result in gridlock. A more detailed system wide analysis would be performed during the innovation period.

Conclusion and Next Steps

Based on a conceptual and high-level review of Bridge Forward's June 2023 Concept it presents numerous technical challenges as well as significant cost implications that will require considerable evaluation prior to considering implementation of the concept. As currently laid out the concept:

- Potentially provides 5 acres of contiguous developable land but results in construction costs at least \$100,000,000 more than Concept I-W and much higher yearly maintenance costs going forward. This number could increase significantly when the concept is studied for constructability and maintenance of traffic during the innovation process.
- Increases the local maintenance (City of Cincinnati) responsibility significantly.
- Requires traffic to pass through additional signals to access existing downtown area.
- Results in a profile change of about 40 feet at 7th Street resulting in both a significantly higher crossing that impacts the surrounding buildings and land use and results in grades on 7th Street between Gest and I-75 of over 9%.

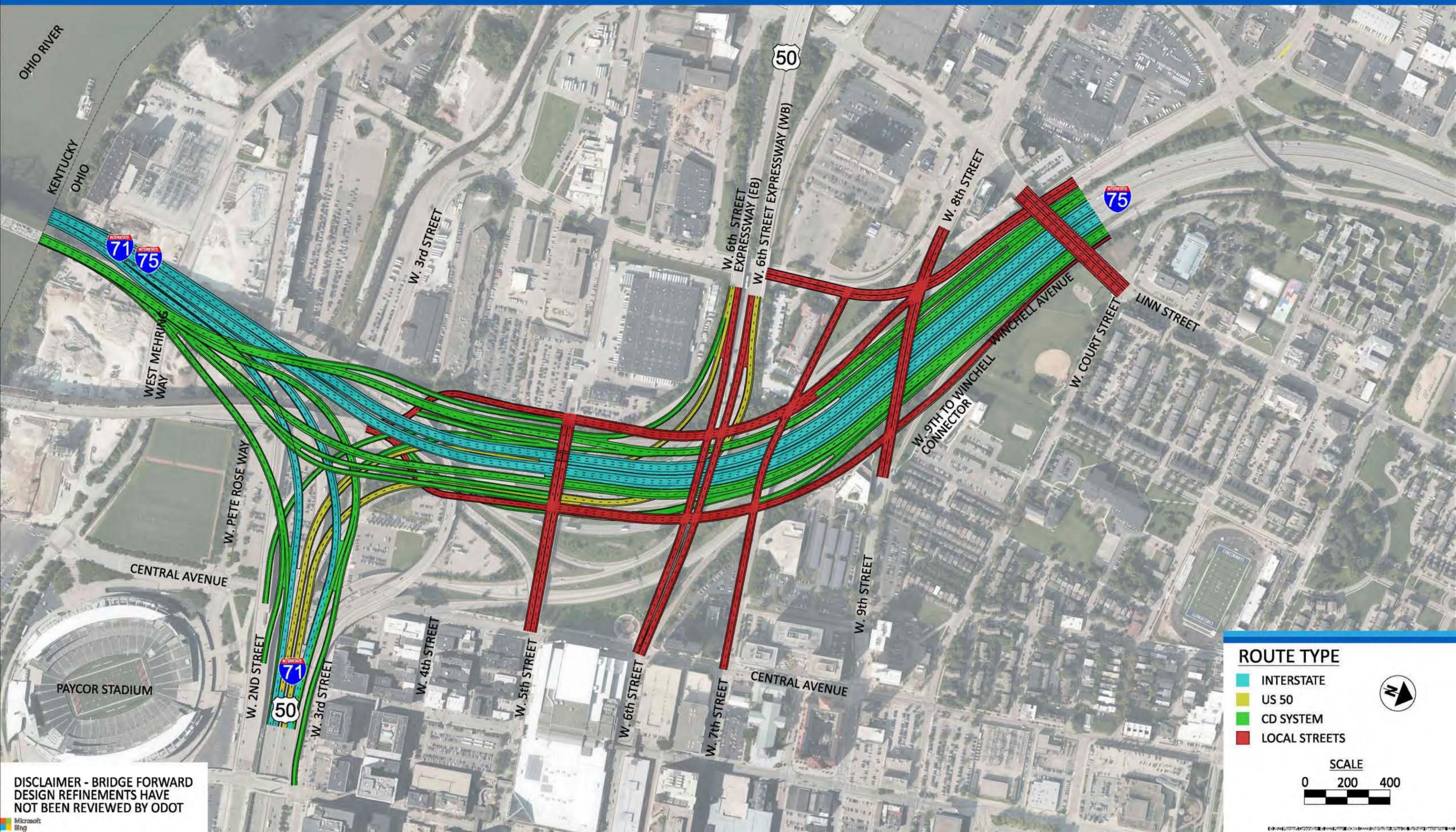
- Proposes the new arterial frontage roads at an elevation about 30 to 40 feet higher than the surrounding land and Central Avenue resulting in a physical and/or a visual barrier between downtown Cincinnati and Queensgate.
- Creates traffic congestion and operational issues on the local road system resulting in queues extended through multiple intersections and potentially creating gridlock during the peak periods.
- Creates potential safety issues due to the added conflict points and additional pedestrian crossing lengths.

The Progressive Design Build process begins with an innovation process. During the innovative process of the Progressive Design Build contract, the design-build team will work with the BSMT to develop and analyze innovative concepts that meet the contract objectives provided in Attachment 1. Local governments will be engaged during this process. The BSMT will share Bridge Forward's Concept with the design-build team at the beginning of this process. At the end of the innovation period the design build team will bring forward any concepts or design revisions that meet established project goals and objectives or reduce project cost and schedule.

Attachment 1: Progressive Design-Build Contract Objectives

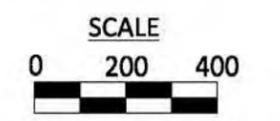
- Maximize the project scope within the programmed funding amounts through innovation, design optimization and effective risk mitigation;
- Build the project with a context sensitive design that fits within the community;
- Maximize the public investment in the project by minimizing the footprint;
- Minimize the footprint of the interstate system to maximize potential developable space;
- Improve neighborhood connectivity across the interstate;
- Minimize traffic disruption during construction, with minimal detours or diversion of traffic to local streets;
- Provide opportunities for workforce development and DBE utilization;
- Provide strong aesthetic value along the project corridor;
- Achieve effective project delivery;
- Minimize physical intrusion and impact;
- Create best environmental outcomes;
- Design for sustained quality of life;
- Improve the local road aesthetics when crossing the interstate; and
- Open the new companion bridge to traffic by July 15, 2029.

Attachment 2:
Bridge Forward Concept and Refined Alternative I (Concept I-W) Layouts



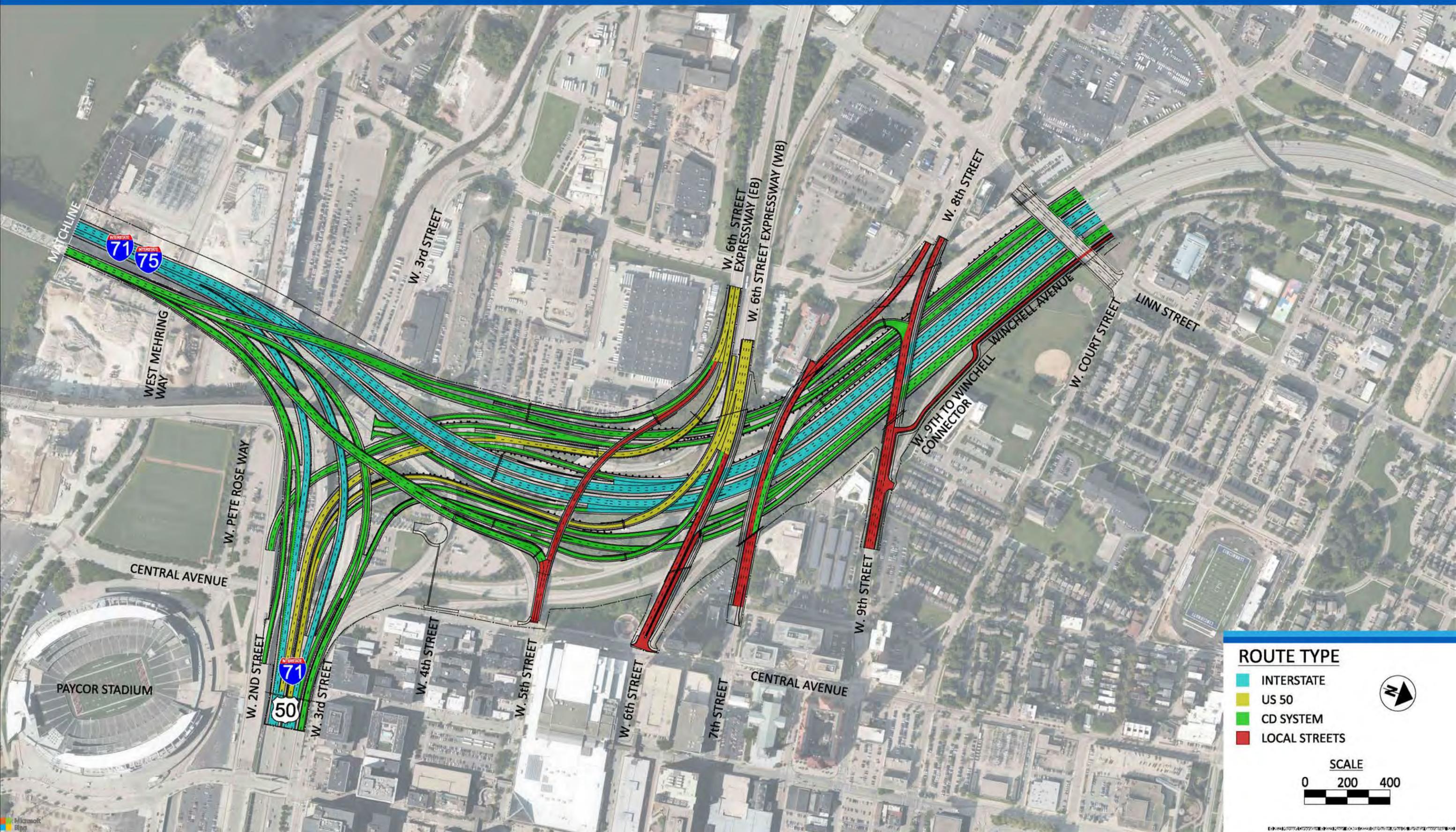
ROUTE TYPE

- INTERSTATE
- US 50
- CD SYSTEM
- LOCAL STREETS



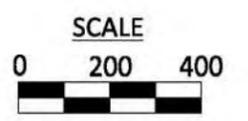
**DISCLAIMER - BRIDGE FORWARD
 DESIGN REFINEMENTS HAVE
 NOT BEEN REVIEWED BY ODOT**

Microsoft
 Bing



ROUTE TYPE

- INTERSTATE
- US 50
- CD SYSTEM
- LOCAL STREETS



Attachment 3:
Bridge Forward Concept Cross Sections

8TH STREET

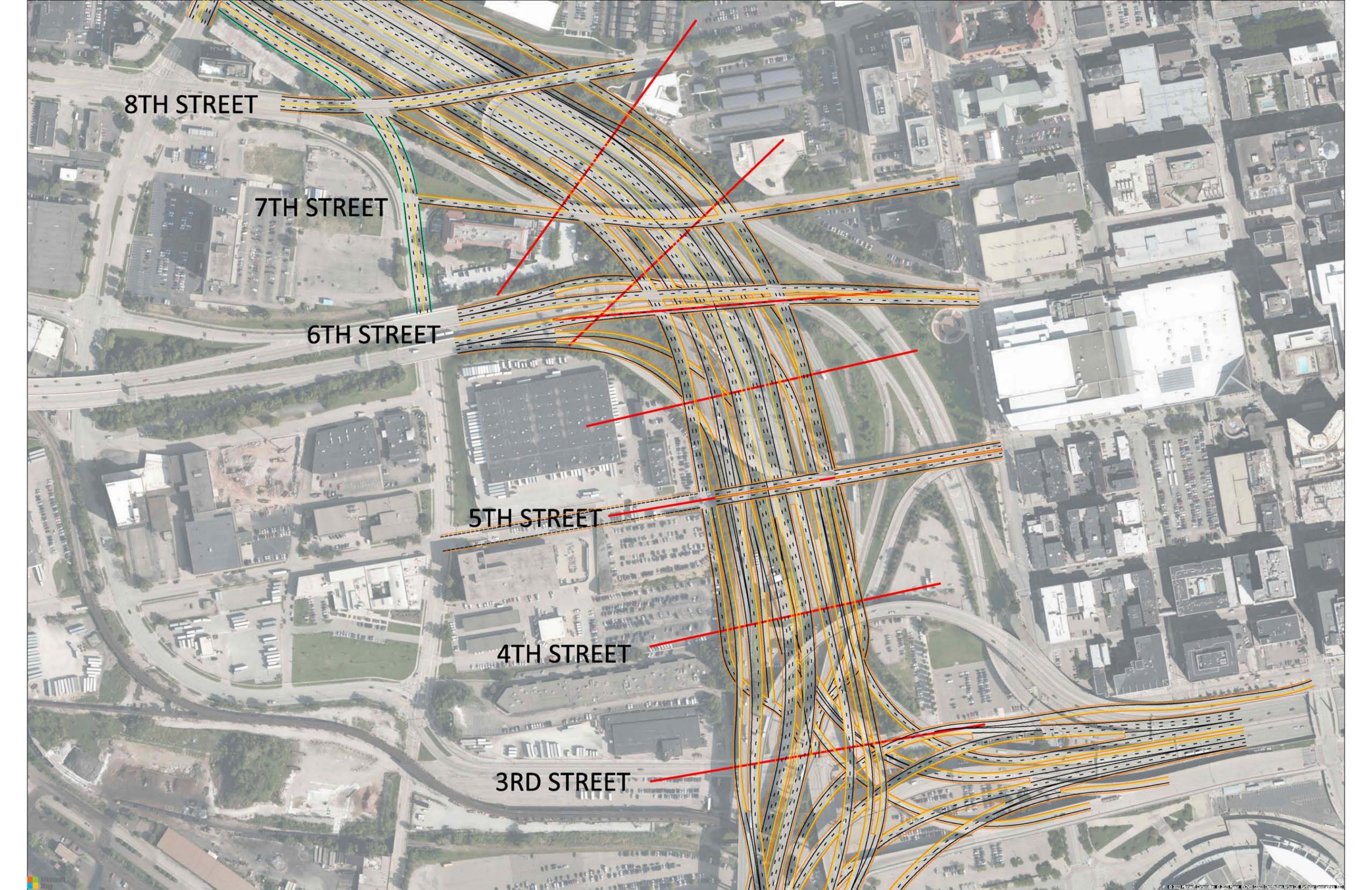
7TH STREET

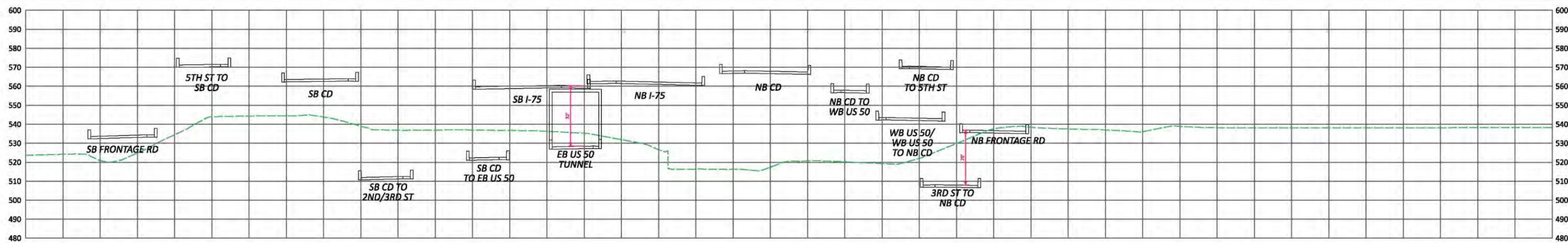
6TH STREET

5TH STREET

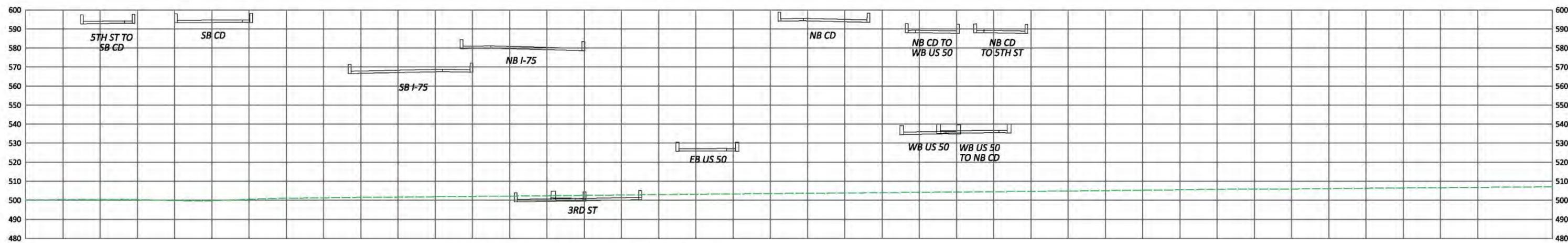
4TH STREET

3RD STREET

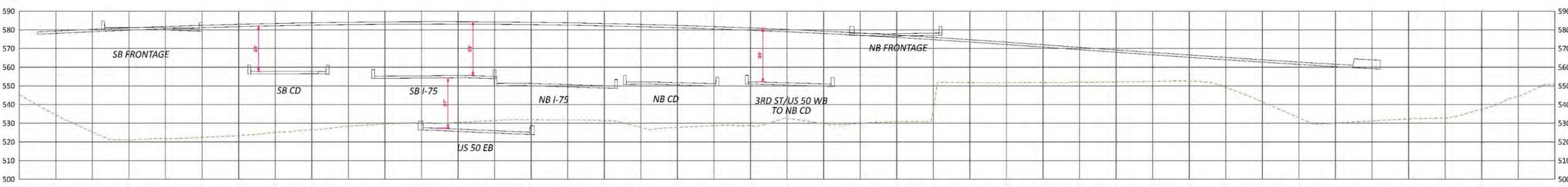




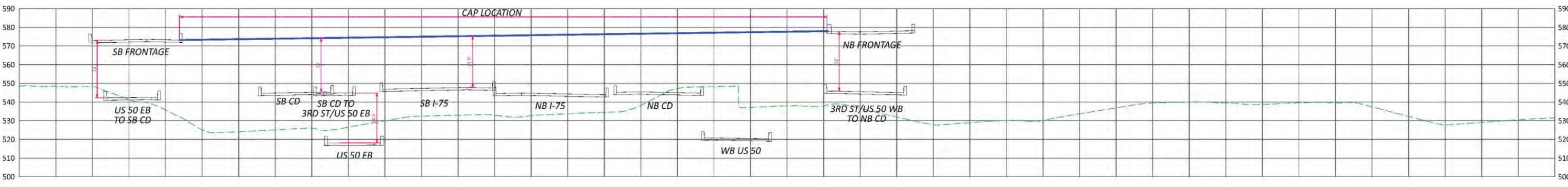
4TH STREET



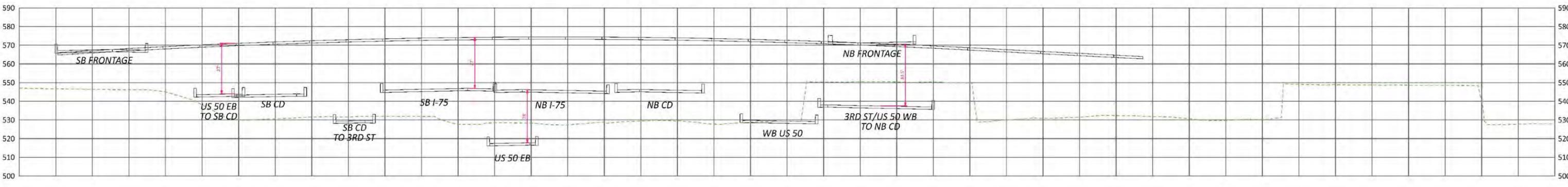
3RD STREET AT CLAY WADE BAILEY



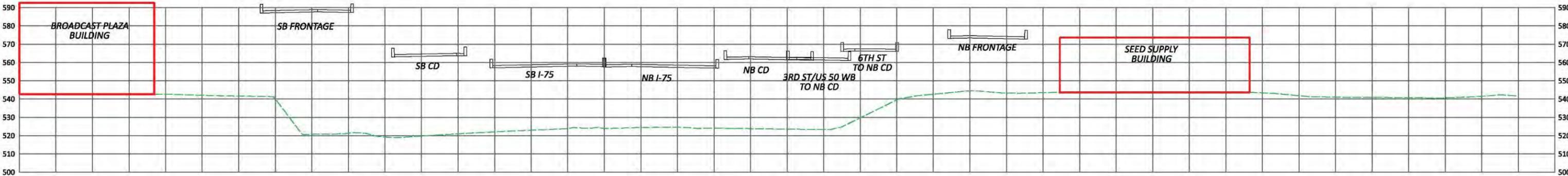
6TH STREET CROSSING



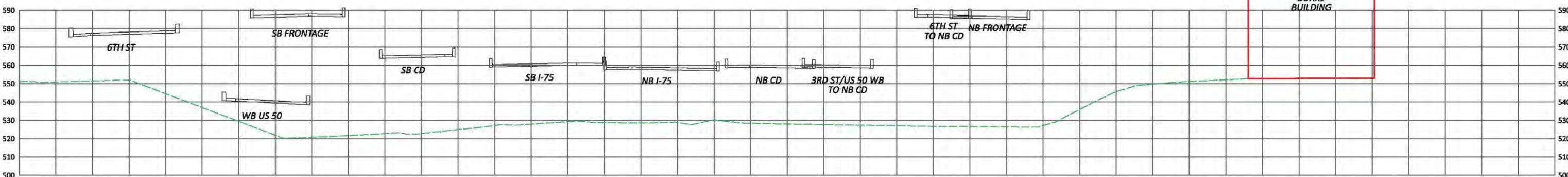
MID-POINT 5TH/6TH STREETS



5TH STREET CROSSING



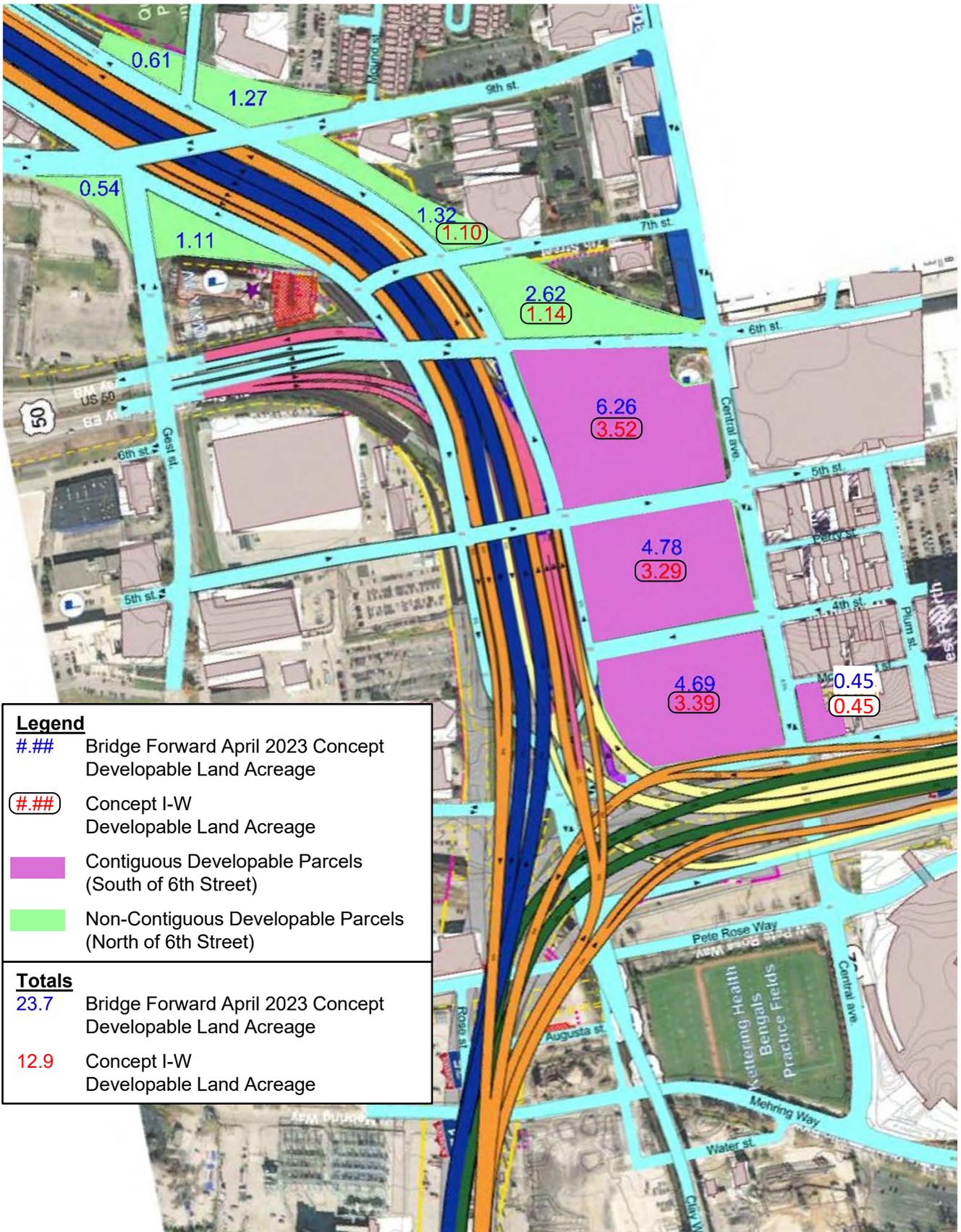
MID-POINT 7TH/8TH STREET



MID-POINT 6TH/7TH STREET



Attachment 4:
Contiguous Developable Land



Source: Bridge Forward April 2023 Concept, as presented to the City of Cincinnati on May 3, 2023.



Attachment 5:
Construction Costs

Bridge Forward Lane Miles Total - June Update

	Roadway	Cost	Structures	Cost	Tunnel	Cost	Total Miles	Total Cost
Local	3.29	\$ 4,400,000	1.37	\$ 27,600,000	-	-	4.67	\$ 32,000,000
CD	1.49	\$ 4,800,000	2.94	\$ 75,550,000	-	-	4.43	\$ 80,350,000
Interstate	1.43	\$ 5,000,000	2.87	\$ 89,000,000	-	-	4.3	\$ 94,000,000
US 50	1.13	\$ 2,400,000	1.13	\$ 22,000,000	0.38	\$ 90,500,000	2.64	\$ 114,900,000
Total	7.34		8.31		0.38		16.04	\$ 321,250,000

Concept I-W Lane Miles Total

	Roadway	Cost	Structures	Cost	Tunnel	Cost	Total Miles	Total Cost
Local	0.92	\$ 450,000	1.02	\$ 12,000,000	-	-	1.94	\$ 12,450,000
CD	2.02	\$ 4,800,000	2.77	\$ 61,000,000	-	-	4.79	\$ 65,800,000
Interstate	1.65	\$ 3,500,000	2.90	\$ 90,000,000	-	-	4.55	\$ 93,500,000
US 50	1.95	\$ 4,500,000	2.03	\$ 45,000,000	-	-	3.99	\$ 49,500,000
Total	6.54		8.73				15.27	\$ 221,250,000.00

Brent Spence Bridge Corridor Project Correspondence

From: Stefan.Spinosa@dot.ohio.gov
Sent: Tuesday, October 4, 2022 12:53 PM
To: Laber, Ryan M. <rmlaber@gmail.com>
CC: Brian Bolan <urban513@yahoo.com>
Subject: RE: Brent Spence Bridge proposal
Attachments: BSB Position Paper 211214.pdf; Attachments.html

Mr. Laber,

The Brent Spence Project Team has reviewed and evaluated the information provided to us (attached). The information submitted and the attached response will be included in the project record. In addition, because of the public interest in the concepts presented to us, we have placed this response on our website: [Environmental Process | Brent Spence Bridge | Investing in Local Communities, Growing America's Economy \(brentspencebridgecorridor.com\)](https://www.brentspencebridgecorridor.com)

We will be sending an e-newsletter this week that will discuss the attached response.

Citrix Attachments Expires November 3, 2022

2022-09-27_Response to Public Comment_...ary.pdf 16.1 MB

[Download Attachments](#)

Stefan Spinosa uses Citrix Files to share documents securely.

Respectfully,

Stefan C. Spinosa, P.E.

Brent Spence Bridge Corridor Project Manager

ODOT District 8 Capital Programs

505 South SR 741, Lebanon, Ohio 45036

513.933.6639

[transportation.ohio.gov](https://www.transportation.ohio.gov)



From: Laber, Ryan M. <rmlaber@gmail.com>
Sent: Sunday, January 23, 2022 10:02 PM
To: Spinosa, Stefan <Stefan.Spinosa@dot.ohio.gov>
Subject: Fwd: Brent Spence Bridge proposal

Hi Stefan,

I hope all is well. I'm writing to quickly follow-up on the email I sent to you last week. I know the paper I sent over may be too long for you to review. That being said, if you're open to a short conversation this week, I would really appreciate speaking with you about the Brent Spence Bridge project.

Thank you for your attention. Please feel free to reach me in any way.

Very Respectfully,
Ryan

Ryan Laber
rmlaber@gmail.com | (859) 445-2257 (c)

----- Forwarded message -----

From: Laber, Ryan M. <rmlaber@gmail.com>
Date: Sun, Jan 16, 2022 at 11:59 PM
Subject: Brent Spence Bridge proposal
To: <Stefan.spinosa@dot.ohio.gov>

Hi Stefan,

My name is Ryan Laber. I'm writing to informally follow-up on the meeting you had with Mark Mallory and Brian Boland a couple of weeks ago. I am loosely coordinating with Mark and Brian, but I am working separately at the moment.

For about a year, I have been developing a position paper related to the Brent Spence Bridge project. Kathleen Fuller and Elizabeth Lyons in your office have been tremendous resources to me over the past year as I've developed this paper.

I'm wondering if you would be willing to review at least the executive summary of the position paper (attached). The paper makes a case for a project redesign, establishes goals for a redesign, and describes why I believe a redesign may be possible.

After you review the paper, I'm hoping that we could then have a short, 30-minute conversation over the phone to discuss a few questions I have about the project and about the process for project implementation going forward (see second attachment). Frankly, I will be advocating for a project recalibration to some of our elected leaders, and I hope to be as accurate as possible when communicating what improvements may actually be achievable, within the ODOT process.

Thanks for your attention, Stefan. I'll hope to hear from you. Please feel free to reach out in any way going forward.

Very Respectfully,
Ryan

Ryan Laber
rmlaber@gmail.com | (859) 445-2257 (c)

From: Brian Boland
Sent: Friday, January 6, 2023 2:37 PM
To: Leffler, Laurie
Cc: Spinosa, Stefan; Marchbanks, Jack
Subject: Brent Spence Engagement and clarification

Hi Laurie,

Bridge Forward has been invited to attend a meeting with ODOT/KYTC in a few weeks, and we are pleased to be invited to talk. Before that time, however, I thought it would be good to give a recap of our group's efforts over the last 12-14 months. Also, since an official reply was sent to one of our groups members and I was copied on it, I thought it appropriate to address some of the items in that letter. Lastly, I will share my hopes for the outcome of the upcoming meeting.

I. About Bridge Forward

Our ideas were developed during the time the Brent Spence was in a stalemate over tolls, and were never intended to be or to bypass the proper engineering processes. They were, rather, analyzed to the point of showing validity of concept with the hope that this plan would lower costs enough and create post-project ROI significant enough to convince the parties in the stalemate to come together to get the bridge built. Our effort has never been opposed to the bridge.

Some of our plans were 'works in progress' plans and 'artists renderings' to spur discussion, and were often modified as we learned more about changes from ODOT as time went on (e.g. alternative I-W in late 2021). Our ideas were shared with both ODOT and KYTC in a spirit of openness, but instead of being receptive to our ideas, ODOT took the known issues of our plan and amplified them and then used our artistic renderings and treated as if they were final, vetted iterations of a plan and summarily dismissed them

The points that follow illustrate how we attempted to engage and share information with both ODOT and KYTC and how our ideas were ignored or misrepresented in order to dismiss and our ideas/concerns about the bridge.

II. The engagement process:

On several occasions, different volunteer members of our group spoke with, emailed or otherwise communicated with members of ODOT and KYTC. The most significant of those communications for this rebuttal are as follows:

**On January 7, 2022, I had a zoom presentation with Stefan Spinoza of ODOT, and several others including former Cincinnati Mayor Mark Mallory. On this call I shared with him the detailed 3D modeling that our group had done where I expressed my wish that 4th street be able to cross if possible, but due to the known slope issues, the presentation showed no connection of 4th street. We then discussed the simplified exiting proposal, as well, and discussed how the 2nd/3rd street exits on FWW perform. Stefan confirmed at that time that they have no issues with the 2nd/3rd performance.

After that meeting, at his request, I shared a link to the file with the 3D modeling so that Stefan could refer back to it if he wanted.

****On February 3rd, 2022,** I presented the same information via a zoom call with Stacey Hans and Gary Valentine of KYTC. I shared with them the entire plan for both sides of the river then focused more with them on the possibilities for reducing the footprint of the bridge in Kentucky via a 2-way collector distributor system similar on the east side of the highway with two roundabouts, similar to what they built for the extension of KY Rt. 9 in Newport KY.

After this meeting, again, I shared a link to the file with the 3D modeling so that they could refer back to it if they wanted.

Based on these interactions and sharing of information, I am perplexed as to why ODOT would hire HNTB--at taxpayer expense--to draft a response based on an artistic rendering of our proposal, and not the 3D modeling that was both reviewed and shared with them. Modeling that would have shown that we had considered and developed work arounds to most of their concerns. These renderings were designed to spark public interest in the project and to hopefully prod ODOT/KYTC into an actual two-way conversation, they were not intended to be the basis of their response.

Page 2, "Lowered Alignment of I-75"

As stated above, our actual 3D Modeled plan does not call for any significant lowering of the existing main trunk section. This issue is moot.

Page 3, "Extended Local Street Grid"

As shown in our 3D Modeling, the I-75 main line path can run almost exactly as it does currently, rising from 520' north of 7th, to approximately 530' at 5th before rising up to cross the CSX RR bridge with proper 23.5' clearance and with a slope of approximately 3.5%. This relatively gentle slope is well under the requirements for urban highway sections and would, in fact, allow for some lowering of the highway main line if that benefitted the plan.

The Cincinnati street grid from 5th to 7th runs nearly flat at 549', and all ODOT renderings show these streets crossing the highway main section in roughly the same locations as they do today. Our street grid extension is nothing more than a modification of this system. In fact, if the interstate section can be reduced in width, then the supporting structure of the extended streets can be minimized, thus making it potentially easier, for the city street grid to cross there.

Page 6, "Access Points"

Access points to downtown Cincinnati currently consist of:

NB - 2nd street (Via I-71) and 5th Street

SB - 7th, 5th and 2nd (along with US 50 path)

Additionally there is the connection to US 50 WB

Bridge Forward Proposes a C-D system comprised of 2 city streets in a one-way couplet setup as part of the extended street grid mentioned above. These will have more than enough capacity to accept current and future local/exiting traffic. It is simply unfounded to suggest that this system would not be able to handle or would "substantially" increase travel times for local traffic. This assertion is based on the success of the existing one-way couplet of 2nd/3rd streets flanking Fort Washington way and Stefan's own statement of their performance.

Our C-D system greatly reduces the number, size, and scale of roadway and (and thus lowers both construction and life-cycle costs) and returns land used for exiting and entering to local control. It is THIS land that can be returned from ODOT ownership for redevelopment and allowed to rise to it's highest and best use.

Page 6, "Local Connectivity"

Consistent with changes over the last 20 years, urban connectivity is more impactful now than at any time since the beginning of the interstate era. Increasing the connections across the interstate and making those crossings as similar in design as possible with the surrounding city scape is crucial.

As has been demonstrated by ODOT in our sister city Columbus, first with the Columbus Cap in 2005, and now with the continuing enhancements as part of their inner loop plan, it is possible to make these crossings almost invisible to the user on the street level. Crossings like Long Street demonstrate how seamless a well-designed connection can be.

The currently proposed high speed CD system takes up great amounts of space, has wide lane widths that are shown to increase speeding, and as a result will be noisy, dangerous to pedestrians and not enhance the neighborhood around the highway that has many traditionally underserved communities. This is directly contrary to the stated goal to improve safety for pedestrians.

Bridge Forward asks that ODOT cede control of local roads to the the local DOT (DOTE) for design control in a way that is consistent with community goals and desired outcomes, and in a manner similar to our sister city in Columbus, these connections should be as invisible as possible to the user.

In Kentucky, Bridge forward suggests a CD system to be a dual carriageway setup similar to the Rt. 9 extension in Newport, KY. This setup can be made east of the interstate and reduce further the footprint of the highway and its impacts in Covington. This includes a proposal that provides a modified path for Route 8 that takes traffic out of Covington's historic residential neighborhoods.

Page 7, "Multi-Modal Design"

For local streets, regardless of the agency tasked with designing them, the NACTO Urban Street Design Guide should be used, not the AASHTO guid book. Cities have very different needs from suburban/rural areas, and the NACTO Design Guide provides proven ways to design street to pedestrian scale and speed needs in a city.

Our proposal asks ODOT to give street design authority to the local DOT for these crossings, and we request that for the exiting C-D streets, design control be given over to the local DOT at the earliest point from exiting the main line as far as possible.

Page 7, "Economic Development"

The Bridge Forward Westway plan only hints at what any future development might look like by providing some artist renderings, which is necessary to communicate the main points of any idea. After all, as the saying goes "a picture paints a thousand words". Our plan does nothing more than ask ODOT/KYTC to allow their design to result in a project for a future where the local planning agencies may in fact have a master plan. And where that plan necessarily interfaces with the interstate, and not to create a plan that is, in fact, a barrier to such future plans.

III. Underlying Assumptions

As ODOT states, it is not the role of ODOT or KYTC to develop Master Plans for cities with the goal of fostering economic development. That is the role of the city, community groups and others, and those goals are beyond the scope of a transportation project.

In many ways, to residents of Cincinnati & Covington, of Ohio and Kentucky, as users and owners of the system and taxpayers to the state, this section is the most frustrating. Our tax dollars fund ODOT and KYTC's operations and thus we fund the projects that they build. By virtue of this WE are the customers, and in any transactions, providers meet the customers needs, not the other way around.

When ODOT states that economic development is "beyond the scope of a transportation project" they ignore this more fundamental point:

DOT's are created as a governmental agency with the underlying assumption that their work of **building transportation facilities has the result of promoting economic development.** We don't build roads to nowhere for no reason. We build roads because we get an economic return from their use. If that were not the case, we would not fund our DOTs every year.

With an understanding of this underlying concept, all work that DOTs do must remember this point with every project. Our DOTs must not lose sight of the WHY of what they do as they busy themselves in the WHAT of what they do.

IF they don't, then as taxpayers, citizens and owners we should dismiss them and we should hire someone else who delivers what we want.

IV. CONCLUSION

My conclusion is that this costly, cursory rebuttal to our proposal is only intended to have the effect of stifling true, two-way conversations about the bridge. We feel that the public engagement process is perfunctory at best to this point, and will not result in a better project.

We feel our ideas have merit and deserve more consideration and we request a seat at the table during the Supplemental EA process and throughout the Progressive Design-Build process to ensure that institutional inertia doesn't result in a plan that fails to let our cities thrive, because ultimately, that IS the role of our DOTs.

As President Biden said at his press conference here this past Wednesday, "We CAN work together, we CAN get things done." We hope that the upcoming meeting with ODOT, KYTC and others will be productive and that the input that we have—which has been nearly unanimously supported by the public--will be given proper consideration.

Ultimately, we hope to help craft a design that will be an example for others around the nation to emulate; a design that will win awards for innovation, design and construction; and finally a design that will achieve the fundamental underlying assumption of all such projects— to spur economic return on our investment.

Respectfully,
Brian Boland

"You can resume your flight whenever you like," they said to me, "but you will arrive at another Trude, absolutely the same, detail by detail. The world is covered by a sole Trude which does not begin and does not end. Only the name of the airport changes." Italo Calvino, *Invisible Cities*

"Any escape might help to soothe the unattractive truth, that the suburbs have no charms to soothe the restless dreams of youth" N. Peart, Rush, "Subdivisions"

From: Ryan Laber <rmlaber@gmail.com>
Sent: Thursday, January 5, 2023 3:34 PM
To: info@brentspencebridgecorridor.com; Ohio.FHWA@dot.gov; Kentucky.FHWA@dot.gov
Subject: Re: Bridge Forward Survey - Progress Report #1
Attachments: Camp Washington CC Letter of Support - Bridge Forward BSB Design.pdf; WECC-BSB-IMPROVE-SUPPORT-2022.pdf; OTRCC_Letter regarding Bridge Forward Coalition_Brent Spence Project_12.20.22.pdf; WECC-BSB-IMPROVE-SUPPORT-2021.pdf; Bridge Forward - BSB PDB Suggested Procurement Language.docx

Please also see attached for letters of support for Bridge Forward's design direction goals to date from local Community Councils. More will follow.

The final attachment is procurement language that Bridge Forward is requesting be included in the initial RFQ or as an addendum to the RFQ.

Ryan Laber
rmlaber@gmail.com | (859) 445-2257 (c)

On Thu, Jan 5, 2023 at 12:01 PM Ryan Laber <rmlaber@gmail.com> wrote:

Hello:

Please see attached for a progress report taken today of results from a survey that Bridge Forward Cincinnati has been circulating in the community, [here](#). Each response is from a unique respondent. Final results and an analysis will be forwarded when the survey period is complete.

Very Respectfully,

Ryan Laber
rmlaber@gmail.com | (859) 445-2257 (c)



Camp Washington Community Council

2951 Sidney Avenue

Cincinnati, OH 45225

communitycouncil@camp-washington.org

To the City of Cincinnati, Cincinnati Department of Transportation and Engineering (“DOTE”), Ohio Department of Transportation (“ODOT”), and whomever it may concern,

The Camp Washington Community Council is writing this letter to express our support for Bridge Forward’s alternative design for the Brent Spence Bridge (the “BSB”).

Bridge Forward’s proposal is a superior design for the BSB that reflects the national movement to minimize the amount of urban land taken up by freeways and looping on/off ramps. The City and region has a unique opportunity to regain and reconnect urban land, and it should not be squandered. ODOT has claimed that alterations to the existing 10+-year old design are not possible, when they themselves just recently made alterations that they declare now regains 10 acres of land. This new 10 acres flanks the BSB approach too closely to be nearly as usable as the reclaimed 30+ acres of land proposed by the Bridge Forward Design.

As a community that was severed by the creation of I-75, we feel that reconnecting communities torn apart by I-75’s construction should be a regional priority. This makes sense from an urban design standpoint and an economic standpoint. It will furthermore offer the opportunity to build thousands of sorely needed housing units in the central core.

We deserve a BSB that reflects what is happening now, today in Cincinnati and Covington; we deserve a bridge that reconnects what was lost and realizes the full economic development potential offered by the Bridge Forward BSB design.

We thank you for your time and consideration, and please do not hesitate to reach out to us at communitycouncil@camp-washington.org.

Sincerely,

The Camp Washington Community Council



Over-the-Rhine Community Council
P.O. Box 662
Cincinnati OH 45201

December 20, 2022

Mayor Aftab Pureval
Cincinnati City Council
Ohio Department of Transportation

Subject: Over-the-Rhine Community Council Support for Bridge Forward Vision and Measurable Priorities for the Brent Spence Corridor Project

To Whom it May Concern:

On October 28, 2022, Bridge Forward presented their current design modification recommendations for the Brent Spence Bridge Project to the Over-the-Rhine Community Council (OTRCC). Along with their design recommendations, they presented measurable priorities that they believe should be adopted for this project. Those priorities are:

- *Maximize the amount of returned land contiguous to the **City's** urban core;*
- *Maximize the amount of walkable street frontage that is conducive to street-facing development within and adjacent to the project footprint;*
- *Maximize the amount of pedestrian connection between the CBD, Queensgate, and the West End, minimizing the walking distance of each of those connections;*
- *Maximize the number of local streets that are designed according to the NACTO Urban Design Guide;*
- *Minimize the number of lane miles using high-speed directional ramps;*
- *Achieve best expenditure of public dollars measured in life-cycle return on investment.*

*This discussion prompted a motion for the OTRCC to support Bridge **Forward's** Vision, and to ask City Council to secure a seat at the table with respect to the project and to adopt their measurable priorities for The Brent Spence Corridor Project. These priorities should be adopted even after the 11/10/2022 press conference regarding limited Chamber-led design modifications. The motion was seconded and passed by the general body membership of the OTRCC.*

*Please Accept this notice as full documented support for the Bridge Forward **Coalition's** vision and proposed measurable priorities for the project.*

Respectfully,

A handwritten signature in black ink that reads "Danny Klingler". The signature is written in a cursive, flowing style.

Danny Klingler
President
Over-the-Rhine Community Council

West End Community Council

PO Box 14424
Cincinnati, Ohio 45250

Date: 10/25/2021

Notice: West End Community Council Motion to Encourage Feasibility Study of Brent Spence Bridge Design Improvements

On October 19, 2021, the West End Community Council general body reviewed and discussed the Brent Spence Bridge realignment proposal created by Ryan Laber and the improved submerged design of the current alignment created by the Bridge Forward Coalition. This discussion included information about the negative historical context and impact of the current design and the development opportunities and historical rectification of the proposed design improvements. The general body voted to encourage relevant governmental agencies to further explore the feasibility of both proposed designs improvements.

Please accept this notice as documented support from the West End Community Council to explore of the feasibility of these Brent Spence Bridge design improvements.

Additional Information:

Bridge Forward Design: <https://www.bridge-forward.org>

Ryan Laber Design: rlaber@gmail.com (available on request)

Respectfully,



Chris Griffin
President, West End Community Council

West End Community Council

PO Box 14424
Cincinnati, Ohio 45250

Date: October 28, 2022

Subject: West End Community Council Support for Bridge Forward Design Recommendations for the Brent Spence Bridge Project and Feasibility Assessments

On October 18, 2022, Steve Kenat, an architect with GBBN, presented the current Bridge Forward design-modification recommendations for the Brent Spence Bridge project to the West End Community Council. There was a discussion about the negative historical racial context of the current design and the disproportionate impact it has had on the West End community. The need for this community to be assertive in our support for these design improvements was also included in the discussion.

This discussion prompted a motion to be made for the West End Community Council to reaffirm our support for the Bridge Forward Coalition and their design recommendations. The motion was seconded and passed by the general body membership of the West End Community Council.

Please accept this notice as full documented support from the West End Community Council for the Brent Spence Bridge project design improvements proposed by the Bridge Forward Coalition and for the appropriate feasibility assessments to be conducted.

Note: Last year, on October 19, 2021, the West End Community Council general body reviewed and discussed potential design modifications to the Brent Spence Bridge project. At the meeting, a motion was passed to request relevant governmental agencies to explore the feasibility of these design improvements. Please note the West End Community Council's long-standing support for design improvements to the Brent Spence Bridge project that could lessen the highway's impact on our historic community.

Respectfully,



Chris Griffin
President, West End Community Council

*Brent Spence Bridge Corridor Project
Progressive Design-Build Project*

Suggested Procurement (RFQ) Language

1. Alternative concept proposals that reduce footprint and simplify operations will be required to be developed and evaluated during Phase 1A using performance measures identified by the Bi-State Management Team (BSMT), with input from local communities, against the ODOT/KYTC “30% Plans” design concept using a progressive design build process to incorporate more complete measures of social and economic value.

ODOT fully support the continued refinement and reduction in footprint impacts. Exhibit E, Section 1 of the RFP, Technical Requirements, includes the following text in Section 1: “...The Design Build Team (DBT) shall use LD-07 (Concept I-W Corridor Exhibit) as a basis for the development and validation of the Base Design. Additional geometric modifications and Alternative Technical Concepts that meet project goals, environmental commitments, and local government expectations can be evaluated during Sub-Phase 1A...” In addition, numerous contract objectives including minimization of footprint are part of the requirements the DBT and the Department will be considering. Involvement from the City and County governments will be part of the progressive design build process.

2. The design-build team (DBT) will engage community partners on both sides of the river in developing the scope, performance measures, and evaluation criteria for the Brent Spence Bridge Corridor Project.

ODOT understands the need to fully engage community partners for the BSB project. Communities, the Project Advisory Committee, and other stakeholders have been engaged since the start of NEPA and were key in the establishment of measures used to evaluate alternatives. This evaluation led to the establishment of the preferred alternative I. Through value engineering and continued studies, improvements have been incorporated into Concept I-W. Any alternative technical concepts or innovations developed during Sub-phase 1 A of the progressive design build will be evaluated against the contract requirements including environmental commitments, project goals, and contract objectives and of course, community partners will continue to be fully engaged.

3. Performance measures will include social and economic net benefits, including value of land recapture and access changes.

Social and economic net benefits as well as working to minimize negative impacts continues to be a primary focus of the BSB project. Any alternative technical concepts or innovations developed during Sub-phase 1 A of the progressive design build, will be evaluated against the contract requirements including environmental commitments, project goals, and contract objectives. The technical requirements of the progressive design build include these specific social and economic objectives:

- *Build a good project well by building a project with a context sensitive design that fits within the community;*
- *Maximize the public investment in the Project by minimizing the footprint;*
- *Minimize the footprint of the interstate system to maximize potential developable space;*
- *Improve neighborhood connectivity across the interstate;*
- *Minimize traffic disruption during construction, with minimal detours or diversion of traffic to local streets;*
- *Provide opportunities for Workforce Development and DBE utilization;*
- *Provide strong aesthetic value along the Project corridor;*
- *Minimize physical intrusion and impact;*
- *Create best environmental outcomes;*
- *Design for sustained quality of life;*
- *Improve the local road aesthetics when crossing the interstate;*

4. Performance standards achieved by the current “30% Plans” will be established; for example, system to system access for I71/I75/I471/US50, LOS, Travel Times, Access, Geometrics, and Safety as a basis for comparison.

The purpose and need for the BSB project is focused on safety, congestion relief and improving the interstate system. Because of this attention, Concept I-W, alternative technical concepts, and any identified innovations incorporated into the project will continue to satisfy operational and design requirements.

5. The design-build scope may be a hybrid of prescriptive and performance elements in order to achieve both agency and community goals, and may incorporate IDIQ, Incentive/Disincentive, and Job Order contracting elements during phase 2.

The progressive design build contract will include various contract mechanisms including incentives/disincentive, disadvantage business enterprise goals, on the job training goals, etc. The contract also is written to encourage alternative technical concepts and innovations in order to continue to improve on the project.

6. The evaluation criteria will incorporate environmental commitments and associated required performance measures resulting from the supplemental EA being conducted for the project.

Environmental commitments are a normal part of the project development process and one that ODOT always takes seriously on all of programmed projects. ODOT will ensure all environmental commitments are addressed appropriately and will ensure the contract language reinforces this fact.

7. The DBT, in coordination with the BSMT, shall undertake updated analyses and studies to support its performance claims, for example, sensitivity analysis and alternative scenarios for opening and design year traffic, including assignments to specific links.

The DBT is currently updating technical documentation including traffic projections and analysis. All the analysis will be publicly available when completed.

8. A life-cycle benefit/cost analysis, consistent with FHWA’s guidance and including the identified value measures for both the “30% plans” and an alternative technical concept, will be required to support proof of concept.

As part of both Bipartisan Infrastructure Law and the grant applications, benefit cost analysis was completed for Concept I-W. The B/C ratio calculated was 3.67:1. All alternative technical concepts identified and evaluated will include cost evaluation as part of the progressive design build process. This cost will be based primarily on initial cost; however, cost decisions made by ODOT typically include long-term maintenance considerations even though specific life-cycle costing is not performed. Because of this, life-cycle cost analysis are not required.

From: Ryan Laber <rmlaber@gmail.com>
Sent: Sunday, February 26, 2023 1:15 PM
To: info@brentspencebridgecorridor.com; Ohio.FHWA@dot.gov; Kentucky.FHWA@dot.gov
Subject: Re: Bridge Forward Survey - Progress Report #2
Attachments: BF_Responses_All_230221 PII Removed.pdf; MACC bridge forward 1_19_23.pdf

Please see attached for additional survey results from unique respondents submitted between 1/5/23 and 2/21/23 (with PII removed), and for a letter from the Mt. Auburn Community Council.

Ryan Laber
rmlaber@gmail.com | (859) 445-2257 (c)

On Thu, Jan 5, 2023 at 3:33 PM Ryan Laber <rmlaber@gmail.com> wrote:

Please also see attached for letters of support for Bridge Forward's design direction goals to date from local Community Councils. More will follow.

The final attachment is procurement language that Bridge Forward is requesting be included in the initial RFQ or as an addendum to the RFQ.

Ryan Laber
rmlaber@gmail.com | (859) 445-2257 (c)

On Thu, Jan 5, 2023 at 12:01 PM Ryan Laber <rmlaber@gmail.com> wrote:

Hello:

Please see attached for a progress report taken today of results from a survey that Bridge Forward Cincinnati has been circulating in the community, [here](#). Each response is from a unique respondent. Final results and an analysis will be forwarded when the survey period is complete.

Very Respectfully,

Ryan Laber
rmlaber@gmail.com | (859) 445-2257 (c)

#122

COMPLETE

Collector: BF Link (Web Link)
Started: Thursday, January 05, 2023 4:59:26 PM
Last Modified: Thursday, January 05, 2023 5:01:07 PM
Time Spent: 00:01:41
IP Address: 174.239.115.44

Page 1

Q1

Local Government

The Brent Spence Corridor Project will be built using a “progressive design-build process.” This process allows the state highway departments, the design-build team (i.e. the contractor), impacted municipalities, and the local community to consider and address major project topics in a more open format. The design-build team will work with the state highway departments to create the basis of design, overall project requirements, and final design intent, before work substantially begins, during what is being called Phase 1A and Phase 1B of the project. Therefore, the following questions are particularly salient: Which level of government do you believe is better positioned to represent your interests in this Project [choose one]:

Q2

With funding secured, take the time to get the design right before starting construction

In terms of Project schedule, which of the following aligns most closely with your view [choose one]:

Q3

Finally, with respect to what the Project could accomplish, please rank the following priorities according to their importance to you [rank]:

Minimize interstate footprint / return land to the region's urban core	3
Create more walkable street frontage for development	1
Increase multi-modal connectivity across the interstate corridor	10
Ensure local streets impacted by the project are designed according to modern, urban, multi-modal guidelines	2
Reduce high-speed ramps through the urban core	5
Achieve best possible life-cycle return on investment with project funds	4
Improve traffic flow on the interstate	6
Improve safety for automobiles traveling on the interstate corridor	7
Ensure the interstate and ramps are designed according to highway design guideline manuals	9
Maintain all on-ramp and off-ramp access points to/from the interstate	8

Q4

Please provide your contact information. This will be kept confidential.

Name	
State/Province	OH
ZIP/Postal Code	45220
Email Address	

#123

COMPLETE

Collector: BF Link (Web Link)
Started: Thursday, January 05, 2023 7:53:32 PM
Last Modified: Thursday, January 05, 2023 7:57:42 PM
Time Spent: 00:04:10
IP Address: 50.5.36.237

Page 1

Q1**Local Government**

The Brent Spence Corridor Project will be built using a “progressive design-build process.” This process allows the state highway departments, the design-build team (i.e. the contractor), impacted municipalities, and the local community to consider and address major project topics in a more open format. The design-build team will work with the state highway departments to create the basis of design, overall project requirements, and final design intent, before work substantially begins, during what is being called Phase 1A and Phase 1B of the project. Therefore, the following questions are particularly salient: Which level of government do you believe is better positioned to represent your interests in this Project [choose one]:

Q2**With funding secured, take the time to get the design right before starting construction**

In terms of Project schedule, which of the following aligns most closely with your view [choose one]:

Q3

Finally, with respect to what the Project could accomplish, please rank the following priorities according to their importance to you [rank]:

- Minimize interstate footprint / return land to the region's urban core **1**
- Create more walkable street frontage for development **4**
- Increase multi-modal connectivity across the interstate corridor **6**
- Ensure local streets impacted by the project are designed according to modern, urban, multi-modal guidelines **8**
- Reduce high-speed ramps through the urban core **7**
- Achieve best possible life-cycle return on investment with project funds **2**
- Improve traffic flow on the interstate **5**
- Improve safety for automobiles traveling on the interstate corridor **3**
- Ensure the interstate and ramps are designed according to highway design guideline manuals **9**
- Maintain all on-ramp and off-ramp access points to/from the interstate **10**

Q4

Please provide your contact information. This will be kept confidential.

Name

State/Province **OH**

ZIP/Postal Code **45223**

Email Address

#124

COMPLETE

Collector: BF Link (Web Link)
Started: Thursday, January 05, 2023 8:51:11 PM
Last Modified: Thursday, January 05, 2023 8:54:49 PM
Time Spent: 00:03:38
IP Address: 184.54.149.175

Page 1

Q1

Local Government

The Brent Spence Corridor Project will be built using a “progressive design-build process.” This process allows the state highway departments, the design-build team (i.e. the contractor), impacted municipalities, and the local community to consider and address major project topics in a more open format. The design-build team will work with the state highway departments to create the basis of design, overall project requirements, and final design intent, before work substantially begins, during what is being called Phase 1A and Phase 1B of the project. Therefore, the following questions are particularly salient: Which level of government do you believe is better positioned to represent your interests in this Project [choose one]:

Q2

With funding secured, take the time to get the design right before starting construction

In terms of Project schedule, which of the following aligns most closely with your view [choose one]:

Q3

Finally, with respect to what the Project could accomplish, please rank the following priorities according to their importance to you [rank]:

Minimize interstate footprint / return land to the region's urban core	7
Create more walkable street frontage for development	3
Increase multi-modal connectivity across the interstate corridor	5
Ensure local streets impacted by the project are designed according to modern, urban, multi-modal guidelines	10
Reduce high-speed ramps through the urban core	2
Achieve best possible life-cycle return on investment with project funds	4
Improve traffic flow on the interstate	6
Improve safety for automobiles traveling on the interstate corridor	1
Ensure the interstate and ramps are designed according to highway design guideline manuals	9
Maintain all on-ramp and off-ramp access points to/from the interstate	8

Q4

Please provide your contact information. This will be kept confidential.

Name	
State/Province	OH
ZIP/Postal Code	45219
Email Address	

#125

COMPLETE

Collector: BF Link (Web Link)
Started: Thursday, January 05, 2023 9:53:53 PM
Last Modified: Thursday, January 05, 2023 9:57:03 PM
Time Spent: 00:03:09
IP Address: 74.215.3.12

Page 1

Q1

Local Government

The Brent Spence Corridor Project will be built using a “progressive design-build process.” This process allows the state highway departments, the design-build team (i.e. the contractor), impacted municipalities, and the local community to consider and address major project topics in a more open format. The design-build team will work with the state highway departments to create the basis of design, overall project requirements, and final design intent, before work substantially begins, during what is being called Phase 1A and Phase 1B of the project. Therefore, the following questions are particularly salient: Which level of government do you believe is better positioned to represent your interests in this Project [choose one]:

Q2

With funding secured, take the time to get the design right before starting construction

In terms of Project schedule, which of the following aligns most closely with your view [choose one]:

Q3

Finally, with respect to what the Project could accomplish, please rank the following priorities according to their importance to you [rank]:

- Minimize interstate footprint / return land to the region's urban core **1**
- Create more walkable street frontage for development **4**
- Increase multi-modal connectivity across the interstate corridor **5**
- Ensure local streets impacted by the project are designed according to modern, urban, multi-modal guidelines **8**
- Reduce high-speed ramps through the urban core **7**
- Achieve best possible life-cycle return on investment with project funds **2**
- Improve traffic flow on the interstate **3**
- Improve safety for automobiles traveling on the interstate corridor **6**
- Ensure the interstate and ramps are designed according to highway design guideline manuals **9**
- Maintain all on-ramp and off-ramp access points to/from the interstate **10**

Q4

Please provide your contact information. This will be kept confidential.

Name

State/Province **OH**

ZIP/Postal Code **45214**

Email Address

#126

COMPLETE

Collector: BF Link (Web Link)
Started: Friday, January 06, 2023 8:53:51 AM
Last Modified: Friday, January 06, 2023 8:56:56 AM
Time Spent: 00:03:05
IP Address: 216.48.135.104

Page 1

Q1

Local Government

The Brent Spence Corridor Project will be built using a “progressive design-build process.” This process allows the state highway departments, the design-build team (i.e. the contractor), impacted municipalities, and the local community to consider and address major project topics in a more open format. The design-build team will work with the state highway departments to create the basis of design, overall project requirements, and final design intent, before work substantially begins, during what is being called Phase 1A and Phase 1B of the project. Therefore, the following questions are particularly salient: Which level of government do you believe is better positioned to represent your interests in this Project [choose one]:

Q2

With funding secured, take the time to get the design right before starting construction

In terms of Project schedule, which of the following aligns most closely with your view [choose one]:

Q3

Finally, with respect to what the Project could accomplish, please rank the following priorities according to their importance to you [rank]:

- Minimize interstate footprint / return land to the region's urban core **5**
- Create more walkable street frontage for development **1**
- Increase multi-modal connectivity across the interstate corridor **3**
- Ensure local streets impacted by the project are designed according to modern, urban, multi-modal guidelines **2**
- Reduce high-speed ramps through the urban core **6**
- Achieve best possible life-cycle return on investment with project funds **7**
- Improve traffic flow on the interstate **8**
- Improve safety for automobiles traveling on the interstate corridor **4**
- Ensure the interstate and ramps are designed according to highway design guideline manuals **9**
- Maintain all on-ramp and off-ramp access points to/from the interstate **10**

Q4

Please provide your contact information. This will be kept confidential.

- Name
- State/Province **OH**
- ZIP/Postal Code **45014**
- Email Address

#127

COMPLETE

Collector: BF Link (Web Link)
Started: Friday, January 06, 2023 9:37:03 PM
Last Modified: Friday, January 06, 2023 9:43:42 PM
Time Spent: 00:06:39
IP Address: 174.102.13.235

Page 1

Q1

Local Government

The Brent Spence Corridor Project will be built using a “progressive design-build process.” This process allows the state highway departments, the design-build team (i.e. the contractor), impacted municipalities, and the local community to consider and address major project topics in a more open format. The design-build team will work with the state highway departments to create the basis of design, overall project requirements, and final design intent, before work substantially begins, during what is being called Phase 1A and Phase 1B of the project. Therefore, the following questions are particularly salient: Which level of government do you believe is better positioned to represent your interests in this Project [choose one]:

Q2

With funding secured, take the time to get the design right before starting construction

In terms of Project schedule, which of the following aligns most closely with your view [choose one]:

Q3

Finally, with respect to what the Project could accomplish, please rank the following priorities according to their importance to you [rank]:

- Minimize interstate footprint / return land to the region's urban core **1**
- Create more walkable street frontage for development **6**
- Increase multi-modal connectivity across the interstate corridor **8**
- Ensure local streets impacted by the project are designed according to modern, urban, multi-modal guidelines **2**
- Reduce high-speed ramps through the urban core **3**
- Achieve best possible life-cycle return on investment with project funds **9**
- Improve traffic flow on the interstate **4**
- Improve safety for automobiles traveling on the interstate corridor **5**
- Ensure the interstate and ramps are designed according to highway design guideline manuals **7**
- Maintain all on-ramp and off-ramp access points to/from the interstate **10**

Q4

Please provide your contact information. This will be kept confidential.

Name

State/Province **OH**

ZIP/Postal Code **45219-1037**

Email Address

#128

COMPLETE

Collector: BF Link (Web Link)
Started: Saturday, January 07, 2023 7:21:25 PM
Last Modified: Saturday, January 07, 2023 7:32:02 PM
Time Spent: 00:10:36
IP Address: 184.54.83.123

Page 1

Q1

Local Government

The Brent Spence Corridor Project will be built using a “progressive design-build process.” This process allows the state highway departments, the design-build team (i.e. the contractor), impacted municipalities, and the local community to consider and address major project topics in a more open format. The design-build team will work with the state highway departments to create the basis of design, overall project requirements, and final design intent, before work substantially begins, during what is being called Phase 1A and Phase 1B of the project. Therefore, the following questions are particularly salient: Which level of government do you believe is better positioned to represent your interests in this Project [choose one]:

Q2

With funding secured, take the time to get the design right before starting construction

In terms of Project schedule, which of the following aligns most closely with your view [choose one]:

Q3

Finally, with respect to what the Project could accomplish, please rank the following priorities according to their importance to you [rank]:

- Minimize interstate footprint / return land to the region's urban core **1**
- Create more walkable street frontage for development **2**
- Increase multi-modal connectivity across the interstate corridor **3**
- Ensure local streets impacted by the project are designed according to modern, urban, multi-modal guidelines **5**
- Reduce high-speed ramps through the urban core **9**
- Achieve best possible life-cycle return on investment with project funds **4**
- Improve traffic flow on the interstate **8**
- Improve safety for automobiles traveling on the interstate corridor **6**
- Ensure the interstate and ramps are designed according to highway design guideline manuals **7**
- Maintain all on-ramp and off-ramp access points to/from the interstate **10**

Q4

Please provide your contact information. This will be kept confidential.

Name

State/Province **OH**

ZIP/Postal Code **45206**

Email Address

#129

COMPLETE

Collector: BF Link (Web Link)
Started: Sunday, January 08, 2023 2:14:44 PM
Last Modified: Sunday, January 08, 2023 2:49:35 PM
Time Spent: 00:34:51
IP Address: 174.193.115.107

Page 1

Q1

Local Government

The Brent Spence Corridor Project will be built using a “progressive design-build process.” This process allows the state highway departments, the design-build team (i.e. the contractor), impacted municipalities, and the local community to consider and address major project topics in a more open format. The design-build team will work with the state highway departments to create the basis of design, overall project requirements, and final design intent, before work substantially begins, during what is being called Phase 1A and Phase 1B of the project. Therefore, the following questions are particularly salient: Which level of government do you believe is better positioned to represent your interests in this Project [choose one]:

Q2

With funding secured, take the time to get the design right before starting construction

In terms of Project schedule, which of the following aligns most closely with your view [choose one]:

Q3

Finally, with respect to what the Project could accomplish, please rank the following priorities according to their importance to you [rank]:

- Minimize interstate footprint / return land to the region's urban core **1**
- Create more walkable street frontage for development **2**
- Increase multi-modal connectivity across the interstate corridor **6**
- Ensure local streets impacted by the project are designed according to modern, urban, multi-modal guidelines **9**
- Reduce high-speed ramps through the urban core **3**
- Achieve best possible life-cycle return on investment with project funds **5**
- Improve traffic flow on the interstate **7**
- Improve safety for automobiles traveling on the interstate corridor **4**
- Ensure the interstate and ramps are designed according to highway design guideline manuals **8**
- Maintain all on-ramp and off-ramp access points to/from the interstate **10**

Q4

Please provide your contact information. This will be kept confidential.

- Name
- State/Province **OH**
- ZIP/Postal Code **45223**
- Email Address

#130

COMPLETE

Collector: BF Link (Web Link)
Started: Monday, January 09, 2023 3:22:45 PM
Last Modified: Monday, January 09, 2023 3:24:53 PM
Time Spent: 00:02:08
IP Address: 50.5.227.141

Page 1

Q1**Local Government**

The Brent Spence Corridor Project will be built using a “progressive design-build process.” This process allows the state highway departments, the design-build team (i.e. the contractor), impacted municipalities, and the local community to consider and address major project topics in a more open format. The design-build team will work with the state highway departments to create the basis of design, overall project requirements, and final design intent, before work substantially begins, during what is being called Phase 1A and Phase 1B of the project. Therefore, the following questions are particularly salient: Which level of government do you believe is better positioned to represent your interests in this Project [choose one]:

Q2**With funding secured, take the time to get the design right before starting construction**

In terms of Project schedule, which of the following aligns most closely with your view [choose one]:

Q3

Finally, with respect to what the Project could accomplish, please rank the following priorities according to their importance to you [rank]:

Minimize interstate footprint / return land to the region's urban core	1
Create more walkable street frontage for development	4
Increase multi-modal connectivity across the interstate corridor	5
Ensure local streets impacted by the project are designed according to modern, urban, multi-modal guidelines	7
Reduce high-speed ramps through the urban core	6
Achieve best possible life-cycle return on investment with project funds	3
Improve traffic flow on the interstate	9
Improve safety for automobiles traveling on the interstate corridor	2
Ensure the interstate and ramps are designed according to highway design guideline manuals	8
Maintain all on-ramp and off-ramp access points to/from the interstate	10

Q4

Please provide your contact information. This will be kept confidential.

Name	OH
State/Province	45220
ZIP/Postal Code	
Email Address	

#131

COMPLETE

Collector: BF Link (Web Link)
Started: Monday, January 09, 2023 3:31:08 PM
Last Modified: Monday, January 09, 2023 3:33:37 PM
Time Spent: 00:02:28
IP Address: 206.127.53.42

Page 1

Q1

Local Government

The Brent Spence Corridor Project will be built using a “progressive design-build process.” This process allows the state highway departments, the design-build team (i.e. the contractor), impacted municipalities, and the local community to consider and address major project topics in a more open format. The design-build team will work with the state highway departments to create the basis of design, overall project requirements, and final design intent, before work substantially begins, during what is being called Phase 1A and Phase 1B of the project. Therefore, the following questions are particularly salient: Which level of government do you believe is better positioned to represent your interests in this Project [choose one]:

Q2

With funding secured, take the time to get the design right before starting construction

In terms of Project schedule, which of the following aligns most closely with your view [choose one]:

Q3

Finally, with respect to what the Project could accomplish, please rank the following priorities according to their importance to you [rank]:

- Minimize interstate footprint / return land to the region's urban core **1**
- Create more walkable street frontage for development **2**
- Increase multi-modal connectivity across the interstate corridor **3**
- Ensure local streets impacted by the project are designed according to modern, urban, multi-modal guidelines **4**
- Reduce high-speed ramps through the urban core **5**
- Achieve best possible life-cycle return on investment with project funds **7**
- Improve traffic flow on the interstate **8**
- Improve safety for automobiles traveling on the interstate corridor **6**
- Ensure the interstate and ramps are designed according to highway design guideline manuals **10**
- Maintain all on-ramp and off-ramp access points to/from the interstate **9**

Q4

Please provide your contact information. This will be kept confidential.

Name

State/Province **IN**

ZIP/Postal Code **47906**

Email Address

#132

COMPLETE

Collector: BF Link (Web Link)
Started: Monday, January 09, 2023 8:57:16 PM
Last Modified: Monday, January 09, 2023 9:00:38 PM
Time Spent: 00:03:21
IP Address: 75.185.248.227

Page 1

Q1

Local Government

The Brent Spence Corridor Project will be built using a “progressive design-build process.” This process allows the state highway departments, the design-build team (i.e. the contractor), impacted municipalities, and the local community to consider and address major project topics in a more open format. The design-build team will work with the state highway departments to create the basis of design, overall project requirements, and final design intent, before work substantially begins, during what is being called Phase 1A and Phase 1B of the project. Therefore, the following questions are particularly salient: Which level of government do you believe is better positioned to represent your interests in this Project [choose one]:

Q2

With funding secured, take the time to get the design right before starting construction

In terms of Project schedule, which of the following aligns most closely with your view [choose one]:

Q3

Finally, with respect to what the Project could accomplish, please rank the following priorities according to their importance to you [rank]:

- Minimize interstate footprint / return land to the region's urban core **2**
- Create more walkable street frontage for development **1**
- Increase multi-modal connectivity across the interstate corridor **6**
- Ensure local streets impacted by the project are designed according to modern, urban, multi-modal guidelines **3**
- Reduce high-speed ramps through the urban core **5**
- Achieve best possible life-cycle return on investment with project funds **7**
- Improve traffic flow on the interstate **8**
- Improve safety for automobiles traveling on the interstate corridor **4**
- Ensure the interstate and ramps are designed according to highway design guideline manuals **9**
- Maintain all on-ramp and off-ramp access points to/from the interstate **10**

Q4

Please provide your contact information. This will be kept confidential.

- Name
- State/Province **OH**
- ZIP/Postal Code **45103**
- Email Address

#133

COMPLETE

Collector: BF Link (Web Link)
Started: Monday, January 09, 2023 10:38:35 PM
Last Modified: Monday, January 09, 2023 10:43:24 PM
Time Spent: 00:04:49
IP Address: 104.28.32.134

Page 1

Q1**Local Government**

The Brent Spence Corridor Project will be built using a “progressive design-build process.” This process allows the state highway departments, the design-build team (i.e. the contractor), impacted municipalities, and the local community to consider and address major project topics in a more open format. The design-build team will work with the state highway departments to create the basis of design, overall project requirements, and final design intent, before work substantially begins, during what is being called Phase 1A and Phase 1B of the project. Therefore, the following questions are particularly salient: Which level of government do you believe is better positioned to represent your interests in this Project [choose one]:

Q2**With funding secured, take the time to get the design right before starting construction**

In terms of Project schedule, which of the following aligns most closely with your view [choose one]:

Q3

Finally, with respect to what the Project could accomplish, please rank the following priorities according to their importance to you [rank]:

- Minimize interstate footprint / return land to the region's urban core **1**
- Create more walkable street frontage for development **3**
- Increase multi-modal connectivity across the interstate corridor **9**
- Ensure local streets impacted by the project are designed according to modern, urban, multi-modal guidelines **5**
- Reduce high-speed ramps through the urban core **6**
- Achieve best possible life-cycle return on investment with project funds **2**
- Improve traffic flow on the interstate **7**
- Improve safety for automobiles traveling on the interstate corridor **4**
- Ensure the interstate and ramps are designed according to highway design guideline manuals **8**
- Maintain all on-ramp and off-ramp access points to/from the interstate **10**

Q4

Please provide your contact information. This will be kept confidential.

Name

State/Province **OH**

ZIP/Postal Code **45251**

Email Address

#134

COMPLETE

Collector: BF Link (Web Link)
Started: Wednesday, January 11, 2023 2:56:35 PM
Last Modified: Wednesday, January 11, 2023 2:58:36 PM
Time Spent: 00:02:00
IP Address: 98.103.158.146

Page 1

Q1**Local Government**

The Brent Spence Corridor Project will be built using a “progressive design-build process.” This process allows the state highway departments, the design-build team (i.e. the contractor), impacted municipalities, and the local community to consider and address major project topics in a more open format. The design-build team will work with the state highway departments to create the basis of design, overall project requirements, and final design intent, before work substantially begins, during what is being called Phase 1A and Phase 1B of the project. Therefore, the following questions are particularly salient: Which level of government do you believe is better positioned to represent your interests in this Project [choose one]:

Q2**With funding secured, take the time to get the design right before starting construction**

In terms of Project schedule, which of the following aligns most closely with your view [choose one]:

Q3

Finally, with respect to what the Project could accomplish, please rank the following priorities according to their importance to you [rank]:

Minimize interstate footprint / return land to the region's urban core	7
Create more walkable street frontage for development	1
Increase multi-modal connectivity across the interstate corridor	5
Ensure local streets impacted by the project are designed according to modern, urban, multi-modal guidelines	6
Reduce high-speed ramps through the urban core	4
Achieve best possible life-cycle return on investment with project funds	10
Improve traffic flow on the interstate	2
Improve safety for automobiles traveling on the interstate corridor	3
Ensure the interstate and ramps are designed according to highway design guideline manuals	8
Maintain all on-ramp and off-ramp access points to/from the interstate	9

Q4

Please provide your contact information. This will be kept confidential.

Name	
State/Province	KY
ZIP/Postal Code	41071
Email Address	

#135

COMPLETE

Collector: BF Link (Web Link)
Started: Wednesday, January 11, 2023 5:01:53 PM
Last Modified: Wednesday, January 11, 2023 5:05:35 PM
Time Spent: 00:03:41
IP Address: 74.215.235.133

Page 1

Q1

Local Government

The Brent Spence Corridor Project will be built using a “progressive design-build process.” This process allows the state highway departments, the design-build team (i.e. the contractor), impacted municipalities, and the local community to consider and address major project topics in a more open format. The design-build team will work with the state highway departments to create the basis of design, overall project requirements, and final design intent, before work substantially begins, during what is being called Phase 1A and Phase 1B of the project. Therefore, the following questions are particularly salient: Which level of government do you believe is better positioned to represent your interests in this Project [choose one]:

Q2

With funding secured, take the time to get the design right before starting construction

In terms of Project schedule, which of the following aligns most closely with your view [choose one]:

Q3

Finally, with respect to what the Project could accomplish, please rank the following priorities according to their importance to you [rank]:

- Minimize interstate footprint / return land to the region's urban core **2**
- Create more walkable street frontage for development **1**
- Increase multi-modal connectivity across the interstate corridor **6**
- Ensure local streets impacted by the project are designed according to modern, urban, multi-modal guidelines **3**
- Reduce high-speed ramps through the urban core **4**
- Achieve best possible life-cycle return on investment with project funds **7**
- Improve traffic flow on the interstate **9**
- Improve safety for automobiles traveling on the interstate corridor **5**
- Ensure the interstate and ramps are designed according to highway design guideline manuals **8**
- Maintain all on-ramp and off-ramp access points to/from the interstate **10**

Q4

Please provide your contact information. This will be kept confidential.

- Name
- State/Province **OH**
- ZIP/Postal Code **45040**
- Email Address

#136

COMPLETE

Collector: BF Link (Web Link)
Started: Thursday, January 12, 2023 5:36:54 PM
Last Modified: Thursday, January 12, 2023 5:40:31 PM
Time Spent: 00:03:36
IP Address: 50.5.252.209

Page 1

Q1**Local Government**

The Brent Spence Corridor Project will be built using a “progressive design-build process.” This process allows the state highway departments, the design-build team (i.e. the contractor), impacted municipalities, and the local community to consider and address major project topics in a more open format. The design-build team will work with the state highway departments to create the basis of design, overall project requirements, and final design intent, before work substantially begins, during what is being called Phase 1A and Phase 1B of the project. Therefore, the following questions are particularly salient: Which level of government do you believe is better positioned to represent your interests in this Project [choose one]:

Q2**With funding secured, take the time to get the design right before starting construction**

In terms of Project schedule, which of the following aligns most closely with your view [choose one]:

Q3

Finally, with respect to what the Project could accomplish, please rank the following priorities according to their importance to you [rank]:

- Minimize interstate footprint / return land to the region's urban core **4**
- Create more walkable street frontage for development **1**
- Increase multi-modal connectivity across the interstate corridor **2**
- Ensure local streets impacted by the project are designed according to modern, urban, multi-modal guidelines **3**
- Reduce high-speed ramps through the urban core **6**
- Achieve best possible life-cycle return on investment with project funds **5**
- Improve traffic flow on the interstate **8**
- Improve safety for automobiles traveling on the interstate corridor **7**
- Ensure the interstate and ramps are designed according to highway design guideline manuals **9**
- Maintain all on-ramp and off-ramp access points to/from the interstate **10**

Q4

Please provide your contact information. This will be kept confidential.

Name

State/Province **OH**

ZIP/Postal Code **45150**

Email Address

#137

COMPLETE

Collector: BF Link (Web Link)
Started: Saturday, January 14, 2023 9:11:04 AM
Last Modified: Saturday, January 14, 2023 9:14:28 AM
Time Spent: 00:03:24
IP Address: 192.181.164.11

Page 1

Q1

Local Government

The Brent Spence Corridor Project will be built using a “progressive design-build process.” This process allows the state highway departments, the design-build team (i.e. the contractor), impacted municipalities, and the local community to consider and address major project topics in a more open format. The design-build team will work with the state highway departments to create the basis of design, overall project requirements, and final design intent, before work substantially begins, during what is being called Phase 1A and Phase 1B of the project. Therefore, the following questions are particularly salient: Which level of government do you believe is better positioned to represent your interests in this Project [choose one]:

Q2

With funding secured, take the time to get the design right before starting construction

In terms of Project schedule, which of the following aligns most closely with your view [choose one]:

Q3

Finally, with respect to what the Project could accomplish, please rank the following priorities according to their importance to you [rank]:

- Minimize interstate footprint / return land to the region's urban core **1**
- Create more walkable street frontage for development **3**
- Increase multi-modal connectivity across the interstate corridor **2**
- Ensure local streets impacted by the project are designed according to modern, urban, multi-modal guidelines **6**
- Reduce high-speed ramps through the urban core **7**
- Achieve best possible life-cycle return on investment with project funds **5**
- Improve traffic flow on the interstate **8**
- Improve safety for automobiles traveling on the interstate corridor **4**
- Ensure the interstate and ramps are designed according to highway design guideline manuals **10**
- Maintain all on-ramp and off-ramp access points to/from the interstate **9**

Q4

Please provide your contact information. This will be kept confidential.

- Name
- State/Province **OH**
- ZIP/Postal Code **45152**
- Email Address

#138

COMPLETE

Collector: BF Link (Web Link)
Started: Wednesday, January 18, 2023 3:35:49 PM
Last Modified: Wednesday, January 18, 2023 3:37:36 PM
Time Spent: 00:01:47
IP Address: 98.110.144.169

Page 1

Q1

Local Government

The Brent Spence Corridor Project will be built using a “progressive design-build process.” This process allows the state highway departments, the design-build team (i.e. the contractor), impacted municipalities, and the local community to consider and address major project topics in a more open format. The design-build team will work with the state highway departments to create the basis of design, overall project requirements, and final design intent, before work substantially begins, during what is being called Phase 1A and Phase 1B of the project. Therefore, the following questions are particularly salient: Which level of government do you believe is better positioned to represent your interests in this Project [choose one]:

Q2

With funding secured, take the time to get the design right before starting construction

In terms of Project schedule, which of the following aligns most closely with your view [choose one]:

Q3

Finally, with respect to what the Project could accomplish, please rank the following priorities according to their importance to you [rank]:

Minimize interstate footprint / return land to the region's urban core	4
Create more walkable street frontage for development	3
Increase multi-modal connectivity across the interstate corridor	2
Ensure local streets impacted by the project are designed according to modern, urban, multi-modal guidelines	1
Reduce high-speed ramps through the urban core	5
Achieve best possible life-cycle return on investment with project funds	6
Improve traffic flow on the interstate	9
Improve safety for automobiles traveling on the interstate corridor	8
Ensure the interstate and ramps are designed according to highway design guideline manuals	10
Maintain all on-ramp and off-ramp access points to/from the interstate	7

Q4

Please provide your contact information. This will be kept confidential.

Name

State/Province **MA**

ZIP/Postal Code **02155**

Email Address

#139

COMPLETE

Collector: BF Link (Web Link)
Started: Wednesday, January 18, 2023 4:16:51 PM
Last Modified: Wednesday, January 18, 2023 4:19:44 PM
Time Spent: 00:02:52
IP Address: 172.58.138.106

Page 1

Q1

Local Government

The Brent Spence Corridor Project will be built using a “progressive design-build process.” This process allows the state highway departments, the design-build team (i.e. the contractor), impacted municipalities, and the local community to consider and address major project topics in a more open format. The design-build team will work with the state highway departments to create the basis of design, overall project requirements, and final design intent, before work substantially begins, during what is being called Phase 1A and Phase 1B of the project. Therefore, the following questions are particularly salient: Which level of government do you believe is better positioned to represent your interests in this Project [choose one]:

Q2

With funding secured, take the time to get the design right before starting construction

In terms of Project schedule, which of the following aligns most closely with your view [choose one]:

Q3

Finally, with respect to what the Project could accomplish, please rank the following priorities according to their importance to you [rank]:

- Minimize interstate footprint / return land to the region's urban core **1**
- Create more walkable street frontage for development **2**
- Increase multi-modal connectivity across the interstate corridor **4**
- Ensure local streets impacted by the project are designed according to modern, urban, multi-modal guidelines **6**
- Reduce high-speed ramps through the urban core **9**
- Achieve best possible life-cycle return on investment with project funds **3**
- Improve traffic flow on the interstate **7**
- Improve safety for automobiles traveling on the interstate corridor **5**
- Ensure the interstate and ramps are designed according to highway design guideline manuals **8**
- Maintain all on-ramp and off-ramp access points to/from the interstate **10**

Q4

Please provide your contact information. This will be kept confidential.

- Name
- State/Province **OH**
- ZIP/Postal Code **45219**
- Email Address

#140

COMPLETE

Collector: BF Link (Web Link)
Started: Thursday, January 19, 2023 7:52:46 PM
Last Modified: Thursday, January 19, 2023 7:55:52 PM
Time Spent: 00:03:05
IP Address: 216.196.159.18

Page 1

Q1**Local Government**

The Brent Spence Corridor Project will be built using a “progressive design-build process.” This process allows the state highway departments, the design-build team (i.e. the contractor), impacted municipalities, and the local community to consider and address major project topics in a more open format. The design-build team will work with the state highway departments to create the basis of design, overall project requirements, and final design intent, before work substantially begins, during what is being called Phase 1A and Phase 1B of the project. Therefore, the following questions are particularly salient: Which level of government do you believe is better positioned to represent your interests in this Project [choose one]:

Q2**With funding secured, take the time to get the design right before starting construction**

In terms of Project schedule, which of the following aligns most closely with your view [choose one]:

Q3

Finally, with respect to what the Project could accomplish, please rank the following priorities according to their importance to you [rank]:

Minimize interstate footprint / return land to the region's urban core	3
Create more walkable street frontage for development	1
Increase multi-modal connectivity across the interstate corridor	8
Ensure local streets impacted by the project are designed according to modern, urban, multi-modal guidelines	2
Reduce high-speed ramps through the urban core	5
Achieve best possible life-cycle return on investment with project funds	4
Improve traffic flow on the interstate	9
Improve safety for automobiles traveling on the interstate corridor	7
Ensure the interstate and ramps are designed according to highway design guideline manuals	10
Maintain all on-ramp and off-ramp access points to/from the interstate	6

Q4

Please provide your contact information. This will be kept confidential.

Name	
State/Province	OH
ZIP/Postal Code	45214
Email Address	

#141

COMPLETE

Collector: BF Link (Web Link)
Started: Wednesday, January 25, 2023 7:34:55 AM
Last Modified: Wednesday, January 25, 2023 7:36:51 AM
Time Spent: 00:01:56
IP Address: 144.57.47.4

Page 1

Q1

Local Government

The Brent Spence Corridor Project will be built using a “progressive design-build process.” This process allows the state highway departments, the design-build team (i.e. the contractor), impacted municipalities, and the local community to consider and address major project topics in a more open format. The design-build team will work with the state highway departments to create the basis of design, overall project requirements, and final design intent, before work substantially begins, during what is being called Phase 1A and Phase 1B of the project. Therefore, the following questions are particularly salient: Which level of government do you believe is better positioned to represent your interests in this Project [choose one]:

Q2

With funding secured, take the time to get the design right before starting construction

In terms of Project schedule, which of the following aligns most closely with your view [choose one]:

Q3

Finally, with respect to what the Project could accomplish, please rank the following priorities according to their importance to you [rank]:

- Minimize interstate footprint / return land to the region's urban core **1**
- Create more walkable street frontage for development **6**
- Increase multi-modal connectivity across the interstate corridor **7**
- Ensure local streets impacted by the project are designed according to modern, urban, multi-modal guidelines **4**
- Reduce high-speed ramps through the urban core **5**
- Achieve best possible life-cycle return on investment with project funds **2**
- Improve traffic flow on the interstate **9**
- Improve safety for automobiles traveling on the interstate corridor **3**
- Ensure the interstate and ramps are designed according to highway design guideline manuals **8**
- Maintain all on-ramp and off-ramp access points to/from the interstate **10**

Q4

Please provide your contact information. This will be kept confidential.

- Name
- State/Province **OH**
- ZIP/Postal Code **45220**
- Email Address

#142

COMPLETE

Collector: BF Link (Web Link)
Started: Wednesday, January 25, 2023 2:54:07 PM
Last Modified: Wednesday, January 25, 2023 3:00:12 PM
Time Spent: 00:06:05
IP Address: 74.215.49.50

Page 1

Q1

Local Government

The Brent Spence Corridor Project will be built using a “progressive design-build process.” This process allows the state highway departments, the design-build team (i.e. the contractor), impacted municipalities, and the local community to consider and address major project topics in a more open format. The design-build team will work with the state highway departments to create the basis of design, overall project requirements, and final design intent, before work substantially begins, during what is being called Phase 1A and Phase 1B of the project. Therefore, the following questions are particularly salient: Which level of government do you believe is better positioned to represent your interests in this Project [choose one]:

Q2

With funding secured, take the time to get the design right before starting construction

In terms of Project schedule, which of the following aligns most closely with your view [choose one]:

Q3

Finally, with respect to what the Project could accomplish, please rank the following priorities according to their importance to you [rank]:

Minimize interstate footprint / return land to the region's urban core	9
Create more walkable street frontage for development	5
Increase multi-modal connectivity across the interstate corridor	7
Ensure local streets impacted by the project are designed according to modern, urban, multi-modal guidelines	6
Reduce high-speed ramps through the urban core	3
Achieve best possible life-cycle return on investment with project funds	8
Improve traffic flow on the interstate	4
Improve safety for automobiles traveling on the interstate corridor	1
Ensure the interstate and ramps are designed according to highway design guideline manuals	2
Maintain all on-ramp and off-ramp access points to/from the interstate	10

Q4

Please provide your contact information. This will be kept confidential.

Name	
State/Province	OH
ZIP/Postal Code	45237
Email Address	

#143

COMPLETE

Collector: BF Link (Web Link)
Started: Thursday, January 26, 2023 8:28:19 AM
Last Modified: Thursday, January 26, 2023 8:33:42 AM
Time Spent: 00:05:23
IP Address: 192.24.170.77

Page 1

Q1

Local Government

The Brent Spence Corridor Project will be built using a “progressive design-build process.” This process allows the state highway departments, the design-build team (i.e. the contractor), impacted municipalities, and the local community to consider and address major project topics in a more open format. The design-build team will work with the state highway departments to create the basis of design, overall project requirements, and final design intent, before work substantially begins, during what is being called Phase 1A and Phase 1B of the project. Therefore, the following questions are particularly salient: Which level of government do you believe is better positioned to represent your interests in this Project [choose one]:

Q2

Design doesn't matter; get construction started immediately

In terms of Project schedule, which of the following aligns most closely with your view [choose one]:

Q3

Finally, with respect to what the Project could accomplish, please rank the following priorities according to their importance to you [rank]:

Minimize interstate footprint / return land to the region's urban core	8
Create more walkable street frontage for development	10
Increase multi-modal connectivity across the interstate corridor	7
Ensure local streets impacted by the project are designed according to modern, urban, multi-modal guidelines	5
Reduce high-speed ramps through the urban core	6
Achieve best possible life-cycle return on investment with project funds	9
Improve traffic flow on the interstate	1
Improve safety for automobiles traveling on the interstate corridor	2
Ensure the interstate and ramps are designed according to highway design guideline manuals	3
Maintain all on-ramp and off-ramp access points to/from the interstate	4

Q4

Please provide your contact information. This will be kept confidential.

Name	
State/Province	KY
ZIP/Postal Code	41015
Email Address	

#144

COMPLETE

Collector: BF Link (Web Link)
Started: Thursday, January 26, 2023 6:21:05 PM
Last Modified: Thursday, January 26, 2023 6:29:13 PM
Time Spent: 00:08:08
IP Address: 75.187.114.139

Page 1

Q1

Local Government

The Brent Spence Corridor Project will be built using a “progressive design-build process.” This process allows the state highway departments, the design-build team (i.e. the contractor), impacted municipalities, and the local community to consider and address major project topics in a more open format. The design-build team will work with the state highway departments to create the basis of design, overall project requirements, and final design intent, before work substantially begins, during what is being called Phase 1A and Phase 1B of the project. Therefore, the following questions are particularly salient: Which level of government do you believe is better positioned to represent your interests in this Project [choose one]:

Q2

With funding secured, take the time to get the design right before starting construction

In terms of Project schedule, which of the following aligns most closely with your view [choose one]:

Q3

Finally, with respect to what the Project could accomplish, please rank the following priorities according to their importance to you [rank]:

Minimize interstate footprint / return land to the region's urban core	8
Create more walkable street frontage for development	10
Increase multi-modal connectivity across the interstate corridor	3
Ensure local streets impacted by the project are designed according to modern, urban, multi-modal guidelines	2
Reduce high-speed ramps through the urban core	6
Achieve best possible life-cycle return on investment with project funds	1
Improve traffic flow on the interstate	7
Improve safety for automobiles traveling on the interstate corridor	5
Ensure the interstate and ramps are designed according to highway design guideline manuals	9
Maintain all on-ramp and off-ramp access points to/from the interstate	4

Q4

Please provide your contact information. This will be kept confidential.

Name	
State/Province	OH
ZIP/Postal Code	45225
Email Address	

#145

COMPLETE

Collector: BF Link (Web Link)
Started: Friday, January 27, 2023 11:58:53 AM
Last Modified: Friday, January 27, 2023 12:01:33 PM
Time Spent: 00:02:40
IP Address: 66.162.53.200

Page 1

Q1

Local Government

The Brent Spence Corridor Project will be built using a “progressive design-build process.” This process allows the state highway departments, the design-build team (i.e. the contractor), impacted municipalities, and the local community to consider and address major project topics in a more open format. The design-build team will work with the state highway departments to create the basis of design, overall project requirements, and final design intent, before work substantially begins, during what is being called Phase 1A and Phase 1B of the project. Therefore, the following questions are particularly salient: Which level of government do you believe is better positioned to represent your interests in this Project [choose one]:

Q2

With funding secured, take the time to get the design right before starting construction

In terms of Project schedule, which of the following aligns most closely with your view [choose one]:

Q3

Finally, with respect to what the Project could accomplish, please rank the following priorities according to their importance to you [rank]:

- Minimize interstate footprint / return land to the region's urban core **3**
- Create more walkable street frontage for development **1**
- Increase multi-modal connectivity across the interstate corridor **2**
- Ensure local streets impacted by the project are designed according to modern, urban, multi-modal guidelines **5**
- Reduce high-speed ramps through the urban core **9**
- Achieve best possible life-cycle return on investment with project funds **4**
- Improve traffic flow on the interstate **6**
- Improve safety for automobiles traveling on the interstate corridor **7**
- Ensure the interstate and ramps are designed according to highway design guideline manuals **8**
- Maintain all on-ramp and off-ramp access points to/from the interstate **10**

Q4

Please provide your contact information. This will be kept confidential.

- Name **OH**
- State/Province **45202**
- ZIP/Postal Code
- Email Address

#146

COMPLETE

Collector: BF Link (Web Link)
Started: Tuesday, January 31, 2023 7:19:42 PM
Last Modified: Tuesday, January 31, 2023 7:22:22 PM
Time Spent: 00:02:40
IP Address: 50.5.85.209

Page 1

Q1

State Government

The Brent Spence Corridor Project will be built using a “progressive design-build process.” This process allows the state highway departments, the design-build team (i.e. the contractor), impacted municipalities, and the local community to consider and address major project topics in a more open format. The design-build team will work with the state highway departments to create the basis of design, overall project requirements, and final design intent, before work substantially begins, during what is being called Phase 1A and Phase 1B of the project. Therefore, the following questions are particularly salient: Which level of government do you believe is better positioned to represent your interests in this Project [choose one]:

Q2

With funding secured, take the time to get the design right before starting construction

In terms of Project schedule, which of the following aligns most closely with your view [choose one]:

Q3

Finally, with respect to what the Project could accomplish, please rank the following priorities according to their importance to you [rank]:

Minimize interstate footprint / return land to the region's urban core	1
Create more walkable street frontage for development	3
Increase multi-modal connectivity across the interstate corridor	6
Ensure local streets impacted by the project are designed according to modern, urban, multi-modal guidelines	5
Reduce high-speed ramps through the urban core	4
Achieve best possible life-cycle return on investment with project funds	2
Improve traffic flow on the interstate	9
Improve safety for automobiles traveling on the interstate corridor	7
Ensure the interstate and ramps are designed according to highway design guideline manuals	8
Maintain all on-ramp and off-ramp access points to/from the interstate	10

Q4

Please provide your contact information. This will be kept confidential.

Name	
State/Province	OH
ZIP/Postal Code	45219
Email Address	

#147

COMPLETE

Collector: BF Link (Web Link)
Started: Wednesday, February 01, 2023 2:13:17 PM
Last Modified: Wednesday, February 01, 2023 2:16:27 PM
Time Spent: 00:03:09
IP Address: 65.28.253.53

Page 1

Q1

Local Government

The Brent Spence Corridor Project will be built using a “progressive design-build process.” This process allows the state highway departments, the design-build team (i.e. the contractor), impacted municipalities, and the local community to consider and address major project topics in a more open format. The design-build team will work with the state highway departments to create the basis of design, overall project requirements, and final design intent, before work substantially begins, during what is being called Phase 1A and Phase 1B of the project. Therefore, the following questions are particularly salient: Which level of government do you believe is better positioned to represent your interests in this Project [choose one]:

Q2

With funding secured, take the time to get the design right before starting construction

In terms of Project schedule, which of the following aligns most closely with your view [choose one]:

Q3

Finally, with respect to what the Project could accomplish, please rank the following priorities according to their importance to you [rank]:

Minimize interstate footprint / return land to the region's urban core	3
Create more walkable street frontage for development	4
Increase multi-modal connectivity across the interstate corridor	5
Ensure local streets impacted by the project are designed according to modern, urban, multi-modal guidelines	6
Reduce high-speed ramps through the urban core	8
Achieve best possible life-cycle return on investment with project funds	9
Improve traffic flow on the interstate	1
Improve safety for automobiles traveling on the interstate corridor	2
Ensure the interstate and ramps are designed according to highway design guideline manuals	7
Maintain all on-ramp and off-ramp access points to/from the interstate	10

Q4

Please provide your contact information. This will be kept confidential.

Name	
State/Province	OH
ZIP/Postal Code	45237
Email Address	

#148

COMPLETE

Collector: BF Link (Web Link)
Started: Thursday, February 02, 2023 7:16:36 PM
Last Modified: Thursday, February 02, 2023 7:18:06 PM
Time Spent: 00:01:30
IP Address: 107.77.195.183

Page 1

Q1

Local Government

The Brent Spence Corridor Project will be built using a “progressive design-build process.” This process allows the state highway departments, the design-build team (i.e. the contractor), impacted municipalities, and the local community to consider and address major project topics in a more open format. The design-build team will work with the state highway departments to create the basis of design, overall project requirements, and final design intent, before work substantially begins, during what is being called Phase 1A and Phase 1B of the project. Therefore, the following questions are particularly salient: Which level of government do you believe is better positioned to represent your interests in this Project [choose one]:

Q2

With funding secured, take the time to get the design right before starting construction

In terms of Project schedule, which of the following aligns most closely with your view [choose one]:

Q3

Finally, with respect to what the Project could accomplish, please rank the following priorities according to their importance to you [rank]:

- Minimize interstate footprint / return land to the region's urban core **2**
- Create more walkable street frontage for development **3**
- Increase multi-modal connectivity across the interstate corridor **5**
- Ensure local streets impacted by the project are designed according to modern, urban, multi-modal guidelines **1**
- Reduce high-speed ramps through the urban core **4**
- Achieve best possible life-cycle return on investment with project funds **7**
- Improve traffic flow on the interstate **8**
- Improve safety for automobiles traveling on the interstate corridor **9**
- Ensure the interstate and ramps are designed according to highway design guideline manuals **10**
- Maintain all on-ramp and off-ramp access points to/from the interstate **6**

Q4

Please provide your contact information. This will be kept confidential.

- Name
- State/Province **OH**
- ZIP/Postal Code **45219**
- Email Address

#149

COMPLETE

Collector: BF Link (Web Link)
Started: Friday, February 03, 2023 9:58:54 AM
Last Modified: Friday, February 03, 2023 10:01:04 AM
Time Spent: 00:02:09
IP Address: 24.166.33.68

Page 1

Q1

Local Government

The Brent Spence Corridor Project will be built using a “progressive design-build process.” This process allows the state highway departments, the design-build team (i.e. the contractor), impacted municipalities, and the local community to consider and address major project topics in a more open format. The design-build team will work with the state highway departments to create the basis of design, overall project requirements, and final design intent, before work substantially begins, during what is being called Phase 1A and Phase 1B of the project. Therefore, the following questions are particularly salient: Which level of government do you believe is better positioned to represent your interests in this Project [choose one]:

Q2

With funding secured, take the time to get the design right before starting construction

In terms of Project schedule, which of the following aligns most closely with your view [choose one]:

Q3

Finally, with respect to what the Project could accomplish, please rank the following priorities according to their importance to you [rank]:

Minimize interstate footprint / return land to the region's urban core	7
Create more walkable street frontage for development	2
Increase multi-modal connectivity across the interstate corridor	6
Ensure local streets impacted by the project are designed according to modern, urban, multi-modal guidelines	9
Reduce high-speed ramps through the urban core	3
Achieve best possible life-cycle return on investment with project funds	1
Improve traffic flow on the interstate	8
Improve safety for automobiles traveling on the interstate corridor	5
Ensure the interstate and ramps are designed according to highway design guideline manuals	4
Maintain all on-ramp and off-ramp access points to/from the interstate	10

Q4

Please provide your contact information. This will be kept confidential.

Name	
State/Province	OH
ZIP/Postal Code	45050
Email Address	

#150

COMPLETE

Collector: BF Link (Web Link)
Started: Monday, February 06, 2023 7:27:09 PM
Last Modified: Monday, February 06, 2023 7:30:03 PM
Time Spent: 00:02:53
IP Address: 172.56.20.93

Page 1

Q1

Local Government

The Brent Spence Corridor Project will be built using a “progressive design-build process.” This process allows the state highway departments, the design-build team (i.e. the contractor), impacted municipalities, and the local community to consider and address major project topics in a more open format. The design-build team will work with the state highway departments to create the basis of design, overall project requirements, and final design intent, before work substantially begins, during what is being called Phase 1A and Phase 1B of the project. Therefore, the following questions are particularly salient: Which level of government do you believe is better positioned to represent your interests in this Project [choose one]:

Q2

With funding secured, take the time to get the design right before starting construction

In terms of Project schedule, which of the following aligns most closely with your view [choose one]:

Q3

Finally, with respect to what the Project could accomplish, please rank the following priorities according to their importance to you [rank]:

- Minimize interstate footprint / return land to the region's urban core **2**
- Create more walkable street frontage for development **1**
- Increase multi-modal connectivity across the interstate corridor **6**
- Ensure local streets impacted by the project are designed according to modern, urban, multi-modal guidelines **3**
- Reduce high-speed ramps through the urban core **4**
- Achieve best possible life-cycle return on investment with project funds **5**
- Improve traffic flow on the interstate **7**
- Improve safety for automobiles traveling on the interstate corridor **9**
- Ensure the interstate and ramps are designed according to highway design guideline manuals **8**
- Maintain all on-ramp and off-ramp access points to/from the interstate **10**

Q4

Please provide your contact information. This will be kept confidential.

- Name
- State/Province **OH**
- ZIP/Postal Code **45202**
- Email Address

#151

COMPLETE

Collector: BF Link (Web Link)
Started: Wednesday, February 08, 2023 6:42:16 PM
Last Modified: Wednesday, February 08, 2023 6:44:23 PM
Time Spent: 00:02:06
IP Address: 98.102.101.10

Page 1

Q1

Local Government

The Brent Spence Corridor Project will be built using a “progressive design-build process.” This process allows the state highway departments, the design-build team (i.e. the contractor), impacted municipalities, and the local community to consider and address major project topics in a more open format. The design-build team will work with the state highway departments to create the basis of design, overall project requirements, and final design intent, before work substantially begins, during what is being called Phase 1A and Phase 1B of the project. Therefore, the following questions are particularly salient: Which level of government do you believe is better positioned to represent your interests in this Project [choose one]:

Q2

With funding secured, take the time to get the design right before starting construction

In terms of Project schedule, which of the following aligns most closely with your view [choose one]:

Q3

Finally, with respect to what the Project could accomplish, please rank the following priorities according to their importance to you [rank]:

- Minimize interstate footprint / return land to the region's urban core **2**
- Create more walkable street frontage for development **3**
- Increase multi-modal connectivity across the interstate corridor **5**
- Ensure local streets impacted by the project are designed according to modern, urban, multi-modal guidelines **1**
- Reduce high-speed ramps through the urban core **7**
- Achieve best possible life-cycle return on investment with project funds **4**
- Improve traffic flow on the interstate **6**
- Improve safety for automobiles traveling on the interstate corridor **10**
- Ensure the interstate and ramps are designed according to highway design guideline manuals **9**
- Maintain all on-ramp and off-ramp access points to/from the interstate **8**

Q4

Please provide your contact information. This will be kept confidential.

- Name
- State/Province **OH**
- ZIP/Postal Code **45244**
- Email Address

#152

COMPLETE

Collector: BF Link (Web Link)
Started: Monday, February 13, 2023 11:37:03 AM
Last Modified: Monday, February 13, 2023 11:42:55 AM
Time Spent: 00:05:52
IP Address: 74.83.215.149

Page 1

Q1

Local Government

The Brent Spence Corridor Project will be built using a “progressive design-build process.” This process allows the state highway departments, the design-build team (i.e. the contractor), impacted municipalities, and the local community to consider and address major project topics in a more open format. The design-build team will work with the state highway departments to create the basis of design, overall project requirements, and final design intent, before work substantially begins, during what is being called Phase 1A and Phase 1B of the project. Therefore, the following questions are particularly salient: Which level of government do you believe is better positioned to represent your interests in this Project [choose one]:

Q2

With funding secured, take the time to get the design right before starting construction

In terms of Project schedule, which of the following aligns most closely with your view [choose one]:

Q3

Finally, with respect to what the Project could accomplish, please rank the following priorities according to their importance to you [rank]:

Minimize interstate footprint / return land to the region's urban core	3
Create more walkable street frontage for development	7
Increase multi-modal connectivity across the interstate corridor	6
Ensure local streets impacted by the project are designed according to modern, urban, multi-modal guidelines	5
Reduce high-speed ramps through the urban core	4
Achieve best possible life-cycle return on investment with project funds	8
Improve traffic flow on the interstate	2
Improve safety for automobiles traveling on the interstate corridor	1
Ensure the interstate and ramps are designed according to highway design guideline manuals	9
Maintain all on-ramp and off-ramp access points to/from the interstate	10

Q4

Please provide your contact information. This will be kept confidential.

Name	
State/Province	OH
ZIP/Postal Code	45220
Email Address	

#153

COMPLETE

Collector: BF Link (Web Link)
Started: Monday, February 13, 2023 12:27:39 PM
Last Modified: Monday, February 13, 2023 12:31:07 PM
Time Spent: 00:03:27
IP Address: 51.158.236.47

Page 1

Q1

State Government

The Brent Spence Corridor Project will be built using a “progressive design-build process.” This process allows the state highway departments, the design-build team (i.e. the contractor), impacted municipalities, and the local community to consider and address major project topics in a more open format. The design-build team will work with the state highway departments to create the basis of design, overall project requirements, and final design intent, before work substantially begins, during what is being called Phase 1A and Phase 1B of the project. Therefore, the following questions are particularly salient: Which level of government do you believe is better positioned to represent your interests in this Project [choose one]:

Q2

Design doesn't matter; get construction started immediately

In terms of Project schedule, which of the following aligns most closely with your view [choose one]:

Q3

Finally, with respect to what the Project could accomplish, please rank the following priorities according to their importance to you [rank]:

- Minimize interstate footprint / return land to the region's urban core **1**
- Create more walkable street frontage for development **3**
- Increase multi-modal connectivity across the interstate corridor **2**
- Ensure local streets impacted by the project are designed according to modern, urban, multi-modal guidelines **6**
- Reduce high-speed ramps through the urban core **4**
- Achieve best possible life-cycle return on investment with project funds **5**
- Improve traffic flow on the interstate **10**
- Improve safety for automobiles traveling on the interstate corridor **7**
- Ensure the interstate and ramps are designed according to highway design guideline manuals **8**
- Maintain all on-ramp and off-ramp access points to/from the interstate **9**

Q4

Please provide your contact information. This will be kept confidential.

- Name
- State/Province **OH**
- ZIP/Postal Code **45429**
- Email Address

#154

COMPLETE

Collector: BF Link (Web Link)
Started: Monday, February 13, 2023 2:51:00 PM
Last Modified: Monday, February 13, 2023 2:53:02 PM
Time Spent: 00:02:02
IP Address: 74.129.210.246

Page 1

Q1

Local Government

The Brent Spence Corridor Project will be built using a “progressive design-build process.” This process allows the state highway departments, the design-build team (i.e. the contractor), impacted municipalities, and the local community to consider and address major project topics in a more open format. The design-build team will work with the state highway departments to create the basis of design, overall project requirements, and final design intent, before work substantially begins, during what is being called Phase 1A and Phase 1B of the project. Therefore, the following questions are particularly salient: Which level of government do you believe is better positioned to represent your interests in this Project [choose one]:

Q2

With funding secured, take the time to get the design right before starting construction

In terms of Project schedule, which of the following aligns most closely with your view [choose one]:

Q3

Finally, with respect to what the Project could accomplish, please rank the following priorities according to their importance to you [rank]:

- Minimize interstate footprint / return land to the region's urban core **1**
- Create more walkable street frontage for development **3**
- Increase multi-modal connectivity across the interstate corridor **7**
- Ensure local streets impacted by the project are designed according to modern, urban, multi-modal guidelines **9**
- Reduce high-speed ramps through the urban core **2**
- Achieve best possible life-cycle return on investment with project funds **6**
- Improve traffic flow on the interstate **8**
- Improve safety for automobiles traveling on the interstate corridor **4**
- Ensure the interstate and ramps are designed according to highway design guideline manuals **5**
- Maintain all on-ramp and off-ramp access points to/from the interstate **10**

Q4

Please provide your contact information. This will be kept confidential.

- Name
- State/Province **OH**
- ZIP/Postal Code **45229**
- Email Address

#155

COMPLETE

Collector: BF Link (Web Link)
Started: Wednesday, February 15, 2023 10:35:55 AM
Last Modified: Wednesday, February 15, 2023 10:40:13 AM
Time Spent: 00:04:18
IP Address: 216.196.153.2

Page 1

Q1

Local Government

The Brent Spence Corridor Project will be built using a “progressive design-build process.” This process allows the state highway departments, the design-build team (i.e. the contractor), impacted municipalities, and the local community to consider and address major project topics in a more open format. The design-build team will work with the state highway departments to create the basis of design, overall project requirements, and final design intent, before work substantially begins, during what is being called Phase 1A and Phase 1B of the project. Therefore, the following questions are particularly salient: Which level of government do you believe is better positioned to represent your interests in this Project [choose one]:

Q2

With funding secured, take the time to get the design right before starting construction

In terms of Project schedule, which of the following aligns most closely with your view [choose one]:

Q3

Finally, with respect to what the Project could accomplish, please rank the following priorities according to their importance to you [rank]:

Minimize interstate footprint / return land to the region's urban core	2
Create more walkable street frontage for development	6
Increase multi-modal connectivity across the interstate corridor	3
Ensure local streets impacted by the project are designed according to modern, urban, multi-modal guidelines	8
Reduce high-speed ramps through the urban core	7
Achieve best possible life-cycle return on investment with project funds	1
Improve traffic flow on the interstate	5
Improve safety for automobiles traveling on the interstate corridor	4
Ensure the interstate and ramps are designed according to highway design guideline manuals	9
Maintain all on-ramp and off-ramp access points to/from the interstate	10

Q4

Please provide your contact information. This will be kept confidential.

Name	
State/Province	OH
ZIP/Postal Code	45202
Email Address	

#156

COMPLETE

Collector: BF Link (Web Link)
Started: Wednesday, February 15, 2023 5:27:55 PM
Last Modified: Wednesday, February 15, 2023 5:30:31 PM
Time Spent: 00:02:36
IP Address: 104.231.175.15

Page 1

Q1**Local Government**

The Brent Spence Corridor Project will be built using a “progressive design-build process.” This process allows the state highway departments, the design-build team (i.e. the contractor), impacted municipalities, and the local community to consider and address major project topics in a more open format. The design-build team will work with the state highway departments to create the basis of design, overall project requirements, and final design intent, before work substantially begins, during what is being called Phase 1A and Phase 1B of the project. Therefore, the following questions are particularly salient: Which level of government do you believe is better positioned to represent your interests in this Project [choose one]:

Q2**With funding secured, take the time to get the design right before starting construction**

In terms of Project schedule, which of the following aligns most closely with your view [choose one]:

Q3

Finally, with respect to what the Project could accomplish, please rank the following priorities according to their importance to you [rank]:

- Minimize interstate footprint / return land to the region's urban core **1**
- Create more walkable street frontage for development **2**
- Increase multi-modal connectivity across the interstate corridor **4**
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- Ensure the interstate and ramps are designed according to highway design guideline manuals **7**
- Maintain all on-ramp and off-ramp access points to/from the interstate **10**

Q4

Please provide your contact information. This will be kept confidential.

Name

State/Province **OH**

ZIP/Postal Code **45239**

Email Address

#157

COMPLETE

Collector: BF Link (Web Link)
Started: Wednesday, February 15, 2023 7:37:04 PM
Last Modified: Wednesday, February 15, 2023 7:39:52 PM
Time Spent: 00:02:47
IP Address: 71.67.245.72

Page 1

Q1**Local Government**

The Brent Spence Corridor Project will be built using a “progressive design-build process.” This process allows the state highway departments, the design-build team (i.e. the contractor), impacted municipalities, and the local community to consider and address major project topics in a more open format. The design-build team will work with the state highway departments to create the basis of design, overall project requirements, and final design intent, before work substantially begins, during what is being called Phase 1A and Phase 1B of the project. Therefore, the following questions are particularly salient: Which level of government do you believe is better positioned to represent your interests in this Project [choose one]:

Q2**With funding secured, take the time to get the design right before starting construction**

In terms of Project schedule, which of the following aligns most closely with your view [choose one]:

Q3

Finally, with respect to what the Project could accomplish, please rank the following priorities according to their importance to you [rank]:

- Minimize interstate footprint / return land to the region's urban core **1**
- Create more walkable street frontage for development **4**
- Increase multi-modal connectivity across the interstate corridor **8**
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- Reduce high-speed ramps through the urban core **6**
- Achieve best possible life-cycle return on investment with project funds **10**
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- Improve safety for automobiles traveling on the interstate corridor **3**
- Ensure the interstate and ramps are designed according to highway design guideline manuals **2**
- Maintain all on-ramp and off-ramp access points to/from the interstate **9**

Q4

Please provide your contact information. This will be kept confidential.

- Name
- State/Province **OH**
- ZIP/Postal Code **45214**
- Email Address

#158

COMPLETE

Collector: BF Link (Web Link)
Started: Thursday, February 16, 2023 12:54:33 PM
Last Modified: Thursday, February 16, 2023 12:58:00 PM
Time Spent: 00:03:26
IP Address: 174.193.98.117

Page 1

Q1**Local Government**

The Brent Spence Corridor Project will be built using a “progressive design-build process.” This process allows the state highway departments, the design-build team (i.e. the contractor), impacted municipalities, and the local community to consider and address major project topics in a more open format. The design-build team will work with the state highway departments to create the basis of design, overall project requirements, and final design intent, before work substantially begins, during what is being called Phase 1A and Phase 1B of the project. Therefore, the following questions are particularly salient: Which level of government do you believe is better positioned to represent your interests in this Project [choose one]:

Q2**With funding secured, take the time to get the design right before starting construction**

In terms of Project schedule, which of the following aligns most closely with your view [choose one]:

Q3

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- Improve safety for automobiles traveling on the interstate corridor **6**
- Ensure the interstate and ramps are designed according to highway design guideline manuals **9**
- Maintain all on-ramp and off-ramp access points to/from the interstate **10**

Q4

Please provide your contact information. This will be kept confidential.

Name

State/Province **OH**

ZIP/Postal Code **45220**

Email Address

January 19, 2023



PO Box 19138
Cincinnati, OH 45219

COMMUNITY COUNCIL

President
Stephen Gibbs

Treasurer
Tami Hendrickson

Recording Secretary
Cody Sheets

Corresponding Secretary
Carol Gibbs

Board of Trustees
Sandi Allen
Ken Farmer
Daryl Frazier
Roger Hildebrand
Christian Huelsman
Alice Williams
Lee Wilson

On January 17, 2023, Bridge Forward presented its current design priorities for the Brent Spence Bridge Project to the Mt. Auburn Community Council. Bridge Forward's design priorities are as follows:

- Maximize the amount of returned land contiguous to the City's urban core;
- Maximize the amount of walkable street frontage that is conducive to street-facing development within and adjacent to the project footprint;
- Maximize the amount of pedestrian connection between the CBD, Queensgate, and the West End, minimizing the walking distance of each of those connections;
- Maximize the number of local streets that are designed according to the NACTO Urban Design Guide;
- Minimize the number of lane miles using high-speed directional ramps; and
- Achieve best expenditure of public dollars measured in life-cycle return on investment.

Additionally, Bridge Forward explained that, according to ODOT / KYTC, the project will be build using a "progressive design-build process." This approach allows ODOT / KYTC, the design-build team (DBT), impacted municipalities, and the local community to consider and address major project topics in a more open format. The DBT will work with ODOT / KYTC to create the basis of design, overall project requirements, and final design intent, before work substantially begins, during what is being called Phase 1A of the Project.

This discussion prompted a motion for the Mt. Auburn Community Council to support Bridge Forward's design priorities, and to ask City Council to get engaged with the project. Specifically, City Council should ensure the progressive design-build contract, to the greatest extent possible, leverages and enhances Phase 1A of the project, guaranteeing that alternative design concepts consistent with these priorities, separate from Concept I-W, are developed. We also request that Scenario Testing be done before Certified Traffic is finalized.

Please accept this notice as full documented support for Bridge Forward's design priorities for the project and as a petition for City Council to ensure this project is transformative for the urban core of Cincinnati.

A handwritten signature in black ink, appearing to read 'Stephen Gibbs', is written over a light blue circular stamp.



March 4, 2023

Ms. Laura Leffler
Division Administrator
Federal Highway Administration (FHWA), Ohio Division
200 North High Street, Room 328
Columbus, Ohio 43215

Dear Ms. Leffler:

This letter is being submitted by Bridge Forward Cincinnati (“Bridge Forward”), a Cincinnati-based, grassroots, non-profit organized around the idea that the Brent Spence Bridge Corridor Project (“the Project”) should be transformational for the Cincinnati region and for Cincinnati’s urban core especially. Bridge Forward is a true coalition of parties interested in and impacted by the Project. Our desire is for the Cincinnati region to receive the best possible product and the best possible life-cycle return on investment from this nearly \$4 billion project.

Today, the Brent Spence Bridge Corridor (“BSBC”) in downtown Cincinnati occupies approximately 55 acres between 3rd Street and 8th Street, and it creates an infrastructure barrier between the region’s CBD and underdeveloped urban neighborhoods to the west. Bridge Forward’s two highest priorities for the Project are: recapturing land from the interstate right-of-way for local use and economic development, and enhancing community connectivity east-west across the interstate corridor to allow for growth of Cincinnati’s landlocked urban core.

Currently, Bridge Forward is concerned that the state agencies delivering the Project are not allowing for innovation, flexibility, or responsiveness to public feedback during the project development process, despite committing to a progressive design-build procurement/delivery. Rather, the Project’s Bi-State Management Team (BSMT) – composed of the Ohio Department of Transportation (ODOT) and the Kentucky Transportation Cabinet (KYTC) – is employing a perfunctory environmental process designed to maximize the agencies’ control over the design direction of the project and to arrive at a pre-determined design outcome. The result is a major missed opportunity for the Project to deliver significant economic development benefits to the region, beyond the Project’s core scope of reducing highway congestion.

The BSMT is currently advancing a design called “Concept I-W.” This design is based on a Preferred Alternative selected in 2011 (“Alternative I”), plus a set of value engineering changes (labeled “W”) that were mostly developed in 2013. The Project received a Finding of No Significant Impact (FONSI) in 2012, which has received extensions until today. An Environmental Impact Statement (EIS) has never been prepared for the Project. In June, 2022, the BSMT did initiate a Supplemental Environmental Assessment (“SEA”) which includes a public feedback component, but the timing of this SEA does not allow for public feedback to appreciably influence the design direction of the Project. In fact, crucial public hearings included in the scope of the SEA are scheduled for *after* project expectations are set during the execution

of a design-build contract.¹ Additionally, the findings of the SEA are set to be finalized only one month before groundbreaking and five months *after* design-build contract execution.

Bridge Forward believes that the Project's current design, having been substantially developed over a decade ago, and being modeled after an existing condition built in the 1960s, is – unfortunately – antiquated. The design utilizes many high-speed directional ramps, it is not context sensitive, and it is sprawling. Although some plans have been for new multi-modal paths on existing local roads near the interstate, the design lacks new connections across the interstate, and only a very marginal amount of land would be returned to the City for local use.

The Project is a once-in-a-generation opportunity to transform the Cincinnati region's relationship to its infrastructure for the better. Bridge Forward's aim is to recalibrate this long-planned highway expansion project so that it not only achieves the original Purpose & Need but also achieves important economic development and community goals championed by local residents and championed by the celebrated Reconnecting Communities program and the Justice40 Initiative. Bridge Forward aims to seize this opportunity by incorporating design improvements that: provide for better community connectivity east-and-west across the BSBC; right-size urban highway infrastructure and return significant new land for urban and economic development to Cincinnati's landlocked urban core; provide restorative justice to those who were displaced to make way for I-75 when it was first constructed; and enhance the health, safety, and well-being of those who live proximate to the interstate.

Bridge Forward is thankful that the state agencies developing this project have decided to utilize a *progressive design-build* procurement. We believe that this procurement method creates a great opportunity to harness the power of the private market to innovate, and in this case specifically, to develop alternative design concepts separate from Concept I-W. However, if this opportunity is to be realized, the BSMT will need to change course and begin to fully leverage the power of the Project's progressive design-build procurement.

To be clear, Bridge Forward Cincinnati does not oppose the Project. In fact, Bridge Forward is very excited about the economic development opportunity that this Project – if improved – could offer the urban core of the Cincinnati region. Bridge Forward's website, since it was first launched, has listed "Do-No-Harm Objectives," including "achieving established project delivery goals."² Additionally, Bridge Forward is not advocating for a lengthier environmental process. Bridge Forward appreciates that the Project has secured time-consuming and costly environmental clearances. However, Bridge Forward is advocating for a design process that is flexible and responsive to public comments. Bridge Forward believes that its guiding design principles, if incorporated, would *lessen* the Project's overall environmental impact and therefore should not threaten the Project's existing environmental clearances. Bridge Forward also believes that both the City of Cincinnati and Hamilton County stand ready to work with the BSMT on local traffic design, which may need to be coordinated with Project design improvements.

¹ <https://brentspencebridgecorridor.com/environmental/>

² <https://www.bridge-forward.org/do-no-harm>

The Cincinnati region is aligned: a majority of local citizens want the Project to move forward, and so do business leaders and elected officials. However, Bridge Forward has identified a groundswell of community support for its vision, and we believe it is fair to expect a project development process that is flexible and that reflects local priorities.

With this being said, Bridge Forward believes that our local communities and our region as a whole would benefit from a much higher degree of federal involvement and guidance on this Project. We believe there is still ample time to ensure that this project is built to be modern, context sensitive, and transformational—a national, platinum project. However, we fear that without urgent federal assistance, the BSMT will miss this historic opportunity to improve both Cincinnati’s economic potential and the quality of life in Cincinnati’s urban core.

The remainder of this letter: (I) provides additional information about Bridge Forward’s vision and briefly describes the Bridge Forward Coalition; (II) briefly describes the geographical context of the BSBC; (III) explains concerns that Bridge Forward has with the project development and environmental processes; and (IV) provides a list of opportunities/suggestions that Bridge Forward would like to offer.

The specific subtopics covered in section (III) are as follows:

- A. The Project’s decade-old FONSI has commonly been cited as reason why substantial design improvements cannot be considered.
- B. The City of Cincinnati was advised that it could not obtain Project Cooperating Agency status.
- C. ODOT released incomplete analyses of Bridge Forward’s vision to the press, without first engaging with Bridge Forward.
- D. Favored organizations have invited to participate in the project development process, while Bridge Forward and other community groups have intentionally been excluded.
- E. November 10, 2022, press conference mischaracterizes the scale recent ODOT-led of design improvements.
- F. Timing of the Project’s environmental process does not allow for subsequent changes in the Project development process or the Project’s design direction.
- G. During public engagement meetings, the Project has been advertised as “set in stone.”
- H. Outreach to neighborhoods has been inadequate.
- I. Foundational EIS and traffic forecasts were disregarded when developing Certified Traffic for the Project.
- J. IIA and Justice40 priorities are not being addressed.

The specific subtopics covered in section (IV) are as follows:

- A. Consider appointing a special project coordinator/liaison of national importance to be heavily involved in the Project.
- B. Launch independent analyses of the items listed herein to help provide the BSMT and the selected DBT with as much useful and timely information as possible.

- C. Include the specific procurement language listed herein in an addendum to the current Request for Proposals (RFP) for a design-build team (DBT).

Please note: all referenced Exhibits in this letter are available at https://bit.ly/Bridge_Forward_Cincinnati_3-4-23

I. Bridge Forward’s Coalition and Vision

Bridge Forward has identified in the community a groundswell of interest in and support for making this interstate project as beneficial as possible for the urban core of Cincinnati. In fact, the City of Cincinnati passed a Resolution on April 13, 2022, supporting the goal of returning as much land as possible, from the highway right-of-way, back to the City, through the course of the Project (see Exhibit A). Additionally, Bridge Forward has obtained multiple letters of support from the City’s official Community Councils, including the important West End Community Council, which are included in Exhibit B. We expect more community council letters of support to be adopted in the coming weeks. Over 300 people have taken part in Bridge Forward’s letter writing campaigns, and over 150 people have participated in a Bridge Forward survey, where the vast majority have recorded support for Bridge Forward’s community priorities. As of a 12/14/2022 posting, of all substantive public comments submitted on the official Project website (which does not include comments that were very simple questions or requests, such as “please subscribe me to Project updates”), roughly 40% of public comments – a plurality – supported all or key portions of the Bridge Forward vision, not including the 300+ letters or 150+ surveys mentioned above.³ Bridge Forward has been featured in many local new articles and is a go-to source for commentary on the Brent Spence.⁴ Bridge Forward is featured in the Cincinnati Metropolitan Housing Authority’s Choice Neighborhood Plan sponsored by the US Department of Housing and Urban Development.⁵ Finally, as of 3/4/23, Bridge Forward has held nine community meetings in local neighborhoods impacted by the Project to share information about the progressive design-build process and to collect information about the public’s priorities for the Project, as shown below. Bridge Forward is currently planning additional meetings. (We have submitted survey results and information about our public meetings to the FHWA, Ohio Division, separately.)



Bridge Forward Cincinnati meeting on 3/4/23 at Messer Construction’s headquarters in the West End neighborhood of Cincinnati

³ <https://brentspencebridgecorridor.com/wp-content/uploads/2022/12/Public-Comments-and-Responses-2022-12-14.pdf>

⁴ <https://www.bridge-forward.org/news>

⁵ <https://www.westendchoice.org/> see “West End Transformation Plan,” page 145

The general design vision that Bridge Forward has promoted is shown below and much more extensively in Exhibit C. This vision is not a technical, engineered design solution, nor is this vision meant to be prescriptive. However, Bridge Forward is advocating for the following specific, measurable, community priorities in bold text to be primary drivers of the Project’s design:

1. Safety: The Project must address systemic safety issues;
2. Environmental Sustainability: The Project must address stormwater, visual, air, and water quality impacts;
3. Quality of life: The Project must enhance the unique characteristics of the community;
 - **Maximize the number of local streets that are designed according to the NACTO Urban Street Design Guide or similar;**
4. Mobility and community connectivity: The Project must address affordable transportation choices and accessibility for all users, particularly non-motorized users;
 - **Maximize the number of pedestrian connections between the CBD, Queensgate, and the West End, and minimize the walking distance of each of those connections;**
5. Economic competitiveness and opportunity: The Project should increase traffic options, system connectivity, and uncover land to increase access to jobs and affordable housing;
 - **Maximize the amount of returned land contiguous to the City’s urban core;**
 - **Maximize the amount of walkable street frontage that is conducive to street-facing development within and adjacent to the project footprint;**
6. State of good repair: The Project must address current or projected system vulnerabilities for underserved, overburdened, or disadvantaged communities;
7. Partnerships and collaboration: The Project must ensure equity by meaningfully engaging with communities directly and indirectly impacted by this initiative;
 - **Incorporation, in the adjacent communities, of features/elements that serve to memorialize the historic lower West End neighborhood and the systematic displacement of tens of thousands of Black residents, in part, to make way for I-75’s construction through the City of Cincinnati;**
8. Innovation: The Project must deploy technology and other forward-thinking practices to improve safety, control environmental impact, and provide better economic outcomes for underserved, overburdened, or disadvantaged communities;
 - **Minimize the number of lane miles using high-speed directional ramps; and**
 - **Achieve best expenditure of public dollars measured in life-cycle return on investment.**



2013 ODOT Proposal



Reconnecting Westway

The Bridge Forward Cincinnati “Reconnecting Westway” Concept Sketch

Bridge Forward believes that the community priorities it is advancing are quite consistent with USDOT/FHWA stated priorities for projects receiving INFRA and Mega grants, which include the Brent Spence. We believe that Bridge Forward’s community priorities could be considered squarely within the scope of this Project, a position supported by the following passages in the grant programs’ Notice of Funding Opportunity (NOFO):⁶

- “Support integrated land use, economic development and transportation planning to improve the movement of people ... facilitate greater public and private investments and strategies in land-use productivity, ... [and] increase in the production or preservation of location-efficient housing;”
- “Reduce transportation and housing cost burdens, including through public and private investments to support greater commercial and mixed-income residential development near public transportation ... in walkable neighborhoods;”
- “Increase the walkability and accessibility for pedestrians and encourage thriving communities for individuals to work, live, and play by creating transportation choices for individuals to move freely with or without a car;”
- “Improved multimodal and transit access across previously bifurcated disadvantaged neighborhoods;”
- “Include[] physical-barrier-mitigating land bridges, caps, lids, linear parks, and multimodal mobility investments that either redress past barriers to opportunity or that proactively create new connections and opportunities for underserved communities that are underserved by transportation;”
- “Include[] new or improved walking and bicycling infrastructure [and] reduce[] automobile dependence;”
- “Address equity and barriers to opportunity, improve quality of life in ... urbanized areas, and benefit Historically Disadvantaged Communities.”

II. Geographical Context

Bridge Forward aims to ensure that the region’s Environmental Justice communities are represented, protected, and bettered throughout the course of the Project. Before a full discussion of the Project’s development and environmental processes, it is critical to understand that the Brent Spence Bridge Corridor (“BSBC”) passes through a very sensitive area. The construction of the Brent Spence Bridge corridor and the adjacent Queensgate industrial area in the early 1960’s completed the elimination of a dense urban neighborhood, by systematically forcing thousands of people from their homes and destroying the “cultural epicenter of Cincinnati’s Black community.”⁷

The City of Cincinnati’s “Kenyon Barr” urban redevelopment scheme, which later become known as “Queensgate I,” aimed at creating an industrial district anchored by the new highway, the river, and the railroads. At the outset of the project, residents of the lower West End

⁶ https://www.transportation.gov/sites/dot.gov/files/2022-03/FY22%20Multimodal%20Project%20Discretionary%20Grant%20-%20NOFO_final_0.pdf

⁷ <https://www.citybeat.com/news/article/20981774/echoes-of-a-lost-west-end>

neighborhood received letters from the City stating that their buildings had been purchased by the City, and that they would need to find somewhere else to live. In all, 25,737 residents – 25,155 of whom were non-white – were displaced, and “137 food stores, 118 bars and restaurants, 86 barber shops and beauty parlors, 80 churches and missions, 24 dry cleaners, and 6 funeral homes” were eliminated.⁸ This forced relocation eliminated the wealth of thousands of families, caused an immeasurable psychological toll, and destroyed a vibrant neighborhood and the community that called it home.⁹

Local historian and University of Cincinnati professor Anne Delano Steinert, who in 2017 launched an exhibit called “Finding Kenyon Barr: Exploring Photography of Cincinnati's Lost West End” reported that, during interviews for the project, some former Kenyon Barr residents were, to this day, moved to tears when reflecting on the destruction of their former neighborhood.¹⁰ Other former residents are still, to this day, openly angry about what happened to their neighborhood.¹¹ Indeed, the original Brent Spence Bridge Corridor construction, the accompanying Queensgate development, and the destruction of much of Cincinnati’s West End neighborhood, is often held up as one of the worst historic examples of US policy on the environment and transportation.¹² In fact, it was policy disasters precisely like the original BSBC project that were the impetus for passage of the landmark National Environmental Policy Act of 1970.

Today, the West End is a stressed community. I-75 is an infrastructure barrier, cutting the West End off from neighborhoods to the west. Hamilton County, Ohio, Census Tract 263 composes the western side of I-75 through the Cincinnati urban core, and Census Tracts 2, 264, and 265 compose the eastern side of I-75 through the Cincinnati urban core. Census Tract 2 is considered a Transportation Disadvantaged Census Tracts (Historically Disadvantaged Communities) by USDOT.¹³ All four tracts meet the definition of “Area of Persistent Poverty” by USDOT.¹⁴ Tracts 2, 263, and 264 are designated as Qualified Census Tracts (QCTs) by HUD.¹⁵ Tracts 2 and 264 are recognized as Racially or Ethnically Concentrated Areas of Poverty (R/ECAPs) by HUD.¹⁶ And, Tract 263 on the western side of I-75 and Tract 2 on the eastern side of I-75, in particular, are both afflicted by very high diesel particulate matter, high particulate matter 2.5, high ozone levels, high air toxics cancer risk, high air toxics respiratory HI, very high rates of asthma, very low life expectancy, low income levels, and high rates of unemployment.¹⁷

⁸ <https://www.cincinnatiimagazine.com/citywiseblog/lost-city-kenyon-barr-queensgate/>

⁹ <https://www.cincinnatiimagazine.com/citywiseblog/finding-kenyon-barr-documents-wholesale-demolition-lower-west-end/>

¹⁰ <https://www.graypape.com/project/>

¹¹ <https://open.spotify.com/episode/0X8s4sFk4EJ8T0uMdSr8nC?si=hgFvX7ucT4Sct3CqMP2Bdw&context=spotify%3Ashow%3A7kBHB0fPocXdtX4w86aN39>

¹² <https://www.nytimes.com/interactive/2021/05/27/climate/us-cities-highway-removal.html>

¹³ <https://usdot.maps.arcgis.com/apps/dashboards/d6f90dfcc8b44525b04c7ce748a3674a>

¹⁴ <https://datahub.transportation.gov/stories/s/tsyd-k6ij>

¹⁵ https://www.huduser.gov/portal/sadda/sadda_qct.html

¹⁶ <https://hudgis-hud.opendata.arcgis.com/datasets/racially-or-ethnically-concentrated-areas-of-poverty-r-ecaps/explore?location=39.094286%2C-84.513947%2C14.48>

¹⁷ <https://ejscreen.epa.gov/mapper/>

The official West End Community Council has been working in partnership with Bridge Forward for well over a year to advocate for Project improvements. (See Exhibit B for letters of support from the West End Community Council.)

Given both the historical and contemporary contexts of the Brent Spence Bridge Corridor in Cincinnati, and given the Administration's focus on EJ communities, including its Justice40 Initiative,¹⁸ it is reasonable to expect that careful attention will be given to the Project's environmental process. However, as indicated before, Bridge Forward has concerns with the current process to date. A catalogue of these concerns follows.

III. Concerns

A. The Project's decade-old FONSI has commonly been cited as reason why substantial design improvements cannot be considered.

Bridge Forward is concerned that public and private statements by ODOT have shaped both the public's and local leadership's understanding of the Project in ways that are not consistent with NEPA requirements or the Project's planned *progressive design-build* procurement/delivery. Specifically, Bridge Forward is disappointed that ODOT has commonly cited the Project's decade-old FONSI,¹⁹ as a reason why design improvements cannot be considered. ODOT has said both privately and publicly that deviations from its decade-old preferred-alternative design²⁰ could require years of additional environmental review, thereby potentially causing the Project to miss out on the once-in-a-generation funding currently available through the Infrastructure Investment and Jobs Act (IIJA).

Bridge Forward believes these statements have unfortunately had a chilling effect on meaningful public engagement on the Project, at all levels. Many local business leaders and elected leaders – who privately admit they support Bridge Forward's vision for the Project – have disengaged from the Project's public engagement process, for fear of jeopardizing the Project's chance of receiving funding through IIJA. However, Bridge Forward is advocating for a set of design priorities focused on reducing the environmental impact of the Project, consistent with the IIJA and agency priority of investing in context-sensitive infrastructure that will last long into the 22nd century. We are concerned that this Project could rush an antiquated design into construction, despite substantial public interest in design improvements.²¹

Here is one discrete example of messaging which served to stifle legitimate public and political engagement in the Project. On May 6, 2022, the Cincinnati Business Courier ran a [cover story](#)

¹⁸ <https://www.whitehouse.gov/environmentaljustice/justice40/>

¹⁹ The Project's FONSI was granted in 2012 and subsequently renewed twice during years in which there were neither material advancement of, nor media attention on, the Project, due to the Project's lack of funding. <https://brentspencebridgecorridor.com/wp-content/uploads/2022/10/FHWA-Re-Eval-BSB-2018.pdf>

²⁰ <https://brentspencebridgecorridor.com/wp-content/uploads/2022/10/PAVR-May-2011.pdf>

²¹ https://www.fhwa.dot.gov/bipartisan-infrastructure-law/using_bil_resources_build_better_america.cfm

[about Bridge Forward's vision](#) for the Project.²² Other local news organizations quickly ran follow-up stories on the Bridge Forward vision.^{23, 24} On May 10, 2022, the Cincinnati Business Courier itself ran a follow-up stories about how the Mayor of Cincinnati was concerned with the Preferred Alternative and was open to Access Point changes on the Project.^{25, 26} Then, on May 14, 2022, the Cincinnati Enquirer ran an opinion piece²⁷ by its former lead opinion writer, [Jason Williams, disparaging Bridge Forward](#) with the following statements:

- “Sorry, urbanists, you’re too late to the Brent Spence Bridge project conversation. Surely, there’s a bike-lane project for y’all to meddle in. The \$2.9 billion Brent Spence plan is done. Two decades worth of meetings about meetings to dissect and deliberate and dicker are over.”
- “The group's idea has some of the Brent Spence project’s biggest champions a little concerned that somewhere along the line an urbanist-pandering bureaucrat or politician might be intrigued enough to, yep, call for another study. If that happens, federal funding could go elsewhere and the Brent Spence plan could be doomed to languish for many more years. Mark Policinski, for years one of the key leaders on planning the Brent Spence project, told me another study could take 5 to 6 years.”
- “‘I’m not sure what the non-professionals are doing,’ said Policinski, CEO of the Ohio-Kentucky-Indiana Regional Council of Governments, the region’s planning agency on federally funded transportation projects. ‘As we approach the finish line after a 20-year race, somebody is telling us to take a detour,’ he added. ‘The professionals are moving ahead to getting us a new bridge.’”

In the months following the Williams piece, Bridge Forward has been featured in even more media.^{28, 29, 30, 31} However, on the official BSMT website for the Project, there is a *News* page

²² <https://www.bizjournals.com/cincinnati/news/2022/05/06/brent-spence-alternative-plan.html?b=1651769806%5E22078201>

²³ <https://local12.com/news/local/bridge-forward-cincinnati-30-uncovered-acres-fort-washington-way-redevelopment-brent-spence-companion-bridge-project-ohio-department-of-transportation-odot-kentucky-transportation-cabinet-local-12-wkrc-tristate-indiana-news>

²⁴ <https://www.iheart.com/podcast/scott-sloan-on-demand-20785494/episode/the-scott-sloan-show-51222-96813456/>, WLW, Cincinnati, 700 AM, broadcast 5/12/22 (start at 1:13:00)

²⁵ <https://www.bizjournals.com/cincinnati/news/2022/05/10/pureval-on-brent-spence-change.html>

²⁶ <https://www.bizjournals.com/cincinnati/news/2022/05/20/pureval-on-brent-spence-project.html>

²⁷ <https://www.cincinnati.com/story/news/politics/columnists/2022/05/14/biden-infrastructure-plan-too-late-change-brent-spence-bridge-plan-column/9730174002/>

²⁸ <https://www.cincinnati.com/story/opinion/contributors/2022/10/28/opinion-city-hall-needs-to-get-engaged-with-brent-spence-corridor-project/69592098007/>

²⁹ <https://spectrumnews1.com/oh/columbus/news/2022/11/11/new-bridge-project-aims-to-reconnect-black-neighborhoods-->

³⁰ <https://www.cincinnati.com/story/news/2022/12/20/brent-spence-bridge-corridor-project-changes/69637957007/>

³¹ <https://www.wlwt.com/article/brent-spence-bridge-plan-brings-new-possibilities-for-west-end-cincinnati/42411301>

with links to local media stories about the Project. For months, only one of the above-mentioned articles was listed: the Jason Williams opinion piece.³² See Exhibit D. (A link to the Williams piece has since been removed from the BSMT website, to make space for links to more recently published articles.)

The quotes in the Williams piece are representative of the statements that ODOT has made to both local elected and business leaders, advising them to disengage from conversations about the Project. Bridge Forward obtained multiple emails through a FOIA request that highlight these advisory statements, which are included in Exhibit E. Indeed, some local leaders have told Bridge Forward that they were expressly told by ODOT officials to “stop talking to Bridge Forward.” This is inconsistent with the BSMT’s pledged “full[] commit[ment] to robust public involvement.”³³

Finally, it is important to note that the BSMT is currently considering a package of value engineering changes (labeled “W”) that significantly alter the preferred alternative (Alternative I). In the Concept I-W changes, lane configurations are shifted, and thru and local traffic are sorted differently on the pair of bridges. These changes made after the last FONSI extension, which was granted to Alternative I – are very significant. Bridge Forward believes that if the changes contemplated in Concept I-W can be covered under existing environmental permits and the current SEA process, then so could Bridge Forward-championed design improvements intended to lessen environmental impact.

B. The City of Cincinnati was advised that it could not obtain Project Cooperating Agency status.

Bridge Forward has always believed that local jurisdictions and local leaders are best positioned to understand the needs of the local community. For this reason, Bridge Forward and partners have consistently advocated for the City of Cincinnati to obtain a Project Cooperating Agency status. And, City of Cincinnati leadership was receptive. According to an email provided to Bridge Forward in response to a FOIA request, the Mayor of Cincinnati asked ODOT project leadership about the City becoming a Cooperating Agency in the Project at an August 1, 2022, meeting. In response, ODOT incorrectly responded that the City “does not meet [the] definition [of a Cooperating Agency],” and therefore did not qualify to become one.

Bridge Forward fears that this was an effort to influence the City to abandon efforts to gain the integrated role that its leadership desired to take in the Project. Unfortunately, since the request for Cooperating Agency status was denied by ODOT, City leadership and staff have been less receptive to the advocacy of Bridge Forward and other partners. The subject email discussed here is attached as Exhibit F.

C. ODOT released incomplete analyses of Bridge Forward’s vision to the press, without first engaging with Bridge Forward.

³² <https://brentspencebridgecorridor.com/news/>

³³ <https://brentspencebridgecorridor.com/environmental/>

Especially disappointing to Bridge Forward was an incident where ODOT circulated to local press, two incomplete feasibility analyses of Bridge Forward’s vision for the Project. By way of this letter, Bridge Forward is submitting a rebuttal to each of these two feasibility analyses by ODOT. These rebuttals are attached to this letter as Exhibit G and Exhibit H. Bridge Forward requests that this letter and each of these two specific exhibits be included in the Project’s administrative record as three separate responses.

On October 5, 2022, ODOT released two feasibility analyses of Bridge Forward’s vision for the Project, circulated these analyses like press releases, and made ODOT Press Secretary Matt Bruning available to speak with local media about these analyses. The message that Press Secretary Bruning shared was that ODOT had reviewed the Bridge Forward proposal and had rejected it as “not feasible.”³⁴ This conclusion also appears in many of the BSMT’s responses to public comments supporting Bridge Forward.³⁵

First, Bridge Forward believes that the substance of these two analyses both have significant flaws in their conclusions. As noted above, rebuttals to each of these analyses are attached to this letter as Exhibit G and Exhibit H.

Second, the way in which ODOT managed these feasibility analyses was not based on the principles of good public engagement. Bridge Forward only learned of the existence of ODOT’s feasibility analyses 24 hours in advance of their being published online. Bridge Forward was never approached before the feasibility analyses were commissioned to confirm its design vision, to provide up-to-date work products, or to collaborate on how the Project may be improved.

Although Bridge Forward is not a technical organization, materials that Bridge Forward had shared with ODOT many months prior, in a spirit of openness and to convey a general vision, were judged as finalized technical proposals. Technical flaws were used as a reason to dismiss the spirit of Bridge Forward’s proposal. ODOT used its feasibility analyses to claim that Bridge Forward’s work should be excluded from the environmental record altogether.

ODOT’s decision to immediately make these feasibility analyses available to local press, before Bridge Forward had a chance to respond, suggests that ODOT’s motivation for commissioning these feasibility analyses may have been rooted in a desire to diminish public discourse around the Project, rather than encourage public discourse or explore potential design improvements in a more open format.

The first of the two subjects that ODOT chose to analyze was a *working position paper* from Bridge Forward, dated December 14, 2021, which had been sent to an ODOT project manager in a January 16, 2022, email. In this email, the author of the working position paper asked for a meeting to discuss the Project and the ideas in the working position paper, generally. The working position paper was meant to provide context for the meeting request at that time period;

³⁴ <https://local12.com/news/local/i-75-interchange-odot-review-plan-30-new-acres-city-blocks-not-feasible-downtown-cincinnati-queensgate-brent-spence-covington-northern-kentucky-nky-bridge-forward-local-12-wkrc-tristate-ohio-ky-indiana-news>

³⁵ <https://brentspencebridgecorridor.com/public-involvement-and-comments/>

it was never meant to be a definitive or final source of Bridge Forward's thinking. In fact, the Working Position Paper was emailed along with a one-page document titled *Implementation Questions*, in an effort to work with ODOT to advance Bridge Forward's community-oriented goals. ODOT made no effort to reach out to Bridge Forward or the author of the working position paper to ask for updates at the commencement of the feasibility analysis. Instead, ODOT reviewed the dated working position paper out of context, then shut down the possibility of working with the community to consider a Bridge Forward-inspired alternative design.

The second of the two subjects that ODOT specifically chose to analyze was a letter campaign that referenced concept drawings – titled Reconnecting Westway – meant to show Bridge Forward's general design thinking to the public. ODOT based its feasibility analysis in a large part on these Reconnecting Westway concept drawings. The concept drawings were never represented as a prescriptive, engineered design solution. In fact, the concept drawings showed the local 4th Street in Cincinnati extending over a trenched I-75, which is a known design challenge that had been communicated by Bridge Forward to ODOT on January 7, 2022, when Bridge Forward shared three-dimensional models of its concept with ODOT. Nonetheless, this design challenge at 4th Street was a key focus of the feasibility analysis, which rejected Bridge Forward's overall vision as unfeasible, even though a 4th Street overpass is not critical component of Bridge Forward's larger advocacy.

In summary, with less than a 24-hour warning, ODOT used Bridge Forward's works in progress and an acknowledged design challenge to completely dismiss Bridge Forward's conceptual vision in the public sphere. This dismissal damaged Bridge Forward's credibility and further discouraged public engagement in the Project.

D. Favored organizations have been invited to participate in the project development process, while Bridge Forward and other community groups have intentionally been excluded.

Bridge Forward has been disappointed that ODOT leadership has given favor to certain business groups in Cincinnati regarding the Project – namely the Cincinnati USA Regional Chamber of Commerce (“the Chamber”) – as compared to Bridge Forward and other community groups.

In January, 2022, Bridge Forward held two meetings with ODOT – one on 1/7/22 and the next on 1/28/22 – introducing the idea of design improvements aimed at reducing the footprint of the design and improving community connectivity across the interstate. The 12/14/2021 working position paper was introduced during one of these early meetings to provide context to these discussions. At these meetings, ODOT responded by saying that the design's footprint had already been reduced as much as possible and that a preferred alternative had already been selected. ODOT did, however, accept a three-dimensional model of Bridge Forward's vision for the Project following one of these meetings.

On February 2, 2022, ODOT began reaching out to the Chamber to discuss the Project. By the end of February, an influential local political consultant who had previously served on ODOT's Transportation Review Advisory Council (TRAC) was reporting back to ODOT's top leadership on the activities of Bridge Forward. On March 7, 2022, ODOT leadership met with Chamber

leadership and other influential business leaders in Cincinnati. Bridge Forward was not invited, but was subsequently informed, that at this meeting, ODOT told local business leaders to “stop talking to Bridge Forward.” Shortly after this 3/7/22 meeting with the Chamber, a scheduled meeting between Bridge Forward and a committee of influential local business leaders was canceled indefinitely by the committee of business leaders. Additionally, during this 3/7/2022 meeting, the Chamber was invited to submit to ODOT a “value statement for real estate opportunities in the urban core,” according to emails obtained by Bridge Forward through a FOIA request. ODOT hand-picked which local group it solely preferred to work with on environmental concerns—the more well-connected business community. See Exhibit E.

Over the following months this pattern continued. By October 5, 2022, ODOT released the incomplete feasibility analyses of Bridge Forward’s vision to the press. Emails indicate that the Chamber was made aware of ODOT’s plan to release its feasibility analyses at least on 10/3/22, while Bridge Forward was not notified until 10/4/22. On November 10, 2022, ODOT held a press conference with the Chamber announcing that interstate land would be returned to the City thanks to the Project, thanks to ODOT’s engagement with the Chamber.

E. November 10, 2022, press conference mischaracterizes the scale of recent design improvements.

On November 10, 2022, a widely-covered press conference was held, where it was announced that ODOT and the Chamber had incorporated changes to the Project which would result in a smaller interstate right-of-way and therefore in about 10 developable acres of land being returned to the Cincinnati CBD.^{36, 37, 38} However, this was a mischaracterization. According to a Bridge Forward analysis provided in Exhibit I, a majority of the area that was being returned to the City was already going to be returned to the City following the value engineering changes (labeled “W”) that were first developed in 2013. In fact, the Concept I-W changes that were being pursued by the BSMT well before the November, 2022, press conference were already going to return approximately 8 acres to the CBD. In reality then, additional changes announced this past November were only returning 2 additional acres to the City.

Bridge Forward applauds the effort by ODOT and the Chamber to minimize the Project footprint. However, Bridge Forward is advocating for a much more robust process, incorporated in the progressive design-build’s initial scope of work, that invites teams of professional highway engineers to develop alternative design concepts meant to minimize footprint. The basis for the design improvements announced in November were specific design tweaks developed primarily by the Chamber, with help from the City.³⁹ Although well-meaning, our local business leaders

³⁶ <https://spectrumnews1.com/oh/columbus/news/2022/11/10/updated-brent-spence-bridge-plan-aims-to-create-usable-land--better-create-downtown>

³⁷ <https://www.cincinnati.com/story/news/2022/12/20/brent-spence-bridge-corridor-project-changes/69637957007/>

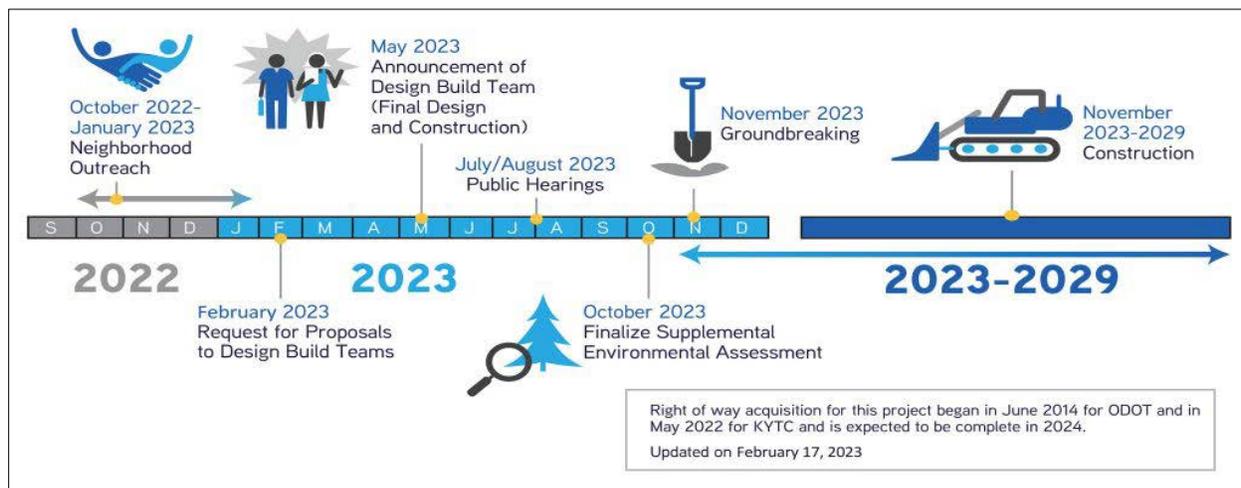
³⁸ <https://www.bizjournals.com/cincinnati/news/2022/11/10/city-odot-propose-adding-10-acres-to-cbd.html>

³⁹ https://brentspencebridgecorridor.com/wp-content/uploads/2022/11/2022-11-04_Response-to-Cincinnati-Chamber-Comments.pdf

do not process the tools that a team of professional highway engineers process to identify design opportunities for footprint minimization and other community goals.

F. Timing of the Project’s environmental process does not allow for subsequent changes in the Project development process or the Project’s design direction.

BSMT’s planned environmental process for the Project is shown below.



BSMT Planned Environmental Process

(taken from: <https://brentspencebridgecorridor.com/environmental/>)

As shown, most public feedback is going to be collected *after* the points in the process with the best chances for such feedback to shape the design direction of the Project. For instance, Neighborhood Outreach was not initiated until October, 2022. Meanwhile, on its Project procurement website, ODOT made MicroStation design files of Concept I-W available for download by bidders in October, 2022.⁴⁰ Indeed, due to lack of funding and an impasse over tolls, this Project was considered dead by the public, and there had been very little public discussion about the project for almost 10 years, until the IJJA was passed in November, 2021. Then, less than 12 months after IJJA passage, ODOT was releasing MicroStation design files to contractors, based on its 2013 design, before nearly any public engagement was had at all. Additionally, it was reported by some local media that there was a 1/5/2023 deadline for the public to make comments on the Project.⁴¹

The BSMT planned no public engagement between 1/5/2023 and the Public Hearings which are scheduled for July/August 2023. The Public Hearings will occur after a contract signing with the design-build team (DBT), and therefore after expectations for the Project’s schedule and design direction are set with the DBT. Based on this timeline, these Public Hearings will be

⁴⁰ <https://www.dot.state.oh.us/Divisions/ConstructionMgt/design-build/bsb-project-procurement/Lists/Announcements/DispForm.aspx?ID=15&Source=https%3A%2F%2Fwww%2Edot%2Estate%2Eoh%2Eus%2FDivisions%2FConstructionMgt%2Fdesign%2Dbuild%2Fbsb%2Dproject%2Dprocurement%2FPages%2Fdefault%2Easpx&ContentTypeId=0x0104003774B832AF09C246BA30FA3641C85723>

⁴¹ <https://www.wvxu.org/local-news/2022-12-14/brent-spence-bridge-corridor-revamp-designs>

performative-only. The schedule does not allow for meaningful engagement during the relevant design processes.

In contrast, Bridge Forward began outreach to the impacted West End neighborhood via its Community Council starting in October, 2021 (see Exhibit C). And, as early as June, 2022, the Bridge Forward coalition started promoting to ODOT the idea of incentivizing DBTs to present alternative, innovative design concepts, separate from Concept I-W during procurement. Going forward, Bridge Forward believes that the Supplemental Environmental Assessment (“SEA”) that is being conducted for this Project needs to be more robust, and it needs to be scheduled in a way that allows the Project’s design direction to be substantially responsive to public comments.

G. During public engagement meetings, the Project has been advertised as “set in stone.”

Bridge Forward has concerns that at BSMT-led public meetings, the Project is being presented in a way that neither invites robust feedback nor conveys to attendees that design improvements during the Project’s progressive design-build procurement/delivery are even an option. Indeed, journalists who have attended ODOT’s public meetings have reported the following:

- “... the design of the Brent Spence Bridge Corridor Project is mostly set in stone...”⁴²
- “For the first time, we are getting a bird's eye view of the I-75 corridor as it approaches Covington and the new companion bridge that will be built next to the Brent Spence Bridge.”⁴³
- “The Ohio Department of Transportation held an informative meeting today to update West End residents on how the Brent Spence Project could impact their neighborhood.”⁴⁴

Bridge Forward fears that valuable input has not been collected, due to a perceived lack of agency on part of the public, due to headlines like these.

H. Outreach to neighborhoods has been inadequate.

ODOT has reported to Bridge Forward that ODOT has relied on two primary methods for advertising its public meetings on the Project: (i) addressing all-volunteer, neighborhood-based Community Councils, and (ii) BSMT’s own communication platforms, such as the Project’s website and ODOT’s social media (See Exhibit J). Therefore, only local neighbors who are very engaged would have received word about public meetings. Still, a Bridge Forward member recently reported the following:

⁴² <https://spectrumnews1.com/ky/louisville/news/2022/12/15/brent-spence-meeting>

⁴³ <https://local12.com/news/local/flyover-gives-first-look-at-how-i-75-will-split-between-brent-spence-companion-bridge-drainage-highway-sound-fly-stacee-hans-project-manager-corridor-project-meeting-separate-barriers-display-covington-kentucky>

⁴⁴ <https://www.fox19.com/video/2022/12/13/odot-hosts-meeting-with-westend-residents-about-brent-spence-bridge/>

“I am a resident of the Cincinnati Central Business District (CBD) neighborhood and live within a half-block of the project area (the 300 block of West 4th Street). A CBD neighborhood outreach meeting was held on 11/29/2022. Despite being a resident of the target neighborhood for the meeting and taking adequate steps to stay informed on the project, I did not receive any communication from ODOT/the project team that a CBD neighborhood outreach meeting was being held. My knowledge of the meeting came from a [work] colleague who has no affiliation to the project team or its partners. Had it not been for that colleague, I would not have known the meeting was happening... To begin, the project’s E-newsletter, the means for communicating project updates and news, failed to adequately advertise the outreach meetings. I signed up for and have received the project’s newsletters beginning with the 10/21/2022 edition as well as the 11/10 and 12/5 editions. The 11/10 newsletter is the first communication regarding the neighborhood outreach; however, no communication was given for the meeting dates, times, or locations. The newsletter did not even indicate which neighborhoods the project team would be visiting nor was any information provided on where an individual could go to learn more of the neighborhood meetings or view the self-service comment website (pulcinput.com/bsbc). This lack of notice comes despite text on the project website’s newsletter sign up form indicating notice of upcoming meetings would be provided [to those who sign] up. ... The 12/5 [newsletter] edition did include the remaining events; however, when comparing with the full list of outreach meetings, ... 5 KY meetings were already held and 2 OH meetings were also already held (an additional KY meeting was held on 12/5—same day notice is hardly effective public engagement).”

Especially for outreach meant to satisfy the requirements of an SEA, this is inadequate. Indeed, even people who sought out information on the Project by signing up for updates on the Project website were only informed of two of the four neighborhood meetings held in Ohio. See Exhibit J.

Ideally, Bridge Forward would like to see this project of national importance more closely conform to the standards established in USDOT’s October, 2022, *Promising Practices for Meaningful Public Involvement in Transportation Decision-Making* guidance: “when engaging the public, it is imperative to have a proactive strategy for seeking input from members of all parts of the project area, especially in underserved communities, including those who have been historically disadvantaged, marginalized, or adversely affected by persistent poverty”.⁴⁵

I. Foundational EIS and traffic forecasts were disregarded when developing Certified Traffic for the Project.

In 2004, the Ohio-Kentucky-Indiana Regional Council of Governments (OKI) and the Miami Valley Regional Planning Commission (MVRPC) completed a major planning study known as the North South Transportation Initiative (“the Initiative”). The Initiative is cited in the Brent Spence Project’s Purpose and Need Statement, but the Initiative also includes agreements /

⁴⁵ <https://www.transportation.gov/sites/dot.gov/files/2022-10/Promising%20Practices%20for%20Meaningful%20Public%20Involvement%20in%20Transportation%20Decision-making.pdf> page 7

preferred alternatives that include highway expansion plus transit expansion to meet transportation demands and mitigate environmental impact.⁴⁶

Appendix G of the Initiative (beginning on page 2076 of the pdf) covers "Modeling Highway & Transit Alternatives" and concludes by recommending a preferred alternative that includes both lanes added to I-75 (and elsewhere), plus LRT options. Additionally, a benefit-cost analysis of the Initiative (beginning on page 2384 of the pdf) shows benefits based on reduced congestion brought by adding transit. In the Initiative, the analysis underpinning lane assignments assumed transit to mitigate congestion and air quality conformity. And at that time, the Initiative's oversight committee reviewed a series of alternatives that included transit to mitigate vehicle miles traveled and congestion.

However, today, we are only presented with highway expansion. Bridge Forward calls into question how the traffic forecasts underpinning these foundational agreements seem to have been discarded. It appears that the recommendation of the EIS was disregarded in developing the traffic forecasts used as the basis for the Certified Traffic.

J. IJA and Justice40 priorities have not been being addressed.

The currently proposed Project design – Concept I-W – which was substantially developed in 2012 and 2013, does not provide the best possible life-cycle value to local, public owners and end-users of the Project. Bridge Forward feels that the BSMT's application for Multimodal Project Discretionary Grant (MPDG) funding signals room for improvement in certain areas of priority for USDOT / FHWA:

- During discussion of how the Project will impact the adjacent disadvantaged communities, the BSMT's MPDG application narrative is that the Project will "reduce barriers to local economic opportunity, including for disadvantaged communities, through better connections [via the interstate] to regional job opportunities" (page 14). And, "I-75 directly connects disadvantaged neighborhoods in Covington and Cincinnati to the greater region and key employment centers, education facilities, and health/cultural institutions" (page 17). Here, Bridge Forward believes that the Project could better serve local residents by delivering a context-sensitive design that reverses the existing BSBC infrastructure barrier. We feel this would be a stronger proposal than the provision of a means, via the interstate, to exit the local neighborhood quickly.
- In BSMT's MPDG application, there is an unsubstantiated claim that this interstate-widening project will support "sustainable development patterns" (page 2). Additionally, in a discussion about how the Project "will support integrated land use, economic development and transportation planning," (page 15) land use planning is not mentioned. Absent from the application is the fact that I-75 serves to landlock the Cincinnati Central Business District on the west, as well as the fact that the BSBC currently occupies 55 acres of extremely valuable real estate in the region's urban

⁴⁶ <https://www.oki.org/studies/pdf/northsouth/ns-complete.pdf>

core. Instead, mention is given to the Uptown area of Cincinnati, which is not located within the BSBC or project area. The Uptown Consortium Inc.'s ongoing Mobility Needs Assessment is mentioned in the funding application, but this study has a clearly defined scope of assessing mobility needs "within Uptown."⁴⁷

Bridge Forward fears that BSMT will deliver a Project that is not worthy of either the nearly \$4 billion requisite investment or a 100-year service life. In contrast, Bridge Forward's vision for a more flexible Project would bring very significant benefits to the Cincinnati region, and it would be a project worthy of national celebration.

IV. Opportunities / Suggestions

Bridge Forward is encouraged by the BSMT's announcement of a progressive design-build procurement/delivery of this project, but Bridge Forward also recognizes that additional steps are needed to take full advantage of the full range of opportunities presented by the progressive design-build. Bridge Forward was particularly encouraged by the below statement included in *ODOT's Response to Public Comment: Reconnecting Westway Emails*. Bridge Forward is hopeful that all commitments made in the passage below are honored:

"The Westway Emails advocate for a design-build process that is flexible and encourages innovation. Given the current environment as it relates to project risk, inflationary trends, the need for ongoing engagement of the adjacent impacted communities, and interested third parties, KYTC and ODOT have made the decision to revise the procurement method from Value-Based Fixed-Price Design-Build (VBDB) to Progressive Design-Build (PDB).

Progressive Design-Build will allow the Design-Build Team (DBT), KYTC, and ODOT to collaboratively consider and address major project topics in a more open format which cannot be achieved with a VBDB contracting method.

Progressive Design-Build uses a best-value selection to determine the preferred DBT, followed by a process whereby the owner and DBT "progress" towards a design and contract price. The DBT is selected through a combination of technical qualifications and "competitive bidding elements" without necessarily a fixed price. The DBT collaborates with the owner to create/confirm the basis of design, overall project requirements, and final design intent. Project design decisions are based on cost, schedule, quality, operability, life cycle and other considerations. Alternative technical concepts will be developed and evaluated over the course of the progressive design-build contract. Alternative concepts that are consistent with the decisions made during the project's environmental process and offer substantial cost savings or additional benefits will be incorporated into the BSB Corridor Project, as appropriate."⁴⁸

⁴⁷ <https://www.uptowncincinnati.com/blog/2022/2/27/uptown-isnt-standing-still-mobility-assessment-now-underway-seeks-your-input?rq=mobility>

⁴⁸ https://brentspencebridgecorridor.com/wp-content/uploads/2022/10/Response-to-Public-Comment-Westway_Attachments-1.pdf

However, currently, ODOT's procurement website suggests that the Project's design direction is not expected to be altered by the environmental process, despite the overwhelming interest in Bridge Forward's vision. Rather, an expectation is being set in procurement that Concept I-W, with a value engineering changes only, will be implemented.⁴⁹ BSMT is not honoring the progressive design-build framework, nor honoring its environmental response above to encourage or incentivize innovation or the development of wholesale alternative design concepts.

Given this dynamic, Bridge Forward believes that this Project and the entire Cincinnati region would benefit greatly from a much higher degree of federal involvement and guidance on this Project. To this end, Bridge Forward has developed a short list of opportunities / suggestions that it would like to offer to USDOT / FHWA leadership for consideration. These suggestions are intended to work within the Project's existing environmental process in a way that ideally would recalibrate this Project into one worth national praise.

A. Consider appointing a special project coordinator/liaison of national importance to be heavily involved in the Project.

Bridge Forward believes that, because this project has been contemplated for so long, and because the profile of this project is so great, a significant leadership profile would be most effective at convening all of the critical parties involved in this project.

Especially given the City of Cincinnati's Resolution (Exhibit A), and given the past confusion regarding the City of Cincinnati's ability to obtain Project Cooperating Agency status, Bridge Forward believes that active outreach to local leadership – the Mayor, City Council, and County Commissioners – would be appropriate.

B. Launch independent analyses of the items listed herein to help provide the BSMT and the selected DBT with as much useful and timely information as possible.

- Alternative design concepts, separate from Concept I-W, which would be based on Bridge Forward's measurable priorities listed in Section I of this letter;
- The assumptions used in the Project's traffic forecasting projections;
- The feasibility of converting US-50 just west of the BSBC into a surface-level boulevard;
- The possibility of adding additional scopes of work and/or additional features to the Project, the associated pools of federal funding which may be available for additional scopes, and the likelihood of securing said federal funding;
- The economic impact analysis of potential expansion of the Cincinnati CBD;
- The feasibility of utilizing alternative design concepts proposed from sources other than the BSMT, guided by Bridge Forward's measurable priorities in Section I of this letter; and

⁴⁹ <https://www.dot.state.oh.us/Divisions/ConstructionMgt/design-build/bsb-project-procurement/Pages/default.aspx>

- The merits and feasibility of the Bridge Forward general design vision.

Ideally, these analyses would be well-established before contract negotiation begins with the DBT and before the commencement of Phase 1A – the proof-of-concept phase – of the Project, so that all findings can be incorporated into the implementation of the Project.

Bridge Forward believes that it would be especially useful to have a professional study of high-level alternative design concepts separate from Concept I-W that remain within the environmental framework, (but for which potential traffic impacts on local streets could be coordinated with the City and County). A robust and trustworthy study of wholesale, community-guided alternative design concepts would give DBTs and the BSMT the confidence they would need to pursue a recalibration.

Additionally, Bridge Forward is suggesting that the assumptions used in the Project’s traffic forecasting projections be re-examined. This is because the current forecasts have been called into question^{50, 51} and because significant amounts of office space in downtown Cincinnati is being converted to residential space, thereby reducing the trips generated by the downtown area. Bridge Forward believes that Scenario Testing of traffic forecasts – which is not yet expected to happen for this Project – would allow the BSMT and DBT to make more well-informed decisions about the implications of design choices on future traffic and congestion, as design priorities are weighed against one another.

C. Include the specific procurement language listed herein in an addendum to the current Request for Proposals (RFP) for a design-build team (DBT).

- Alternative concept proposals that reduce footprint and simplify operations will be required to be developed and evaluated during Phase 1A using performance measures identified by the Bi-State Management Team (BSMT), with input from local communities, against the BSMT/KYTC “30% Plans” design concept using a progressive design build process to incorporate more complete measures of social and economic value.
- The design-build team (DBT) will engage community partners on both sides of the river in developing the scope, performance measures, and evaluation criteria for the Brent Spence Bridge Corridor Project.
- Performance measures will include social and economic net benefits, including value of land recapture and access changes.
- Performance standards achieved by the current “30% Plans” will be established; for example, system to system access for I-71/I-75/I-471/US-50, LOS, Travel Times, Access, Geometrics, and Safety as a basis for comparison.

⁵⁰ <https://usa.streetsblog.org/2022/09/14/highway-boondoggles-part-ii-induced-demand-across-the-ohio-river/>

⁵¹ <https://www.strongtowns.org/journal/2022/4/18/the-brent-spence-boondoggle>

- The design-build scope may be a hybrid of prescriptive and performance elements in order to achieve both agency and community goals, and may incorporate IDIQ, Incentive/Disincentive, and Job Order contracting elements during phase two.
- The evaluation criteria will incorporate environmental commitments and associated required performance measures resulting from the Supplemental EA being conducted for the Project.
- The DBT, in coordination with the BSMT, shall undertake updated analyses and studies to support its performance claims, for example, sensitivity analysis and alternative scenarios for opening and design year traffic, including assignments to specific links.
- A life-cycle benefit/cost analysis, consistent with FHWA's guidance and including the identified value measures for both the "30% plans" and an alternative technical concept, will be required to support proof of concept.
- Ensure that the DBT is evaluated based on its ability to deliver a complex project, in an urban setting, working collaboratively with local government and neighborhoods to deliver community priorities, including by incorporating innovative design concepts throughout the course of the Project.

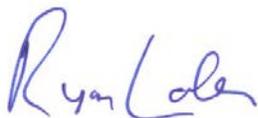
Bridge Forward Cincinnati is committed to ensuring that the Brent Spence Project brings great benefits to the national economy, the local economy, and to local neighborhoods. Bridge Forward hopes that by submitting this letter, the potential of a nationally celebrated Brent Spence Project will be realized. We are available to help in any way.

Thank you for your attention. Please do not hesitate to contact the undersigned with any questions.

Very Respectfully,



Brian Boland
President, Bridge Forward
URBAN513@YAHOO.COM
(859) 907-2305



Ryan Laber
Volunteer for Bridge Forward
RMLABER@GMAIL.COM
(859) 445-2257



4-24-2023

Sent by:

Gina Brock
2449 Riverside Drive
Cincinnati, OH 45202
513-814-4074
brockgrae@gmail.com

On behalf of Bridge Forward

Ms. Laura Leffler,

Bridge Forward is concerned that inadequate meaningful public involvement has taken place.

The Project's E-newsletter failed to adequately advertise the neighborhood outreach meetings.

The November 10, 2022 edition contained the first mention of neighborhood outreach meetings with no information regarding neighborhoods, dates, times or locations. A December 5, 2023 E-newsletter included remaining events, however 5 NKY and 2 OH meetings of a total of 13, had already been held. One additional NKY meeting was held on the same day, December 5, 2023.

This does not constitute effective public involvement.

Bridge Forward has implemented a Community Participation Plan (attached) that seeks to remedy this situation. We invite FHWA, ODOT and KYTC to participate in any of our future outreach activities, and we will continue to collect feedback and forward it to FHWA to be included in the administrative record.

Sincerely,

Gina Brock

From: Ryan Laber <rmlaber@gmail.com>
Sent: Friday, June 23, 2023 3:13 PM
To: info@brentspencebridgecorridor.com; Ohio.FHWA@dot.gov; Kentucky.FHWA@dot.gov
Subject: Re: Brent Spence - Public Comment Docs (email 1 of 2)

Resending the files from email 1 of 2 in a shared folder, because as indicated below, the original email was not received by info@brentspencebridgecorridor.com

Please find an economic study examining the benefits of the Bridge Forward Concept here: https://drive.google.com/drive/folders/1dXSjUll_TZdM7j48l_zkabMZ6FubTdwm?usp=sharing

Thank you!!

Ryan Laber
rmlaber@gmail.com | (859) 445-2257 (c)

----- Forwarded message -----

From: Mail Delivery Subsystem <mailer-daemon@googlemail.com>
Date: Fri, Jun 23, 2023 at 2:53 PM
Subject: Delivery Status Notification (Failure)
To: <rmlaber@gmail.com>



Recipient inbox full

Your message couldn't be delivered to **info@brentspencebridgecorridor.com**. Their inbox is full, or it's getting too much mail right now.

The response from the remote server was:

552 Error: Too much mail data - ImprovMX

Reconnecting Cincinnati Westway

The Economic Benefits of Investing in a Better Brent Spence Bridge Corridor Project

Created for Bridge Forward Cincinnati
Created by Karp Strategies

Full Report Content Available Upon Request

RECONNECTING CINCINNATI WESTWAY

The economic benefits of investing in a better
Brent Spence Bridge Corridor Project

From: Ryan Laber <rmlaber@gmail.com>
Sent: Friday, June 23, 2023 3:00 PM
To: info@brentspencebridgecorridor.com; Ohio.FHWA@dot.gov; Kentucky.FHWA@dot.gov
Subject: Brent Spence - Public Comment Docs (email 2 of 2)
Attachments: Reconnecting Cincinnati Westway Design Improvements - Letters Binder (8.22.22 thru 12.22.22).pdf;
Opinion_ City Hall needs to get engaged with Brent Spence Corridor project.pdf

Also, please find attached a binder of the "Reconnecting Cincinnati Westway Design Improvements" letters sent from 8/22/22 through 12/22/22.

And lastly, an op-ed for inclusion in the SEA / administrative record.

Ryan Laber
rmlaber@gmail.com | (859) 445-2257 (c)

On Fri, Jun 23, 2023 at 2:52 PM Ryan Laber <rmlaber@gmail.com> wrote:

Hello:

Please find attached an economic study examining the benefits of the Bridge Forward Concept.

Ryan Laber
rmlaber@gmail.com | (859) 445-2257 (c)

Note: "Reconnecting Cincinnati Westway Design Improvements" letters referenced above are included in Appendix L.

CONTRIBUTORS | **Opinion** *This piece expresses the views of its author(s), separate from those of this publication.*

Opinion: City Hall needs to get engaged with Brent Spence Corridor project

Ryan Laber Opinion contributor

Published 10:50 p.m. ET Oct. 27, 2022

Two billion eight hundred million dollars. That is the expected cost of the Brent Spence Corridor project. To put that figure into perspective, \$2.8 billion could fund the city of Cincinnati's entire budget for nearly two years; it could pay for 11 TQL Stadiums or 19 streetcar projects; or, it could install the decks that were recently proposed over Fort Washington Way 25 times over.

It makes sense to think that such a long-awaited project will be transformational for our community. After all, three sitting U.S. presidents have come to town to talk about the Brent Spence. Without intervention however, after this once-in-a-lifetime influx of federal money is spent, and after years of disruptive construction are endured, our community is going to be left wondering: Is that it?

The current designs for the Brent Spence Corridor project essentially replicate the existing highway layout that forms the western border of downtown Cincinnati. This layout was designed in the 1950s, and when constructed in the 1960s, it landlocked our downtown and displaced thousands of residents, most of whom were Black. By taking the same, antiquated approach, the current designs miss a major opportunity to reshape how this critical infrastructure interacts with our region's urban core.

The Bridge Forward proposal published earlier this year started a great conversation about what might be possible with the Brent Spence project: tens of acres of land returned to the city; better local connections between downtown, Queensgate and the West End; improved pedestrian and bicycle safety; the accommodation of urban and economic development projects. If our region wants to be competitive with peer regions, we must provide a large, top-tier urban core for those who desire to live or work in one. In this sense, getting the Brent Spence project right is a must.

Fortunately, City Hall leadership still has the power to get engaged on this project and drive an outcome that is much, much better for Cincinnati. Here are three important avenues:

First, the Ohio Department of Transportation is planning to use what is called a "progressive design-build" method of delivery for this project. This means that the contractor who will build the project will also have a large hand in designing the project. If City Hall were to decide soon upon a set of measurable, local priorities for the project – such as land recapture and improved east-west connectivity – those priorities could make their way into the selection criteria for the design-build contractor. This would harness the power of the private market to innovate, and contractors who bid on the project would be incentivized to propose design thinking that directly addresses the city's priorities.

Second, this summer, ODOT officially launched an intensive effort, known as a "Supplemental EA," to collect public feedback on the project. ODOT has pledged to earnestly review and act on public comments provided by individuals, organizations and community councils. Of course, public comments are most useful when they are uniform. Fortunately, the people's representatives in City Hall can, and should, speak about local priorities with one voice – a voice that carries weight – during this critical phase of the project's development.

Third, City Hall can expect to find helpful partners at the national level. USDOT's stated priorities for projects awarded MEGA grants, which will be an important source of capital funding for the project, are far more expansive than what the current Brent Spence designs would deliver. Federal leadership has a significant interest in delivering transformative projects that advance pedestrian safety, that improve connectivity between neighborhoods separated by interstates, and that integrate transportation projects with land use and economic development priorities. City Hall should engage with USDOT and other national experts now to ensure that the best possible product gets delivered to our community.

If any doubts exist about whether all of this is possible, just look to Covington. There, city leadership engaged with the Kentucky Transportation Cabinet to win the opportunity for design changes on the Kentucky side of the river, all of which are aimed at improving local quality of life. Covington started negotiations to align the project's interchanges with local neighborhood plans, ensured that fixing a long-standing, severe stormwater issue will be included in the scope of the project, and secured funding for a full-time city employee who will represent Covington's interests in the project for the next five years.

None of the ideas suggested here have to impact the project's funding or groundbreaking date. However, the project's long-term benefits to our local community, and the true nature of the

young leadership at City Hall, are both yet to be determined. Will City Hall seize on this opportunity, or let it slip?

Ryan is a Cincinnati by identity currently living in Columbus, Ohio. His background is in civil engineering and city planning.



August 28, 2023

The Honorable William J. Seitz
Ohio House of Representatives
77 South High Street, 14th Floor
Columbus, OH 43125

Dear Representative Seitz,

Thank you again for your interest in the Brent Spence Bridge Corridor Project.

Throughout the life of this project, the Ohio Department of Transportation (ODOT) has conducted extensive public engagement, collaborating with local governments and stakeholders in a variety of ways. The Bi-State Management Team, comprised of the Kentucky Transportation Cabinet and the Ohio Department of Transportation, has formally met with Bridge Forward on five different occasions in 2023. During that timeframe, the Bridge Forward group has changed its concept for the project several times. ODOT has actively reviewed the information that members and proponents of Bridge Forward have provided.

At our most recent meeting in June, ODOT confirmed the current Bridge Forward concept was far enough along that we would now engage the Design Build Team in a review, especially in terms of constructability. ODOT has conducted a preliminary review of Bridge Forward's June 2023 concept and can provide the following information:

- *There would be no delay with the Bridge Forward June 2023 Concept because all the changes are confined within the "envelope" of what has been previously approved and permitted.*
 - From an environmental standpoint, ODOT views the Bridge Forward June 2023 concept as a refinement to the current design, and evaluating this concept is not anticipated to cause a delay.
- *There would be no impact on the Western Hills Viaduct project because the Bridge Forward June 2023 Concept and ODOT's proposal are in complete alignment north of the Western Avenue / Union Terminal Exit on Interstate 75.*
 - The Interstate 75 interchange with the Western Hills Viaduct is part of the Brent Spence Bridge Corridor project environmental review, so if there is no delay related to the Bridge Forward evaluation, then the Interstate 75 interchange with the Western Hills Viaduct will remain on schedule. This is just one of the major components for the full project.

- *The Bridge Forward June 2023 Concept is superior to ODOT's proposal not only because more developable land is created thereby, but also because the concept would retain a 5th Street Exit off Interstate 75 that the ODOT plan would eliminate.*
 - There are a variety of changes proposed to the current project under the Bridge Forward concept. Based on our preliminary review, the Bridge Forward concept potentially has significant issues related to traffic operations, constructability, design, cost, and long-term maintenance and operations and therefore may not be feasible
- *The Bridge Forward June 2023 Concept would not upset any construction schedule since construction is not scheduled until 2025 and the "progressive design" is in its earliest stages.*
 - There is a significant amount of detailed design work that must occur after this proposal is reviewed to keep the project on schedule with early construction work anticipated in 2024 and major construction beginning in 2025. If the decision is delayed for any reason, there is a risk that the overall project will be delayed.
- *The added construction cost of the Bridge Forward June 2023 proposal is \$65-80 million, a sum that is a very small fraction of the overall cost and one that would be funded exclusively by local sources.*
 - At the most recent meeting with Bridge Forward, ODOT emphasized the estimate of \$65-80 million was a very preliminary number based on an initial review of the original concept. After reviewing the June concept, ODOT believes this cost will now likely exceed \$100 million. A driving factor will be the constructability aspect of the concept, which could further increase the cost to \$140 million. In short, the question is can this be constructed.

The Ohio Department of Transportation will continue to closely collaborate with the City of Cincinnati to evaluate the Bridge Forward concept as it evolves, along with other potential innovations to accomplish our shared goals. Those goals include minimizing the project footprint, increasing developable land in the downtown Cincinnati area, delivering the project within budget, and completing the project on-time.

A more detailed evaluation of the latest design concept will be published on the project website and distributed to you. We will continue to keep you and your staff updated on the progress of the Brent Spence Bridge Corridor Project.

Respectfully,



Jack Marchbanks, Ph.D.
Director

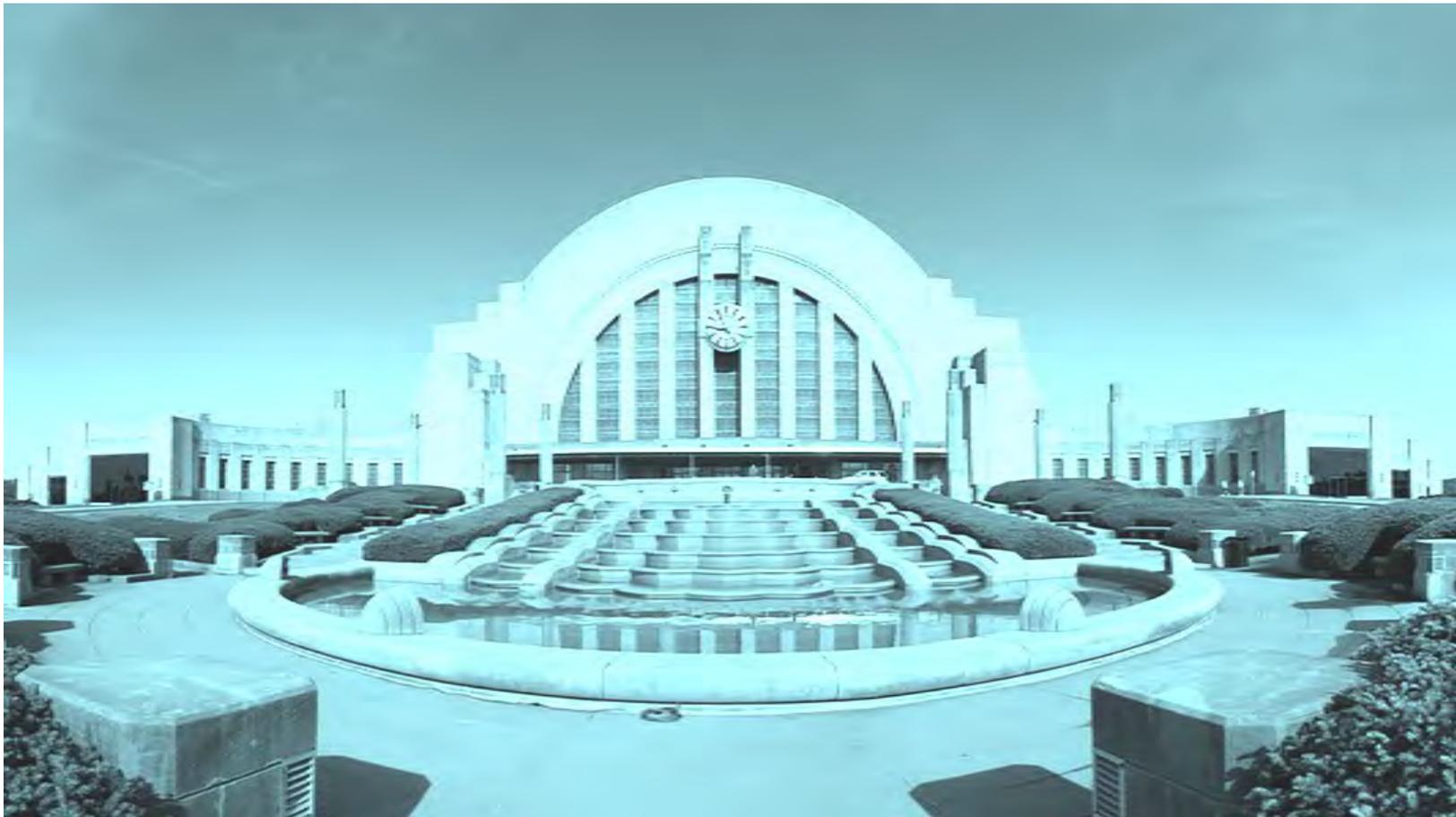
From: Ryan Laber <rmlaber@gmail.com>
Sent: Thursday, August 31, 2023 11:13 AM
To: info@brentspencebridgecorridor.com
Cc: Ohio.FHWA@dot.gov; Kentucky.FHWA@dot.gov; Jodi Heflin; Fuller, Kathleen
Subject: Re: Brent Spence - Public Comment Docs - Community Conversation Event
Attachments: BRENT SPENCE BRIDGE COMMUNITY CONVERSATION SUMMARY DOCUMENT + COVER(2).pdf

Please see attached, and please include in the project record, a report summarizing the community conversation event that took place at Union Terminal on 6/21/23.

Ryan Laber
rmlaber@gmail.com | (859) 445-2257 (c)

SUMMARY DOCUMENT

**Brent Spence Bridge Community
Conversation Event**



**Brent Spence Bridge Corridor Project in
the City of Cincinnati**

Cincinnati Museum Center at Union Terminal
June 21, 2023

Full Report Content Available Upon Request

TO: Bi-State Management Team
FROM: Greg Fischer, Fischer Management & Professional Support Team
DATE: September 11, 2023
RE: Brent Spence Bridge Corridor Project
Response to BSMT Technical Memorandum, Dated August 29, 2023

Thank you for your continuing engagement in our mutual effort to achieve the best solution for the Cincinnati approach to the Ohio River bridges.

Following are comments offered in response to technical memorandum to the Bi-State Management Team (BSMT) providing a “very high-level conceptual review” of the proposed refinements to concept alternative I-W inspired by Bridge Forward’s vision. This concept is referred to throughout this response as the Bridge Forward (BF) US 50 Concept, for clarity, just as the technical memo did, but as with I-W, it is a refinement of the current preferred alternative I.

The purpose of developing this alternative design concept was to demonstrate the feasibility of the Bridge Forward conceptual layout when applying the same design criteria used by the BSMT in developing I-W. Both the BF US 50 Concept and I-W remain at the conceptual design stage, both have design exceptions, and both are considered feasible, meaning that both should be examined during the proof-of-concept phase of design. Exhibit 1 of this memorandum compares and contrasts the known design exceptions for both I-W and the BF US 50 Concept.

Our team has continued to work to address concerns expressed by the BSMT and have subsequently modified the original BF US 50 Concept to reduce design exceptions and costs and have included layouts in Exhibits 2 and 3. We will refer to these subsequent refinements as the I-W Street Grid Refinement.

Our goal is not to recreate Fort Washington Way with any of these refinements, but to achieve many of the same outcomes that resulted from the FWW redesign. These outcomes are effectively incorporated in the progressive design-build contract objectives noted in Attachment 1 of your technical memo.

Responses to specific sections of your technical memorandum follow:

Alignment and Profile

Items 1-4:

Both I-W and the BF US 50 Concept use a “stacked” approach to interchange design. The disagreement appears to be about the definition of a tunnel. Different states have different thresholds for when a structure should be considered a tunnel, ranging from 350 feet to 850 feet in length. ODOT, according to our previous conversations, does not have an identified threshold. Our view is that because the structures will not exceed 800 feet in length, they should be considered bridges.

As a result of previous feedback from BSMT and their technical team, our subsequent engineering efforts have been focused on addressing this concern and reducing the costs associated with the BF US 50 Concept.

With our most recent modifications to arrive at the I-W Street Grid Refinement, we have placed the I-75 mainline and Collector-Distributor roads at the lowest level of the stack, and the grade-separated US 50 ramps above, effectively removing the tunnel vs. bridge debate and reducing the overall bridge surface area.

Our first iteration was aimed at proving that the Bridge Forward inspired refinements of Preferred Alternative I-W were feasible. Our subsequent iterations with the I-W Street Grid Refinement, including “flipping the stack”, have the byproduct of lowering the I-75 mainline and maintaining the local access roads and ramps close to their current elevations.

These modifications have resulted in the I-75 mainline and CD system being lower than the existing freeway between 6th and 9th streets, while the 6th and 7th street overpasses are only slightly higher than the existing overpasses. The 8th/9th street overpass remains at approximately the same elevation as the existing overpass. Typical cross sections are all shown in Exhibit 4 at roughly the same locations as in your technical memo.

Item 5:

The terrain of southwestern Ohio as it approaches the Ohio River is steep. Local access roads between 3rd and 5th streets exceed 9% in the BF US 50 Concept. These should be compared to the grades of the various downtown streets leading from Fort Washington Way between 3rd and 4th streets with grades between 7 and 9 percent. Monastery Street along I-471, just north of its connection with the ramps from US 50, is upwards of 12%. It is not uncommon in the Cincinnati CBD perimeter transition areas to have steep local street grades as is present in the BF US 50 Concept. This is part of the the City’s design context.

For the I-W Street Grid Refinement, the local access roads grades have been reduced to between 7% and 9%, because of the lowering of the overall local access roads system.

Items 6 and 7:

Our team has adopted the BSMT alignment for the I-71/I-75 mainlines and CD connections just north of the Ohio River to avoid both the Duke Energy gas pipeline terminal and property impacts near the Bengals practice facility.

We emphasize that both I-W and the BF US 50 Concept require design exceptions. A number of the design exceptions required in the original BF Concept have been eliminated in the I-W Street Grid Refinement.

At this point, the I-W Street Grid concept, as modified, requires fewer design exceptions and costs about the same as I-W.

The HNTB memorandum refers to “project design criteria” established during the “performance-based design process”. Please provide these criteria to our group along with an explanation of the performance-based design process undertaken.

Access

Our group recognizes that some connections that today do not require stopping at a signalized intersection will require it in the future under the BF US 50 Concept. This is offset by creating a safer, lower-speed, more pedestrian and alternative mobility friendly urban environment.

We recognize and agree that, as the technical memorandum states, additional analysis is required to determine the appropriate number of lanes, signal timing, turning movements, traffic volumes and composition (truck and cars). This analysis should take place during the proof-of-concept phase of the progressive design-build process.

Footprint

Squabbling about the exact amount of reclaimed acreage is pointless. It is the value and accessibility of the acreage that is important. I-W only allows access to the reclaimed land from Central Avenue, while the BF US 50 Concept provides access from all four sides of the re-established local street-grid. Providing lower speed local streets improves safety for all roadway users trying to access these properties, as well as for pedestrians and bicyclists taking advantage of improved access to the Queensgate neighborhood.

Our modifications in the I-W Street Grid Refinement have reduced the elevation differences that might contribute to creating a visual or physical barrier between the Central Business District and the Queensgate neighborhood. However, we would point out that similar elevation differences exist along Fort Washington Way. In this case the development community capitalized on the elevation differences to incorporate parking garages and other amenities below grade while maximizing the development potential. The market will find a way to favorably respond to a well-designed circulation and access template.

Costs

ODOT's preliminary cost estimates are not as detailed as the approach used by our team. ODOT is using a "per lane mile" approach, while including additional costs if that approach does not accurately account for specific items, such as maintenance of traffic.

Our group has utilized the quantities from *OpenRoads*, the common computer aided design (CAD) system used by both our engineering team and by ODOT. This approach considers the quantities derived from the design, for roadways, bridges, retaining walls, drainage, signage and marking, traffic signals, and maintenance of traffic. Additionally, our team has applied a contingency for items that are unquantifiable at this point in time.

Our estimates for the initial iteration, the BF US 50 Concept, based on the above-described methodology amounted to an estimate of \$130M over that of I-W. ODOT's estimates for the initial iteration of the ADC have ranged from \$65M to \$140M above concept I-W.

Estimates for our team's most recent modifications range are essentially equivalent to the estimated cost of I-W. We have included a compilation of our costs for I-W, the BF US 50 Concept, and the I-W Street Grid Refinement in Exhibit 5.

Operations

Operation and maintenance of the "local road system" is completely up to decisions made by the City of Cincinnati. The local access roads could be operated and maintained by the State of Ohio as part of the Interstate system if there are no mid-block access points other than signalized intersections (known as

“limited access”). The Return on Investment and Economic Impact of access should be evaluated if the City desires to provide access from this facility to private entities.

Note that this decision by the City of Cincinnati impacts the traffic analysis discussed above under Access.

Traffic

Minus the details of the BSMT’s technical team’s “high level analysis”, it is not possible to evaluate their conclusions. Please provide this analysis. We agree with the conclusion that a more detailed analysis would be required to make an informed decision about the performance of the local street grid, but we reject that gridlock would be the starting point assumption. We are confident that this is an engineering problem to be solved and not a fatal flaw. We have offered experts in the field as resources to address this concern.

Conclusions

Our team’s goals are to reconnect the West End and Queensgate neighborhoods to downtown by narrowing the cross-section of the freeway, making it traversable by walking and/or bicycling; and creating a local street grid that makes any reclaimed land more valuable, thus spurring economic development.

We recognize there are tradeoffs such as slower speeds through signalized intersections. In addition to achieving the goals mentioned above, we value the offsetting benefits of reliability and safety offered by this treatment.

As discussed above under Footprint, we are not interested simply in raw acreage, but rather, in developable and accessible acreage.

We concur that it is necessary to take a more comprehensive approach to establishing the correct cross-section of the local street grid. If your technical team would share its data and calculations, for example any HCS runs and assumptions, we could engage in a productive conversation about goals and tradeoffs with respect to traffic volumes on local streets.

We note that the City may be asked to take on additional maintenance responsibilities. With respect, this is a policy question, not an engineering issue. The City Council will direct the administration regarding its judgement with respect to the tradeoffs associated with accepting such an obligation. We are interested in reviewing the buildup of your estimate of the costs you think are involved in maintaining the local street grid.

We have listened to your concerns about the BF US 50 Concept and have addressed most of these with the I-W Street Grid Refinement in Exhibit 2. This is evidence that creative engineers can resolve numerous challenges in a short period of time.

We are committed to continue to work collaboratively with the BSMT through the upcoming innovation period and the remainder of the project development process and we appreciate the BSMT’s willingness to do likewise. We firmly believe that together we are striving for a project that will benefit the Cincinnati community for generations to come.

Design files (*OpenRoads* .dgn) are being separately transmitted to HNTB, BSMT's technical advisor. A list of these files has been included in Exhibit 6.

EXHIBIT 1 – Design Exceptions

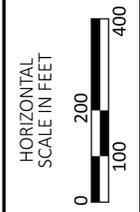
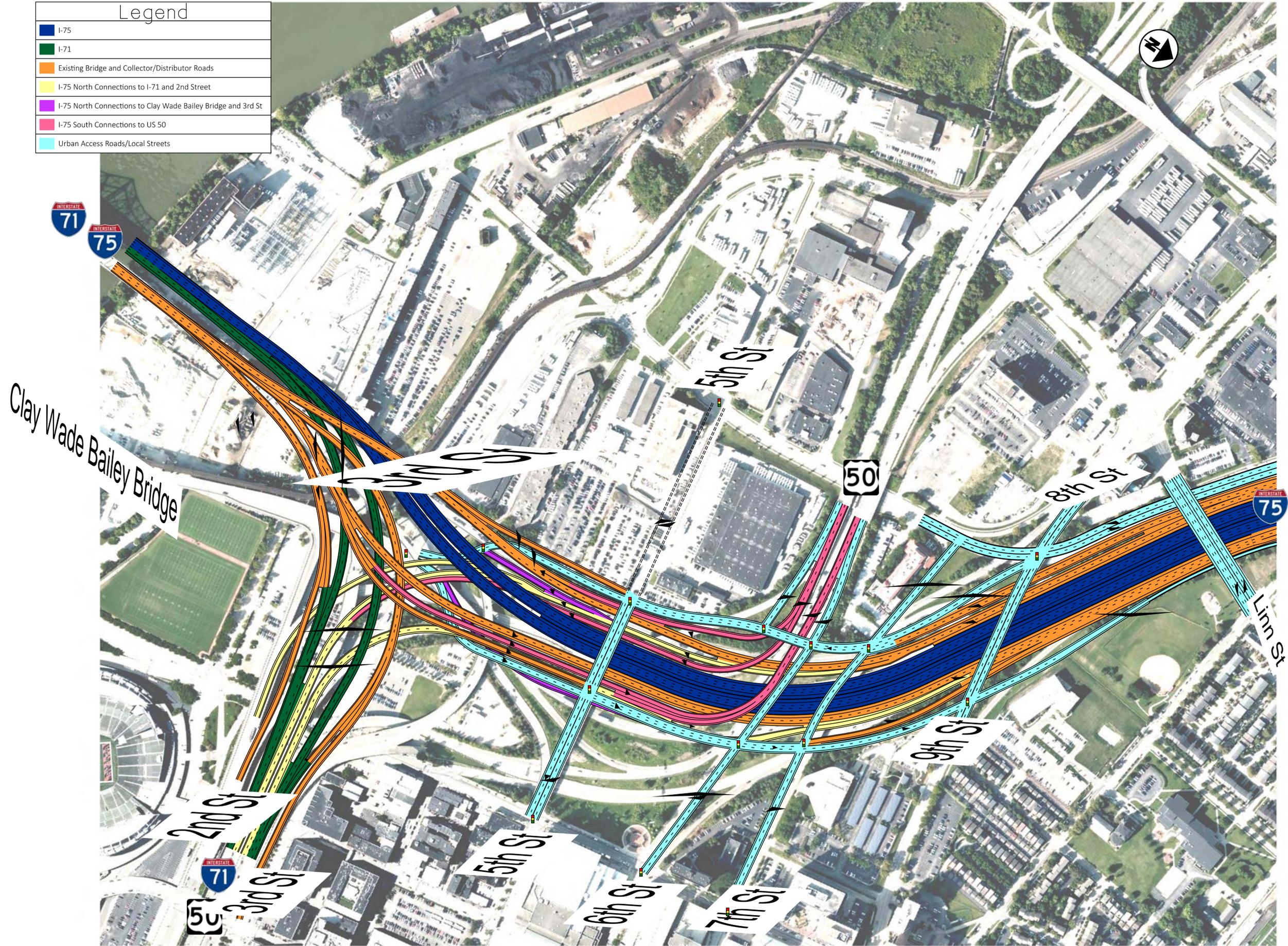
I-W Street Grid Refinement Design Exceptions										
Alignment Name	Design Speed Met	(required)	Horizontal Dc	(max)	Horizontal SSD	(Min)	Max Grade	(Required)	Vertical SSD	(Min)
US 50 EB - I-71 NB	35 mph	(50)	16° 22' 13"	(6° 45' 00")	196	(425)				
US 50 EB - I-71 NB	45 mph	(50)	9° 10' 02"	(6° 45' 00")	247	(425)				
US 50 EB - I-71 NB	45 mph	(50)							360	(425)
I-75 SB - I-71 NB	45 mph	(50)	7° 38' 22"	(6° 45' 00")						
I-75 SB - I-71 NB	45 mph	(50)	8° 54' 19"	(6° 45' 00")						
I-71 SB - US 50 WB	36 mph	(50)	15° 31' 39"	(6° 45' 00")	313	(425)				
I-71 SB - US 50 WB	42 mph	(50)	10° 25' 03"	(6° 45' 00")	226	(425)				
I-71 SB - US 50 WB	45 mph	(50)					6.00%	5.00%		360 (425)
CD NB - US 50							6.50%	6.00%		
CD NB - US 50	35 mph	(45)	16° 22' 13"	(9° 00' 00")	186	(360)				
CD NB - US 50	37 mph	(45)			270	(360)				
US 50 - CD SB	30 mph	(50)	23° 52' 24"	(6° 45' 00")						
US 50 - CD SB							7.50%	5.00%		
US 50 - CD SB	45 mph	(50)							360	(425)
I-71 CD NB	39 mph	(45)			293	(360)				
I-71 CD NB										
I-71 CD SB	35 mph	(45)			254	(360)				
I-71 CD SB							6.40%	6.00%		
I-71 SB - CD NB	43 mph	(45)	9° 32' 57"	(9° 00' 00")	242	(360)				
I-71 NB	52 mph	(55)	6° 21' 58"	(5° 30' 00")	336	(495)				
I-75 ML NB							5.50%	5.00%		
I-75 ML SB	48 mph	(55)			404	(495)				
I-75 ML SB										
I-71 SB	54 mph	(55)	5° 43' 46"	(5° 30' 00")	284	(495)				
I-71 SB							6.50%	5.00%		
CD NB							6.50%	6.00%		
CD SB							6.50%	6.00%		

I-W Street Grid Refinement Design Exceptions - unique alignments										
6th St WB	35 mph	(40)								250 (305)
5th St	35 mph	(40)								250 (305)
I-75 SB - 2nd St	37 mph	(45)			270	(360)				
CD NB - 2nd St	36 mph	(40)			269	(305)				
CD NB - 5th St	43 mph	(45)	9° 32' 57"	(9° 00' 00")	242	(360)				
5th St - CD SB							6.50%	6.00%		
5th St - CD SB	35 mph	(45)								250 (360)
Frontage Rd NB	28 mph	(40)	28° 38' 52"	(11° 45' 00")						
Frontage Rd NB	28 mph	(40)	28° 38' 52"	(11° 45' 00")						
Frontage Rd NB	35 mph	(40)					9.00%	8.00%		
Frontage Rd NB	28 mph	(40)	28° 38' 52"	(11° 45' 00")						250 (305)
Frontage Rd SB	28 mph	(40)	28° 38' 52"	(11° 45' 00")						
Frontage Rd SB	33 mph	(40)			237	(305)				
Frontage Rd SB							8.60%	8.00%		
Frontage Rd SB	35 mph	(40)								250 (305)
SB CD - 7th St	44 mph	(45)	9° 10' 55"	(9° 00' 00")						
SB CD - 7th St							6.50%	6.00%		
Linn St	35 mph	(40)								250 (305)
7th St	35 mph	(40)								250 (305)
6th St - CD NB							6.20%	6.00%		
3rd St - CD NB	37 mph	(45)			279	(360)				
3rd St - CD NB	44 mph	(45)	9° 16' 29"	(9° 00' 00")						
3rd St - CD NB	40 mph	(45)								305 (360)
Gest St	29 mph	(40)			199	(305)				
Gest St	35 mph	(40)								250 (305)

Original I-W Concept Design Exceptions Comparison										
Alignment Name	Design Speed Met	(required)	Horizontal Dc	(max)	Horizontal SSD	(Min)	Max Grade	(Required)	Vertical SSD	(Min)
I-W: US 50 EB	40 mph	(50)	11° 00' 00"	(6° 45' 00")						
I-W: US 50 EB	36 mph	(50)				261	(425)			
I-W: US 50 EB	45 mph	(50)							360	(425)
I-W: US 50 EB	45 mph	(50)							360	(425)
I-W: I-75 SB - I-71 NB	40 mph	(45)	10° 30' 00"	(9° 00' 00")						
I-W: I-75 SB - I-71 NB	43 mph	(45)				341	(360)			
I-W: I-75 SB - I-71 NB	34 mph	(45)				240	(360)			
I-W: US 50 WB	40 mph	(50)	10° 30' 00"	(6° 45' 00")						
I-W: US 50 WB	40 mph	(50)	10° 30' 00"	(6° 45' 00")						
I-W: US 50 WB	34 mph	(50)				242	(425)			
I-W: US 50 WB	45 mph	(50)							360	(425)
I-W: NB CD - WB 50	40 mph	(45)								305 (306)
I-W: NB CD - WB 50	40 mph	(45)								305 (306)
I-W: NB CD - WB 50	40 mph	(45)	11° 30' 00"	(9° 00' 00")						
I-W: US 50 - SB CD	41 mph	(45)	10° 45' 00"	(9° 00' 00")						
I-W: US 50 - SB CD	34 mph	(45)				240	(360)			
I-W: NB CD - NB I-71	42 mph	(45)				325	(360)			
I-W: NB CD - NB I-71	40 mph	(45)								305 (306)
I-W: I-71 SB - SB CD	41 mph	(45)				315	(360)			
I-W: I-71 SB - SB CD								6.44%	5.00%	
I-W: I-71 SB - NB CD	33 mph	(45)				230	(360)			
I-W: I-71 SB - NB CD	40 mph	(45)				305	(360)			
I-W: I-71 SB - NB CD	40 mph	(45)	11° 45' 00"	(9° 00' 00")						
I-W: I-71 SB - NB CD	40 mph	(45)								305 (306)
I-W: I-71 SB - NB CD	40 mph	(45)								305 (306)
I-W: I-71 NB	50 mph	(55)	6° 30' 00"	(5° 30' 00")						
I-W: I-71 NB	50 mph	(55)	5° 37' 00"	(5° 30' 00")						
I-W: I-71 NB	43 mph	(55)				340	(495)			
I-W: I-71 NB								6.51%	5.00%	
I-W: I-75 CL	51 mph	(55)				443	(495)			
I-W: I-71 SB	45 mph	(55)				360	(495)		6.50%	5.00%
I-W: I-71 SB										
I-W: NB CD to Local								6.97%	5.00%	
I-W: NB CD to Local	40 mph	(45)								305 (306)
I-W: NB CD to Local	40 mph	(45)								305 (306)
I-W: SB CD from I-75								6.02%	5.00%	
I-W: SB CD from I-75								6.50%	5.00%	

EXHIBITS 2 & 3 – I-W Street Grid Refinement Layouts

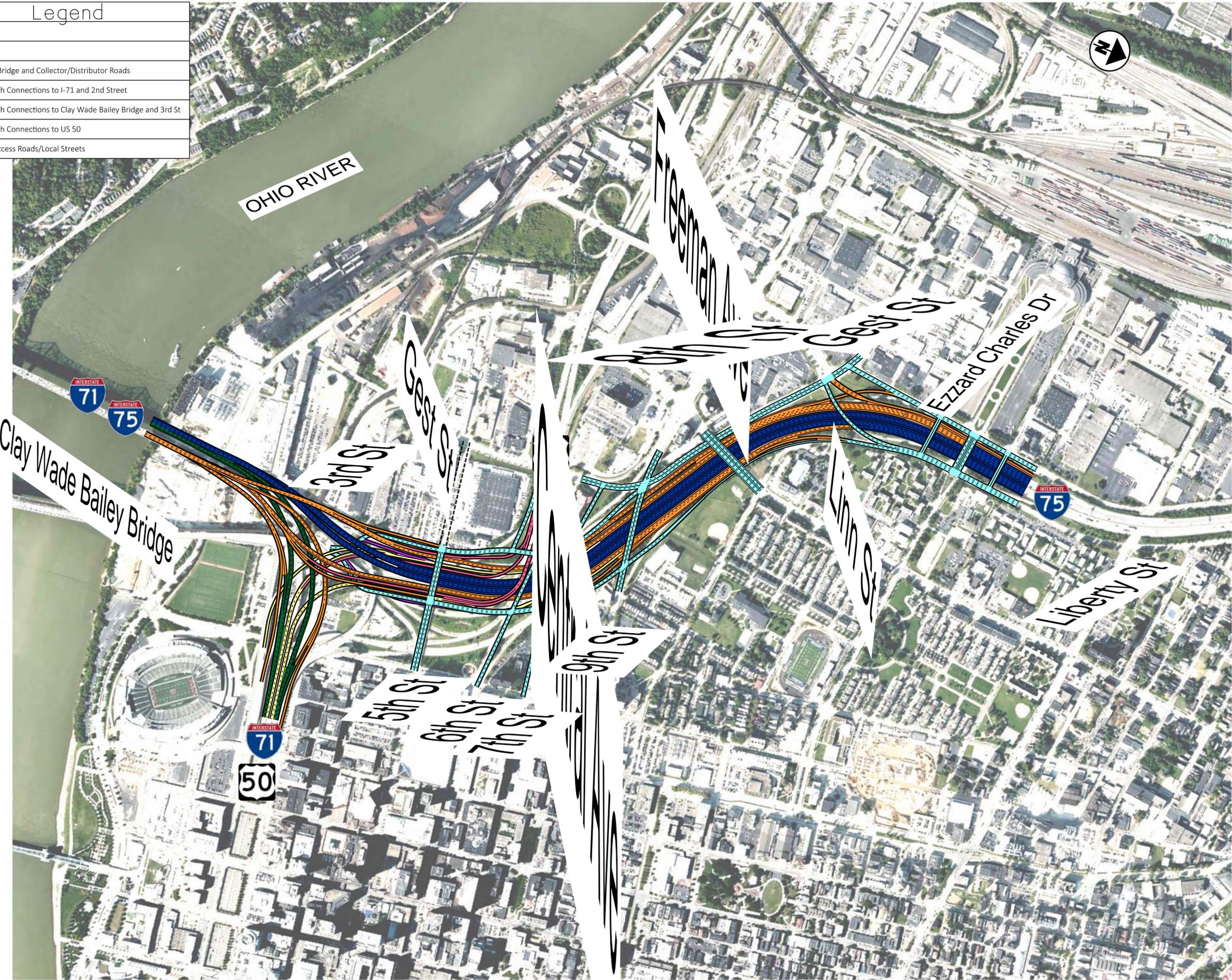
Legend	
	I-75
	I-71
	Existing Bridge and Collector/Distributor Roads
	I-75 North Connections to I-71 and 2nd Street
	I-75 North Connections to Clay Wade Bailey Bridge and 3rd St
	I-75 South Connections to US 50
	Urban Access Roads/Local Streets



ALTERNATE PLAN EXHIBIT

DESIGN AGENCY	
DESIGNER	JRW
REVIEWER	XXX MM-DD-YY
PROJECT ID	0
SHEET	TOTAL
1	1

Legend	
	I-75
	I-71
	Existing Bridge and Collector/Distributor Roads
	I-75 North Connections to I-71 and 2nd Street
	I-75 North Connections to Clay Wade Bailey Bridge and 3rd St
	I-75 South Connections to US 50
	Urban Access Roads/Local Streets



ALTERNATE PLAN EXHIBIT

DESIGN AGENCY

DESIGNER
JRW

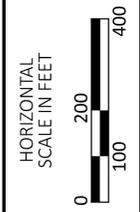
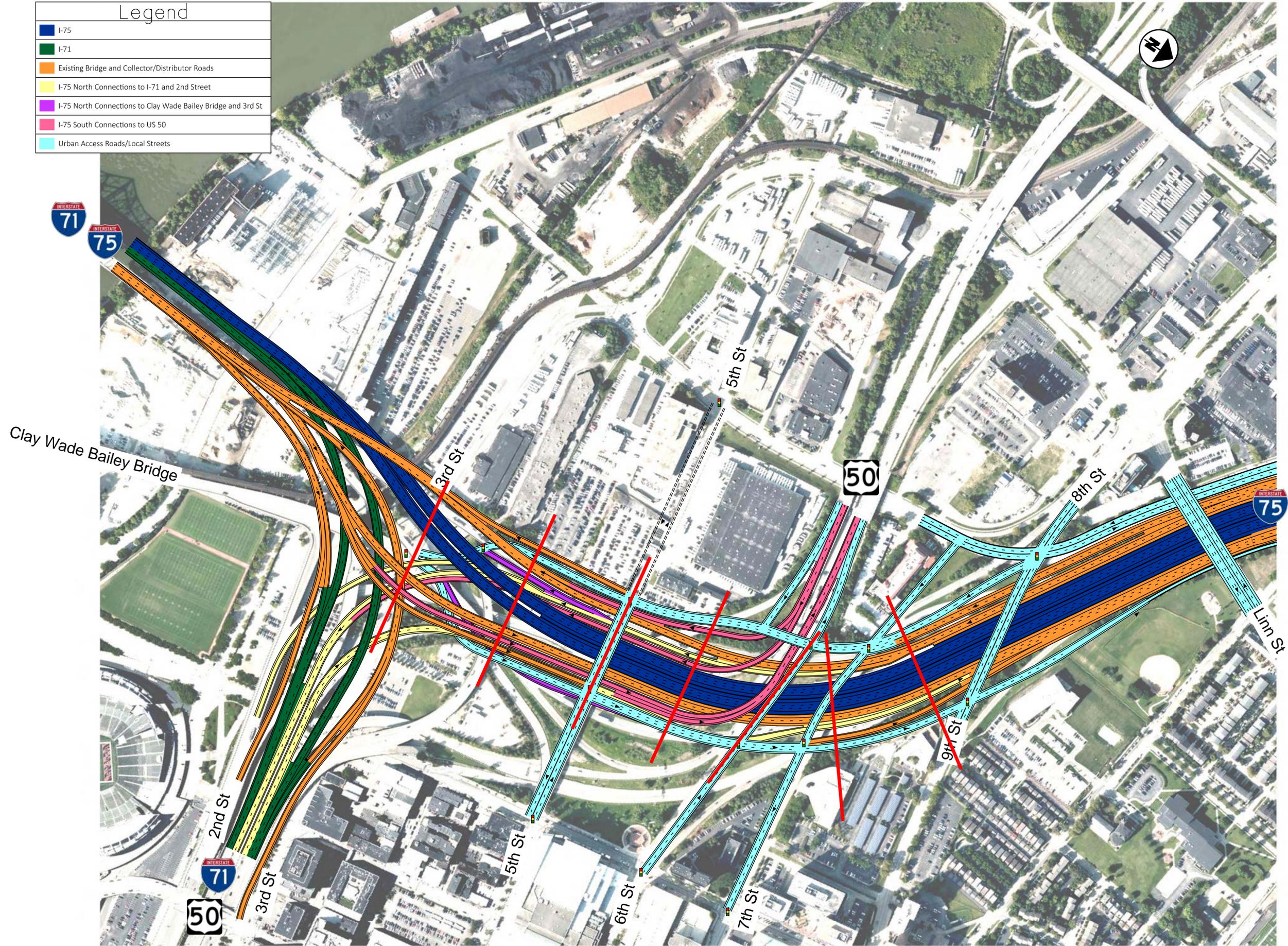
REVIEWER
XXX MM-DD-YY

PROJECT ID
0

SHEET	TOTAL
1	1

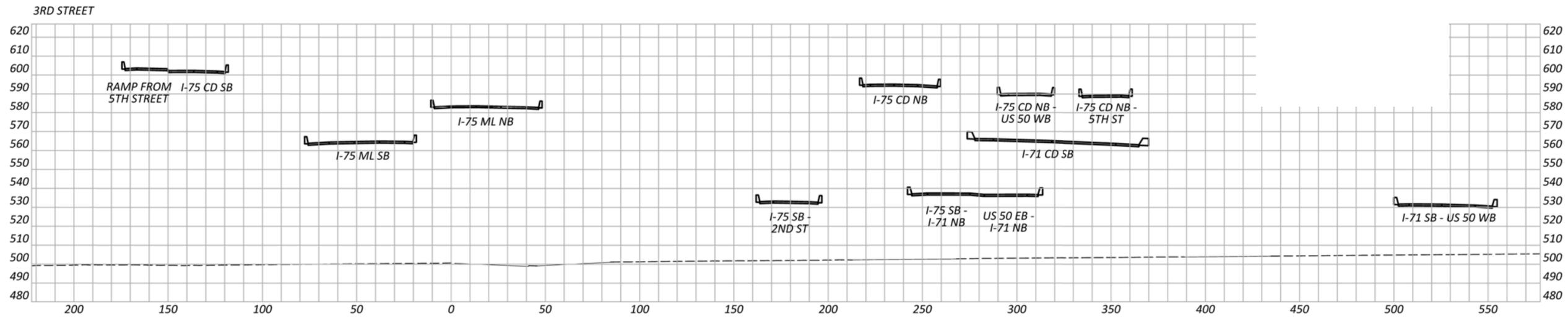
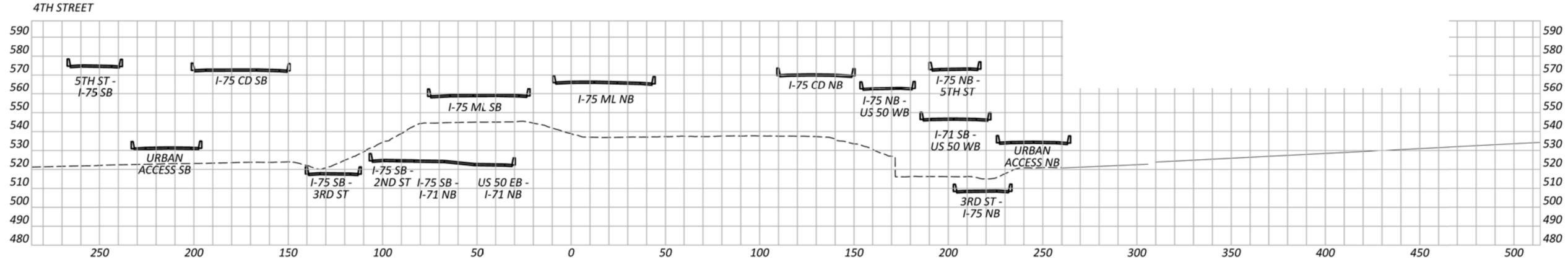
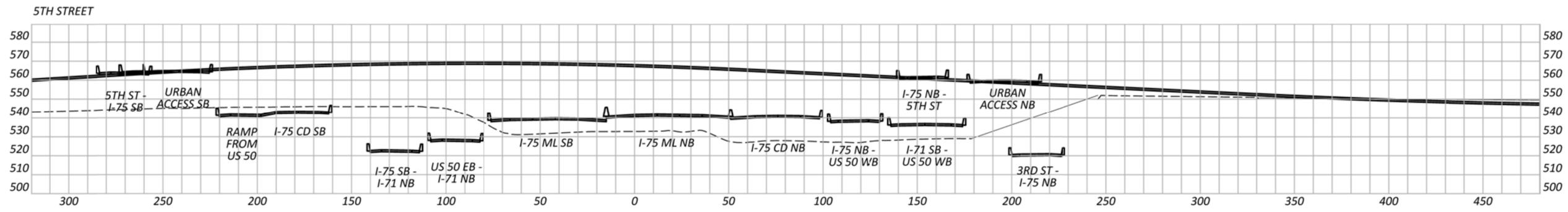
EXHIBIT 4 – Typical Cross Sections

Legend	
	I-75
	I-71
	Existing Bridge and Collector/Distributor Roads
	I-75 North Connections to I-71 and 2nd Street
	I-75 North Connections to Clay Wade Bailey Bridge and 3rd St
	I-75 South Connections to US 50
	Urban Access Roads/Local Streets



ALTERNATE PLAN EXHIBIT

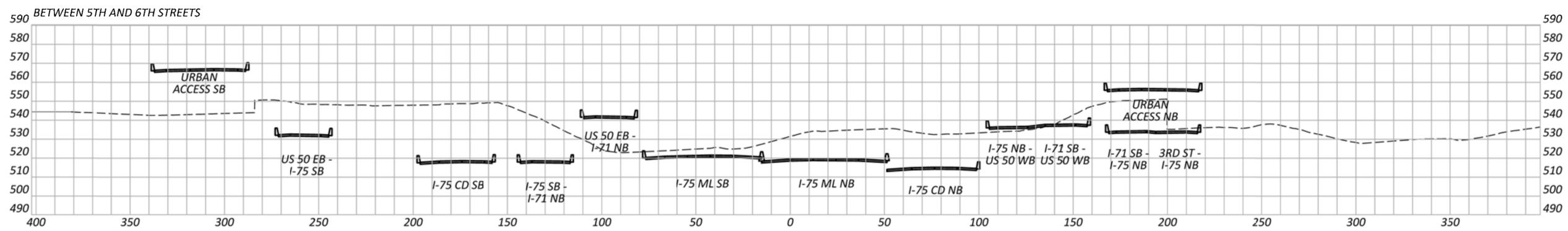
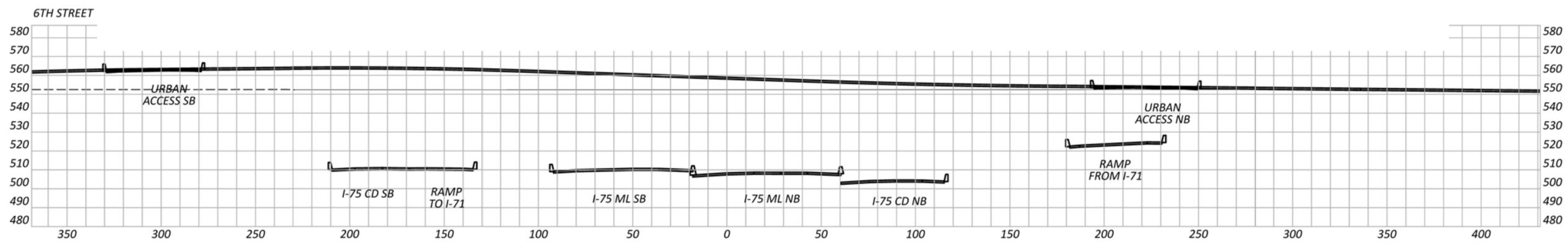
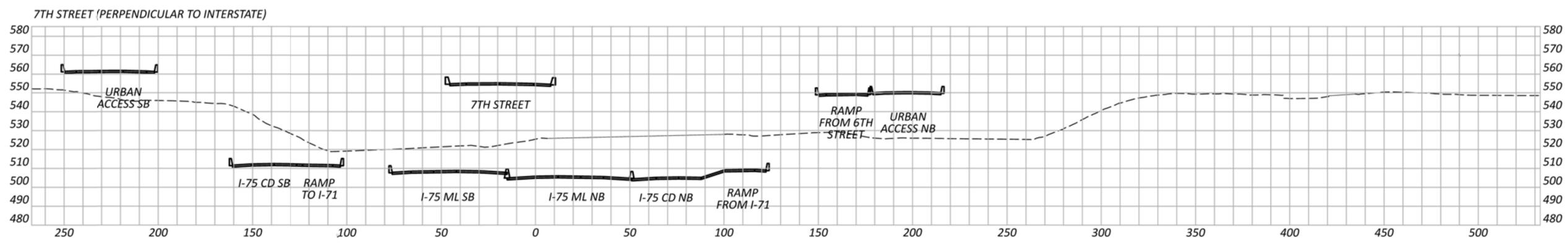
DESIGN AGENCY	
DESIGNER	JRW
REVIEWER	XXX MM-DD-YY
PROJECT ID	0
SHEET	TOTAL
1	1



SHEET TITLE
SHEET SUB-TITLE

DESIGN AGENCY	
DESIGNER	XXX
REVIEWER	XXX MM-DD-YY
PROJECT ID	

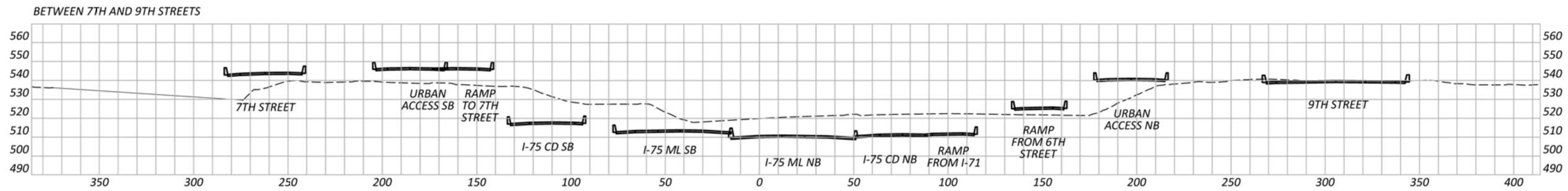
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SHEET P.O.	TOTAL	0



SHEET TITLE
 SHEET SUB-TITLE

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DESIGNER	XXX
REVIEWER	XXX MM-DD-YY
PROJECT ID	

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Sheet Totals			SHEET TOTAL	
Seeding	Cut	Fill	P.O	0
				0

DESIGN AGENCY
DESIGNER
REVIEWER
PROJECT ID

SHEET TITLE
 SHEET SUB-TITLE

EXHIBIT 5 – Preliminary Opinions of Construction Costs Compilation

Brent Spence Bridge Ohio Approach
Preliminary Opinions of Costs

Category	ODOT Preferred Alternative I-W	Bridge Forward US 50 Concept	I-W Street Grid Refinement
Earthwork/Removals	\$38,501,660	\$69,360,087	\$52,311,168
Pavement	\$56,982,273	\$60,046,705	\$59,920,113
Bridges & Retaining Walls	\$556,951,935	\$600,586,475	\$538,029,287
Signals, Signing & Lighting	\$31,596,271	\$38,072,891	\$34,086,256
Drainage & Utilities	\$56,378,288	\$63,581,204	\$56,405,261
Miscellaneous	\$145,120,443	\$163,002,882	\$145,187,408
Contingency (20%)	<u>\$177,106,174</u>	<u>\$198,930,049</u>	<u>\$177,187,899</u>
Grand Total	\$1,062,637,000	\$1,193,580,000	\$1,063,127,000
<i>Compared to Alt I-W</i>	<i>N/A</i>	<i>-\$130,943,000</i>	<i>-\$490,000</i>

EXHIBIT 6 – List of Design Files Transmitted to BSMT

Name	Date modified	Type	Size
 116649_BC000.dgn	5/25/2023 14:13	Bentley MicroStati...	53,211 KB
 116649_BC001.dgn	4/18/2023 13:23	Bentley MicroStati...	17,061 KB
 116649_BF001_rev1.dgn	4/18/2023 13:23	Bentley MicroStati...	12,029 KB
 116649_BK001_rev1.dgn	5/3/2023 14:35	Bentley MicroStati...	10,452 KB
 116649_BP001_rev1.dgn	5/2/2023 12:35	Bentley MicroStati...	4,023 KB
 116649_KD001 (2).dgn	4/5/2023 15:13	Bentley MicroStati...	75,936 KB
 116649_KD001.dgn	4/27/2023 8:14	Bentley MicroStati...	64,604 KB
 116649_KD003 (2).dgn	4/5/2023 15:12	Bentley MicroStati...	59,344 KB
 116649_KD003.dgn	4/27/2023 8:00	Bentley MicroStati...	59,344 KB
 alignments and profiles.dgn	9/11/2023 7:43	Bentley MicroStati...	4,032 KB
 Cost Estimate - BF US 50 Concept.xlsx	9/12/2023 8:00	Microsoft Excel W...	37 KB
 Cost Estimate - I-W Street Grid Refinement.xlsx	9/12/2023 8:00	Microsoft Excel W...	41 KB
 Cost Estimate - I-W.xlsx	9/12/2023 7:59	Microsoft Excel W...	40 KB
 lanes and shoulders.dgn	9/7/2023 13:23	Bentley MicroStati...	1,405 KB

From: Ryan Laber <rmlaber@gmail.com>
Sent: Monday, October 30, 2023 5:09 PM
To: Matt.Bruning@dot.ohio.gov
Cc: info@brentspencebridgecorridor.com; Ohio.FHWA@dot.gov; Kentucky.FHWA@dot.gov; Jodi Heflin; Fuller, Kathleen
Subject: Re: Brent Spence - Public Comment Docs - Community Conversation Event
Attachments: BF Petitions 10-30-2023_addresses.pdf

All,

Please see attached for a binder of letters submitted to date supporting the Bridge Forward street grid concept (local urban roads added on both sides of the interstate; east-west connection at 5th Street; etc.). This binder of letters is separate and apart from the "Reconnecting Westway" letter binder that I previously sent to this list of recipients on 6/23/23. Please ensure this is included in the project record.

Thank you!

Ryan Laber
rmlaber@gmail.com | (859) 445-2257 (c)

Bridge Forward

David Bushong

576 Delta Ave

Cincinnati, OH 45226
United States

Phone:

Email Address: davidbushong@gmail.com

Date of Submission: Tue, 08/01/2023 - 11:48

Submission Letter:
I am writing to express my support for the Bridge Forward design concept for the Brent Spence Bridge Project. While I recognize the importance of this national level transportation infrastructure project, this once-in-a-century opportunity has far-reaching implications for our local communities.

I believe that this design offers numerous benefits to our community and should be seriously considered as the starting point for the project. I urge you to advocate for the Ohio Department of Transportation to adopt the Bridge Forward design as the initial model given to the engineering firms that will soon be chosen.

Time is of the essence to ensure that this opportunity does not pass us by!

As our representative, I trust that you have carefully considered the needs and priorities of our community and will make decisions that will have a positive impact for years to come.

The automobile centric design is inherently flawed to begin with. Our complete subservience to car dependency has resulted in a century of cultural decline. We must transition to rail travel as quickly as possible to reduce our emissions and poor land use. America doesn't build great spaces and as long as the car rules our transportation ideology we can't. This bridge plan is the best we can do now. The future will have to take it the rest of the way.

Thank you for your service to our community, and I look forward to your response.

199

Bridge Forward

Logan Stryker

142 Warner Street

Cincinnati, OH 45219
United States

Phone:

Email Address: loganstryker@outlook.com

Date of Submission: Sat, 07/29/2023 - 08:41

Submission Letter:
I am writing to express my support for the Bridge Forward design concept for the Brent Spence Bridge Project. While I recognize the importance of this national level transportation infrastructure project, this once-in-a-century opportunity has far-reaching implications for our local communities.

I believe that this design offers numerous benefits to our community and should be seriously considered as the starting point for the project. I urge you to advocate for the Ohio Department of Transportation to adopt the Bridge Forward design as the initial model given to the engineering firms that will soon be chosen.

Time is of the essence to ensure that this opportunity does not pass us by!

As our representative, I trust that you have carefully considered the needs and priorities of our community and will make decisions that will have a positive impact for years to come.

Thank you for your service to our community, and I look forward to your response.

205

Bridge Forward

Leonard French

3420 Brotherton rd.

Cincinnati, OH 45209
United States

Phone:

Email Address: French61@hotmail.com

Date of Submission: Thu, 07/13/2023 - 23:51

Submission Letter:
I am writing to express my support for the Bridge Forward design concept for the Brent Spence Bridge Project. While I recognize the importance of this national level transportation infrastructure project, this once-in-a-century opportunity has far-reaching implications for our local communities.

I believe that this design offers numerous benefits to our community and should be seriously considered as the starting point for the project. I urge you to advocate for the Ohio Department of Transportation to adopt the Bridge Forward design as the initial model given to the engineering firms that will soon be chosen.

Time is of the essence to ensure that this opportunity does not pass us by!

As our representative, I trust that you have carefully considered the needs and priorities of our community and will make decisions that will have a positive impact for years to come.

Thank you for your service to our community, and I look forward to your response.

211

Bridge Forward

David Wright

2910 Woodrow Avenue

Cincinnati, OH 45211
United States

Phone:

Email Address: dtwright85@gmail.com

Date of Submission: Mon, 07/31/2023 - 22:15

Submission Letter:
I am writing to express my support for the Bridge Forward design concept for the Brent Spence Bridge Project. While I recognize the importance of this national level transportation infrastructure project, this once-in-a-century opportunity has far-reaching implications for our local communities.

Whether or not this project gets approved, it at least deserves consideration. Elected officials who are serious about being good stewards of this city's future and trust owe it to the voters to consider it.

I believe that this design offers numerous benefits to our community and should be seriously considered as the starting point for the project. I urge you to advocate for the Ohio Department of Transportation to adopt the Bridge Forward design as the initial model given to the engineering firms that will soon be chosen.

Time is of the essence to ensure that this opportunity does not pass us by!

As our representative, I trust that you have carefully considered the needs and priorities of our community and will make decisions that will have a positive impact for years to come.

Thank you for your service to our community, and I look forward to your response.

200

Bridge Forward

Rene Thibodeaux

3619 Eileen Road

Kettering, OH 45429
United States

Phone:

Email Address: rene.thibodeaux.1@betageneral.com

Date of Submission: Fri, 07/28/2023 - 22:10

Submission Letter:
The Bridge Forward design concept for the Brent Spence Bridge Project does not go far enough. The problem was never I-75, it was I-71 along the Riverfront. I-71 required building a "tight spaghetti" interchange at I-71/I-74 to cross the river. Building I-71 along the river left no room for a wider "T" interchange. I-71 should have NEVER run along the river, instead I-71 should have crossed the Ohio River where I-471 is now. Downtown Cincinnati would have been bounded by I-75 on the west and I-71 on the east with NO freeway between the two through the downtown. I-75 would have a narrow footprint, leaving most of the neighborhoods alone. It will not take long for even the new bridge to become over congested in the future because of the fundamental flaw of running I-71 along the Riverfront. If a freeway should be removed, it should be I-71, then converting I-471 to I-71, then running I-71 along I-275 to I-75. By removing I-71 along the Riverfront, I-71 has a "smoother" route south and I-75 has a "smoother" route west of the downtown with a small footprint and no expensive, tight interchange. The removed I-71 expressway along the Riverfront could be replaced by a surface boulevard, reconnecting the downtown and the Riverfront sports/entertainment complex.

Thank you for your service to our community, and I look forward to your response.

206

Bridge Forward

Milan Kulics

5912 Madison Rd, Apt. 412

Cincinnati, OH 45227
United States

Phone:

Email Address: milan.kulics1@gmail.com

Date of Submission: Thu, 07/13/2023 - 16:53

Submission Letter:
I am writing to express my support for the Bridge Forward design concept for the Brent Spence Bridge Project. While I recognize the importance of this national level transportation infrastructure project, this once-in-a-century opportunity has far-reaching implications for our local communities.

I believe that this design offers numerous benefits to our community and should be seriously considered as the starting point for the project. I urge you to advocate for the Ohio Department of Transportation to adopt the Bridge Forward design as the initial model given to the engineering firms that will soon be chosen.

Time is of the essence to ensure that this opportunity does not pass us by!

As our representative, I trust that you have carefully considered the needs and priorities of our community and will make decisions that will have a positive impact for years to come.

Thank you for your service to our community, and I look forward to your response.

212

Bridge Forward

Nolan Brown

4136 Georgia Ave

Cincinnati, OH 45223
United States

Phone:

Email Address: nolanbrown01@gmail.com

Date of Submission: Mon, 07/31/2023 - 21:36

Submission Letter:
I am writing to express my support for the Bridge Forward design concept for the Brent Spence Bridge Project. While I recognize the importance of this national level transportation infrastructure project, this once-in-a-century opportunity has far-reaching implications for our local communities.

I believe that this design offers numerous benefits to our community and should be seriously considered as the starting point for the project. I urge you to advocate for the Ohio Department of Transportation to adopt the Bridge Forward design as the initial model given to the engineering firms that will soon be chosen.

Time is of the essence to ensure that this opportunity does not pass us by!

As our representative, I trust that you have carefully considered the needs and priorities of our community and will make decisions that will have a positive impact for years to come.

Thank you for your service to our community, and I look forward to your response.

201

Bridge Forward

Chuck Libourel

1028 Mound Street

Cincinnati, OH 45203
United States

Phone:

Email Address: chuck.libourel@gmail.com

Date of Submission: Fri, 07/21/2023 - 13:18

Submission Letter:
I am writing to express my support for the Bridge Forward design concept for the Brent Spence Bridge Project. While I recognize the importance of this national level transportation infrastructure project, this once-in-a-century opportunity has far-reaching implications for our local communities.

I believe that this design offers numerous benefits to our community and should be seriously considered as the starting point for the project. I urge you to advocate for the Ohio Department of Transportation to adopt the Bridge Forward design as the initial model given to the engineering firms that will soon be chosen.

Time is of the essence to ensure that this opportunity does not pass us by!

As our representative, I trust that you have carefully considered the needs and priorities of our community and will make decisions that will have a positive impact for years to come.

Thank you for your service to our community, and I look forward to your response.

207

Bridge Forward

Michael Jarrold-Grapes

6529 SPINDLEWICK LN

CINCINNATI, OH 45230-2434
United States

Phone:

Email Address: jfgfamilycinc1@gmail.com

Date of Submission: Sat, 07/08/2023 - 16:53

Submission Letter:
I am writing to express my support for the Bridge Forward design concept for the Brent Spence Bridge Project. While I recognize the importance of this national level transportation infrastructure project, this once-in-a-century opportunity has far-reaching implications for our local communities.

I believe that this design offers numerous benefits to our community and should be seriously considered as the starting point for the project. I urge you to advocate for the Ohio Department of Transportation to adopt the Bridge Forward design as the initial model given to the engineering firms that will soon be chosen.

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Thank you for your service to our community, and I look forward to your response.

213

Bridge Forward

Brooke Singleton

3708 Parfore Court

Cincinnati, OH 45245
United States

Phone:

Email Address: rbrookesingleton@gmail.com

Date of Submission: Mon, 07/31/2023 - 21:34

Submission Letter:
I am writing to express my support for the Bridge Forward design concept for the Brent Spence Bridge Project. While I recognize the importance of this national level transportation infrastructure project, this once-in-a-century opportunity has far-reaching implications for our local communities.

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Thank you for your service to our community, and I look forward to your response.

202

Bridge Forward

Susan Pare

3924 East

Silverton, OH 45966
United States

Phone:

Email Address: alwaysgettingtaller@gmail.com

Date of Submission: Wed, 07/19/2023 - 08:48

Submission Letter:
I am writing to express my support for the Bridge Forward design concept for the Brent Spence Bridge Project. While I recognize the importance of this national level transportation infrastructure project, this once-in-a-century opportunity has far-reaching implications for our local communities.

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Thank you for your service to our community, and I look forward to your response.

208

Bridge Forward

Jason Taylor

243 Foote Avenue

Bellevue, KY 41073
United States

Phone:

Email Address: jason.taylor@yahoo.com

Date of Submission: Thu, 07/06/2023 - 14:30

Submission Letter:
I am writing to express my support for the Bridge Forward design concept for the Brent Spence Bridge Project. While I recognize the importance of this national level transportation infrastructure project, this once-in-a-century opportunity has far-reaching implications for our local communities.

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Thank you for your service to our community, and I look forward to your response.

214

Bridge Forward

Lauren Hendrix

7211 McMillian W, 6, 6

Cincinnati, OH 45230
United States

Phone:

Email Address: wishgirl101@gmail.com

Date of Submission: Mon, 07/29/2023 - 17:21

Submission Letter:
I am writing to express my support for the Bridge Forward design concept for the Brent Spence Bridge Project. While I recognize the importance of this national level transportation infrastructure project, this once-in-a-century opportunity has far-reaching implications for our local communities.

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Thank you for your service to our community, and I look forward to your response.

203

Bridge Forward

Joe Kuhn

4211 State Route 66

Minster, OH 45965
United States

Phone:

Email Address: joek90621@gmail.com

Date of Submission: Tue, 07/18/2023 - 09:37

Submission Letter:
I am writing to express my support for the Bridge Forward design concept for the Brent Spence Bridge Project. While I recognize the importance of this national level transportation infrastructure project, this once-in-a-century opportunity has far-reaching implications for our local communities.

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Thank you for your service to our community, and I look forward to your response.

209

Bridge Forward

Madison Smith

4230 Cherry Street

Cincinnati, OH 45223
United States

Phone:

Email Address: smithmadisonsmith@gmail.com

Date of Submission: Wed, 07/05/2023 - 11:28

Submission Letter:
I am writing to express my support for the Bridge Forward design concept for the Brent Spence Bridge Project. While I recognize the importance of this national level transportation infrastructure project, this once-in-a-century opportunity has far-reaching implications for our local communities.

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Thank you for your service to our community, and I look forward to your response.

215

Bridge Forward

Juliana Weis

222 McMillian W, 6, 6

Cincinnati, OH 45219
United States

Phone:

Email Address: julyweis@gmail.com

Date of Submission: Sat, 07/29/2023 - 09:50

Submission Letter:
I am writing to express my support for the Bridge Forward design concept for the Brent Spence Bridge Project. While I recognize the importance of this national level transportation infrastructure project, this once-in-a-century opportunity has far-reaching implications for our local communities.

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Thank you for your service to our community, and I look forward to your response.

204

Bridge Forward

Kenny Galdeen

511 N Broadway Street No. 4212

Lebanon, OH 45036
United States

Phone:

Email Address: kgaldeen@sbcglobal.net

Date of Submission: Mon, 07/17/2023 - 10:23

Submission Letter:
I am writing to express my support for the Bridge Forward design concept for the Brent Spence Bridge Project. While I recognize the importance of this national level transportation infrastructure project, this once-in-a-century opportunity has far-reaching implications for our local communities.

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Thank you for your service to our community, and I look forward to your response.

210

Bridge Forward

Deborah Hildebrand

115 Dorsey Street

Cincinnati, OH 45202
United States

Phone:

Email Address: ddixon12@cinci.rr.com

Date of Submission: Fri, 06/30/2023 - 09:02

Submission Letter:
I am writing to express my support for the Bridge Forward design concept for the Brent Spence Bridge Project. While I recognize the importance of this national level transportation infrastructure project, this once-in-a-century opportunity has far-reaching implications for our local communities.

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Thank you for your service to our community, and I look forward to your response.

216

Bridge Forward

Anne Snider
119 Dorsey St

Cincinnati, OH 45202
United States

Phone:
Email Address: dsnyder25@gmail.com
Date of Submission: Wed, 06/28/2023 - 17:46

Submission Letter:
I am writing to express my support for the Bridge Forward design concept for the Brent Spence Bridge Project. While I recognize the importance of this national level transportation infrastructure project, this once-in-a-century opportunity has far-reaching implications for our local communities.

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Thank you for your service to our community, and I look forward to your response.

217

Bridge Forward

Dana Snider
832 Maple Avenue

Newport, KY 41071
United States

Phone:
Email Address: dsnyder25@gmail.com
Date of Submission: Wed, 06/28/2023 - 15:44

Submission Letter:
I am writing to express my support for the Bridge Forward design concept for the Brent Spence Bridge Project. While I recognize the importance of this national level transportation infrastructure project, this once-in-a-century opportunity has far-reaching implications for our local communities.

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Thank you for your service to our community, and I look forward to your response.

218

Bridge Forward

Nicholas Bolton
4974 Relleum Avenue

Cincinnati, OH 45238
United States

Phone:
Email Address: bolton.nicholasd@gmail.com
Date of Submission: Wed, 06/28/2023 - 12:55

Submission Letter:
I am writing to express my support for the Bridge Forward design concept for the Brent Spence Bridge Project. While I recognize the importance of this national level transportation infrastructure project, this once-in-a-century opportunity has far-reaching implications for our local communities.

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Thank you for your service to our community, and I look forward to your response.

219

Bridge Forward

Jarred Panganiban
601 East Pete Rose Way Unit 537

Cincinnati, OH 45202
United States

Phone:
Email Address: jarredhp3@gmail.com
Date of Submission: Tue, 06/27/2023 - 01:53

Submission Letter:
I am writing to express my support for the Bridge Forward design concept for the Brent Spence Bridge Project. While I recognize the importance of this national level transportation infrastructure project, this once-in-a-century opportunity has far-reaching implications for our local communities.

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Thank you for your service to our community, and I look forward to your response.

220

Bridge Forward

Samantha Pearson
2107 Eastern Avenue

Covington, OH 41015
United States

Phone:
Email Address: samantha.pearson1616@gmail.com
Date of Submission: Fri, 06/23/2023 - 19:20

Submission Letter:
I am writing to express my support for the Bridge Forward design concept for the Brent Spence Bridge Project. While I recognize the importance of this national level transportation infrastructure project, this once-in-a-century opportunity has far-reaching implications for our local communities.

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Thank you for your service to our community, and I look forward to your response.

221

Bridge Forward

Deepika Joshi
580 Walnut Street

Cincinnati, OH 45202
United States

Phone:
Email Address: deepika.primout@gmail.com
Date of Submission: Fri, 06/23/2023 - 14:09

Submission Letter:
I am writing to express my support for the Bridge Forward design concept for the Brent Spence Bridge Project. While I recognize the importance of this national level transportation infrastructure project, this once-in-a-century opportunity has far-reaching implications for our local communities.

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Thank you for your service to our community, and I look forward to your response.

222

Bridge Forward

Hailey Tobler
6905 April Drive

Cincinnati, OH 45239
United States

Phone:
Email Address: htobler1@hotmail.com
Date of Submission: Thu, 06/22/2023 - 12:12

Submission Letter:
I am writing to express my support for the Bridge Forward design concept for the Brent Spence Bridge Project. While I recognize the importance of this national level transportation infrastructure project, this once-in-a-century opportunity has far-reaching implications for our local communities.

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Thank you for your service to our community, and I look forward to your response.

223

Bridge Forward

David Roland
6303 Mayflower Ave

Cincinnati, OH 45237
United States

Phone:
Email Address: eastdrol121@gmail.com
Date of Submission: Thu, 06/22/2023 - 11:28

Submission Letter:
I am writing to express my support for the Bridge Forward design concept for the Brent Spence Bridge Project. While I recognize the importance of this national level transportation infrastructure project, this once-in-a-century opportunity has far-reaching implications for our local communities.

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Thank you for your service to our community, and I look forward to your response.

224

Bridge Forward

EVAN SLAVIK
4168 Paxton Woods Drive

Cincinnati, OH 45209
United States

Phone:
Email Address: evanslavi1k@gmail.com
Date of Submission: Thu, 06/22/2023 - 05:54

Submission Letter:
I am writing to express my support for the Bridge Forward design concept for the Brent Spence Bridge Project. While I recognize the importance of this national level transportation infrastructure project, this once-in-a-century opportunity has far-reaching implications for our local communities.

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Thank you for your service to our community, and I look forward to your response.

225

Bridge Forward

Chris McCormick
1124 Belvedere St.

Cincinnati, OH 45202
United States

Phone:
Email Address: chrismccormick@gmail.com
Date of Submission: Tue, 06/13/2023 - 15:27

Submission Letter:
I am writing to express my support for the Bridge Forward design concept for the Brent Spence Bridge Project. While I recognize the importance of this national level transportation infrastructure project, this once-in-a-century opportunity has far-reaching implications for our local communities.

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Thank you for your service to our community, and I look forward to your response.

226

Bridge Forward

Matthew Yochim
4629 Miller Rd

Blue Ash, OH 45242
United States

Phone:
Email Address: yochimmatthew@gmail.com
Date of Submission: Sat, 06/03/2023 - 09:51

Submission Letter:
I am writing to express my support for the Bridge Forward design concept for the Brent Spence Bridge Project. While I recognize the importance of this national level transportation infrastructure project, this once-in-a-century opportunity has far-reaching implications for our local communities.

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Thank you for your service to our community, and I look forward to your response.

227

Bridge Forward

Greg Franseth
205 Crescent Ave

Wyoming, OH 45215
United States

Phone:
Email Address: gregfranseth@yahoo.com
Date of Submission: Thu, 06/01/2023 - 16:42

Submission Letter:
I am writing to express my support for the Bridge Forward design concept for the Brent Spence Bridge Project. While I recognize the importance of this national level transportation infrastructure project, this once-in-a-century opportunity has far-reaching implications for our local communities.

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Thank you for your service to our community, and I look forward to your response.

228

Bridge Forward

Dale Kleinschmidt
422 Grove Ave

Wyoming, OH 45215
United States

Phone:
Email Address: dckleins@hotmail.com
Date of Submission: Thu, 06/01/2023 - 16:20

Submission Letter:
I am writing to express my support for the Bridge Forward design concept for the Brent Spence Bridge Project. While I recognize the importance of this national level transportation infrastructure project, this once-in-a-century opportunity has far-reaching implications for our local communities.

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Thank you for your service to our community, and I look forward to your response.

229

Bridge Forward

Charles Schartung
127 E Mills Ave

Wyoming, OH 45215
United States

Phone:
Email Address: todcschartung@gmail.com
Date of Submission: Thu, 06/01/2023 - 15:22

Submission Letter:
I am writing to express my support for the Bridge Forward design concept for the Brent Spence Bridge Project. While I recognize the importance of this national level transportation infrastructure project, this once-in-a-century opportunity has far-reaching implications for our local communities.

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Thank you for your service to our community, and I look forward to your response.

230

Bridge Forward

Matthew Vogt
425 East 5th Street

Newport, KY 41071
United States

Phone:
Email Address: matthew.vogtg@gmail.com
Date of Submission: Fri, 05/26/2023 - 13:44

Submission Letter:
I am writing to express my support for the Bridge Forward design concept for the Brent Spence Bridge Project. While I recognize the importance of this national level transportation infrastructure project, this once-in-a-century opportunity has far-reaching implications for our local communities.

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Thank you for your service to our community, and I look forward to your response.

231

Bridge Forward

Evan Eagle
4001 Ballard Avenue

Cincinnati, OH 45209
United States

Phone:
Email Address: evan.eagle@gmail.com
Date of Submission: Tue, 05/23/2023 - 17:50

Submission Letter:
I am writing to express my support for the Bridge Forward design concept for the Brent Spence Bridge Project. While I recognize the importance of this national level transportation infrastructure project, this once-in-a-century opportunity has far-reaching implications for our local communities.

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Thank you for your service to our community, and I look forward to your response.

232

Bridge Forward

Kyle Eagle
4001 Ballard Avenue

Cincinnati, OH 45209
United States

Phone:
Email Address: keagle427@gmail.com
Date of Submission: Tue, 05/23/2023 - 16:21

Submission Letter:
Hello, my name is Kyle Eagle and I am an 18 year old living in Cincinnati. I've lived here all my life and have always been proud of the history Cincinnati has. Unfortunately I have noticed many wrongs taking place in the city's history as well.

I am writing to express my support for the Bridge Forward design concept for the Brent Spence Bridge Project.

I believe their plan could most definitely help right the wrongs that have been made with the previous making of the Interstate 75.

Multiple communities were torn down in the making of I75 and many minorities lived in those places and had functioning neighborhoods. Union Terminal or the Cincinnati Museum Center used to be a hub of transportation before all the development was torn down for the Interstate. These places were never the same since.

Ever since that time the people have continued to be displaced from their communities and never came back the way they were despite the promises of relocation from the government.

As well as the racist origin and displacement of people Interstate 75 also enhanced the traffic problem.

Since we didn't focus on our urban cores and tore them down we are left with only suburbs. These suburbs relied on cars. These cars cause all of the traffic congestion issues and will always cause those issues. If everyone uses cars to go to downtown you have to destroy your downtown for parking.

The other problem with Interstate 75 is that it offered no other alternative modes

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Bridge Forward

of transport. We have almost no other ways of getting to the urban core other than the highway. We could have improved bus, stretcar, rail, bike, and pedestrian transport which offers a whole variety of modes for not just commuting but enjoying the city as well as making it a hotspot for business and activity.

This plan is what's best for Cincinnati and I'm sure loads of Cincinnatians will agree.

This infrastructure project will really impact our future. The city depends on a good design that will take history as well as other factors into account.

There are many examples of places across the world and even in the U.S that recognize these highways have caused lots of issues in the urban core. Boston is realizing their highway issues while Paris, France is rapidly fixing their issues, and Amsterdam in the Netherlands realized this a long time ago and is now providing great transport for city vehicles and cars but as well as prioritizing pedestrians and other modes of public transport. This system actually made all forms of transport better within their city.

I urge you to consider these facts and my pleas, and do what is right for the city and for our nations future.

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Bridge Forward
<ul style="list-style-type: none"> Use of alternative design concepts proposed from sources other than BSMT; Investigation of adding additional scopes of work and/or additional features to the Project, the associated pools of federal funding which may be available for additional scopes, and the likelihood of securing said federal funding; <p>I call upon the City and County to assume codified roles in the Project, such as Cooperating Agency and/or Participating Agency. In doing so, there will be much more local control over the Project's outcome. Also, in doing so, the City / County will have an opportunity to ensure that the DBT is selected in large part based on its demonstrated ability to deliver a context-sensitive urban project while working closely with a large municipality to develop innovative design direction.</p>
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Bridge Forward
<p>Sister Sally Duffy, SC</p> <p>5062 Sidney Road</p> <p>Cincinnati, OH 45238 United States</p> <p>Phone:</p> <p>Email Address: sduffy@srcharitycinti.org</p> <p>Date of Submission: Sun, 01/01/2023 - 11:53</p> <p>Submission Letter: I write to you today, advocating for a transformative Brent Spence Corridor Project. I strongly support the following priorities for the Project's design, which should inform all stages of design development, including innovative concepts and alternative concepts:</p> <ul style="list-style-type: none"> Maximize the amount of returned land contiguous to the City's urban core; Maximize the amount of walkable street frontage that is conducive to street-facing development within and adjacent to the project footprint; Maximize the number of pedestrian connections between the CBD, Queensgate, and the West End, and minimize the walking distance of each of those connections; Maximize the number of local streets that are designed according to the NACTO Urban Street Design Guide or similar; Minimize the number of lane miles using high-speed directional ramps; and Achieve best expenditure of public dollars measured in life-cycle return on investment. <p>According to ODOT / KYTC, the Project will be build using a "progressive design-build process." This approach allows ODOT / KYTC, the design-build team (DBT), impacted municipalities, and the local community to consider and address major Project topics in a more open format. The DBT will work with ODOT / KYTC to create the basis of design, overall project requirements, and final design intent, before work substantially begins, during what is being called Phase 1A and Phase 1B of the Project.</p> <p>Therefore, I am calling upon ODOT, the City of Cincinnati, and Hamilton County to ensure that an expectation is set, at least in contract language with the DBT, and ideally in an addendum to the Project's RFO, for a very robust Phase 1A and Phase 1B of the Project. Specifically, I demand the following expectations be set for Phase 1A and Phase 1B of the Project:</p> <ul style="list-style-type: none"> Incorporation of innovative design concepts; Re-evaluation of assumptions used in the Project's traffic forecasting projections;
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Bridge Forward
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Bridge Forward
<p>Craig Schoenberger</p> <p>556 McAlpin Avenue</p> <p>Cincinnati, OH 45220 United States</p> <p>Phone:</p> <p>Email Address: schoenberger.c@outlook.com</p> <p>Date of Submission: Sun, 01/01/2023 - 11:51</p> <p>Submission Letter: First off, I am writing to CANCEL this entire boondoggle of a project that is 1) not needed and 2) can be solved by either tolls, congestion charges, rerouting more traffic to the 275 bypass which is the entire stated goal of a bypass. Destroying yet more of our cities is NOT the solution. In the event that somehow is not possible (and we decide to waste 3 billion plus on something we don't need.)</p> <p>I write to you today, advocating for a transformative Brent Spence Corridor Project. I strongly support the following priorities for the Project's design, which should inform all stages of design development, including innovative concepts and alternative concepts:</p> <ul style="list-style-type: none"> Maximize the amount of returned land contiguous to the City's urban core; Maximize the amount of walkable street frontage that is conducive to street-facing development within and adjacent to the project footprint; Maximize the number of pedestrian connections between the CBD, Queensgate, and the West End, and minimize the walking distance of each of those connections; Maximize the number of local streets that are designed according to the NACTO Urban Street Design Guide or similar; Minimize the number of lane miles using high-speed directional ramps; and Achieve best expenditure of public dollars measured in life-cycle return on investment. <p>According to ODOT / KYTC, the Project will be build using a "progressive design-build process." This approach allows ODOT / KYTC, the design-build team (DBT), impacted municipalities, and the local community to consider and address major Project topics in a more open format. The DBT will work with ODOT / KYTC to create the basis of design, overall project requirements, and final design intent, before work substantially begins, during what is being called Phase 1A and Phase 1B of the Project.</p> <p>Therefore, I am calling upon ODOT, the City of Cincinnati, and Hamilton County to ensure that an expectation is set, at least in contract language with the DBT, and</p>
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Bridge Forward
<p>Mitchell Mustain</p> <p>1705 Pleasant Street</p> <p>Cincinnati, OH 45202 United States</p> <p>Phone:</p> <p>Email Address: mitchell_mustain@yahoo.com</p> <p>Date of Submission: Sat, 12/31/2022 - 09:52</p> <p>Submission Letter: I write to you today, advocating for a transformative Brent Spence Corridor Project. I strongly support the following priorities for the Project's design, which should inform all stages of design development, including innovative concepts and alternative concepts:</p> <ul style="list-style-type: none"> Maximize the amount of returned land contiguous to the City's urban core; Maximize the amount of walkable street frontage that is conducive to street-facing development within and adjacent to the project footprint; Maximize the number of pedestrian connections between the CBD, Queensgate, and the West End, and minimize the walking distance of each of those connections; Maximize the number of local streets that are designed according to the NACTO Urban Street Design Guide or similar; Minimize the number of lane miles using high-speed directional ramps; and Achieve best expenditure of public dollars measured in life-cycle return on investment. <p>According to ODOT / KYTC, the Project will be build using a "progressive design-build process." This approach allows ODOT / KYTC, the design-build team (DBT), impacted municipalities, and the local community to consider and address major Project topics in a more open format. The DBT will work with ODOT / KYTC to create the basis of design, overall project requirements, and final design intent, before work substantially begins, during what is being called Phase 1A and Phase 1B of the Project.</p> <p>Therefore, I am calling upon ODOT, the City of Cincinnati, and Hamilton County to ensure that an expectation is set, at least in contract language with the DBT, and ideally in an addendum to the Project's RFO, for a very robust Phase 1A and Phase 1B of the Project. Specifically, I demand the following expectations be set for Phase 1A and Phase 1B of the Project:</p> <ul style="list-style-type: none"> Incorporation of innovative design concepts; Re-evaluation of assumptions used in the Project's traffic forecasting projections;
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Bridge Forward
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Bridge Forward
<p>Brian Evans</p> <p>1910 Sterling Avenue,</p> <p>Cincinnati, OH 45239 United States</p> <p>Phone:</p> <p>Email Address: bomberbee@gmail.com</p> <p>Date of Submission: Fri, 12/30/2022 - 18:00</p> <p>Submission Letter: I write to you today, advocating for a transformative Brent Spence Corridor Project. I strongly support the following priorities for the Project's design, which should inform all stages of design development, including innovative concepts and alternative concepts:</p> <ul style="list-style-type: none"> Maximize the amount of returned land contiguous to the City's urban core; Maximize the amount of walkable street frontage that is conducive to street-facing development within and adjacent to the project footprint; Maximize the number of pedestrian connections between the CBD, Queensgate, and the West End, and minimize the walking distance of each of those connections; Maximize the number of local streets that are designed according to the NACTO Urban Street Design Guide or similar; Minimize the number of lane miles using high-speed directional ramps; and Achieve best expenditure of public dollars measured in life-cycle return on investment. <p>According to ODOT / KYTC, the Project will be build using a "progressive design-build process." This approach allows ODOT / KYTC, the design-build team (DBT), impacted municipalities, and the local community to consider and address major Project topics in a more open format. The DBT will work with ODOT / KYTC to create the basis of design, overall project requirements, and final design intent, before work substantially begins, during what is being called Phase 1A and Phase 1B of the Project.</p> <p>Therefore, I am calling upon ODOT, the City of Cincinnati, and Hamilton County to ensure that an expectation is set, at least in contract language with the DBT, and ideally in an addendum to the Project's RFO, for a very robust Phase 1A and Phase 1B of the Project. Specifically, I demand the following expectations be set for Phase 1A and Phase 1B of the Project:</p> <ul style="list-style-type: none"> Incorporation of innovative design concepts; Re-evaluation of assumptions used in the Project's traffic forecasting projections;
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Bridge Forward
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Bridge Forward
<p>Gerry Paff</p> <p>4368 Carnation Circle</p> <p>Cincinnati, OH 45238 United States</p> <p>Phone:</p> <p>Email Address: gtpaff@fuse.net</p> <p>Date of Submission: Fri, 12/30/2022 - 15:16</p> <p>Submission Letter: I write to you today, advocating for a transformative Brent Spence Corridor Project. I strongly support the following priorities for the Project's design, which should inform all stages of design development, including innovative concepts and alternative concepts:</p> <ul style="list-style-type: none"> Maximize the amount of returned land contiguous to the City's urban core; Maximize the amount of walkable street frontage that is conducive to street-facing development within and adjacent to the project footprint; Maximize the number of pedestrian connections between the CBD, Queensgate, and the West End, and minimize the walking distance of each of those connections; Maximize the number of local streets that are designed according to the NACTO Urban Street Design Guide or similar; Minimize the number of lane miles using high-speed directional ramps; and Achieve best expenditure of public dollars measured in life-cycle return on investment. <p>According to ODOT / KYTC, the Project will be build using a "progressive design-build process." This approach allows ODOT / KYTC, the design-build team (DBT), impacted municipalities, and the local community to consider and address major Project topics in a more open format. The DBT will work with ODOT / KYTC to create the basis of design, overall project requirements, and final design intent, before work substantially begins, during what is being called Phase 1A and Phase 1B of the Project.</p> <p>Therefore, I am calling upon ODOT, the City of Cincinnati, and Hamilton County to ensure that an expectation is set, at least in contract language with the DBT, and ideally in an addendum to the Project's RFO, for a very robust Phase 1A and Phase 1B of the Project. Specifically, I demand the following expectations be set for Phase 1A and Phase 1B of the Project:</p> <ul style="list-style-type: none"> Incorporation of innovative design concepts; Re-evaluation of assumptions used in the Project's traffic forecasting projections;
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Bridge Forward
<p>Robert Bonham</p> <p>3525 Centurion Drive</p> <p>Cincinnati, OH 45211 United States</p> <p>Phone:</p> <p>Email Address: bilbonham7@gmail.com</p> <p>Date of Submission: Fri, 12/30/2022 - 08:23</p> <p>Submission Letter: I write to you today, advocating for a transformative Brent Spence Corridor Project. I strongly support the following priorities for the Project's design, which should inform all stages of design development, including innovative concepts and alternative concepts:</p> <ul style="list-style-type: none"> Maximize the amount of returned land contiguous to the City's urban core; Maximize the amount of walkable street frontage that is conducive to street-facing development within and adjacent to the project footprint; Maximize the number of pedestrian connections between the CBD, Queensgate, and the West End, and minimize the walking distance of each of those connections; Maximize the number of local streets that are designed according to the NACTO Urban Street Design Guide or similar; Minimize the number of lane miles using high-speed directional ramps; and Achieve best expenditure of public dollars measured in life-cycle return on investment. <p>According to ODOT / KYTC, the Project will be build using a "progressive design-build process." This approach allows ODOT / KYTC, the design-build team (DBT), impacted municipalities, and the local community to consider and address major Project topics in a more open format. The DBT will work with ODOT / KYTC to create the basis of design, overall project requirements, and final design intent, before work substantially begins, during what is being called Phase 1A and Phase 1B of the Project.</p> <p>Therefore, I am calling upon ODOT, the City of Cincinnati, and Hamilton County to ensure that an expectation is set, at least in contract language with the DBT, and ideally in an addendum to the Project's RFO, for a very robust Phase 1A and Phase 1B of the Project. Specifically, I demand the following expectations be set for Phase 1A and Phase 1B of the Project:</p> <ul style="list-style-type: none"> Incorporation of innovative design concepts; Re-evaluation of assumptions used in the Project's traffic forecasting projections;
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Bridge Forward
<p>Rene Thibodeaux</p> <p>3619 Eileen Rd, Kettering, OH, USA</p> <p>Kettering, OH 45429 United States</p> <p>Phone:</p> <p>Email Address: info@futureinterstatecorridors.com</p> <p>Date of Submission: Thu, 12/29/2022 - 23:45</p> <p>Submission Letter: The Brent Spence Bridge Corridor Project now has a chance to start because the \$1.5B pledge of the U.S. Department of Transportation to fund the 90% Federal share. This project is only the beginning of the necessary interstate infrastructure projects not only in the Cincinnati region but for the entire transcontinental I-75, which will reach traffic capacity design limits in the next 20 years. I am advocating planning an alternative interstate traffic route to I-75, one that will interconnect Midwestern state to the ports of Charleston South Carolina directly.</p> <p>Even though Cincinnati built the I-75 beltway, which helps somewhat relieving traffic inside Cincinnati, but even this is reaching its design limit. Unfortunately, ODOT and KYTC have limited options when it comes to improving I-75 capacity. The 60-year-old Brent Spence Bridge MUST be replaced, no question. Even if I-75 widened by only one lane in either direction through Cincinnati, both local and transcontinental traffic will overcome its capacity in 20 years. Some local business interests have proposed a 4-lane Eastern Bypass, but both ODOT and KYTC have correctly assessed this as an expensive and functionally obsolete concept from the beginning. The I-75 corridor through both Ohio and Kentucky is an increasing growing freight-truck traffic corridor.</p> <p>Local interests in Cincinnati would like to build I-75 suburface and to 'cap' the highway to 'reconnect and restore' the pre-interstate neighborhoods. While this has become a popular option to 'correct' problems of constructing urban interstates in the 1960's and 1970's, it is a very expensive option that in this particular case will produce a freeway with a dangerous steep grade between the bridge and the subsurface section. ODOT has determined this concept is not feasible from traffic safety. While local interests believe the 'capping' option will create an opportunity to build low-cost housing, these redevelopment projects more often become 'new urbanist' neighborhoods that because of the need for a positive financial return, become expensive upper-income 'gentrified' neighborhoods. It is much safer if the highway remains ground level and that large pedestrian walkways and transit overpasses are built to connect the existing neighborhood west of the highway to the downtown on the east. This will be a</p>
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<p>wiser use of taxpayer money that will not provide any real opportunity of low-cost housing.</p> <p>It is time to re-evaluate the purpose of the Interstate System, shifting from endlessly widening urban interstates to building future rural interstates bypassing the major cities. In this case, it means re-evaluating the old Future Interstate 73 concept once proposed through the Columbus area that was cancelled. The State of Ohio and Kentucky should support and work with their Congressional delegations to create a 'new' Future Interstate 73 from U.S. 127 (or U.S. 131) in Michigan, generally parallel to I-75 along U.S. 127 in Ohio, connecting to U.S. 35 west of Dayton. To help relieve traffic from I-75 in Dayton, a bypass connector should be constructed from I-73 west of Dayton to I-75 south of Dayton, intersecting I-75 either at the existing I-675 or further south near Springboro based on feasibility. I-73 would continue using U.S. 35 through Dayton and around Xenia. Feasibility studies should select if I-73 continues along U.S. 35 to bypass west of Chillicothe to U.S. 23 or if I-73 should follow U.S. 68 east of Xenia to OH 73 near Wilmington OH to the new OH 823 Portsmouth bypass, then U.S. 52 and U.S. 23 through eastern Kentucky and Virginia, terminating at I-26 in Kingsport TN forming a traffic corridor from the port of Charleston SC to Michigan.</p>
987

Bridge Forward
<p>Christopher Retzler</p> <p>4248 Langland St</p> <p>Cincinnati, OH 45223 United States</p> <p>Phone:</p> <p>Email Address: chris.retzler@gmail.com</p> <p>Date of Submission: Thu, 12/29/2022 - 20:21</p> <p>Submission Letter: I write to you today, advocating for a transformative Brent Spence Corridor Project. I strongly support the following priorities for the Project's design, which should inform all stages of design development, including innovative concepts and alternative concepts:</p> <ul style="list-style-type: none"> Maximize the amount of returned land contiguous to the City's urban core; Maximize the amount of walkable street frontage that is conducive to street-facing development within and adjacent to the project footprint; Maximize the number of pedestrian connections between the CBD, Queensgate, and the West End, and minimize the walking distance of each of those connections; Maximize the number of local streets that are designed according to the NACTO Urban Street Design Guide or similar; Minimize the number of lane miles using high-speed directional ramps; and Achieve best expenditure of public dollars measured in life-cycle return on investment. <p>According to ODOT / KYTC, the Project will be build using a "progressive design-build process." This approach allows ODOT / KYTC, the design-build team (DBT), impacted municipalities, and the local community to consider and address major Project topics in a more open format. The DBT will work with ODOT / KYTC to create the basis of design, overall project requirements, and final design intent, before work substantially begins, during what is being called Phase 1A and Phase 1B of the Project.</p> <p>Therefore, I am calling upon ODOT, the City of Cincinnati, and Hamilton County to ensure that an expectation is set, at least in contract language with the DBT, and ideally in an addendum to the Project's RFO, for a very robust Phase 1A and Phase 1B of the Project. Specifically, I demand the following expectations be set for Phase 1A and Phase 1B of the Project:</p> <ul style="list-style-type: none"> Incorporation of innovative design concepts; Re-evaluation of assumptions used in the Project's traffic forecasting projections;
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Bridge Forward
<p>Kerry Martin O'Connor</p> <p>7118 Willowood Dr</p> <p>Cincinnati, OH 45241 United States</p> <p>Phone:</p> <p>Email Address: kmartyoc@hotmail.com</p> <p>Date of Submission: Thu, 12/29/2022 - 17:48</p> <p>Submission Letter: I write to you today, advocating for a transformative Brent Spence Corridor Project. I strongly support the following priorities for the Project's design, which should inform all stages of design development, including innovative concepts and alternative concepts:</p> <ul style="list-style-type: none"> Maximize the amount of returned land contiguous to the City's urban core; Maximize the amount of walkable street frontage that is conducive to street-facing development within and adjacent to the project footprint; Maximize the number of pedestrian connections between the CBD, Queensgate, and the West End, and minimize the walking distance of each of those connections; Maximize the number of local streets that are designed according to the NACTO Urban Street Design Guide or similar; Minimize the number of lane miles using high-speed directional ramps; and Achieve best expenditure of public dollars measured in life-cycle return on investment. <p>According to ODOT / KYTC, the Project will be build using a "progressive design-build process." This approach allows ODOT / KYTC, the design-build team (DBT), impacted municipalities, and the local community to consider and address major Project topics in a more open format. The DBT will work with ODOT / KYTC to create the basis of design, overall project requirements, and final design intent, before work substantially begins, during what is being called Phase 1A and Phase 1B of the Project.</p> <p>Therefore, I am calling upon ODOT, the City of Cincinnati, and Hamilton County to ensure that an expectation is set, at least in contract language with the DBT, and ideally in an addendum to the Project's RFO, for a very robust Phase 1A and Phase 1B of the Project. Specifically, I demand the following expectations be set for Phase 1A and Phase 1B of the Project:</p> <ul style="list-style-type: none"> Incorporation of innovative design concepts; Re-evaluation of assumptions used in the Project's traffic forecasting projections;
990

From: [Long, Timothy \(FHWA\)](#)
To: [Laber, Ryan M.](#)
Cc: [Ross, Eric \(FHWA\)](#); [Lowry, Sara \(FHWA\)](#); [Moore, Robert \(FHWA\)](#); [Biondi, Emily \(FHWA\)](#); [Weingroff, Richard \(FHWA\)](#); [Baughman, Pamela \(FHWA\)](#)
Subject: RE: Brent Spence Bridge Corridor
Date: Tuesday, January 9, 2024 11:03:49 AM

Good morning Mr. Laber,
I'm writing in response to your inquiry below, the letter dated March 4, 2023 and its exhibits are part of the project record.

Respectfully,

Tim Long
Planning, Environment and Realty Team Leader
Federal Highway Administration
Ohio Division Office
614-280-6879

From: Ryan Laber <rmlaber@gmail.com>
Sent: Friday, December 1, 2023 5:03 PM
To: Weingroff, Richard (FHWA) <Richard.Weingroff@dot.gov>
Cc: Biondi, Emily (FHWA) <Emily.Biondi@dot.gov>; Leffler, Laurie (FHWA) <Laurie.Leffler@dot.gov>
Subject: Re: Brent Spence Bridge Corridor

CAUTION: This email originated from outside of the Department of Transportation (DOT). Do not click on links or open attachments unless you recognize the sender and know the content is safe.

Richard / Emily / Laurie:

Hope you are all well.

Could I ask a quick question? Would our letter dated 3/4/23 plus its exhibits have been included in the project administrative record?

Ryan Laber
rmlaber@gmail.com | (859) 445-2257 (c)

Appendix L:
Westway Comments and Responses



Brent Spence Bridge Corridor Project Response to "Westway Emails"



Response to Public Comment

Reconnecting Cincinnati Westway Design Improvements

TO: KYTC and ODOT

FROM: HNTB Corporation

DATE: September 14, 2022

RE: Brent Spence Bridge Corridor Project | ODOT PID 89068 | KYTC Project Item No. 6-17

Introduction

The Kentucky Transportation Cabinet (KYTC) and the Ohio Department of Transportation (ODOT) received numerous emails beginning in August 2022 suggesting potential changes to Preferred Alternative I (Concept I-W) for the Brent Spence Bridge (BSB) Corridor Project. The content of the emails was the same, and in most cases the subject line of the emails read: “Brent Spence Bridge Project – Reconnecting Cincinnati Westway Design Improvements” (hereafter referred to as the “Westway Emails”). As expressed in the Westway Emails, KYTC and ODOT are also excited by the momentum gained for the BSB Corridor Project and are dedicated to helping the Cincinnati region receive the best possible outcome for this large infrastructure investment. The following sections provide a response to the concepts and ideas outlined in the emailed comments.

Purpose and Need

The Westway Emails suggest an expansion of the project’s purpose and need statement to include community priorities such as east-west connectivity, multi-modal improvements, and economic development as primary goals (among others). The project’s purpose and need statement was developed based on extensive analyses of existing and proposed conditions in the greater Cincinnati region and in collaboration with local stakeholders – including the cities of Cincinnati and Covington and Kenton and Hamilton counties – and the general public. It has undergone extensive state and federal review and was approved early in the project’s development. While the project’s primary needs reflect the existing and future conditions that are truly causing the transportation problem, they do not preclude KYTC and ODOT from pursuing other goals in the project’s development. To that end, KYTC and ODOT have intentionally incorporated features that reduce the project’s footprint, promote and improve bicycle and pedestrian connectivity, improve water quality by separating storm water from the sanitary system, balance regional and local traffic needs, and reduce community impacts throughout the project’s development. Many of these features were added and refined based on local agency, stakeholder, and public feedback.

Alternatives Development and Evaluation

The Westway Emails also advocate for additional alternatives that do not adhere to Preferred Alternative I (Concept I-W) for the BSB Corridor Project. In August 2012, the Federal Highway Administration (FHWA)



issued a Finding of No Significant Impact (FONSI), which approved the preferred alternative for the BSB Corridor Project after extensive evaluation of numerous alternatives which incorporated feedback from local agencies, stakeholders, and the public. Introducing additional alternatives would invalidate the FONSI issued by FHWA and would require the project's environmental process to be re-opened. The extensive preliminary design, study, evaluation, and public involvement required to re-open the alternatives analysis would delay the project's targeted groundbreaking for two years or more and could potentially jeopardize the project's eligibility for federal grants for construction. These realities conflict with the goals advocated in the Westway Emails, which include capitalizing on funding from the Bipartisan Infrastructure Law, applying for and receiving funding through the federal INFRA and Mega grant programs, and adhering to the project's targeted groundbreaking in late 2023.

Traffic Forecasting

The Westway Emails include comments that traffic forecasting assumptions used to drive the project's design should be validated by actual, historic traffic counts and trends. In addition, the forecasting methodology should consider econometric factors, individual trip preference, and alternative transportation modes, among others. KYTC and ODOT fully support this approach to traffic forecasting and modeling. The traffic forecasts developed for the BSB Corridor Project consider historical traffic counts, trends, regional land use projections, and the planned regional network as forecasted by the Ohio-Kentucky-Indiana Regional Council of Governments (OKI), which is the Metropolitan Planning Organization (MPO) for the greater Cincinnati area. The traffic forecasts consider the full regional network and the ability for trips to shift to other corridors. Trip modes, telecommuting, and changed trips are modeled by OKI using the best available data.

Lowered Alignment of I-75

ODOT supports the idea of lowering freeways through urban areas where feasible, as evidenced by the Fort Washington Way project that is prominently referenced in the Westway Emails. However, the concept simply is not feasible for this specific location due to the following limitations:

- Any bridge over the Ohio River must meet U.S. Coast Guard clearance requirements, which means the bottom of the bridge will need to be 55 feet above the flowline of the Ohio River.
- Once the interstate passes over the Ohio River, it cannot descend directly into downtown Cincinnati. First, it must stay elevated to cross active CSX rail lines between Pete Rose Avenue and 3rd Street.
- Any design must accommodate a complicated system of mainline and ramp movements to provide local access and continuity along I-71, I-75, and US 50. Weaving these ramps through the project area requires a complex tiered bridge system with the highest tier rising over 50 feet above the ground (see Attachment 1).



To meet these geometric constraints, the preferred alternative for the BSB Corridor Project currently descends toward downtown Cincinnati at a 4- to 5.5-percent grade. Lowering the roadway any further would require substantially steeper roadway grades (up to 7.7 percent). For reference, ODOT's *Location and Design Manual, Volume 1* lists the maximum grade for urban interstates in similar terrain as 6 percent. Furthermore, given the complicated geometry, any changes to the mainline profile would cascade throughout the BSB interchange area, resulting in prohibitively steep grades along other ramps and C-D roads. Introducing grades of this magnitude would require design exceptions and present traffic operational and safety concerns, particularly considering the high volumes of heavy truck traffic traveling through the corridor. This would be contrary to the project's approved purpose and need, which includes improving traffic flow and safety and correcting geometric deficiencies through a key regional and national transportation corridor.



Long, steep roadway grades can negatively affect traffic flow on interstate highways such as on southbound I-71/I-75 between Kyles Lane and MLK Jr. Boulevard in Kentucky – also known as the “Cut in the Hill” (pictured above). Truck traffic climbing the hill often moves at a much slower speed than other traffic. The substantial difference in speed between trucks and cars contributes to elevated traffic congestion and crashes.¹ (Photo: Traffic cam for I-71/I-75 at Cut in the Hill)

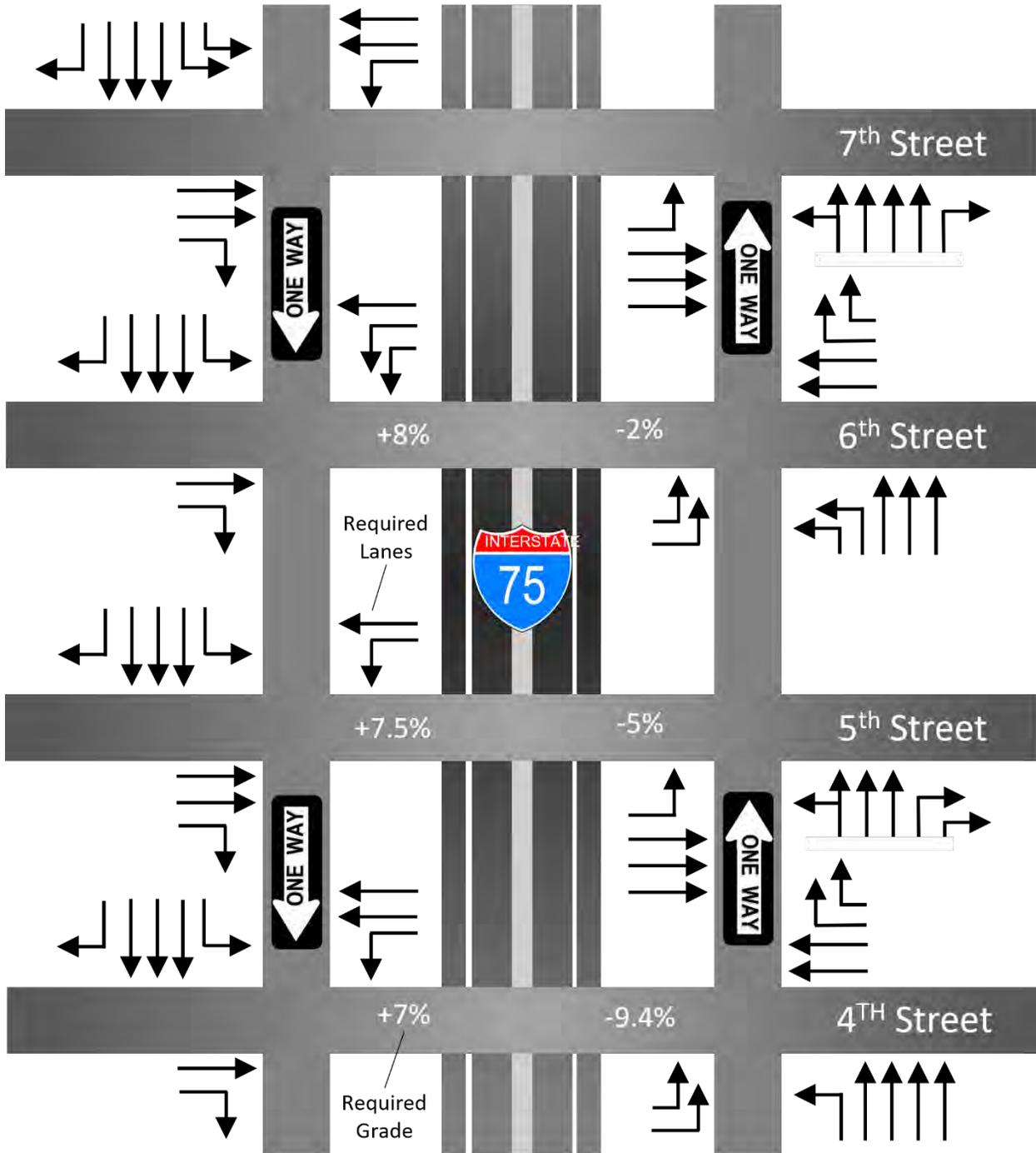
Extended Local Street Grid

The Westway Emails suggest extending the local street grid over I-75 to better connect the Cincinnati Central Business District (CBD) to Queensgate and the West End and to accommodate additional street-facing development. The geometric constraints discussed previously also affect the design of local cross streets. Because the I-75 mainline and access ramps cannot be depressed further, Cincinnati's cross streets must be raised substantially if they are reconfigured in a grid-type configuration that passes over I-75. These changes would require grades ranging from 5 to 9 percent on 4th Street, 5th Street, and 6th Street¹ (see Figure 1 and Attachment 2). ODOT's *Location and Design Manual, Volume 1* lists the maximum grade for urban arterial roadways as 7 percent. However, the manual states that maximum design grades should be used infrequently, rather than a value to be used in most cases. These required grades would not support a “capped” design for the BSB corridor. Furthermore, grades of this magnitude are not only undesirable for vehicular traffic, but they also present substantial mobility and accessibility concerns for cyclists and pedestrians. The Americans with Disabilities Act (ADA) requires facilities that accommodate pedestrians to be constructed with grades of 5 percent or less.

¹ Potential exists to reduce the grades on 6th Street with more extensive engineering study and refinement. However, the constraints on 4th and 5th Street would remain.



Figure 1: Schematic Depiction of Lowered I-75 Concept with Frontage Road Intersections





The Westway Emails maintain that the final width of the BSB Corridor should be no larger than the width of Fort Washington Way. A high-level screening of traffic operations was conducted using available traffic data to determine the minimum number of lanes required to provide acceptable levels of service² for a roadway configuration similar to Fort Washington Way, which includes parallel frontage roads on both sides of the interstate. Based on the screening, the frontage roads would require three to four lanes for through traffic with additional left and right turn lanes at each intersection to eliminate any adverse operations on the freeway system. The local cross streets would also require additional lanes for turning traffic. The minimum lanes required for a configuration similar to Fort Washington Way are depicted schematically in Figure 1.

Project Footprint

When compared to the preferred alternative for the BSB Corridor Project, extending and elevating the downtown Cincinnati cross streets over I-75 would substantially increase the project footprint on the west side of I-75 to tie into the existing elevations near 4th Street. This additional footprint would result in new impacts to existing businesses and potential developable land. For example, the elevated cross streets would create steep slopes that would negatively affect potential development opportunities for urban, street-facing development on 2.5 to 3.0 acres of land east of I-75 that is currently slated to be transferred to the City of Cincinnati once the project is completed.

Since 2012, KYTC and ODOT have been refining the preferred alternative for the BSB Corridor Project to further reduce the project footprint. Measures already incorporated into Preferred Alternative I (Concept I-W) include:

- Optimizing interchange geometry by utilizing the land formerly occupied by the Dunn-Humby building in Cincinnati.
- Allowing the inside and outside shoulder widths on ramps to be flipped to reduce overall width and improve horizontal stopping sight distance.
- Providing narrower inside and outside shoulder widths for I-71/I-75 and the C-D roads according to current design standards.
- Reconfiguring the lanes on the existing BSB and new companion bridge to keep through (interstate) and local (C-D) traffic on separate facilities and reducing the width of the new companion bridge from 172 feet to 107 feet.

² Level of Service (LOS) is a way of describing the amount of traffic congestion on a roadway by “grading” it on a letter scale from A (best) to F (worst). LOS A represents near ideal traffic flow, while LOS F represents a breakdown of traffic flow. LOS D is considered acceptable urban intersections such as those in downtown Cincinnati.



Access Points

The Westway Emails state that interstate entry and exit ramps should be simplified and consolidated while promoting lower speeds through the urban core. KYTC and ODOT recognize that the BSB Corridor Project must serve both long-distance interstate traffic and local trips. As a result, the Preferred Alternative includes a collector-distributor (C-D) system that serves local movements in both Cincinnati and Covington while minimizing access points on the freeway. Any further removal of access points would increase congestion on local streets and could substantially increase travel times for local traffic.

The Westway Email also advocates for a design that reduces intensive, high-speed design requirements in Cincinnati's urban core. All entrance and exit ramps now connect to the freeway through a C-D roadway system. The C-D roads will be lower-speed (45 mph) roadways, which will result in lower speeds on the entrance and exit ramps. The ramp connections with local streets are being designed as lower-speed urban intersections in accordance with City of Cincinnati design standards. In addition, recent refinements to the preferred alternative have lowered the design speed to match the posted speed limit (as opposed to 5 mph above the posted speed).

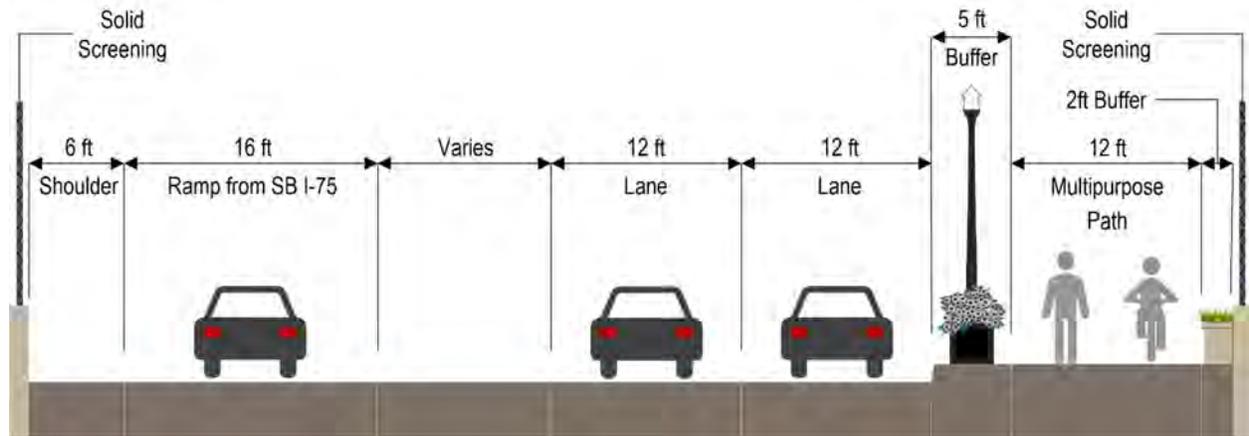
Local Connectivity

The Westway Emails advocate for improving connections across the interstate between local east-west streets and improving north-south connections between Queensgate and the West End. The preferred alternative for the BSB Corridor Project maintains or improves local connectivity for both vehicular and pedestrian/bicycle traffic. In Ohio, a system of C-D roads and ramps provide connections between the interstate highways and 4th Street, 5th Street, 6th Street, 7th Street and other streets in Queensgate, the Riverfront, and the CBD. In Kentucky, the C-D roads and ramps provide connections between I-71/I-75 and West 12th Street, West Pike Street, West 9th Street, West 5th Street, West 4th Street and other streets in Lewisburg, Westside, Mainstrasse, and the Covington CBD. The C-D roads also provide north-south interconnectivity between local streets and cross-river connectivity between Cincinnati and Covington. Furthermore, the east-west connectivity is maintained in both Cincinnati (US 50, West 6th Street, West 7th Street, West 9th Street, Linn Street, Winchell Avenue, Ezzard Charles Drive) and Covington (West 12th Street, West 9th Street, West 5th Street, West 4th Street, West 3rd Street). Attachment 3 includes a color-coded map illustrating the interstate and local roadway connections in the Cincinnati and Covington areas.

Multi-Modal Design

The Westway Emails advocate for local streets that are “designed in a way that is safe and enjoyable for pedestrians and cyclists to navigate and in a way that is supportive of local businesses and conducive to urban development.” KYTC and ODOT fully support these priorities. To that end, the preferred alternative maintains existing sidewalk connections and adds new pedestrian and bicycle shared use paths parallel to and across I-71/I-75. Bike lanes will also be added in some locations (see Attachment 4). The multi-modal features incorporated into the BSB Corridor Project will connect to and enhance existing pedestrian and bicycle facilities in the Cincinnati and Covington areas. In addition, aesthetic guidelines developed for the project will guide the design of multi-modal facilities to provide a network that is not only safe and utilitarian, but enjoyable. Multi-modal facilities will help to create a “sense-of-place” for residents that will be compatible with local, urban land uses. Figure 2 shows an example of the multi-modal facilities that will be incorporated into the design of West 7th Street bridge over I-75 in Cincinnati.

Figure 2: West 7th Street Bridge Over I-75 (Looking East)



Economic Development

KYTC and ODOT have been working collaboratively with local jurisdictions to design the BSB Corridor Project in a way that provides critical transportation infrastructure that is compatible with and supports local land use, zoning, growth, and development plans.

The Westwood Emails articulate goals for expanding the Cincinnati CBD west of I-75, elevating the highest and best use for land, creating new opportunities for economic and housing development, and implementing regional development priorities, among others. These goals are beyond the scope of a transportation project. Local jurisdictions are responsible for developing and approving Master Plans and other similar documents to guide local economic and land use goals. Nonetheless, KYTC and ODOT have worked collaboratively with local jurisdictions to design a project that is compatible with local Master Plans and initiatives and have refined the design in several locations in response to local requests.



Design-Build Process

The Westway Emails advocate for a design-build process that is flexible and encourages innovation. Given the current environment as it relates to project risk, inflationary trends, the need for ongoing engagement of the adjacent impacted communities, and interested third parties, KYTC and ODOT have made the decision to revise the procurement method from Value-Based Fixed-Price Design-Build (VBDB) to Progressive Design-Build (PDB).

Progressive Design-Build will allow the Design-Build Team (DBT), KYTC, and ODOT to collaboratively consider and address major project topics in a more open format which cannot be achieved with a VBDB contracting method.

Progressive Design-Build uses a best-value selection to determine the preferred DBT, followed by a process whereby the owner and DBT “progress” towards a design and contract price. The DBT is selected through a combination of technical qualifications and “competitive bidding elements” without necessarily a fixed price. The DBT collaborates with the owner to create/confirm the basis of design, overall project requirements, and final design intent. Project design decisions are based on cost, schedule, quality, operability, life cycle and other considerations. Alternative technical concepts will be developed and evaluated over the course of the progressive design-build contract. Alternative concepts that are consistent with the decisions made during the project’s environmental process and offer substantial cost savings or additional benefits will be incorporated into the BSB Corridor Project, as appropriate.

Local Agency Coordination

The Westway Emails propose designating the City of Cincinnati and Hamilton County as cooperating agencies for the BSB Corridor Project. Cooperating agencies are defined by federal law and are limited to federal agencies that have jurisdiction by law or special expertise with respect to any environmental impact involved in a proposed project. State and local agencies of similar qualifications can also be designated as cooperating agencies. The BSB project does not have any designated cooperating agencies, although several state and federal agencies have been designated as participating agencies, which are agencies with an interest in the project but a lower degree of authority than a cooperating agency. While not identified as participating agencies, the City of Cincinnati and Hamilton County have been involved in the Project Advisory Committee and Aesthetics Design Committee since the project’s inception. In addition, ODOT regularly coordinates design details directly with these agencies and has refined the design in several locations in response to feedback received. This collaboration will continue through the procurement and construction phases of the project, with the City of Cincinnati being involved in the evaluation of the design-build teams.



Conclusion

KYTC and ODOT share many of the goals articulated in the Westway Emails, including:

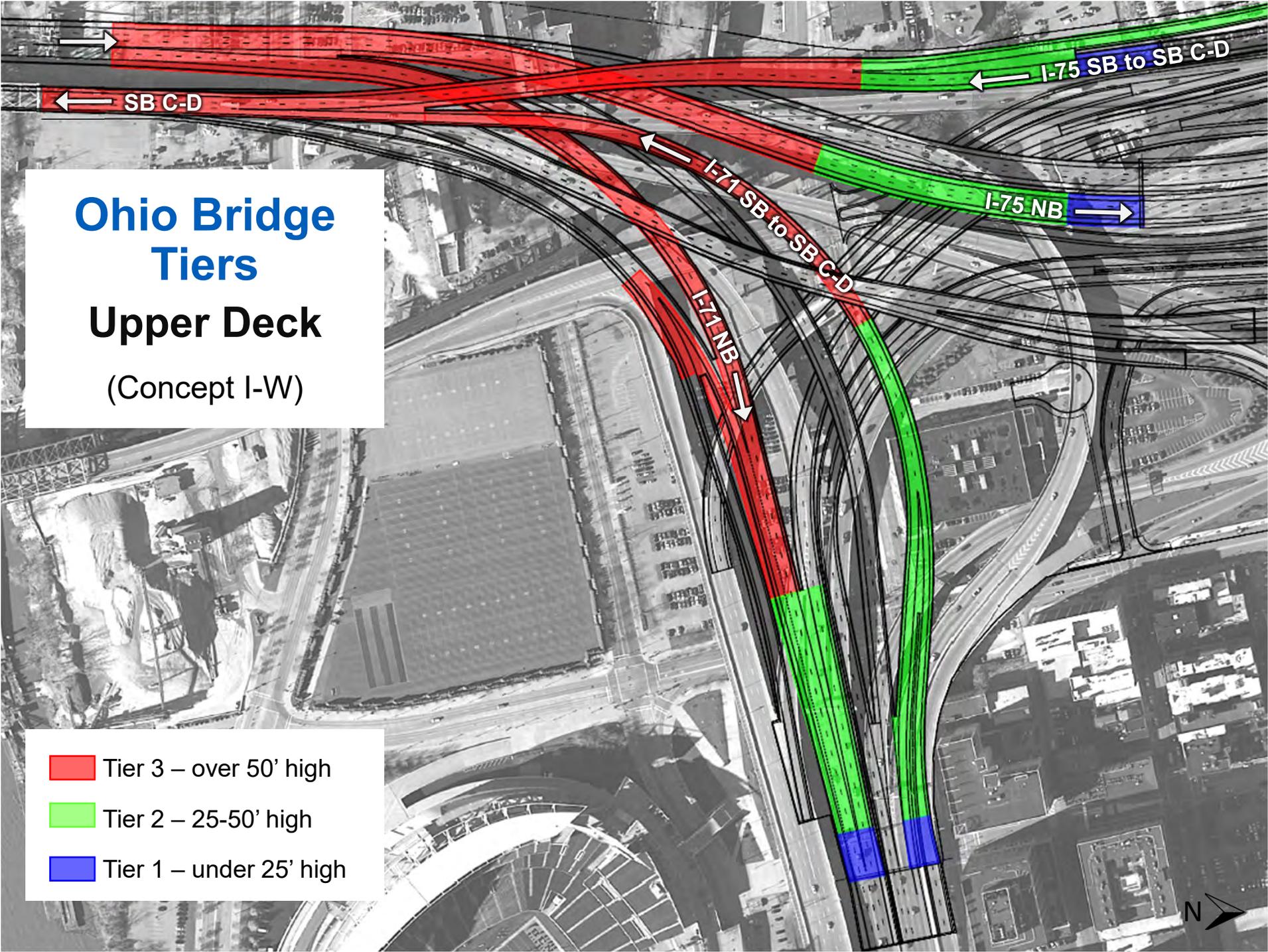
- Minimizing the footprint of the highway;
- Maintaining and improving local access;
- Providing a network of safe, multi-modal streets for local traffic;
- Providing transportation infrastructure that supports local development goals and initiatives; and
- Engaging in a design-build process that provides flexibility and opportunities to maximize benefits and minimize costs.

To accomplish those goals, KYTC and ODOT have developed a set of refinements to the preferred alternative since the approval of the project's Environmental Assessment (EA) and Finding of No Significant Impact (FONSI) in 2012. These refinements reconfigured the river crossing to use the existing BSB for local traffic and a new double decker companion bridge to the west for through (interstate) traffic. In addition, performance-based design principles have been incorporated into the project, substantially reducing the project's footprint and associated impacts. Multi-modal facilities have been incorporated into the project, and KYTC and ODOT are continuing to coordinate the project with the cities of Cincinnati and Covington to address local concerns while further reducing the highway's footprint and impacts to the communities in the project area.

While the specific concepts presented in the Westway Emails are not feasible and, as such, will not be considered in the project's Supplemental Environmental Assessment, KYTC and ODOT will continue to incorporate the overriding principles championed in the Westway Emails as the BSB Corridor Project moves toward implementation.



Attachment 1: BSB Corridor Project Tiered Bridge System

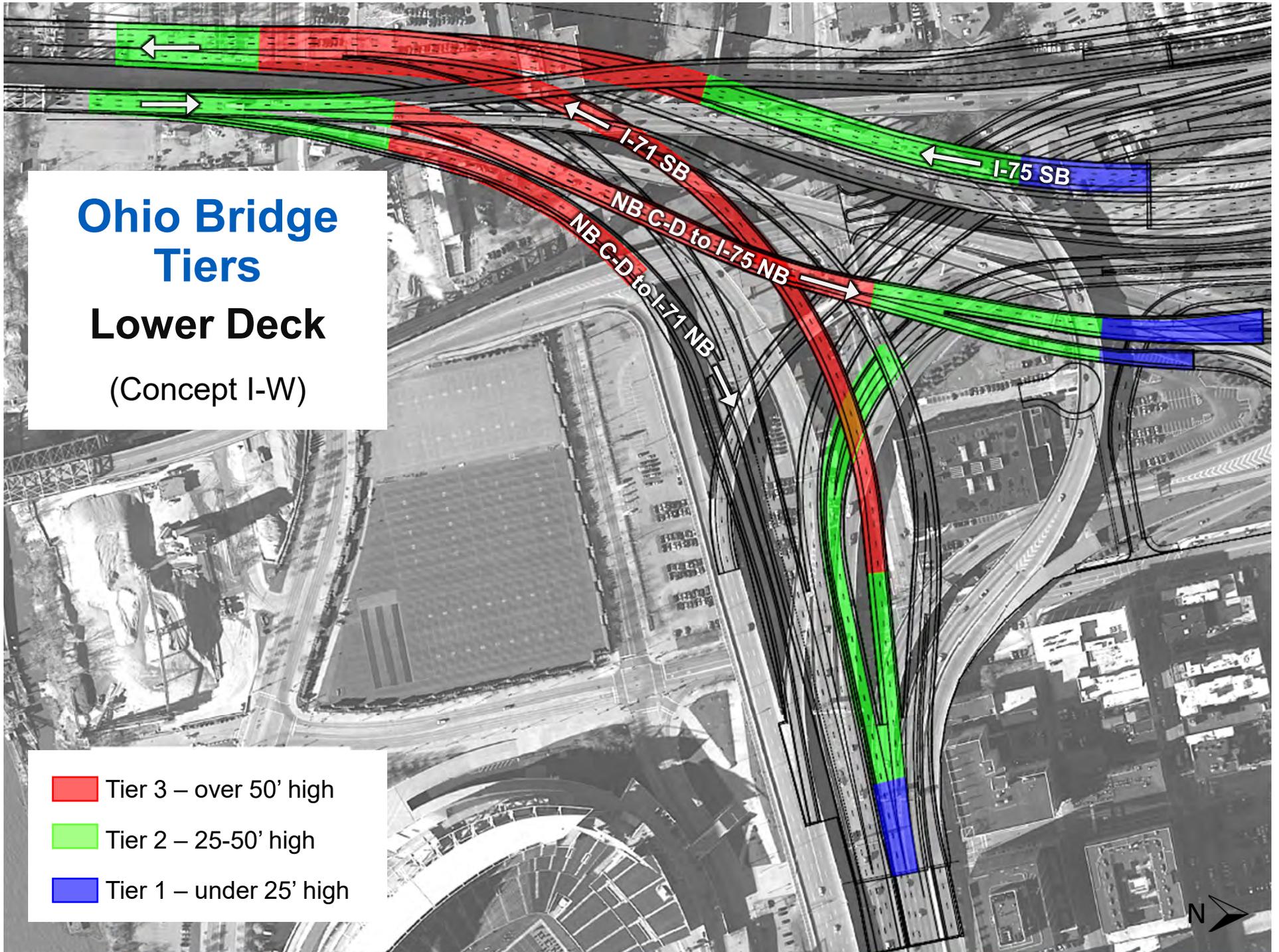


Ohio Bridge Tiers

Lower Deck

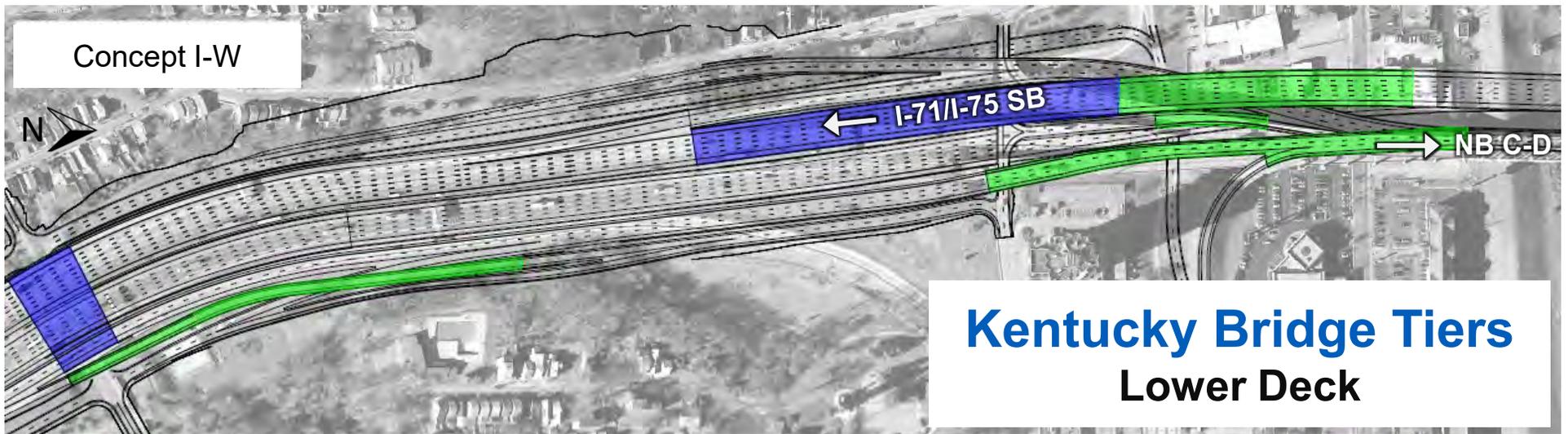
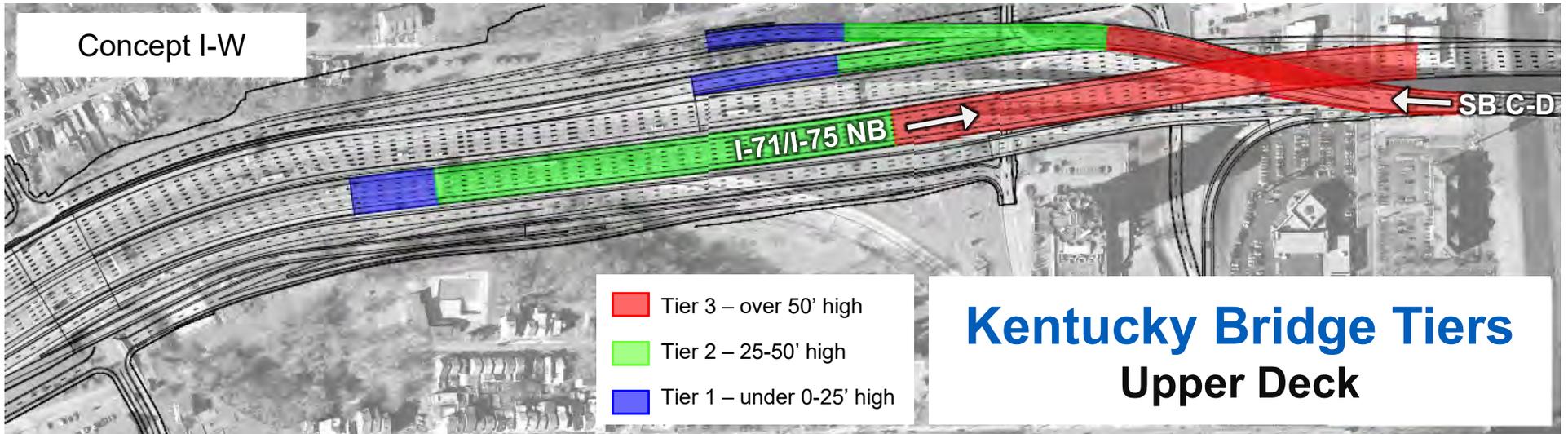
(Concept I-W)

-  Tier 3 – over 50' high
-  Tier 2 – 25-50' high
-  Tier 1 – under 25' high





Project Overview





Attachment 2: BSB Corridor Project Cross Sectional Views

*BSB - CONCEPT I-W
XS CUTS IN PLAN VIEW*

CENTERLINE XS

6TH ST

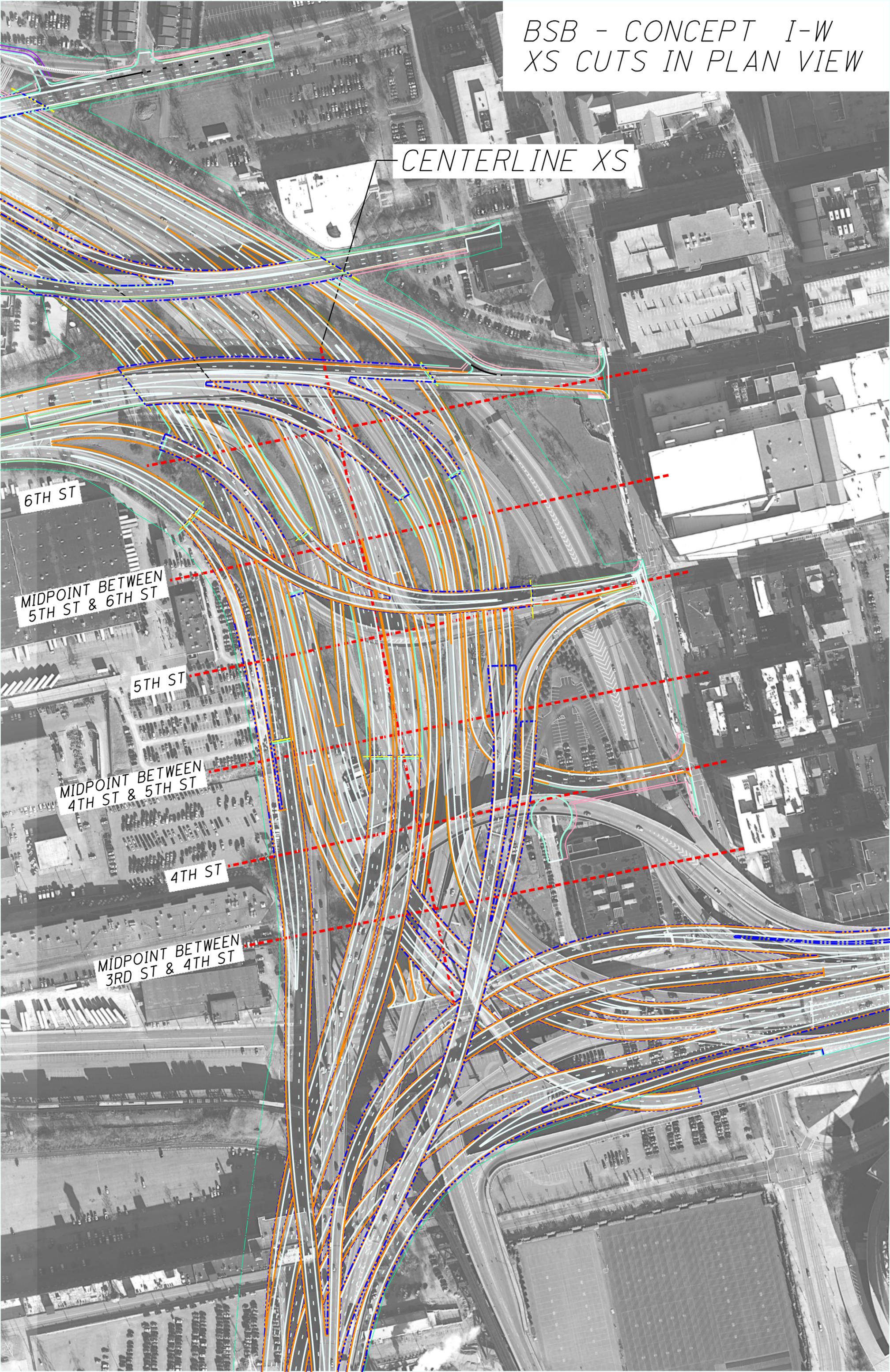
MIDPOINT BETWEEN
5TH ST & 6TH ST

5TH ST

MIDPOINT BETWEEN
4TH ST & 5TH ST

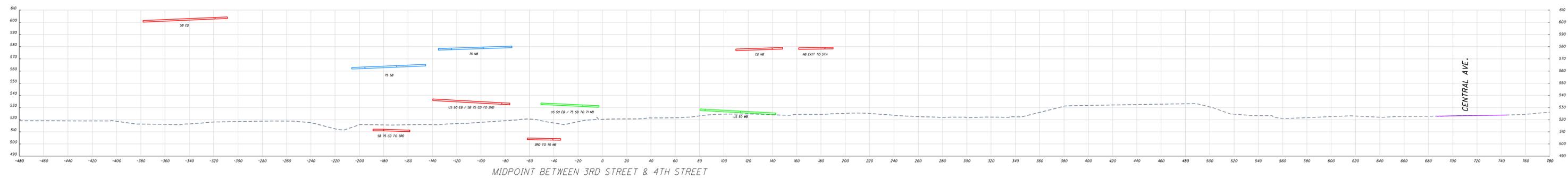
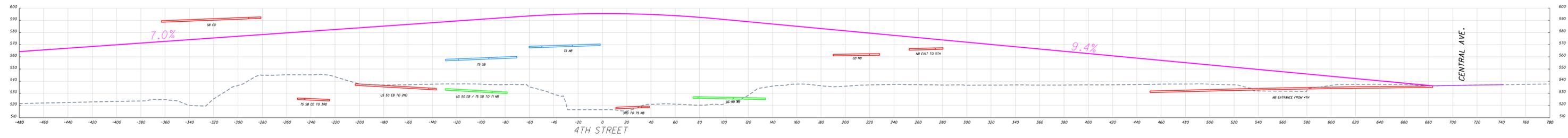
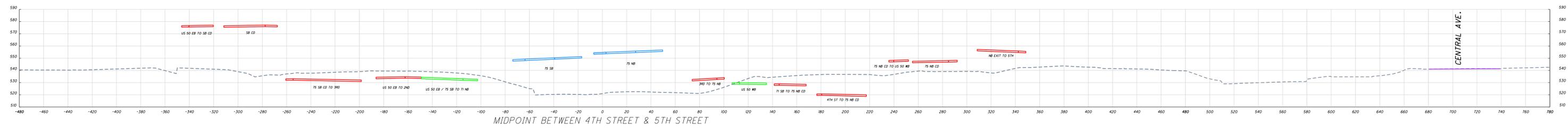
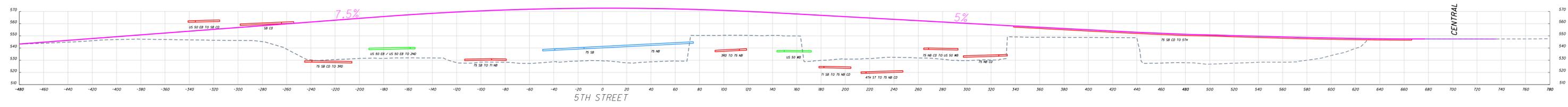
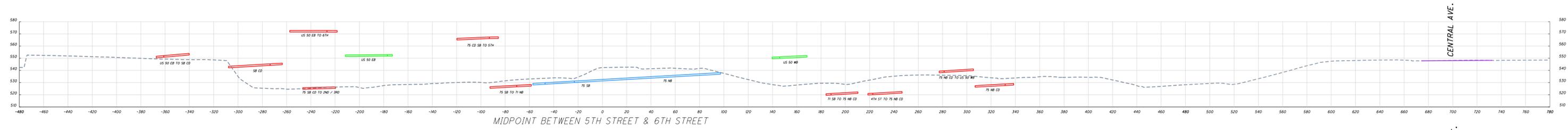
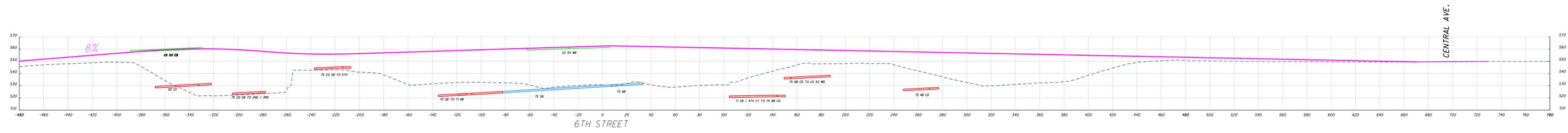
4TH ST

MIDPOINT BETWEEN
3RD ST & 4TH ST



- INTERSTATE
- CD / RAMPS
- US 50
- LOCAL ROADS

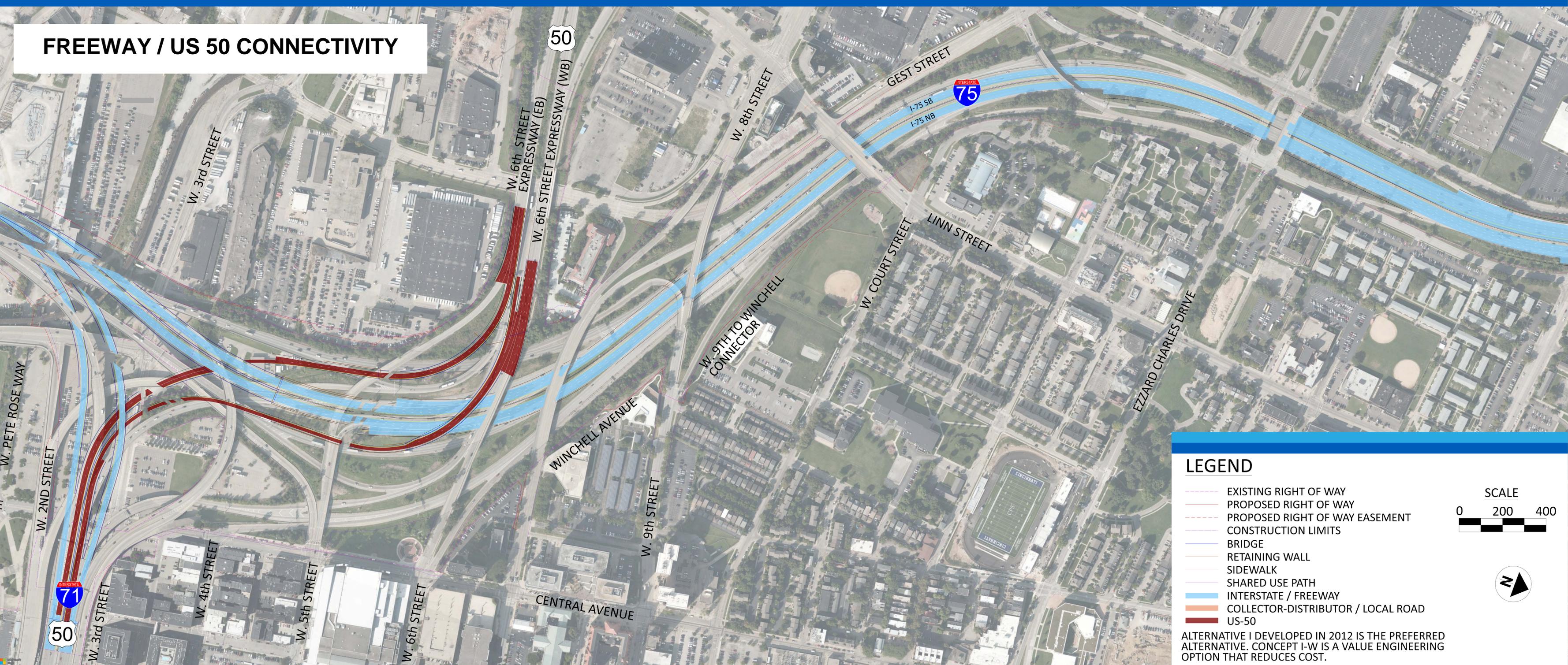
BSB CONCEPT I-W CROSS SECTIONS 4TH ST TO 6TH ST CINCINNATI





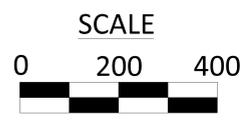
Attachment 3: BSB Corridor Project Interstate and Local Continuity

FREEWAY / US 50 CONNECTIVITY



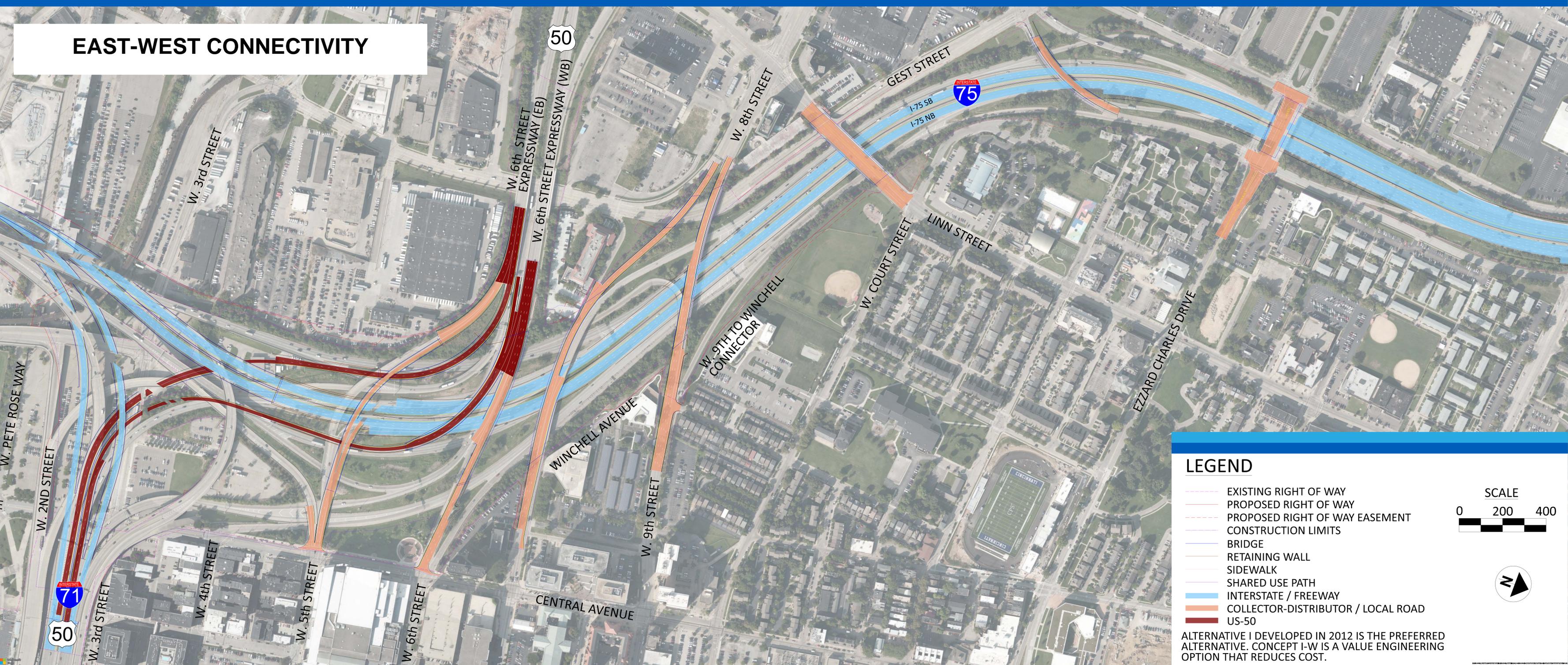
LEGEND

-  EXISTING RIGHT OF WAY
-  PROPOSED RIGHT OF WAY
-  PROPOSED RIGHT OF WAY EASEMENT
-  CONSTRUCTION LIMITS
-  BRIDGE
-  RETAINING WALL
-  SIDEWALK
-  SHARED USE PATH
-  INTERSTATE / FREEWAY
-  COLLECTOR-DISTRIBUTOR / LOCAL ROAD
-  US-50



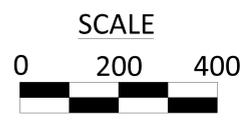
ALTERNATIVE I DEVELOPED IN 2012 IS THE PREFERRED ALTERNATIVE. CONCEPT I-W IS A VALUE ENGINEERING OPTION THAT REDUCES COST.

EAST-WEST CONNECTIVITY



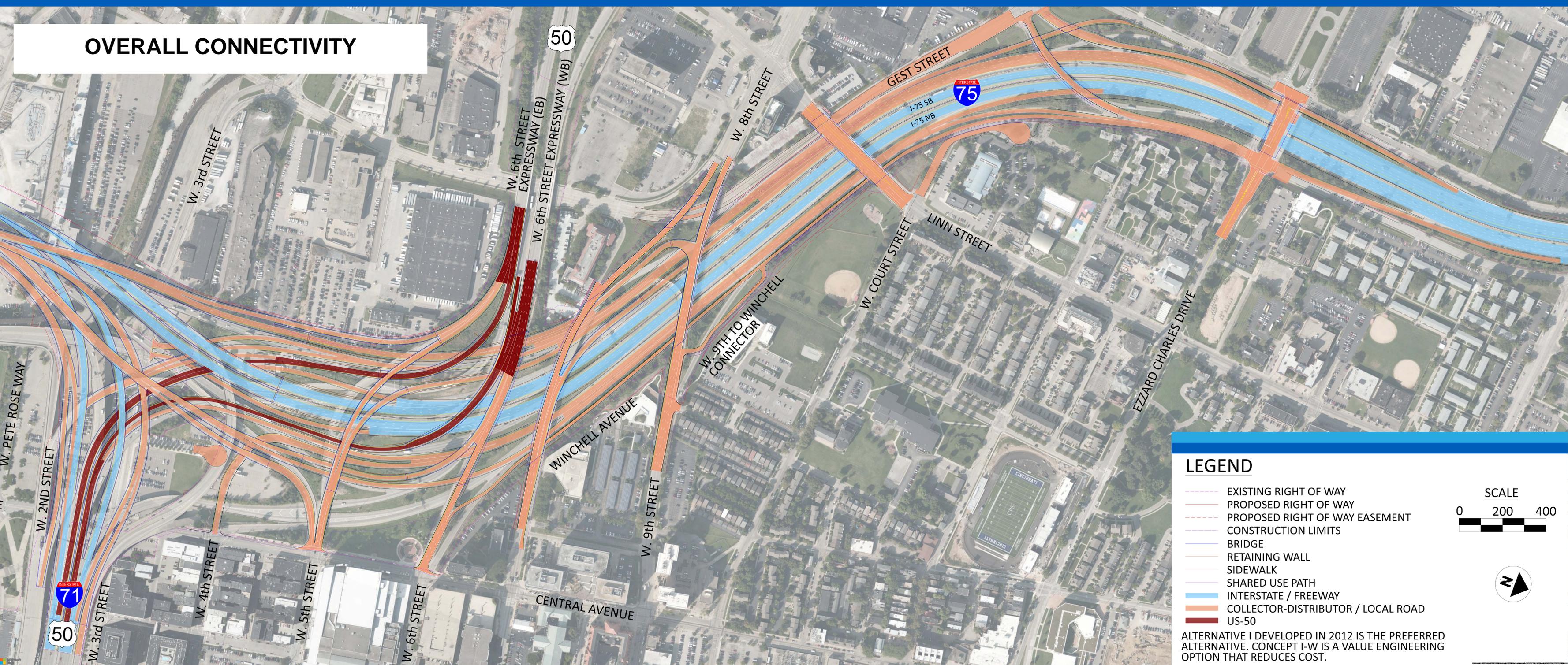
LEGEND

-  EXISTING RIGHT OF WAY
-  PROPOSED RIGHT OF WAY
-  PROPOSED RIGHT OF WAY EASEMENT
-  CONSTRUCTION LIMITS
-  BRIDGE
-  RETAINING WALL
-  SIDEWALK
-  SHARED USE PATH
-  INTERSTATE / FREEWAY
-  COLLECTOR-DISTRIBUTOR / LOCAL ROAD
-  US-50



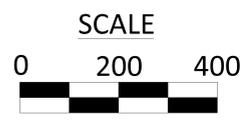
ALTERNATIVE I DEVELOPED IN 2012 IS THE PREFERRED ALTERNATIVE. CONCEPT I-W IS A VALUE ENGINEERING OPTION THAT REDUCES COST.

OVERALL CONNECTIVITY



LEGEND

-  EXISTING RIGHT OF WAY
-  PROPOSED RIGHT OF WAY
-  PROPOSED RIGHT OF WAY EASEMENT
-  CONSTRUCTION LIMITS
-  BRIDGE
-  RETAINING WALL
-  SIDEWALK
-  SHARED USE PATH
-  INTERSTATE / FREEWAY
-  COLLECTOR-DISTRIBUTOR / LOCAL ROAD
-  US-50



ALTERNATIVE I DEVELOPED IN 2012 IS THE PREFERRED ALTERNATIVE. CONCEPT I-W IS A VALUE ENGINEERING OPTION THAT REDUCES COST.



Attachment 4: BSB Corridor Project Multi-Modal Facilities

Legend

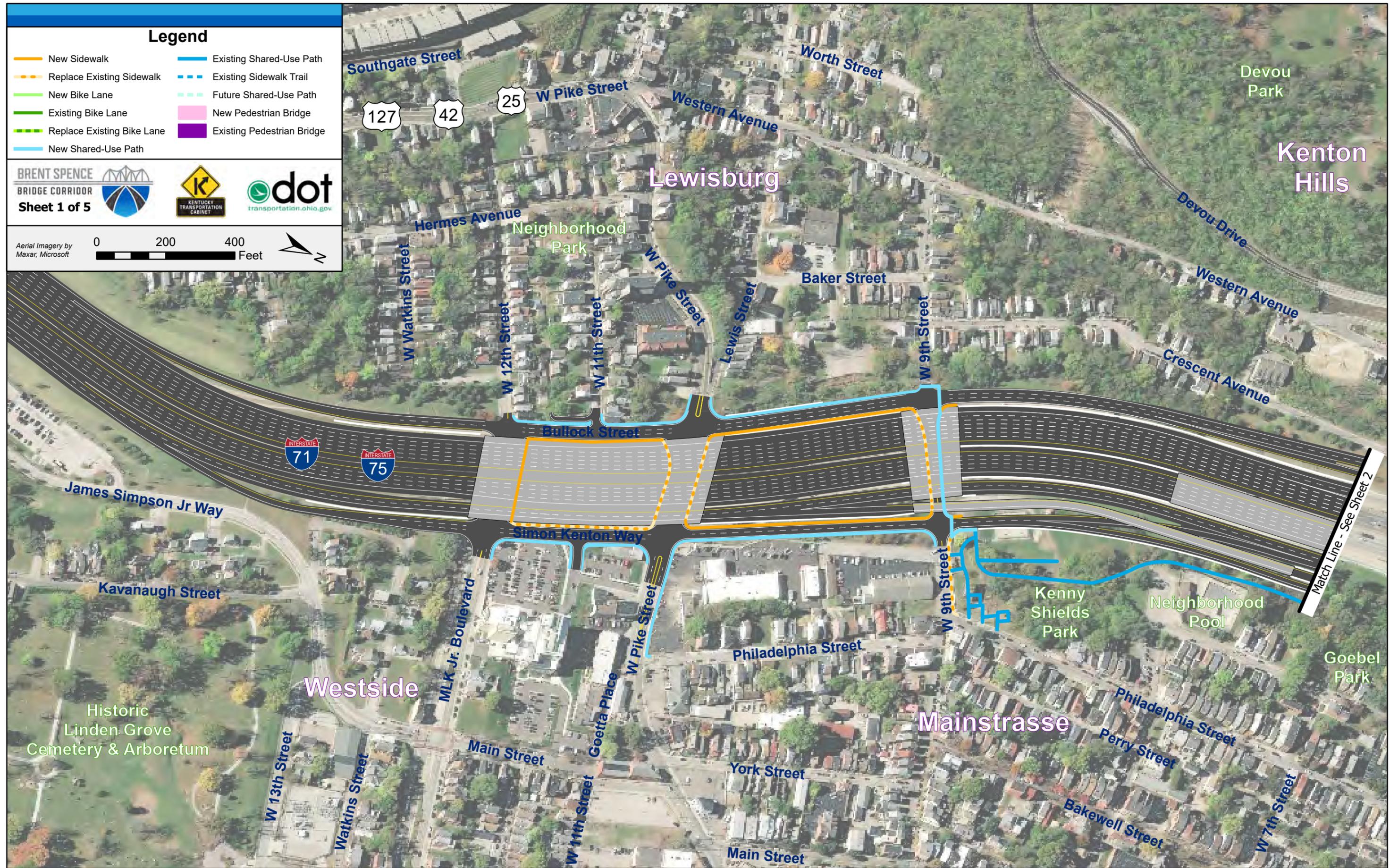
- New Sidewalk
- - - Replace Existing Sidewalk
- New Bike Lane
- - - Existing Bike Lane
- - - Replace Existing Bike Lane
- New Shared-Use Path
- Existing Shared-Use Path
- - - Existing Sidewalk Trail
- - - Future Shared-Use Path
- New Pedestrian Bridge
- Existing Pedestrian Bridge





Sheet 1 of 5

Aerial Imagery by Maxar, Microsoft
 0 200 400 Feet
 



Legend

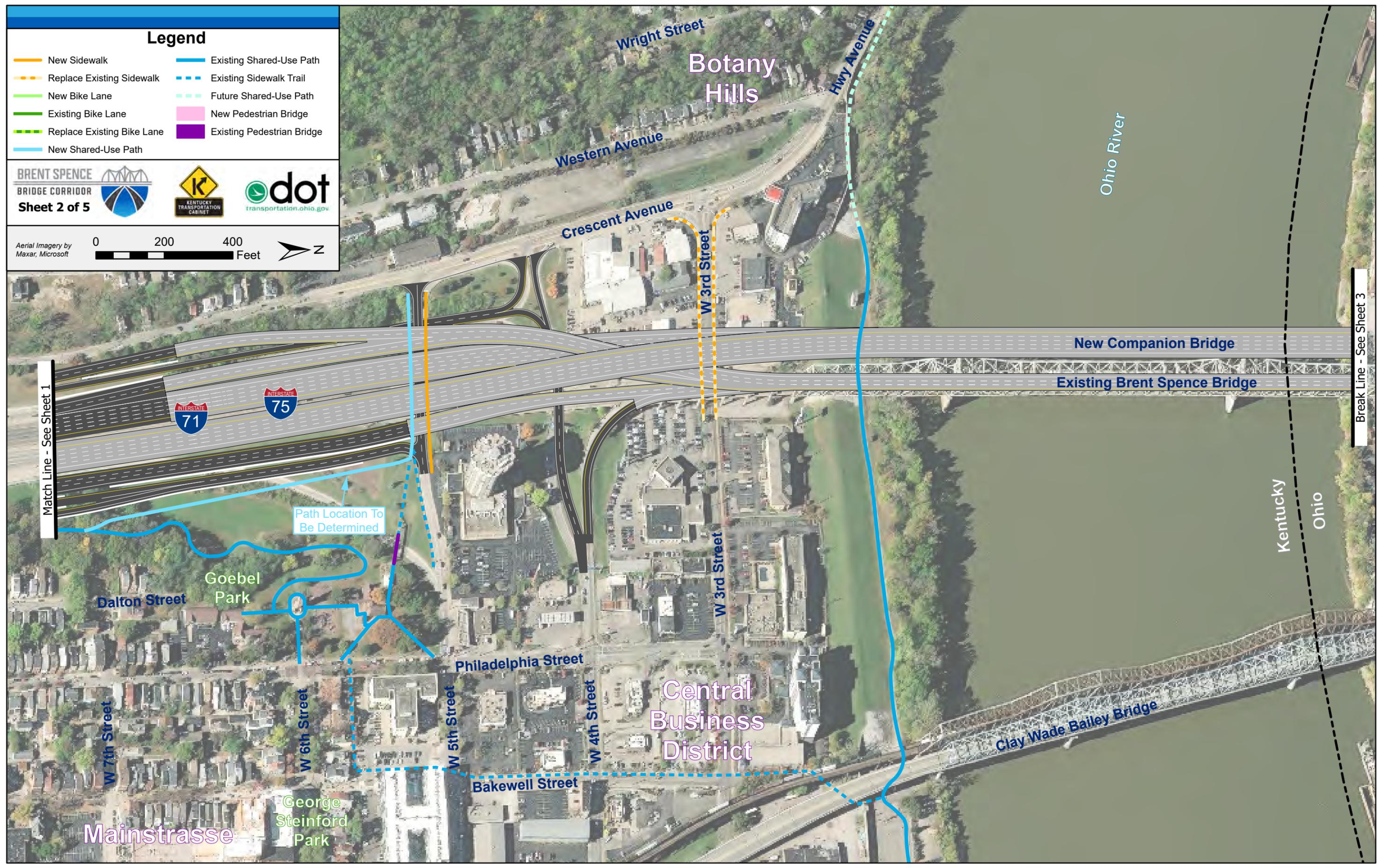
- New Sidewalk
- Replace Existing Sidewalk
- New Bike Lane
- Existing Bike Lane
- Replace Existing Bike Lane
- New Shared-Use Path
- Existing Shared-Use Path
- Existing Sidewalk Trail
- Future Shared-Use Path
- New Pedestrian Bridge
- Existing Pedestrian Bridge

BRENT SPENCE BRIDGE CORRIDOR
Sheet 2 of 5

transportation.ohio.gov

Aerial Imagery by Maxar, Microsoft

0 200 400 Feet



Match Line - See Sheet 1

Break Line - See Sheet 3

Path Location To Be Determined

Legend

- New Sidewalk
- - - Replace Existing Sidewalk
- New Bike Lane
- - - Existing Bike Lane
- - - Replace Existing Bike Lane
- New Shared-Use Path
- Existing Shared-Use Path
- - - Existing Sidewalk Trail
- - - Future Shared-Use Path
- New Pedestrian Bridge
- Existing Pedestrian Bridge





Sheet 3 of 5

Aerial Imagery by Maxar, Microsoft

0 200 400 Feet




Break Line - See Sheet 2

Match Line - See Sheet 4

Clay Wade Bailey Bridge

W Pete Rose Way

W 2nd Street

W 3rd Street

W 4th Street

W 5th Street

W 6th Street

W 7th Street

W 9th Street

W 8th Street

6th Street Expressway

Central Avenue

Gest Street

Queensgate

CBD Riverfront

West End

Firefighters Memorial

Paul Brown Stadium

27

52

71

75

71

75

52

Legend

- New Sidewalk
- - - Replace Existing Sidewalk
- New Bike Lane
- - - Existing Bike Lane
- · - · - Replace Existing Bike Lane
- New Shared-Use Path
- Existing Shared-Use Path
- - - Existing Sidewalk Trail
- · - · - Future Shared-Use Path
- New Pedestrian Bridge
- Existing Pedestrian Bridge

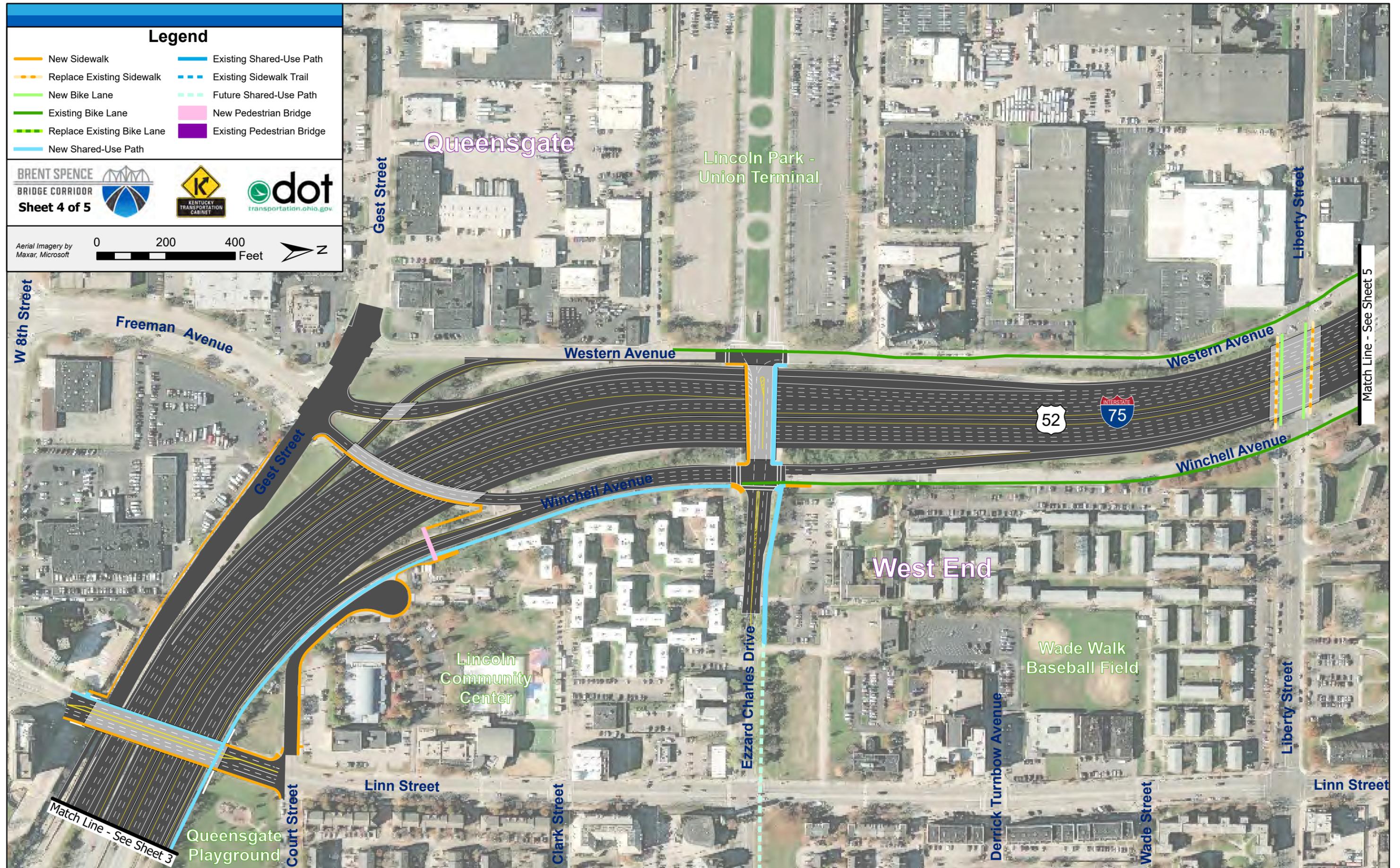




Sheet 4 of 5

Aerial Imagery by Maxar, Microsoft

0 200 400 Feet

Match Line - See Sheet 5

Match Line - See Sheet 3

Legend

- New Sidewalk
- - - Replace Existing Sidewalk
- New Bike Lane
- - - Existing Bike Lane
- - - Replace Existing Bike Lane
- New Shared-Use Path
- Existing Shared-Use Path
- - - Existing Sidewalk Trail
- - - Future Shared-Use Path
- New Pedestrian Bridge
- Existing Pedestrian Bridge





Aerial Imagery by Maxar, Microsoft
 0 200 400 Feet 



Match Line - See Sheet 4

Brent Spence Bridge Corridor Project Correspondence

From: Ryan Laber <info@sg.actionnetwork.org>
Sent: Friday, August 5, 2022 11:47 PM
To: Spinosa, Stefan
Subject: Brent Spence Bridge Project - Design Improvements

ODOT Capital Programs Administrator Stefan Spinosa,

I am thrilled that the Brent Spence Bridge Corridor Project (“the Project”) is gaining momentum, as a result of the recently passed Bipartisan Infrastructure Law (BIL). Earnestly, thank you for your leadership to date in helping our community reach this point. However, given the massive sum of federal and state funding being committed to the Cincinnati region for this Project, and given the lengthy lifespan of the physical infrastructure to be constructed, I am advocating for design improvements to the Project, so that the Cincinnati region receives the best possible outcome.

Before us stands a once-in-a-generation opportunity to shape the Cincinnati region’s relationship to its infrastructure. Unfortunately, the currently proposed Project design – Alternative I-W – which was substantially developed in 2012 and 2013, does not provide the best possible life-cycle value to local, public owners and end users of the Project. However, a Project design that reduces the interstate’s footprint and that improves local community connectivity between the east and west sides of I-75 would provide huge economic benefits to the Cincinnati region, thereby maximizing return on investment in this Project. The Cincinnati region should not miss out on this tremendous opportunity to advance its long-term priorities.

While I appreciate that the Project’s current design has a smaller right-of-way footprint than previous designs, I believe that additional, significant right-of-way reduction is crucial to the future of Cincinnati. With a landlocked urban core, Cincinnati’s urban growth is currently constrained, and the Cincinnati region’s ability to develop urban-core real estate for those who desire it – including businesses and top talent – is limited. Improved connectivity between the east and west sides of I-75 in Cincinnati is also crucial. I believe a design solution that not only reduces the right-of-way footprint, but that also includes a recessed highway and an overpassing local street grid, like that of ODOT’s celebrated Fort Washington Way project, would represent a better alternative than the current design and would deliver

important priorities for the community: the expansion of Cincinnati's central business district to the west of I-75, the elevation of the highest and best use for land, and the creation of new opportunities for economic development and housing development. Any design solution that is implemented should deliver these priorities and facilitate a new/improved network of safe, multi-modal local streets connecting the Cincinnati CBD, Queensgate, the West End, and Covington.

Specifically, I advocate for the alignment of the interstate through the urban core, from West 3rd Street to Ezzard Charles Drive, to be lowered and consolidated into a trench. This trench should have as small a width as possible. The local street grid should remain at-grade and extend over the interstate. This concept borrows from the successful urban interstate upgrades that have been ongoing in Columbus, Ohio, for years. Connections should be made across the interstate between as many local east-west streets as possible, but especially between West 4th, 5th, 6th, and 7th Streets. Improved connections should also be made north-south between Queensgate and the West End. Local streets extending over the interstate should be designed in a way that is safe and enjoyable for pedestrians and cyclists to navigate, and in a way that is supportive of local businesses and conducive to urban development. A good example of this is the completed East Long Street bridge crossing I-71 in Columbus, Ohio. With respect to land recapture thanks to Project footprint reduction, enough space should be returned to local control to facilitate the development of long-awaited regional priorities: an expanded convention center and a new arena. Optimally, the interstate would be capped for as much road-length as possible between West 3rd Street and Ezzard Charles Drive. Interstate entry and exit ramps in the urban core should be simplified and consolidated, thereby reducing intensive, high-speed design requirements. Traffic forecasting assumptions used to drive the Project's design should be validated by actual, historic traffic counts and trends. Design input data should be based on a hybrid of econometric and network-based models built to consider individual trips that shift to other corridors, occur at a different time of day, involve a different mode of transportation, or disappear due to telecommuting or a shifted trip. When the specifics of these priorities cannot be implemented, the spirit of them should be.

The Project priorities for which I am advocating are extremely consistent with USDOT's stated priorities for its INFRA and Mega grant programs, which are likely to fund the Project.

The outcomes and design elements advocated for in this letter should be squarely within the scope of the Project, as made clear by the following passages in the grant programs' Notice of Funding Opportunity (NOFO):

- “Support integrated land use, economic development and transportation planning to improve the movement of people ... facilitate greater public and private investments and strategies in land-use productivity, ... [and] increase in the production or preservation of location-efficient housing”
- “Reduce transportation and housing cost burdens, including through public and private investments to support greater commercial and mixed-income residential development near public transportation ... in walkable neighborhoods”
- “Increase the walkability and accessibility for pedestrians and encourage thriving communities for individuals to work, live, and play by creating transportation choices for individuals to move freely with or without a car”
- “Improved multimodal and transit access across previously bifurcated disadvantaged neighborhoods”
- “Include[] physical-barrier-mitigating land bridges, caps, lids, linear parks, and multimodal mobility investments that either redress past barriers to opportunity or that proactively create new connections and opportunities for underserved communities that are underserved by transportation”
- “Include[] new or improved walking and bicycling infrastructure [and] reduce[] automobile dependence”
- “Address equity and barriers to opportunity, improve quality of life in ... urbanized areas, and benefit Historically Disadvantaged Communities”

o Note: Hamilton County, Ohio, Census Tracts 2 and 263 – both within the Project footprint, with one on each side of I-75 – are considered Transportation Disadvantaged, and furthermore Tract 2 is considered a Historically Disadvantaged Community by USDOT.

Although ODOT selected a Preferred Alternative design in 2011, upgrades to this design are achievable before the Project's targeted groundbreaking in late 2023. As local business, community, and political leaders, we expect flexibility from ODOT in designing the procurement process. ODOT should adopt a custom, alternative design-build process that is responsive to the community priorities established here. Such a procurement should be

performance- and value-based rather than prescriptive, in the sense that the designer/contracting community should be proactively incentivized to provide creative alternative design concepts responsive to community goals. Importantly, in this value- and performance-based procurement, community priorities must be sufficiently scored and weighted so that they are critical aspects of the design and selection process.

Representative, objective metrics that could be used to evaluate alternative designs in a competitive procurement include: maximization of the amount of returned, contiguous land in Cincinnati's urban core, maximization of street frontage that is walkable and conducive to urban, street-facing development, and minimization of walking distance between the Cincinnati CBD and Queensgate. Also, aspects of progressive design-build procurement should be utilized, to allow for innovative design improvements throughout the course of the project, without delaying Project schedule.

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I pledge to support ODOT and the entire Cincinnati region in shaping the project procurement process and ultimate end-product of this Project to all parties' best interests.

Ryan Laber

rmlaber@gmail.com

391 Oak Street #3A

Columbus, Ohio 43215

CAUTION: This is an external email and may not be safe. If the email looks suspicious, please do not click links or open attachments and forward the email to csc@ohio.gov or click the Phish Alert Button if available.

From: Spinosa, Stefan
Sent: Wednesday, August 10, 2022 6:52 AM
To: rmlaber@gmail.com
Subject: RE: Brent Spence Bridge Project - Design Improvements

Mr. Laber,

Thank you for your email and comments on the Brent Spence Bridge project. First, I want to ensure you that your comments below will be included in our project record.

We continue to receive and address public comments on our project, and as is our practice, will be addressing them as part of our public comment summary documentation that is being developed as part of the environmental re-evaluation process.

As you and I have communicated in the past, we are aware of the comments to include concepts described in your email: lower interstate alignment, reduce width of footprint, connect streets over the interstate, improve pedestrian and cyclist accommodations, land recapture, etc. This has also been brought to our attention as a "Fort Washington Way" concept by others in the community.

We share your desire to utilize a procurement process that allows for both performance and value-based selection as well as allowing opportunities for the development of alternative technical concepts to continue to improve the project. Our agency will continue to work with our partners in Kentucky and FHWA to deliver the project in the best manner to address risks, provide innovation, and reduce project costs.

ODOT has a strong working partnership with the City on the Brent Spence Bridge project. Throughout the project development, City staff has engaged and supported our efforts by providing feedback and guidance with respects to the project and specifically how it impacts their transportation network. Our goal is to continue this cooperation and seek out City input throughout further development and construction. We continue to encourage the City to provide combined comments to us on how we can develop the project to meet the City's current and future goals for their community.

With respects to establishment of Cooperating Agencies, a cooperating agency is essentially an entity that has jurisdiction by law or special expertise with respect to any environmental impact in a proposed project. A good example is the US Coast Guard, as a federal agency, has approval authority by law for the height and span of the bridge over the Ohio River. Though the City and County do not meet this definition, we plan to work closely with them as discussed previously.

Again, thank you for your comments on the project and the support you offered.

Stefan C. Spinosa, P.E.

Brent Spence Bridge Corridor Project Manager

ODOT District 8 Capital Programs

505 South SR 741, Lebanon, Ohio 45036

513.933.6639

transportation.ohio.gov

From: Amanda Lorman <info@sg.actionnetwork.org>
Sent: Friday, August 12, 2022 8:53 PM
To: Spinosa, Stefan
Subject: Brent Spence Bridge Project - Reconnecting Cincinnati Westway Design Improvements

ODOT Capital Programs Administrator Stefan Spinosa,

I am thrilled that the Brent Spence Bridge Corridor Project (“the Project”) is gaining momentum, as a result of the recently passed Bipartisan Infrastructure Law. Earnestly, thank you for your leadership to date in helping our community reach this point. However, given the massive sum of federal and state funding being committed to the Cincinnati region for this Project, and given the lengthy lifespan of the physical infrastructure to be constructed, I am advocating for design improvements to the Project, so that the Cincinnati region receives the best possible outcome.

Before us stands a once-in-a-generation opportunity to shape the Cincinnati region’s relationship to its infrastructure. Unfortunately, the currently proposed Project design – Alternative I-W – which was substantially developed in 2012 and 2013, does not provide the best possible life-cycle value to local, public owners and end users of the Project. However, a Project design that reduces the interstate’s footprint and that improves local community connectivity between the east and west sides of I-75 would provide huge economic benefits to the Cincinnati region, thereby maximizing return on investment in this Project. The Cincinnati region should not miss out on this tremendous opportunity to advance its long-term urban and economic development priorities.

Generally, I advocate that the design of the Project be recalibrated in a way that:

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- Extends the local street grid over the interstate in order to better connect the Cincinnati

CBD to Queensgate and the West End via multiple modes, and in order to accommodate local, urban, street-facing development

While I appreciate that the Project's current design has a smaller right-of-way footprint than previous designs, I believe that additional, significant right-of-way reduction is crucial to the future of Cincinnati. With a landlocked urban core, Cincinnati's urban growth is currently constrained, and the Cincinnati region's ability to develop urban-core real estate for those who desire it – including businesses and top talent – is limited. Improved connectivity between the east and west sides of I-75 in Cincinnati is also crucial. I believe a design solution that not only reduces the right-of-way footprint, but that also includes a recessed highway and an overpassing local street grid, like that of ODOT's celebrated Fort Washington Way project, is just one example of a configuration that could deliver important priorities for the community: the expansion of Cincinnati's central business district to the west of I-75, the elevation of the highest and best use for land, and the creation of new opportunities for economic development and housing development. Any design solution that is implemented should deliver these priorities and facilitate a new/improved network of safe, multi-modal local streets connecting the Cincinnati CBD, Queensgate, the West End, and Covington.

Specific design recommendations capturing the spirit of community priorities:

Specifically, I advocate for the alignment of the interstate through the urban core, from West 3rd Street to Ezzard Charles Drive, to be lowered and consolidated in a way that minimizes right-of-way width, thereby creating space for development and reducing environmental impacts of the interstate on the urban core. The final right-of-way width should be as small as possible, but no larger than the width of Fort Washington Way—about 155 feet. The local street grid should remain at-grade and extend over the interstate, improving community connectivity between currently bifurcated neighborhoods. This concept borrows from the successful urban interstate upgrades that have been ongoing in Columbus, Ohio, for years. Connections should be made across the interstate between as many local east-west streets as possible, but especially between West 4th, 5th, 6th, and 7th Streets. Improved connections should also be made north-south between Queensgate and the West End. Local streets extending over the interstate should be designed in a way that is safe and enjoyable for pedestrians and cyclists to navigate, and in a way that is supportive of local businesses

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The Project priorities for which I am advocating are extremely consistent with USDOT's stated priorities for its INFRA and Mega grant programs, which are likely to fund the Project. The outcomes and design elements advocated for in this letter should be squarely within the scope of the Project, as made clear by the following passages in the grant programs' Notice of Funding Opportunity (NOFO):

- "Support integrated land use, economic development and transportation planning to improve the movement of people ... facilitate greater public and private investments and strategies in land-use productivity, ... [and] increase in the production or preservation of location-efficient housing"
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Local stakeholders – especially the City of Cincinnati – should have power throughout the procurement and progressive design-build decision-making process:

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Thank you for your attention. I pledge to support ODOT and the entire Cincinnati region in shaping the Project procurement process and ultimate end-product of this Project to all parties' best interests.

Very Respectfully,

Amanda Lorman

Amanda Lorman
lormanamanda813@gmail.com
1951 Knob Ct
Cincinnati, Ohio 45225

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From: Rakshit Chandrasaha <info@sg.actionnetwork.org>
Sent: Sunday, August 14, 2022 9:27 PM
To: Spinosa, Stefan
Subject: Brent Spence Bridge Project - Reconnecting Cincinnati Westway Design Improvements

ODOT Capital Programs Administrator Stefan Spinosa,

I am thrilled that the Brent Spence Bridge Corridor Project (“the Project”) is gaining momentum, as a result of the recently passed Bipartisan Infrastructure Law. Earnestly, thank you for your leadership to date in helping our community reach this point. However, given the massive sum of federal and state funding being committed to the Cincinnati region for this Project, and given the lengthy lifespan of the physical infrastructure to be constructed, I am advocating for design improvements to the Project, so that the Cincinnati region receives the best possible outcome.

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Rakshit Chandrasana
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802 Autumn Ln
Mason, Ohio 45040

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From: Chris Colley <info@sg.actionnetwork.org>
Sent: Sunday, August 14, 2022 3:17 PM
To: Spinosa, Stefan
Subject: Brent Spence Bridge Project - Reconnecting Cincinnati Westway Design Improvements

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Thank you for your attention. I pledge to support ODOT and the entire Cincinnati region in shaping the Project procurement process and ultimate end-product of this Project to all parties' best interests.

Very Respectfully,

Chris Colley, concerned citizen

Chris Colley
cwcolley@gmail.com
3441 Hudson Ave
Cincinnati, Ohio 45207

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From: TJ Jones <info@sg.actionnetwork.org>
Sent: Sunday, August 14, 2022 3:52 PM
To: Spinosa, Stefan
Subject: Brent Spence Bridge Project - Reconnecting Cincinnati Westway Design Improvements

ODOT Capital Programs Administrator Stefan Spinosa,

I am thrilled that the Brent Spence Bridge Corridor Project (“the Project”) is gaining momentum, as a result of the recently passed Bipartisan Infrastructure Law. Earnestly, thank you for your leadership to date in helping our community reach this point. However, given the massive sum of federal and state funding being committed to the Cincinnati region for this Project, and given the lengthy lifespan of the physical infrastructure to be constructed, I am advocating for design improvements to the Project, so that the Cincinnati region receives the best possible outcome.

Before us stands a once-in-a-generation opportunity to shape the Cincinnati region’s relationship to its infrastructure. Unfortunately, the currently proposed Project design – Alternative I-W – which was substantially developed in 2012 and 2013, does not provide the best possible life-cycle value to local, public owners and end users of the Project. However, a Project design that reduces the interstate’s footprint and that improves local community connectivity between the east and west sides of I-75 would provide huge economic benefits to the Cincinnati region, thereby maximizing return on investment in this Project. The Cincinnati region should not miss out on this tremendous opportunity to advance its long-term urban and economic development priorities.

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- Does not require adherence to the Alternative I-W design
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- Extends the local street grid over the interstate in order to better connect the Cincinnati

CBD to Queensgate and the West End via multiple modes, and in order to accommodate local, urban, street-facing development

While I appreciate that the Project's current design has a smaller right-of-way footprint than previous designs, I believe that additional, significant right-of-way reduction is crucial to the future of Cincinnati. With a landlocked urban core, Cincinnati's urban growth is currently constrained, and the Cincinnati region's ability to develop urban-core real estate for those who desire it – including businesses and top talent – is limited. Improved connectivity between the east and west sides of I-75 in Cincinnati is also crucial. I believe a design solution that not only reduces the right-of-way footprint, but that also includes a recessed highway and an overpassing local street grid, like that of ODOT's celebrated Fort Washington Way project, is just one example of a configuration that could deliver important priorities for the community: the expansion of Cincinnati's central business district to the west of I-75, the elevation of the highest and best use for land, and the creation of new opportunities for economic development and housing development. Any design solution that is implemented should deliver these priorities and facilitate a new/improved network of safe, multi-modal local streets connecting the Cincinnati CBD, Queensgate, the West End, and Covington.

Specific design recommendations capturing the spirit of community priorities:

Specifically, I advocate for the alignment of the interstate through the urban core, from West 3rd Street to Ezzard Charles Drive, to be lowered and consolidated in a way that minimizes right-of-way width, thereby creating space for development and reducing environmental impacts of the interstate on the urban core. The final right-of-way width should be as small as possible, but no larger than the width of Fort Washington Way—about 155 feet. The local street grid should remain at-grade and extend over the interstate, improving community connectivity between currently bifurcated neighborhoods. This concept borrows from the successful urban interstate upgrades that have been ongoing in Columbus, Ohio, for years. Connections should be made across the interstate between as many local east-west streets as possible, but especially between West 4th, 5th, 6th, and 7th Streets. Improved connections should also be made north-south between Queensgate and the West End. Local streets extending over the interstate should be designed in a way that is safe and enjoyable for pedestrians and cyclists to navigate, and in a way that is supportive of local businesses

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TJ Jones

TJ Jones
bobbyjones2187@gmail.com
1845 Duck Creek
Cincinnati, Ohio 45207

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From: Joshua Junker <info@sg.actionnetwork.org>
Sent: Sunday, August 14, 2022 10:20 PM
To: Spinosa, Stefan
Subject: Brent Spence Bridge Project - Reconnecting Cincinnati Westway Design Improvements

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Joshua Junker

junkerj9@gmail.com

4465 Hubble Rd
Cincinnati, Ohio 45247

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From: Andrew Kokaliaries <info@sg.actionnetwork.org>
Sent: Sunday, August 14, 2022 10:40 PM
To: Spinosa, Stefan
Subject: Brent Spence Bridge Project - Reconnecting Cincinnati Westway Design Improvements

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Andrew Kokaliaries

tinges_wringer_0l@icloud.com

7058 Ruwes Oak Dr
Cincinnati, Ohio 45248

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From: Julie Luna <info@sg.actionnetwork.org>
Sent: Sunday, August 14, 2022 12:35 PM
To: Spinosa, Stefan
Subject: Brent Spence Bridge Project - Reconnecting Cincinnati Westway Design Improvements

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While I appreciate that the Project's current design has a smaller right-of-way footprint than previous designs, I believe that additional, significant right-of-way reduction is crucial to the future of Cincinnati. With a landlocked urban core, Cincinnati's urban growth is currently constrained, and the Cincinnati region's ability to develop urban-core real estate for those who desire it – including businesses and top talent – is limited. Improved connectivity between the east and west sides of I-75 in Cincinnati is also crucial. I believe a design solution that not only reduces the right-of-way footprint, but that also includes a recessed highway and an overpassing local street grid, like that of ODOT's celebrated Fort Washington Way project, is just one example of a configuration that could deliver important priorities for the community: the expansion of Cincinnati's central business district to the west of I-75, the elevation of the highest and best use for land, and the creation of new opportunities for economic development and housing development. Any design solution that is implemented should deliver these priorities and facilitate a new/improved network of safe, multi-modal local streets connecting the Cincinnati CBD, Queensgate, the West End, and Covington.

Specific design recommendations capturing the spirit of community priorities:

Specifically, I advocate for the alignment of the interstate through the urban core, from West 3rd Street to Ezzard Charles Drive, to be lowered and consolidated in a way that minimizes right-of-way width, thereby creating space for development and reducing environmental impacts of the interstate on the urban core. The final right-of-way width should be as small as possible, but no larger than the width of Fort Washington Way—about 155 feet. The local street grid should remain at-grade and extend over the interstate, improving community connectivity between currently bifurcated neighborhoods. This concept borrows from the successful urban interstate upgrades that have been ongoing in Columbus, Ohio, for years. Connections should be made across the interstate between as many local east-west streets as possible, but especially between West 4th, 5th, 6th, and 7th Streets. Improved connections should also be made north-south between Queensgate and the West End. Local streets extending over the interstate should be designed in a way that is safe and enjoyable for pedestrians and cyclists to navigate, and in a way that is supportive of local businesses

and conducive to urban development. A good example of this is the completed East Long Street bridge crossing I-71 in Columbus, Ohio. With respect to land recapture thanks to Project footprint reduction, enough space should be returned to local control to facilitate the development of long-awaited regional priorities: an expanded convention center and a new arena. Optimally, the interstate would be capped for as much road-length as possible between West 3rd Street and Ezzard Charles Drive. Interstate entry and exit ramps (i.e. Access Points) in the urban core should be simplified and consolidated, thereby reducing intensive, high-speed design requirements. Traffic forecasting assumptions used to drive the Project's design should be validated by actual, historic traffic counts and trends. Design input data could be based on a hybrid of econometric and network-based models built to consider individual trips that shift to other corridors, occur at a different time of day, involve a different mode of transportation, or disappear due to telecommuting or a shifted trip. When the specifics of these priorities cannot be implemented, the spirit of them should be.

These priorities can be incorporated within the scope of projects awarded funding through INFRA and/or Mega grants:

The Project priorities for which I am advocating are extremely consistent with USDOT's stated priorities for its INFRA and Mega grant programs, which are likely to fund the Project. The outcomes and design elements advocated for in this letter should be squarely within the scope of the Project, as made clear by the following passages in the grant programs' Notice of Funding Opportunity (NOFO):

- "Support integrated land use, economic development and transportation planning to improve the movement of people ... facilitate greater public and private investments and strategies in land-use productivity, ... [and] increase in the production or preservation of location-efficient housing"
- "Reduce transportation and housing cost burdens, including through public and private investments to support greater commercial and mixed-income residential development near public transportation ... in walkable neighborhoods"
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- “Improved multimodal and transit access across previously bifurcated disadvantaged neighborhoods”
- “Include[] physical-barrier-mitigating land bridges, caps, lids, linear parks, and multimodal mobility investments that either redress past barriers to opportunity or that proactively create new connections and opportunities for underserved communities that are underserved by transportation”
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- “Address equity and barriers to opportunity, improve quality of life in ... urbanized areas, and benefit Historically Disadvantaged Communities”

o Note: Hamilton County, Ohio, Census Tracts 2 and 263 – both within the Project footprint, with one on each side of I-75 – are considered Transportation Disadvantaged, and Tract 2 is considered a Historically Disadvantaged Community by USDOT. Additionally Tract 2 and Tract 264, which is also adjacent to the Project, are both HUD-designated Racially or Ethnically Concentrated Areas of Poverty (R/ECAPs).

o The Project presents an opportunity for restorative justice. The construction of the Brent Spence Bridge corridor and the accompanying Queensgate industrial area in the early 1960’s eliminated a dense urban neighborhood and displaced 25,737 residents, 25,155 of whom were non-white. 49 CFR 21.5(b)(7) sets an expectation that the relevant agency “must take affirmative action to remove or overcome the effects of the prior discriminatory practice.”

A custom procurement and design-build processes can accommodate a design recalibration while maintaining Project budget and schedule:

Although ODOT selected a Preferred Alternative design in 2011, upgrades to this design are achievable before the Project’s targeted groundbreaking in late 2023. As local business, community, and political leaders, we expect flexibility from ODOT in designing the procurement process. ODOT should adopt a custom, alternative design-build process that is responsive to the community priorities established here. Such a procurement should be performance- and value-based rather than prescriptive, in the sense that the designer/contracting community should be proactively incentivized to provide creative alternative design concepts, not adhering to Alternative I-W, which are responsive to community goals. Importantly, in this value- and performance-based procurement, community

priorities must be sufficiently scored and weighted so that they are critical aspects of the design and selection process. Representative, objective metrics that could be used to evaluate alternative designs in a competitive procurement include: maximization of the amount of returned, contiguous land in Cincinnati's urban core, maximization of street frontage that is walkable and conducive to urban, street-facing development, and minimization of walking distance between the Cincinnati CBD and Queensgate. Also, aspects of progressive design-build procurement should be utilized, to allow for innovative design improvements throughout the course of the project, without delaying Project schedule. When weighing the risks of various Project delivery options, it must be realized that the real risk is delivering this once-in-a-generation investment in a way that only improves highway capacity & safety and misses the opportunity to advance additional, critical regional priorities.

Local stakeholders – especially the City of Cincinnati – should have power throughout the procurement and progressive design-build decision-making process:

Finally, having an engaged local owner of the Project will be critical to the success of Project design improvements. The City of Cincinnati and Hamilton County should be engaged as active partners to ODOT throughout the Project procurement, progressive design, and implementation processes. Specifically, the City of Cincinnati and Hamilton County should become Cooperating Agencies as well as have key votes during the selection of the design-build contractor and associated design concept. A new, City-assembled committee of local stakeholders should help craft the procurement documents and scoring criteria for the selection of a design-builder. Recently, the City of Covington and KYTC signed two MOUs related to the Project, giving Covington a significant seat at the table. ODOT, the City of Cincinnati, and Hamilton County should take similar steps.

Thank you for your attention. I pledge to support ODOT and the entire Cincinnati region in shaping the Project procurement process and ultimate end-product of this Project to all parties' best interests.

Very Respectfully,

Julie Luna

julielunatorres@gmail.com

699 1/2 City Park Avenue
Columbus , Ohio 43206

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From: Dylan Lurk <info@sg.actionnetwork.org>
Sent: Sunday, August 14, 2022 6:07 PM
To: Spinosa, Stefan
Subject: Brent Spence Bridge Project - Reconnecting Cincinnati Westway Design Improvements

ODOT Capital Programs Administrator Stefan Spinosa,

Dear public officials,

This letter concerns the Brent Spence Bridge Corridor Project (“the Project”) where I-71 and I-75 cross the Ohio River in Cincinnati, OH.

I am thrilled this Project is gaining renewed attention and momentum as a result of the recently passed Bipartisan Infrastructure Law. Earnestly, thank you for your leadership to date in helping our community reach this point. Given that the acknowledgement for a bridge project is decades old with the most recent proposal released in 2013; and given the massive sum of federal and state funding being committed to the Cincinnati region for this Project; and given the lengthy lifespan of the physical infrastructure to be constructed, I am gravely concerned designs and due diligence is out of date with what the local community wants and could benefit from.

As a practicing engineer, I believe this is an entirely unnecessary project. Rather than spending ~\$2.5 billion to build a new bridge and approaches, I believe we have not adequately considered alternatives that focus on routing through traffic around the city on the beltway. It makes little physical or economic sense to route through traffic through the city, especially during rush hour. Cars and trucks that pass through do very little to add to the local economy. I'd argue they worsen the quality of life through increased interstate traffic and increased emissions, all for a chance that a small number stop for gas or eat in the region. If we adopt a transportation policy that tilts the equation so it becomes economically enticing to take the beltway instead of the direct route through the city, I believe we can improve traffic conditions within the metro area, focus on serving the transportation needs of local/originating/destination traffic, and save hundreds of millions of dollars to direct towards other regional needs (such as expanded bike facilities and public transit facilities). One example of enacting this policy is to use automated tolling for cars and trucks that enter and

exit the metro area within a certain amount of time (indicating they did not stop in the region). It would be assumed that vehicles that entered but did not exit within the time period stopped in the region and likely contributed to the local economy in some way (they would not be tolled). I'd be happy to discuss this concept further with anyone interested.

Despite my concern and proposed solution, it seems that the only choice politically available is a new bridge. Thus, I'd like to advocate and ask that we reconsider ways to advance needs of the region through this project.

Before us stands a once-in-a-generation opportunity to shape the Cincinnati region's relationship to its infrastructure. Unfortunately, the currently proposed Project design – Alternative I-W – which was substantially developed in 2012 and 2013, does not provide the best possible life-cycle value to local, public owners and end users of the Project. However, a Project design that reduces the interstate's footprint and that improves local community connectivity between the east and west sides of I-75 would provide huge economic benefits to the Cincinnati region, thereby maximizing return on investment in this Project. The Cincinnati region should not miss out on this tremendous opportunity to advance its long-term urban and economic development priorities.

Generally, I advocate that the design of the Project be recalibrated in a way that:

- Does not require adherence to the Alternative I-W design
- Expands the original Purpose & Needs Statement to include the community priorities established here as primary goals
- Reduces the footprint of the interstate and associated ramping
- Lowers the alignment of I-75 through the urban core of Cincinnati, much like that of Fort Washington Way
- Extends the local street grid over the interstate in order to better connect the Cincinnati CBD to Queensgate and the West End via multiple modes, and in order to accommodate local, urban, street-facing development
- Emphasizes the importance of pedestrian facilities and public transit facilities
- Includes design components that seek to beautify the project with landscaping, signage, and parks that let people know they are in The Queen City of Cincinnati.

While I appreciate that the Project's current design has a smaller right-of-way footprint than previous designs including the current roadway structure, I believe that additional, significant right-of-way reduction is crucial to the future of Cincinnati. With a landlocked urban core, Cincinnati's urban growth is currently constrained, and the Cincinnati region's ability to develop urban-core real estate for those who desire it – including businesses and top talent – is limited. Improved connectivity between the east and west sides of I-75 in Cincinnati is also crucial. I believe a design solution that not only reduces the right-of-way footprint, but that also includes a recessed highway and an overpassing local street grid, like that of ODOT's celebrated Fort Washington Way project, is just one example of a configuration that could deliver important priorities for the community: the expansion of Cincinnati's central business district to the west of I-75, the elevation of the highest and best use for land, and the creation of new opportunities for economic development and housing development. Any design solution that is implemented should deliver these priorities and facilitate a new/improved network of safe, multi-modal local streets connecting the Cincinnati CBD, Queensgate, the West End, and Covington.

Specific design recommendations capturing the spirit of community priorities:

Specifically, I advocate for the alignment of the interstate through the urban core, from West 3rd Street to Ezzard Charles Drive, to be lowered and consolidated in a way that minimizes right-of-way width, thereby creating space for development and reducing environmental impacts of the interstate on the urban core. The final right-of-way width should be as small as possible, but no larger than the width of Fort Washington Way—about 155 feet. The local street grid should remain at-grade and extend over the interstate, improving community connectivity between currently bifurcated neighborhoods. This concept borrows from the successful urban interstate upgrades that have been ongoing in Columbus, Ohio, for years. Connections should be made across the interstate between as many local east-west streets as possible, but especially between West 4th, 5th, 6th, and 7th Streets. Improved connections should also be made north-south between Queensgate and the West End. Local streets extending over the interstate should be designed in a way that is safe and enjoyable for pedestrians and cyclists to navigate, and in a way that is supportive of local businesses and conducive to urban development. A good example of this is the completed East Long Street bridge crossing I-71 in Columbus, Ohio. With respect to land recapture thanks to

Project footprint reduction, enough space should be returned to local control to facilitate the development of long-awaited regional priorities: an expanded convention center and a new arena. Optimally, the interstate would be capped for as much road-length as possible between West 3rd Street and Ezzard Charles Drive. Interstate entry and exit ramps (i.e. Access Points) in the urban core should be simplified and consolidated, thereby reducing intensive, high-speed design requirements. Traffic calming for traffic entering/exiting the interstate is a must. Traffic forecasting assumptions used to drive the Project's design should be validated by actual, historic traffic counts and trends. Design input data could be based on a hybrid of econometric and network-based models built to consider individual trips that shift to other corridors, occur at a different time of day, involve a different mode of transportation, or disappear due to telecommuting or a shifted trip. When the specifics of these priorities cannot be implemented, the spirit of them should be.

These priorities can be incorporated within the scope of projects awarded funding through INFRA and/or Mega grants:

The Project priorities for which I am advocating are extremely consistent with USDOT's stated priorities for its INFRA and Mega grant programs, which are likely to fund the Project. The outcomes and design elements advocated for in this letter should be squarely within the scope of the Project, as made clear by the following passages in the grant programs' Notice of Funding Opportunity (NOFO):

- "Support integrated land use, economic development and transportation planning to improve the movement of people ... facilitate greater public and private investments and strategies in land-use productivity, ... [and] increase in the production or preservation of location-efficient housing"
- "Reduce transportation and housing cost burdens, including through public and private investments to support greater commercial and mixed-income residential development near public transportation ... in walkable neighborhoods"
- "Increase the walkability and accessibility for pedestrians and encourage thriving communities for individuals to work, live, and play by creating transportation choices for individuals to move freely with or without a car"
- "Improved multimodal and transit access across previously bifurcated disadvantaged

neighborhoods”

- “Include[] physical-barrier-mitigating land bridges, caps, lids, linear parks, and multimodal mobility investments that either redress past barriers to opportunity or that proactively create new connections and opportunities for underserved communities that are underserved by transportation”

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- “Address equity and barriers to opportunity, improve quality of life in ... urbanized areas, and benefit Historically Disadvantaged Communities”

- o Note: Hamilton County, Ohio, Census Tracts 2 and 263 – both within the Project footprint, with one on each side of I-75 – are considered Transportation Disadvantaged, and Tract 2 is considered a Historically Disadvantaged Community by USDOT. Additionally Tract 2 and Tract 264, which is also adjacent to the Project, are both HUD-designated Racially or Ethnically Concentrated Areas of Poverty (R/ECAPs).

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Thank you for your attention. I pledge to support ODOT and the entire Cincinnati region in shaping the Project procurement process and ultimate end-product of this Project to all parties' best interests. I look forward to continuing to engage with this project as it moves forward.

Very Respectfully,
Dylan C. Lurk

Resident of Central Business District
Cincinnati, OH

Dylan Lurk
dlurk97@hotmail.com
322 W 4th St, APT 607
Cincinnati, Ohio 45202

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From: Maddy McGuire <info@sg.actionnetwork.org>
Sent: Sunday, August 14, 2022 1:20 PM
To: Spinosa, Stefan
Subject: Brent Spence Bridge Project - Reconnecting Cincinnati Westway Design Improvements

ODOT Capital Programs Administrator Stefan Spinosa,

I am thrilled that the Brent Spence Bridge Corridor Project (“the Project”) is gaining momentum, as a result of the recently passed Bipartisan Infrastructure Law. Earnestly, thank you for your leadership to date in helping our community reach this point. However, given the massive sum of federal and state funding being committed to the Cincinnati region for this Project, and given the lengthy lifespan of the physical infrastructure to be constructed, I am advocating for design improvements to the Project, so that the Cincinnati region receives the best possible outcome.

Before us stands a once-in-a-generation opportunity to shape the Cincinnati region’s relationship to its infrastructure. Unfortunately, the currently proposed Project design – Alternative I-W – which was substantially developed in 2012 and 2013, does not provide the best possible life-cycle value to local, public owners and end users of the Project. However, a Project design that reduces the interstate’s footprint and that improves local community connectivity between the east and west sides of I-75 would provide huge economic benefits to the Cincinnati region, thereby maximizing return on investment in this Project. The Cincinnati region should not miss out on this tremendous opportunity to advance its long-term urban and economic development priorities.

Generally, I advocate that the design of the Project be recalibrated in a way that:

- Does not require adherence to the Alternative I-W design
- Expands the original Purpose & Needs Statement to include the community priorities established here as primary goals
- Reduces the footprint of the interstate and associated ramping
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Very Respectfully,

Maddy McGuire

mmcguire0104@gmail.com

2723 Edroy Ct
Cincinnati , Ohio 45209

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From: Josh Murphy <info@sg.actionnetwork.org>
Sent: Sunday, August 14, 2022 11:27 PM
To: Spinosa, Stefan
Subject: Brent Spence Bridge Project - Reconnecting Cincinnati Westway Design Improvements

ODOT Capital Programs Administrator Stefan Spinosa,

I am thrilled that the Brent Spence Bridge Corridor Project (“the Project”) is gaining momentum, as a result of the recently passed Bipartisan Infrastructure Law. Earnestly, thank you for your leadership to date in helping our community reach this point. However, given the massive sum of federal and state funding being committed to the Cincinnati region for this Project, and given the lengthy lifespan of the physical infrastructure to be constructed, I am advocating for design improvements to the Project, so that the Cincinnati region receives the best possible outcome.

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Very Respectfully,

Josh Murphy

jjmurph14@yahoo.com

2207 Burnet Avenue, #2
Cincinnati, Ohio 45219

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From: Phillip Ott <info@sg.actionnetwork.org>
Sent: Sunday, August 14, 2022 9:23 PM
To: Spinosa, Stefan
Subject: Brent Spence Bridge Project - Reconnecting Cincinnati Westway Design Improvements

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Very Respectfully,

-Phillip Ott

Hamilton County Resident and Proud Ohioan

Phillip Ott
phillipott@gmail.com
3302 Jessup Road,
Cincinnati, Ohio 45239

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From: Christopher Randall <info@sg.actionnetwork.org>
Sent: Sunday, August 14, 2022 9:56 PM
To: Spinosa, Stefan
Subject: Brent Spence Bridge Project - Reconnecting Cincinnati Westway Design Improvements

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Chris Randall

Christopher Randall
chris.randall13@gmail.com
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Local stakeholders – especially the City of Cincinnati – should have power throughout the procurement and progressive design-build decision-making process:

Finally, having an engaged local owner of the Project will be critical to the success of Project design improvements. The City of Cincinnati and Hamilton County should be engaged as active partners to ODOT throughout the Project procurement, progressive design, and implementation processes. Specifically, the City of Cincinnati and Hamilton County should become Cooperating Agencies as well as have key votes during the selection of the design-build contractor and associated design concept. A new, City-assembled committee of local stakeholders should help craft the procurement documents and scoring criteria for the selection of a design-builder. Recently, the City of Covington and KYTC signed two MOUs related to the Project, giving Covington a significant seat at the table. ODOT, the City of Cincinnati, and Hamilton County should take similar steps.

Thank you for your attention. I pledge to support ODOT and the entire Cincinnati region in shaping the Project procurement process and ultimate end-product of this Project to all parties' best interests.

Very Respectfully,

Don Ray

djrjr84@gmail.com

5675 Chestnut Ridge Drive
Cincinnati , Ohio 45230

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From: Latasha Williams <info@sg.actionnetwork.org>
Sent: Sunday, August 14, 2022 2:10 PM
To: Spinosa, Stefan
Subject: Brent Spence Bridge Project - Reconnecting Cincinnati Westway Design Improvements

ODOT Capital Programs Administrator Stefan Spinosa,

I am thrilled that the Brent Spence Bridge Corridor Project (“the Project”) is gaining momentum, as a result of the recently passed Bipartisan Infrastructure Law. Earnestly, thank you for your leadership to date in helping our community reach this point. However, given the massive sum of federal and state funding being committed to the Cincinnati region for this Project, and given the lengthy lifespan of the physical infrastructure to be constructed, I am advocating for design improvements to the Project, so that the Cincinnati region receives the best possible outcome.

Before us stands a once-in-a-generation opportunity to shape the Cincinnati region’s relationship to its infrastructure. Unfortunately, the currently proposed Project design – Alternative I-W – which was substantially developed in 2012 and 2013, does not provide the best possible life-cycle value to local, public owners and end users of the Project. However, a Project design that reduces the interstate’s footprint and that improves local community connectivity between the east and west sides of I-75 would provide huge economic benefits to the Cincinnati region, thereby maximizing return on investment in this Project. The Cincinnati region should not miss out on this tremendous opportunity to advance its long-term urban and economic development priorities.

Generally, I advocate that the design of the Project be recalibrated in a way that:

- Does not require adherence to the Alternative I-W design
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CBD to Queensgate and the West End via multiple modes, and in order to accommodate local, urban, street-facing development

While I appreciate that the Project's current design has a smaller right-of-way footprint than previous designs, I believe that additional, significant right-of-way reduction is crucial to the future of Cincinnati. With a landlocked urban core, Cincinnati's urban growth is currently constrained, and the Cincinnati region's ability to develop urban-core real estate for those who desire it – including businesses and top talent – is limited. Improved connectivity between the east and west sides of I-75 in Cincinnati is also crucial. I believe a design solution that not only reduces the right-of-way footprint, but that also includes a recessed highway and an overpassing local street grid, like that of ODOT's celebrated Fort Washington Way project, is just one example of a configuration that could deliver important priorities for the community: the expansion of Cincinnati's central business district to the west of I-75, the elevation of the highest and best use for land, and the creation of new opportunities for economic development and housing development. Any design solution that is implemented should deliver these priorities and facilitate a new/improved network of safe, multi-modal local streets connecting the Cincinnati CBD, Queensgate, the West End, and Covington.

Specific design recommendations capturing the spirit of community priorities:

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Latasha Williams

Latasha Williams
tasha7756@gmail.com
4791 Este Ave
Cincinnati, Ohio 45232

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From: Benjamin Wissel <info@sg.actionnetwork.org>
Sent: Sunday, August 14, 2022 12:24 PM
To: Spinosa, Stefan
Subject: Brent Spence Bridge Project - Reconnecting Cincinnati Westway Design Improvements

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Benjamin Wissel
wissbe@gmail.com

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From: Spencer Deutsch <info@sg.actionnetwork.org>
Sent: Monday, August 15, 2022 2:32 PM
To: Spinosa, Stefan
Subject: Brent Spence Bridge Project - Reconnecting Cincinnati Westway Design Improvements

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Very Respectfully,

Spencer Deutsch
spencer@springdot.com

2611 colrain ave
cincinnati, Ohio 45214

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From: Danny McKelvey <info@sg.actionnetwork.org>
Sent: Monday, August 15, 2022 11:33 AM
To: Spinosa, Stefan
Subject: Brent Spence Bridge Project - Reconnecting Cincinnati Westway Design Improvements

ODOT Capital Programs Administrator Stefan Spinosa,

I am thrilled that the Brent Spence Bridge Corridor Project (“the Project”) is gaining momentum, as a result of the recently passed Bipartisan Infrastructure Law. Earnestly, thank you for your leadership to date in helping our community reach this point. However, given the massive sum of federal and state funding being committed to the Cincinnati region for this Project, and given the lengthy lifespan of the physical infrastructure to be constructed, I am advocating for design improvements to the Project, so that the Cincinnati region receives the best possible outcome.

Before us stands a once-in-a-generation opportunity to shape the Cincinnati region’s relationship to its infrastructure. Unfortunately, the currently proposed Project design – Alternative I-W – which was substantially developed in 2012 and 2013, does not provide the best possible life-cycle value to local, public owners and end users of the Project. However, a Project design that reduces the interstate’s footprint and that improves local community connectivity between the east and west sides of I-75 would provide huge economic benefits to the Cincinnati region, thereby maximizing return on investment in this Project. The Cincinnati region should not miss out on this tremendous opportunity to advance its long-term urban and economic development priorities.

Generally, I advocate that the design of the Project be recalibrated in a way that:

- Does not require adherence to the Alternative I-W design
- Expands the original Purpose & Needs Statement to include the community priorities established here as primary goals
- Reduces the footprint of the interstate and associated ramping
- Lowers the alignment of I-75 through the urban core of Cincinnati, much like that of Fort Washington Way
- Extends the local street grid over the interstate in order to better connect the Cincinnati

CBD to Queensgate and the West End via multiple modes, and in order to accommodate local, urban, street-facing development

While I appreciate that the Project's current design has a smaller right-of-way footprint than previous designs, I believe that additional, significant right-of-way reduction is crucial to the future of Cincinnati. With a landlocked urban core, Cincinnati's urban growth is currently constrained, and the Cincinnati region's ability to develop urban-core real estate for those who desire it – including businesses and top talent – is limited. Improved connectivity between the east and west sides of I-75 in Cincinnati is also crucial. I believe a design solution that not only reduces the right-of-way footprint, but that also includes a recessed highway and an overpassing local street grid, like that of ODOT's celebrated Fort Washington Way project, is just one example of a configuration that could deliver important priorities for the community: the expansion of Cincinnati's central business district to the west of I-75, the elevation of the highest and best use for land, and the creation of new opportunities for economic development and housing development. Any design solution that is implemented should deliver these priorities and facilitate a new/improved network of safe, multi-modal local streets connecting the Cincinnati CBD, Queensgate, the West End, and Covington.

Specific design recommendations capturing the spirit of community priorities:

Specifically, I advocate for the alignment of the interstate through the urban core, from West 3rd Street to Ezzard Charles Drive, to be lowered and consolidated in a way that minimizes right-of-way width, thereby creating space for development and reducing environmental impacts of the interstate on the urban core. The final right-of-way width should be as small as possible, but no larger than the width of Fort Washington Way—about 155 feet. The local street grid should remain at-grade and extend over the interstate, improving community connectivity between currently bifurcated neighborhoods. This concept borrows from the successful urban interstate upgrades that have been ongoing in Columbus, Ohio, for years. Connections should be made across the interstate between as many local east-west streets as possible, but especially between West 4th, 5th, 6th, and 7th Streets. Improved connections should also be made north-south between Queensgate and the West End. Local streets extending over the interstate should be designed in a way that is safe and enjoyable for pedestrians and cyclists to navigate, and in a way that is supportive of local businesses

and conducive to urban development. A good example of this is the completed East Long Street bridge crossing I-71 in Columbus, Ohio. With respect to land recapture thanks to Project footprint reduction, enough space should be returned to local control to facilitate the development of long-awaited regional priorities: an expanded convention center and a new arena. Optimally, the interstate would be capped for as much road-length as possible between West 3rd Street and Ezzard Charles Drive. Interstate entry and exit ramps (i.e. Access Points) in the urban core should be simplified and consolidated, thereby reducing intensive, high-speed design requirements. Traffic forecasting assumptions used to drive the Project's design should be validated by actual, historic traffic counts and trends. Design input data could be based on a hybrid of econometric and network-based models built to consider individual trips that shift to other corridors, occur at a different time of day, involve a different mode of transportation, or disappear due to telecommuting or a shifted trip. When the specifics of these priorities cannot be implemented, the spirit of them should be.

These priorities can be incorporated within the scope of projects awarded funding through INFRA and/or Mega grants:

The Project priorities for which I am advocating are extremely consistent with USDOT's stated priorities for its INFRA and Mega grant programs, which are likely to fund the Project. The outcomes and design elements advocated for in this letter should be squarely within the scope of the Project, as made clear by the following passages in the grant programs' Notice of Funding Opportunity (NOFO):

- "Support integrated land use, economic development and transportation planning to improve the movement of people ... facilitate greater public and private investments and strategies in land-use productivity, ... [and] increase in the production or preservation of location-efficient housing"
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A custom procurement and design-build processes can accommodate a design recalibration while maintaining Project budget and schedule:

Although ODOT selected a Preferred Alternative design in 2011, upgrades to this design are achievable before the Project’s targeted groundbreaking in late 2023. As local business, community, and political leaders, we expect flexibility from ODOT in designing the procurement process. ODOT should adopt a custom, alternative design-build process that is responsive to the community priorities established here. Such a procurement should be performance- and value-based rather than prescriptive, in the sense that the designer/contracting community should be proactively incentivized to provide creative alternative design concepts, not adhering to Alternative I-W, which are responsive to community goals. Importantly, in this value- and performance-based procurement, community

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Very Respectfully,

Danny McKelvey

danny.mckelvey@cigproperties.com

100 Aqua Way, APT 406
Newport, Kentucky 41071

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From: Stephanie Minges <info@sg.actionnetwork.org>
Sent: Monday, August 15, 2022 7:36 AM
To: Spinosa, Stefan
Subject: Brent Spence Bridge Project - Reconnecting Cincinnati Westway Design Improvements

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Very Respectfully,
Stephanie Minges

(former cincy resident and frequent visitor who uses 75/71 every time to visit family!)

Stephanie Minges
s.minges@dow.com
2007 Prestwick Dr
La Grange, Kentucky 40031

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From: Brenden Pulte <info@sg.actionnetwork.org>
Sent: Monday, August 15, 2022 1:24 PM
To: Spinosa, Stefan
Subject: Brent Spence Bridge Project - Reconnecting Cincinnati Westway Design Improvements

ODOT Capital Programs Administrator Stefan Spinosa,

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Brenden Pulte
pulteb@gmail.com

2805 Digby Ave, 12
Cincinnati, Ohio 45220

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From: Morgan Smith <info@sg.actionnetwork.org>
Sent: Monday, August 15, 2022 10:27 AM
To: Spinosa, Stefan
Subject: Brent Spence Bridge Project; Reconnecting Cincinnati Westway

ODOT Capital Programs Administrator Stefan Spinosa,

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Very Respectfully,
Morgan

Morgan Smith
morgan.smith0519@gmail.com
527 E 13th St, Apt 4
Cincinnati, Ohio 45202

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From: Alexander Antony <info@sg.actionnetwork.org>
Sent: Wednesday, August 17, 2022 7:03 PM
To: Spinosa, Stefan
Subject: Brent Spence Bridge Project - Reconnecting Cincinnati Westway Design Improvements

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Alex

Alexander Antony
alexanderk.antony@gmail.com
4256 Langland Street
CINCINNATI, Ohio 45223

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From: Kerry Devery <info@sg.actionnetwork.org>
Sent: Wednesday, August 17, 2022 3:17 PM
To: Spinosa, Stefan
Subject: Brent Spence Bridge Project - Reconnecting Cincinnati Westway Design Improvements

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Kerry Devery

kerry.devery@gmail.com

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Cincinnati, Ohio 45227

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From: Arielle Goldberg <info@sg.actionnetwork.org>
Sent: Wednesday, August 17, 2022 8:40 PM
To: Spinosa, Stefan
Subject: Brent Spence Bridge Project - Reconnecting Cincinnati Westway Design Improvements

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Thank you for your attention. I pledge to support ODOT and the entire Cincinnati region in shaping the Project procurement process and ultimate end-product of this Project to all parties' best interests.

Very Respectfully,

Arielle Goldberg
ariellemg@me.com

634 Bakewell Street,
Covington, Kentucky 41011-1226

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From: JOSEPH HARMON <info@sg.actionnetwork.org>
Sent: Wednesday, August 17, 2022 3:17 PM
To: Spinosa, Stefan
Subject: Brent Spence Bridge Project - Reconnecting Cincinnati Westway Design Improvements

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CBD to Queensgate and the West End via multiple modes, and in order to accommodate local, urban, street-facing development

While I appreciate that the Project's current design has a smaller right-of-way footprint than previous designs, I believe that additional, significant right-of-way reduction is crucial to the future of Cincinnati. With a landlocked urban core, Cincinnati's urban growth is currently constrained, and the Cincinnati region's ability to develop urban-core real estate for those who desire it – including businesses and top talent – is limited. Improved connectivity between the east and west sides of I-75 in Cincinnati is also crucial. I believe a design solution that not only reduces the right-of-way footprint, but that also includes a recessed highway and an overpassing local street grid, like that of ODOT's celebrated Fort Washington Way project, is just one example of a configuration that could deliver important priorities for the community: the expansion of Cincinnati's central business district to the west of I-75, the elevation of the highest and best use for land, and the creation of new opportunities for economic development and housing development. Any design solution that is implemented should deliver these priorities and facilitate a new/improved network of safe, multi-modal local streets connecting the Cincinnati CBD, Queensgate, the West End, and Covington.

Specific design recommendations capturing the spirit of community priorities:

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JOSEPH HARMON

joseph.harmon@gmail.com

6936 WINDING WAY
CINCINNATI, Ohio 45236

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From: Brad Ostendorf <info@sg.actionnetwork.org>
Sent: Wednesday, August 17, 2022 4:03 PM
To: Spinosa, Stefan
Subject: Brent Spence Bridge Project - Reconnecting Cincinnati Westway Design Improvements

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Finally, having an engaged local owner of the Project will be critical to the success of Project design improvements. The City of Cincinnati and Hamilton County should be engaged as active partners to ODOT throughout the Project procurement, progressive design, and implementation processes. Specifically, the City of Cincinnati and Hamilton County should become Cooperating Agencies as well as have key votes during the selection of the design-build contractor and associated design concept. A new, City-assembled committee of local stakeholders should help craft the procurement documents and scoring criteria for the selection of a design-builder. Recently, the City of Covington and KYTC signed two MOUs related to the Project, giving Covington a significant seat at the table. ODOT, the City of Cincinnati, and Hamilton County should take similar steps.

Thank you for your attention. I pledge to support ODOT and the entire Cincinnati region in shaping the Project procurement process and ultimate end-product of this Project to all parties' best interests.

Very Respectfully,
Gracia Ostendorf

Gracia Ostendorf
gracia.goldberg@gmail.com
1221 Parkway Ave
Covington, Kentucky 41011

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From: Craig Schoenberger <info@sg.actionnetwork.org>
Sent: Wednesday, August 17, 2022 9:13 AM
To: Spinosa, Stefan
Subject: Brent Spence Bridge Project - Reconnecting Cincinnati Westway Design Improvements

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Before us stands a once-in-a-generation opportunity to shape the Cincinnati region’s relationship to its infrastructure. Unfortunately, the currently proposed Project design – Alternative I-W – which was substantially developed in 2012 and 2013, does not provide the best possible life-cycle value to local, public owners and end users of the Project. However, a Project design that reduces the interstate’s footprint and that improves local community connectivity between the east and west sides of I-75 would provide huge economic benefits to the Cincinnati region, thereby maximizing return on investment in this Project. The Cincinnati region should not miss out on this tremendous opportunity to advance its long-term urban and economic development priorities.

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- Does not require adherence to the Alternative I-W design
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- Reduces the footprint of the interstate and associated ramping
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- Extends the local street grid over the interstate in order to better connect the Cincinnati

CBD to Queensgate and the West End via multiple modes, and in order to accommodate local, urban, street-facing development

While I appreciate that the Project's current design has a smaller right-of-way footprint than previous designs, I believe that additional, significant right-of-way reduction is crucial to the future of Cincinnati. With a landlocked urban core, Cincinnati's urban growth is currently constrained, and the Cincinnati region's ability to develop urban-core real estate for those who desire it – including businesses and top talent – is limited. Improved connectivity between the east and west sides of I-75 in Cincinnati is also crucial. I believe a design solution that not only reduces the right-of-way footprint, but that also includes a recessed highway and an overpassing local street grid, like that of ODOT's celebrated Fort Washington Way project, is just one example of a configuration that could deliver important priorities for the community: the expansion of Cincinnati's central business district to the west of I-75, the elevation of the highest and best use for land, and the creation of new opportunities for economic development and housing development. Any design solution that is implemented should deliver these priorities and facilitate a new/improved network of safe, multi-modal local streets connecting the Cincinnati CBD, Queensgate, the West End, and Covington.

Specific design recommendations capturing the spirit of community priorities:

Specifically, I advocate for the alignment of the interstate through the urban core, from West 3rd Street to Ezzard Charles Drive, to be lowered and consolidated in a way that minimizes right-of-way width, thereby creating space for development and reducing environmental impacts of the interstate on the urban core. The final right-of-way width should be as small as possible, but no larger than the width of Fort Washington Way—about 155 feet. The local street grid should remain at-grade and extend over the interstate, improving community connectivity between currently bifurcated neighborhoods. This concept borrows from the successful urban interstate upgrades that have been ongoing in Columbus, Ohio, for years. Connections should be made across the interstate between as many local east-west streets as possible, but especially between West 4th, 5th, 6th, and 7th Streets. Improved connections should also be made north-south between Queensgate and the West End. Local streets extending over the interstate should be designed in a way that is safe and enjoyable for pedestrians and cyclists to navigate, and in a way that is supportive of local businesses

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Craig Schoenberger
schoenberger.c@outlook.com

McAlpin Avenue
Cincinnati, Ohio 45220

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From: Andrew Vielhaber <info@sg.actionnetwork.org>
Sent: Wednesday, August 17, 2022 3:16 PM
To: Spinosa, Stefan
Subject: Brent Spence Bridge Project - Reconnecting Cincinnati Westway Design Improvements

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vielhaber8@gmail.com

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Cincinnati, Ohio 45207

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From: Ben Conniff <info@sg.actionnetwork.org>
Sent: Thursday, August 18, 2022 9:50 AM
To: Spinosa, Stefan
Subject: Brent Spence Bridge Project - Reconnecting Cincinnati Westway Design Improvements

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benconniff24@gmail.com

4481 Andreas Ave
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From: Susan Goldberg <info@sg.actionnetwork.org>
Sent: Thursday, August 18, 2022 9:08 PM
To: Spinosa, Stefan
Subject: Brent Spence Bridge Project - Reconnecting Cincinnati Westway Design Improvements

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Very Respectfully,
Susan Goldberg

Susan Goldberg
skmgoldberg@gmail.com
965 Asbury Road
Cincinnati, Ohio 45255

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From: Keenan Cooper <info@sg.actionnetwork.org>
Sent: Saturday, August 20, 2022 3:34 PM
To: Spinosa, Stefan
Subject: Brent Spence Bridge Project - Reconnecting Cincinnati Westway Design Improvements

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Keenan Cooper

Keenan Cooper
keenan.p.cooper@gmail.com
1308 Broadway St. - Unit 2
Cincinnati, Ohio 45202

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From: Casey Goldman <info@sg.actionnetwork.org>
Sent: Saturday, August 20, 2022 3:48 PM
To: Spinosa, Stefan
Subject: Brent Spence Bridge Project - Reconnecting Cincinnati Westway Design Improvements

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Casey Goldman
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404 Reading Rd 301
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Sent: Saturday, August 20, 2022 2:50 PM
To: Spinosa, Stefan
Subject: Brent Spence Bridge Project - Reconnecting Cincinnati Westway Design Improvements

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Local stakeholders – especially the City of Cincinnati – should have power throughout the procurement and progressive design-build decision-making process:

Finally, having an engaged local owner of the Project will be critical to the success of Project design improvements. The City of Cincinnati and Hamilton County should be engaged as active partners to ODOT throughout the Project procurement, progressive design, and implementation processes. Specifically, the City of Cincinnati and Hamilton County should become Cooperating Agencies as well as have key votes during the selection of the design-build contractor and associated design concept. A new, City-assembled committee of local stakeholders should help craft the procurement documents and scoring criteria for the selection of a design-builder. Recently, the City of Covington and KYTC signed two MOUs related to the Project, giving Covington a significant seat at the table. ODOT, the City of Cincinnati, and Hamilton County should take similar steps.

Thank you for your attention. I pledge to support ODOT and the entire Cincinnati region in shaping the Project procurement process and ultimate end-product of this Project to all parties' best interests.

Very Respectfully,

Brennan Knotts

rbrennanknotts@gmail.com

404 Reading Rd Apt 301
Cincinnati, Ohio 45202

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From: Luke Brockett <info@sg.actionnetwork.org>
Sent: Monday, August 22, 2022 5:08 PM
To: Spinosa, Stefan
Subject: Brent Spence Bridge Project - Reconnecting Cincinnati Westway Design Improvements

ODOT Capital Programs Administrator Stefan Spinosa,

I am thrilled that the Brent Spence Bridge Corridor Project (“the Project”) is gaining momentum, as a result of the recently passed Bipartisan Infrastructure Law. Earnestly, thank you for your leadership to date in helping our community reach this point. However, given the massive sum of federal and state funding being committed to the Cincinnati region for this Project, and given the lengthy lifespan of the physical infrastructure to be constructed, I am advocating for design improvements to the Project, so that the Cincinnati region receives the best possible outcome.

Before us stands a once-in-a-generation opportunity to shape the Cincinnati region’s relationship to its infrastructure. Unfortunately, the currently proposed Project design – Alternative I-W – which was substantially developed in 2012 and 2013, does not provide the best possible life-cycle value to local, public owners and end users of the Project. However, a Project design that reduces the interstate’s footprint and that improves local community connectivity between the east and west sides of I-75 would provide huge economic benefits to the Cincinnati region, thereby maximizing return on investment in this Project. The Cincinnati region should not miss out on this tremendous opportunity to advance its long-term urban and economic development priorities.

Generally, I advocate that the design of the Project be recalibrated in a way that:

- Does not require adherence to the Alternative I-W design
- Expands the original Purpose & Needs Statement to include the community priorities established here as primary goals
- Reduces the footprint of the interstate and associated ramping
- Lowers the alignment of I-75 through the urban core of Cincinnati, much like that of Fort Washington Way
- Extends the local street grid over the interstate in order to better connect the Cincinnati

CBD to Queensgate and the West End via multiple modes, and in order to accommodate local, urban, street-facing development

While I appreciate that the Project's current design has a smaller right-of-way footprint than previous designs, I believe that additional, significant right-of-way reduction is crucial to the future of Cincinnati. With a landlocked urban core, Cincinnati's urban growth is currently constrained, and the Cincinnati region's ability to develop urban-core real estate for those who desire it – including businesses and top talent – is limited. Improved connectivity between the east and west sides of I-75 in Cincinnati is also crucial. I believe a design solution that not only reduces the right-of-way footprint, but that also includes a recessed highway and an overpassing local street grid, like that of ODOT's celebrated Fort Washington Way project, is just one example of a configuration that could deliver important priorities for the community: the expansion of Cincinnati's central business district to the west of I-75, the elevation of the highest and best use for land, and the creation of new opportunities for economic development and housing development. Any design solution that is implemented should deliver these priorities and facilitate a new/improved network of safe, multi-modal local streets connecting the Cincinnati CBD, Queensgate, the West End, and Covington.

Specific design recommendations capturing the spirit of community priorities:

Specifically, I advocate for the alignment of the interstate through the urban core, from West 3rd Street to Ezzard Charles Drive, to be lowered and consolidated in a way that minimizes right-of-way width, thereby creating space for development and reducing environmental impacts of the interstate on the urban core. The final right-of-way width should be as small as possible, but no larger than the width of Fort Washington Way—about 155 feet. The local street grid should remain at-grade and extend over the interstate, improving community connectivity between currently bifurcated neighborhoods. This concept borrows from the successful urban interstate upgrades that have been ongoing in Columbus, Ohio, for years. Connections should be made across the interstate between as many local east-west streets as possible, but especially between West 4th, 5th, 6th, and 7th Streets. Improved connections should also be made north-south between Queensgate and the West End. Local streets extending over the interstate should be designed in a way that is safe and enjoyable for pedestrians and cyclists to navigate, and in a way that is supportive of local businesses

and conducive to urban development. A good example of this is the completed East Long Street bridge crossing I-71 in Columbus, Ohio. With respect to land recapture thanks to Project footprint reduction, enough space should be returned to local control to facilitate the development of long-awaited regional priorities: an expanded convention center and a new arena. Optimally, the interstate would be capped for as much road-length as possible between West 3rd Street and Ezzard Charles Drive. Interstate entry and exit ramps (i.e. Access Points) in the urban core should be simplified and consolidated, thereby reducing intensive, high-speed design requirements. Traffic forecasting assumptions used to drive the Project's design should be validated by actual, historic traffic counts and trends. Design input data could be based on a hybrid of econometric and network-based models built to consider individual trips that shift to other corridors, occur at a different time of day, involve a different mode of transportation, or disappear due to telecommuting or a shifted trip. When the specifics of these priorities cannot be implemented, the spirit of them should be.

These priorities can be incorporated within the scope of projects awarded funding through INFRA and/or Mega grants:

The Project priorities for which I am advocating are extremely consistent with USDOT's stated priorities for its INFRA and Mega grant programs, which are likely to fund the Project. The outcomes and design elements advocated for in this letter should be squarely within the scope of the Project, as made clear by the following passages in the grant programs' Notice of Funding Opportunity (NOFO):

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Thank you for your attention. I pledge to support ODOT and the entire Cincinnati region in shaping the Project procurement process and ultimate end-product of this Project to all parties' best interests.

Very Respectfully,

Luke Brockett

luke.brockett@gmail.com

202 Center St
Bellevue, Kentucky 41073

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From: Mike Ciccone <info@sg.actionnetwork.org>
Sent: Monday, August 22, 2022 4:16 PM
To: Spinosa, Stefan
Subject: Brent Spence Bridge Project - We can do better

ODOT Capital Programs Administrator Stefan Spinosa,

I am thrilled that the Brent Spence Bridge Corridor Project (“the Project”) is gaining momentum, as a result of the recently passed Bipartisan Infrastructure Law. Earnestly, thank you for your leadership to date in helping our community reach this point. However, given the massive sum of federal and state funding being committed to the Cincinnati region for this Project, and given the lengthy lifespan of the physical infrastructure to be constructed, I am advocating for design improvements to the Project, so that the Cincinnati region receives the best possible outcome.

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CBD to Queensgate and the West End via multiple modes, and in order to accommodate local, urban, street-facing development

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Thank you for your attention. Although no longer a resident of Cincinnati, I called the city home for 3 years and ultimately had to move away for work, but still have family and friends in the city and the project is still important to me. I pledge to support ODOT and the entire Cincinnati region in shaping the Project procurement process and ultimate end-product of this Project to all parties' best interests.

Very Respectfully,

Mike Ciccone

Mike Ciccone

mcicconeiv@gmail.com

333 E 19th Street

Minneapolis , Minnesota 55404

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From: Samuel Deetz <info@sg.actionnetwork.org>
Sent: Monday, August 22, 2022 2:08 PM
To: Spinosa, Stefan
Subject: Brent Spence Bridge Project - Reconnecting Cincinnati Westway Design Improvements

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Finally, having an engaged local owner of the Project will be critical to the success of Project design improvements. The City of Cincinnati and Hamilton County should be engaged as active partners to ODOT throughout the Project procurement, progressive design, and implementation processes. Specifically, the City of Cincinnati and Hamilton County should become Cooperating Agencies as well as have key votes during the selection of the design-build contractor and associated design concept. A new, City-assembled committee of local stakeholders should help craft the procurement documents and scoring criteria for the selection of a design-builder. Recently, the City of Covington and KYTC signed two MOUs related to the Project, giving Covington a significant seat at the table. ODOT, the City of Cincinnati, and Hamilton County should take similar steps.

Thank you for your attention. I pledge to support ODOT and the entire Cincinnati region in shaping the Project procurement process and ultimate end-product of this Project to all parties' best interests.

While I do not live in the Cincinnati region, I do visit almost every year for the Western and Southern Open held in Mason and would be thrilled to see the improvements proposed by Bridge Forward. Cincinnati has so much wasted potential with numerous freeways bisecting downtown, and this project would help to regain that potential. In particular though, I would

like to see the Western and Southern Open and its' associated tennis facilities move to this reclaimed space so close to downtown. It would provide significantly better transportation access and increase tourism to the area.

Very Respectfully,
Sam

Samuel Deetz
samueldeetz@gmail.com
947 Division St
La Crosse, Wisconsin 54601

CAUTION: This is an external email and may not be safe. If the email looks suspicious, please do not click links or open attachments and forward the email to csc@ohio.gov or click the Phish Alert Button if available.

From: Edward Funnell <info@sg.actionnetwork.org>
Sent: Monday, August 22, 2022 9:10 PM
To: Spinosa, Stefan
Subject: I drive the west side every day, please modernize the Brent Spence replacement plan

ODOT Capital Programs Administrator Stefan Spinosa,

I am thrilled that the Brent Spence Bridge Corridor Project (“the Project”) is gaining momentum, as a result of the recently passed Bipartisan Infrastructure Law. Earnestly, thank you for your leadership to date in helping our community reach this point. However, given the massive sum of federal and state funding being committed to the Cincinnati region for this Project, and given the lengthy lifespan of the physical infrastructure to be constructed, I am advocating for design improvements to the Project, so that the Cincinnati region receives the best possible outcome.

Before us stands a once-in-a-generation opportunity to shape the Cincinnati region’s relationship to its infrastructure. Unfortunately, the currently proposed Project design – Alternative I-W – which was substantially developed in 2012 and 2013, does not provide the best possible life-cycle value to local, public owners and end users of the Project. However, a Project design that reduces the interstate’s footprint and that improves local community connectivity between the east and west sides of I-75 would provide huge economic benefits to the Cincinnati region, thereby maximizing return on investment in this Project. The Cincinnati region should not miss out on this tremendous opportunity to advance its long-term urban and economic development priorities.

Generally, I advocate that the design of the Project be recalibrated in a way that:

- Does not require adherence to the Alternative I-W design
- Expands the original Purpose & Needs Statement to include the community priorities established here as primary goals
- Reduces the footprint of the interstate and associated ramping
- Lowers the alignment of I-75 through the urban core of Cincinnati, much like that of Fort Washington Way
- Extends the local street grid over the interstate in order to better connect the Cincinnati

CBD to Queensgate and the West End via multiple modes, and in order to accommodate local, urban, street-facing development

While I appreciate that the Project's current design has a smaller right-of-way footprint than previous designs, I believe that additional, significant right-of-way reduction is crucial to the future of Cincinnati. With a landlocked urban core, Cincinnati's urban growth is currently constrained, and the Cincinnati region's ability to develop urban-core real estate for those who desire it – including businesses and top talent – is limited. Improved connectivity between the east and west sides of I-75 in Cincinnati is also crucial. I believe a design solution that not only reduces the right-of-way footprint, but that also includes a recessed highway and an overpassing local street grid, like that of ODOT's celebrated Fort Washington Way project, is just one example of a configuration that could deliver important priorities for the community: the expansion of Cincinnati's central business district to the west of I-75, the elevation of the highest and best use for land, and the creation of new opportunities for economic development and housing development. Any design solution that is implemented should deliver these priorities and facilitate a new/improved network of safe, multi-modal local streets connecting the Cincinnati CBD, Queensgate, the West End, and Covington.

Specific design recommendations capturing the spirit of community priorities:

Specifically, I advocate for the alignment of the interstate through the urban core, from West 3rd Street to Ezzard Charles Drive, to be lowered and consolidated in a way that minimizes right-of-way width, thereby creating space for development and reducing environmental impacts of the interstate on the urban core. The final right-of-way width should be as small as possible, but no larger than the width of Fort Washington Way—about 155 feet. The local street grid should remain at-grade and extend over the interstate, improving community connectivity between currently bifurcated neighborhoods. This concept borrows from the successful urban interstate upgrades that have been ongoing in Columbus, Ohio, for years. Connections should be made across the interstate between as many local east-west streets as possible, but especially between West 4th, 5th, 6th, and 7th Streets. Improved connections should also be made north-south between Queensgate and the West End. Local streets extending over the interstate should be designed in a way that is safe and enjoyable for pedestrians and cyclists to navigate, and in a way that is supportive of local businesses

and conducive to urban development. A good example of this is the completed East Long Street bridge crossing I-71 in Columbus, Ohio. With respect to land recapture thanks to Project footprint reduction, enough space should be returned to local control to facilitate the development of long-awaited regional priorities: an expanded convention center and a new arena. Optimally, the interstate would be capped for as much road-length as possible between West 3rd Street and Ezzard Charles Drive. Interstate entry and exit ramps (i.e. Access Points) in the urban core should be simplified and consolidated, thereby reducing intensive, high-speed design requirements. Traffic forecasting assumptions used to drive the Project's design should be validated by actual, historic traffic counts and trends. Design input data could be based on a hybrid of econometric and network-based models built to consider individual trips that shift to other corridors, occur at a different time of day, involve a different mode of transportation, or disappear due to telecommuting or a shifted trip. When the specifics of these priorities cannot be implemented, the spirit of them should be.

These priorities can be incorporated within the scope of projects awarded funding through INFRA and/or Mega grants:

The Project priorities for which I am advocating are extremely consistent with USDOT's stated priorities for its INFRA and Mega grant programs, which are likely to fund the Project. The outcomes and design elements advocated for in this letter should be squarely within the scope of the Project, as made clear by the following passages in the grant programs' Notice of Funding Opportunity (NOFO):

- "Support integrated land use, economic development and transportation planning to improve the movement of people ... facilitate greater public and private investments and strategies in land-use productivity, ... [and] increase in the production or preservation of location-efficient housing"
- "Reduce transportation and housing cost burdens, including through public and private investments to support greater commercial and mixed-income residential development near public transportation ... in walkable neighborhoods"
- "Increase the walkability and accessibility for pedestrians and encourage thriving communities for individuals to work, live, and play by creating transportation choices for individuals to move freely with or without a car"

- “Improved multimodal and transit access across previously bifurcated disadvantaged neighborhoods”
- “Include[] physical-barrier-mitigating land bridges, caps, lids, linear parks, and multimodal mobility investments that either redress past barriers to opportunity or that proactively create new connections and opportunities for underserved communities that are underserved by transportation”
- “Include[] new or improved walking and bicycling infrastructure [and] reduce[] automobile dependence”
- “Address equity and barriers to opportunity, improve quality of life in ... urbanized areas, and benefit Historically Disadvantaged Communities”

o Note: Hamilton County, Ohio, Census Tracts 2 and 263 – both within the Project footprint, with one on each side of I-75 – are considered Transportation Disadvantaged, and Tract 2 is considered a Historically Disadvantaged Community by USDOT. Additionally Tract 2 and Tract 264, which is also adjacent to the Project, are both HUD-designated Racially or Ethnically Concentrated Areas of Poverty (R/ECAPs).

o The Project presents an opportunity for restorative justice. The construction of the Brent Spence Bridge corridor and the accompanying Queensgate industrial area in the early 1960’s eliminated a dense urban neighborhood and displaced 25,737 residents, 25,155 of whom were non-white. 49 CFR 21.5(b)(7) sets an expectation that the relevant agency “must take affirmative action to remove or overcome the effects of the prior discriminatory practice.”

A custom procurement and design-build processes can accommodate a design recalibration while maintaining Project budget and schedule:

Although ODOT selected a Preferred Alternative design in 2011, upgrades to this design are achievable before the Project’s targeted groundbreaking in late 2023. As local business, community, and political leaders, we expect flexibility from ODOT in designing the procurement process. ODOT should adopt a custom, alternative design-build process that is responsive to the community priorities established here. Such a procurement should be performance- and value-based rather than prescriptive, in the sense that the designer/contracting community should be proactively incentivized to provide creative alternative design concepts, not adhering to Alternative I-W, which are responsive to community goals. Importantly, in this value- and performance-based procurement, community

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Thank you for your attention. I pledge to support ODOT and the entire Cincinnati region in shaping the Project procurement process and ultimate end-product of this Project to all parties' best interests.

Very Respectfully,

Edward Funnell

ned.funnell@gmail.com

5027 Francisvalley Ct.
Delhi, Ohio 45238

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From: Samuel Gray <info@sg.actionnetwork.org>
Sent: Monday, August 22, 2022 10:27 PM
To: Spinosa, Stefan
Subject: Brent Spence Bridge Project - Reconnecting Cincinnati Westway Design Improvements

ODOT Capital Programs Administrator Stefan Spinosa,

I am thrilled that the Brent Spence Bridge Corridor Project (“the Project”) is gaining momentum, as a result of the recently passed Bipartisan Infrastructure Law. Earnestly, thank you for your leadership to date in helping our community reach this point. However, given the massive sum of federal and state funding being committed to the Cincinnati region for this Project, and given the lengthy lifespan of the physical infrastructure to be constructed, I am advocating for design improvements to the Project, so that the Cincinnati region receives the best possible outcome.

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o Note: Hamilton County, Ohio, Census Tracts 2 and 263 – both within the Project footprint, with one on each side of I-75 – are considered Transportation Disadvantaged, and Tract 2 is considered a Historically Disadvantaged Community by USDOT. Additionally Tract 2 and Tract 264, which is also adjacent to the Project, are both HUD-designated Racially or Ethnically Concentrated Areas of Poverty (R/ECAPs).

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A custom procurement and design-build processes can accommodate a design recalibration while maintaining Project budget and schedule:

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Thank you for your attention. I pledge to support ODOT and the entire Cincinnati region in shaping the Project procurement process and ultimate end-product of this Project to all parties' best interests.

Very Respectfully,

Samuel Gray

Samuel Gray
ayellowshoe@gmail.com
1406 ELM ST, 1F
Cincinnati, Ohio 45202

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From: Joe Haverkos <info@sg.actionnetwork.org>
Sent: Monday, August 22, 2022 4:22 PM
To: Spinosa, Stefan
Subject: Brent Spence Bridge Project - Reconnecting Cincinnati Westway Design Improvements

ODOT Capital Programs Administrator Stefan Spinosa,

I am thrilled that the Brent Spence Bridge Corridor Project (“the Project”) is gaining momentum, as a result of the recently passed Bipartisan Infrastructure Law. Earnestly, thank you for your leadership to date in helping our community reach this point. However, given the massive sum of federal and state funding being committed to the Cincinnati region for this Project, and given the lengthy lifespan of the physical infrastructure to be constructed, I am advocating for design improvements to the Project, so that the Cincinnati region receives the best possible outcome.

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Thank you for your attention. I pledge to support ODOT and the entire Cincinnati region in shaping the Project procurement process and ultimate end-product of this Project to all parties' best interests.

Very Respectfully,

Joseph Haverkos

Joe Haverkos
josephhaverkos@gmail.com
4166 Georgetown
Blue ash, Ohio 45236

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From: Matt Pace
Date: Mon, Aug 22, 2022 at 10:12 PM
Subject: Brent Spence Bridge Project - Design Improvements

I am advocating that the Cincinnati region receive the best possible outcome from the Brent Spence Bridge Project (“the Project”). Such an outcome would achieve more than simply congestion reduction and safety improvements on the interstate. I support a recalibrated design for the Brent Spence Bridge Project (“the Project”) that follows the general vision of Reconnecting Cincinnati Westway. Specifically, I believe that the design of the Project should be recalibrated in a way that:

- Does not require adherence to the Alternative I-W design;
- Expands the original Purpose & Needs Statement to include the community priorities established here as primary goals;
- Reduces the footprint of the interstate and associated ramping in a way that provides enough land recapture to accommodate long-standing economic development goals such as an expanded convention center or a new arena;
- Minimizes the impact of the interstate and minimizes the barrier created by the interstate, perhaps by lowering the alignment of I-75 through the urban core of Cincinnati, much like that of Fort Washington Way;
- Extends the local street grid over the interstate in order to better connect the Cincinnati CBD to Queensgate and the West End via multiple modes, and in order to accommodate local, urban, street-facing development;
- When the specifics of these priorities cannot be accommodated, the spirit and the magnitude of them should be.

In order to accomplish this, design-builders should be incentivized to propose alternative designs addressing these priorities. The City of Cincinnati should be involved throughout the entire procurement and design process of the Project, and should have a key vote in selecting the design-builder of the Project.

Thank you for your attention. I pledge to support ODOT and the entire Cincinnati region in shaping the Project procurement process and ultimate end-product of this Project to all parties’ best interests.

Very Respectfully,

Matt Pace
matthew.dp.96@gmail.com
2746 Markbreit Ave.
Cincinnati, Ohio 45209

From: Adam Plaver <info@sg.actionnetwork.org>
Sent: Monday, August 22, 2022 7:45 PM
To: Spinosa, Stefan
Subject: Brent Spence Bridge Project - Reconnecting Cincinnati Westway Design Improvements

ODOT Capital Programs Administrator Stefan Spinosa,

I am thrilled that the Brent Spence Bridge Corridor Project (“the Project”) is gaining momentum, as a result of the recently passed Bipartisan Infrastructure Law. Earnestly, thank you for your leadership to date in helping our community reach this point. However, given the massive sum of federal and state funding being committed to the Cincinnati region for this Project, and given the lengthy lifespan of the physical infrastructure to be constructed, I am advocating for design improvements to the Project, so that the Cincinnati region receives the best possible outcome.

Before us stands a once-in-a-generation opportunity to shape the Cincinnati region’s relationship to its infrastructure. Unfortunately, the currently proposed Project design – Alternative I-W – which was substantially developed in 2012 and 2013, does not provide the best possible life-cycle value to local, public owners and end users of the Project. However, a Project design that reduces the interstate’s footprint and that improves local community connectivity between the east and west sides of I-75 would provide huge economic benefits to the Cincinnati region, thereby maximizing return on investment in this Project. The Cincinnati region should not miss out on this tremendous opportunity to advance its long-term urban and economic development priorities.

Generally, I advocate that the design of the Project be recalibrated in a way that:

- Does not require adherence to the Alternative I-W design
- Expands the original Purpose & Needs Statement to include the community priorities established here as primary goals
- Reduces the footprint of the interstate and associated ramping
- Lowers the alignment of I-75 through the urban core of Cincinnati, much like that of Fort Washington Way
- Extends the local street grid over the interstate in order to better connect the Cincinnati

CBD to Queensgate and the West End via multiple modes, and in order to accommodate local, urban, street-facing development

While I appreciate that the Project's current design has a smaller right-of-way footprint than previous designs, I believe that additional, significant right-of-way reduction is crucial to the future of Cincinnati. With a landlocked urban core, Cincinnati's urban growth is currently constrained, and the Cincinnati region's ability to develop urban-core real estate for those who desire it – including businesses and top talent – is limited. Improved connectivity between the east and west sides of I-75 in Cincinnati is also crucial. I believe a design solution that not only reduces the right-of-way footprint, but that also includes a recessed highway and an overpassing local street grid, like that of ODOT's celebrated Fort Washington Way project, is just one example of a configuration that could deliver important priorities for the community: the expansion of Cincinnati's central business district to the west of I-75, the elevation of the highest and best use for land, and the creation of new opportunities for economic development and housing development. Any design solution that is implemented should deliver these priorities and facilitate a new/improved network of safe, multi-modal local streets connecting the Cincinnati CBD, Queensgate, the West End, and Covington.

Specific design recommendations capturing the spirit of community priorities:

Specifically, I advocate for the alignment of the interstate through the urban core, from West 3rd Street to Ezzard Charles Drive, to be lowered and consolidated in a way that minimizes right-of-way width, thereby creating space for development and reducing environmental impacts of the interstate on the urban core. The final right-of-way width should be as small as possible, but no larger than the width of Fort Washington Way—about 155 feet. The local street grid should remain at-grade and extend over the interstate, improving community connectivity between currently bifurcated neighborhoods. This concept borrows from the successful urban interstate upgrades that have been ongoing in Columbus, Ohio, for years. Connections should be made across the interstate between as many local east-west streets as possible, but especially between West 4th, 5th, 6th, and 7th Streets. Improved connections should also be made north-south between Queensgate and the West End. Local streets extending over the interstate should be designed in a way that is safe and enjoyable for pedestrians and cyclists to navigate, and in a way that is supportive of local businesses

and conducive to urban development. A good example of this is the completed East Long Street bridge crossing I-71 in Columbus, Ohio. With respect to land recapture thanks to Project footprint reduction, enough space should be returned to local control to facilitate the development of long-awaited regional priorities: an expanded convention center and a new arena. Optimally, the interstate would be capped for as much road-length as possible between West 3rd Street and Ezzard Charles Drive. Interstate entry and exit ramps (i.e. Access Points) in the urban core should be simplified and consolidated, thereby reducing intensive, high-speed design requirements. Traffic forecasting assumptions used to drive the Project's design should be validated by actual, historic traffic counts and trends. Design input data could be based on a hybrid of econometric and network-based models built to consider individual trips that shift to other corridors, occur at a different time of day, involve a different mode of transportation, or disappear due to telecommuting or a shifted trip. When the specifics of these priorities cannot be implemented, the spirit of them should be.

These priorities can be incorporated within the scope of projects awarded funding through INFRA and/or Mega grants:

The Project priorities for which I am advocating are extremely consistent with USDOT's stated priorities for its INFRA and Mega grant programs, which are likely to fund the Project. The outcomes and design elements advocated for in this letter should be squarely within the scope of the Project, as made clear by the following passages in the grant programs' Notice of Funding Opportunity (NOFO):

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o The Project presents an opportunity for restorative justice. The construction of the Brent Spence Bridge corridor and the accompanying Queensgate industrial area in the early 1960’s eliminated a dense urban neighborhood and displaced 25,737 residents, 25,155 of whom were non-white. 49 CFR 21.5(b)(7) sets an expectation that the relevant agency “must take affirmative action to remove or overcome the effects of the prior discriminatory practice.”

A custom procurement and design-build processes can accommodate a design recalibration while maintaining Project budget and schedule:

Although ODOT selected a Preferred Alternative design in 2011, upgrades to this design are achievable before the Project’s targeted groundbreaking in late 2023. As local business, community, and political leaders, we expect flexibility from ODOT in designing the procurement process. ODOT should adopt a custom, alternative design-build process that is responsive to the community priorities established here. Such a procurement should be performance- and value-based rather than prescriptive, in the sense that the designer/contracting community should be proactively incentivized to provide creative alternative design concepts, not adhering to Alternative I-W, which are responsive to community goals. Importantly, in this value- and performance-based procurement, community

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Finally, having an engaged local owner of the Project will be critical to the success of Project design improvements. The City of Cincinnati and Hamilton County should be engaged as active partners to ODOT throughout the Project procurement, progressive design, and implementation processes. Specifically, the City of Cincinnati and Hamilton County should become Cooperating Agencies as well as have key votes during the selection of the design-build contractor and associated design concept. A new, City-assembled committee of local stakeholders should help craft the procurement documents and scoring criteria for the selection of a design-builder. Recently, the City of Covington and KYTC signed two MOUs related to the Project, giving Covington a significant seat at the table. ODOT, the City of Cincinnati, and Hamilton County should take similar steps.

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Adam Plaver

adamplaver@gmail.com

2424 Maryland Avenue
Cincinnati , Ohio 45204

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From: Cody Pyle <info@sg.actionnetwork.org>
Sent: Monday, August 22, 2022 1:55 PM
To: Spinosa, Stefan
Subject: Brent Spence Bridge Project - Reconnecting Cincinnati Westway Design Improvements

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Very Respectfully,

Cody Pyle

pyle.cody@gmail.com

535 Cooper Ave
MILFORD, Ohio 45150

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From: Colin Reusch <info@sg.actionnetwork.org>
Sent: Monday, August 22, 2022 5:25 PM
To: Spinosa, Stefan
Subject: Brent Spence Bridge Project - Reconnecting Cincinnati Westway Design Improvements

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Very Respectfully,
Colin Reusch

Colin Reusch
reusch.colin@gmail.com
4227 Chambers Street,
Cincinnati, Ohio 45223

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From: Dylan Robinson <info@sg.actionnetwork.org>
Sent: Monday, August 22, 2022 7:45 PM
To: Spinosa, Stefan
Subject: Brent Spence Bridge Project - Reconnecting Cincinnati Westway Design Improvements

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Thank you for your attention. I pledge to support ODOT and the entire Cincinnati region in shaping the Project procurement process and ultimate end-product of this Project to all parties' best interests.

Very Respectfully,

Dylan Robinson

dylan.robinson@artacademy.edu

1310 Sycamore Street Apt 403
Cincinnati, Ohio 45202

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From: Tyler Scull-McWilliams <info@sg.actionnetwork.org>
Sent: Monday, August 22, 2022 6:00 PM
To: Spinosa, Stefan
Subject: Brent Spence Bridge Project - Reconnecting Cincinnati Westway Design Improvements

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Tyler Scull-McWilliams
tscull@gmail.com

1639 Sycamore st
Cincinnati , Ohio 45202

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From: Jacob Shifman <info@sg.actionnetwork.org>
Sent: Monday, August 22, 2022 9:43 PM
To: Spinosa, Stefan
Subject: Brent Spence Bridge Project - Reconnecting Cincinnati Westway Design Improvements

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Sent: Monday, August 22, 2022 4:13 PM
To: Spinosa, Stefan
Subject: Brent Spence Bridge Project - Reconnecting Cincinnati Westway Design Improvements

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Very Respectfully,
Jonathan Tagg

Jonathan Tagg
j.tagg4@gmail.com
4437 Brazee st
Cincinnati, Ohio 45209

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From: Ross Thompson <info@sg.actionnetwork.org>
Sent: Monday, August 22, 2022 3:37 PM
To: Spinosa, Stefan
Subject: Brent Spence Bridge Project - Reconnecting Cincinnati Westway Design Improvements

ODOT Capital Programs Administrator Stefan Spinosa,

I am thrilled that the Brent Spence Bridge Corridor Project (“the Project”) is gaining momentum, as a result of the recently passed Bipartisan Infrastructure Law. Earnestly, thank you for your leadership to date in helping our community reach this point. However, given the massive sum of federal and state funding being committed to the Cincinnati region for this Project, and given the lengthy lifespan of the physical infrastructure to be constructed, I am advocating for design improvements to the Project, so that the Cincinnati region receives the best possible outcome.

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CBD to Queensgate and the West End via multiple modes, and in order to accommodate local, urban, street-facing development

While I appreciate that the Project's current design has a smaller right-of-way footprint than previous designs, I believe that additional, significant right-of-way reduction is crucial to the future of Cincinnati. With a landlocked urban core, Cincinnati's urban growth is currently constrained, and the Cincinnati region's ability to develop urban-core real estate for those who desire it – including businesses and top talent – is limited. Improved connectivity between the east and west sides of I-75 in Cincinnati is also crucial. I believe a design solution that not only reduces the right-of-way footprint, but that also includes a recessed highway and an overpassing local street grid, like that of ODOT's celebrated Fort Washington Way project, is just one example of a configuration that could deliver important priorities for the community: the expansion of Cincinnati's central business district to the west of I-75, the elevation of the highest and best use for land, and the creation of new opportunities for economic development and housing development. Any design solution that is implemented should deliver these priorities and facilitate a new/improved network of safe, multi-modal local streets connecting the Cincinnati CBD, Queensgate, the West End, and Covington.

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Ross Thompson

Ross Thompson
brentspence@wintermute115.fastmail.com
4795 Circle Dr
Fairfield , Ohio 45014

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From: Luke Ciccone <info@sg.actionnetwork.org>
Sent: Tuesday, August 23, 2022 3:27 PM
To: Spinosa, Stefan
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Luke Ciccone

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Mason, Ohio 45040

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To: Spinosa, Stefan
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Thank you for your attention. I pledge to support ODOT and the entire Cincinnati region in shaping the Project procurement process and ultimate end-product of this Project to all parties' best interests.

Very Respectfully,

Grant Dosch
doschgrm@gmail.com

4137 Maple Dr
Cincinnati, Ohio 45209

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From: Warren Hinkle <info@sg.actionnetwork.org>
Sent: Tuesday, August 23, 2022 10:17 AM
To: Spinosa, Stefan
Subject: Brent Spence Bridge Project - Reconnecting Cincinnati Westway Design Improvements

ODOT Capital Programs Administrator Stefan Spinosa,

I am thrilled that the Brent Spence Bridge Corridor Project (“the Project”) is gaining momentum, as a result of the recently passed Bipartisan Infrastructure Law. Earnestly, thank you for your leadership to date in helping our community reach this point. However, given the massive sum of federal and state funding being committed to the Cincinnati region for this Project, and given the lengthy lifespan of the physical infrastructure to be constructed, I am advocating for design improvements to the Project, so that the Cincinnati region receives the best possible outcome.

Before us stands a once-in-a-generation opportunity to shape the Cincinnati region’s relationship to its infrastructure. Unfortunately, the currently proposed Project design – Alternative I-W – which was substantially developed in 2012 and 2013, does not provide the best possible life-cycle value to local, public owners and end users of the Project. However, a Project design that reduces the interstate’s footprint and that improves local community connectivity between the east and west sides of I-75 would provide huge economic benefits to the Cincinnati region, thereby maximizing return on investment in this Project. The Cincinnati region should not miss out on this tremendous opportunity to advance its long-term urban and economic development priorities.

Generally, I advocate that the design of the Project be recalibrated in a way that:

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- Lowers the alignment of I-75 through the urban core of Cincinnati, much like that of Fort Washington Way
- Extends the local street grid over the interstate in order to better connect the Cincinnati

CBD to Queensgate and the West End via multiple modes, and in order to accommodate local, urban, street-facing development

While I appreciate that the Project's current design has a smaller right-of-way footprint than previous designs, I believe that additional, significant right-of-way reduction is crucial to the future of Cincinnati. With a landlocked urban core, Cincinnati's urban growth is currently constrained, and the Cincinnati region's ability to develop urban-core real estate for those who desire it – including businesses and top talent – is limited. Improved connectivity between the east and west sides of I-75 in Cincinnati is also crucial. I believe a design solution that not only reduces the right-of-way footprint, but that also includes a recessed highway and an overpassing local street grid, like that of ODOT's celebrated Fort Washington Way project, is just one example of a configuration that could deliver important priorities for the community: the expansion of Cincinnati's central business district to the west of I-75, the elevation of the highest and best use for land, and the creation of new opportunities for economic development and housing development. Any design solution that is implemented should deliver these priorities and facilitate a new/improved network of safe, multi-modal local streets connecting the Cincinnati CBD, Queensgate, the West End, and Covington.

Specific design recommendations capturing the spirit of community priorities:

Specifically, I advocate for the alignment of the interstate through the urban core, from West 3rd Street to Ezzard Charles Drive, to be lowered and consolidated in a way that minimizes right-of-way width, thereby creating space for development and reducing environmental impacts of the interstate on the urban core. The final right-of-way width should be as small as possible, but no larger than the width of Fort Washington Way—about 155 feet. The local street grid should remain at-grade and extend over the interstate, improving community connectivity between currently bifurcated neighborhoods. This concept borrows from the successful urban interstate upgrades that have been ongoing in Columbus, Ohio, for years. Connections should be made across the interstate between as many local east-west streets as possible, but especially between West 4th, 5th, 6th, and 7th Streets. Improved connections should also be made north-south between Queensgate and the West End. Local streets extending over the interstate should be designed in a way that is safe and enjoyable for pedestrians and cyclists to navigate, and in a way that is supportive of local businesses

and conducive to urban development. A good example of this is the completed East Long Street bridge crossing I-71 in Columbus, Ohio. With respect to land recapture thanks to Project footprint reduction, enough space should be returned to local control to facilitate the development of long-awaited regional priorities: an expanded convention center and a new arena. Optimally, the interstate would be capped for as much road-length as possible between West 3rd Street and Ezzard Charles Drive. Interstate entry and exit ramps (i.e. Access Points) in the urban core should be simplified and consolidated, thereby reducing intensive, high-speed design requirements. Traffic forecasting assumptions used to drive the Project's design should be validated by actual, historic traffic counts and trends. Design input data could be based on a hybrid of econometric and network-based models built to consider individual trips that shift to other corridors, occur at a different time of day, involve a different mode of transportation, or disappear due to telecommuting or a shifted trip. When the specifics of these priorities cannot be implemented, the spirit of them should be.

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The Project priorities for which I am advocating are extremely consistent with USDOT's stated priorities for its INFRA and Mega grant programs, which are likely to fund the Project. The outcomes and design elements advocated for in this letter should be squarely within the scope of the Project, as made clear by the following passages in the grant programs' Notice of Funding Opportunity (NOFO):

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A custom procurement and design-build processes can accommodate a design recalibration while maintaining Project budget and schedule:

Although ODOT selected a Preferred Alternative design in 2011, upgrades to this design are achievable before the Project’s targeted groundbreaking in late 2023. As local business, community, and political leaders, we expect flexibility from ODOT in designing the procurement process. ODOT should adopt a custom, alternative design-build process that is responsive to the community priorities established here. Such a procurement should be performance- and value-based rather than prescriptive, in the sense that the designer/contracting community should be proactively incentivized to provide creative alternative design concepts, not adhering to Alternative I-W, which are responsive to community goals. Importantly, in this value- and performance-based procurement, community

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-Warren Hinkle

Warren Hinkle
warrenphinkle@gmail.com
806 Overton Street
Newport, Kentucky 41071

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From: Tyler Roberts <info@sg.actionnetwork.org>
Sent: Tuesday, August 23, 2022 8:48 AM
To: Spinosa, Stefan
Subject: Brent Spence Bridge Project - Reconnecting Cincinnati Westway Design Improvements

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Tyler Roberts

tyler.keith.roberts@gmail.com

3155 Markbreit Ave
Cincinnati, Ohio 45209

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From: Benjamin Skove <info@sg.actionnetwork.org>
Sent: Tuesday, August 23, 2022 3:34 AM
To: Spinosa, Stefan
Subject: Brent Spence Bridge Project - Reconnecting Cincinnati Westway Design Improvements

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Benjamin Skove

Benjamin Skove
bskove@gmail.com
133 Wentworth Ave,
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From: Jason Taylor <info@sg.actionnetwork.org>
Sent: Tuesday, August 23, 2022 10:30 AM
To: Spinosa, Stefan
Subject: Brent Spence Bridge Project - Reconnecting Cincinnati Westway Design Improvements

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Thank you for your attention. I pledge to support ODOT and the entire Cincinnati region in shaping the Project procurement process and ultimate end-product of this Project to all parties' best interests.

Very Respectfully,
Jason Taylor

Jason Taylor
jason.taylor@yahoo.com
243 Foote Ave
Bellevue, Kentucky 41073

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From: Andrew Campbell
Date: Wed, Aug 24, 2022 at 9:25 AM
Subject: Brent Spence Bridge Project - Reconnecting Cincinnati Westway Design Improvements

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Before us stands a once-in-a-generation opportunity to shape the Cincinnati region’s relationship to its infrastructure. Unfortunately, the currently proposed Project design – Alternative I-W – which was substantially developed in 2012 and 2013, does not provide the best possible life-cycle value to local, public owners and end users of the Project. However, a Project design that reduces the interstate’s footprint and that improves local community connectivity between the east and west sides of I-75 would provide huge economic benefits to the Cincinnati region, thereby maximizing return on investment in this Project. The Cincinnati region should not miss out on this tremendous opportunity to advance its long-term urban and economic development priorities.

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Very Respectfully,

Andrew Scott Campbell
campbellandrewscott@gmail.com
4411 Schulte Drive
Cincinnati, Ohio 45205

From: Connor Brand <info@sg.actionnetwork.org>
Sent: Wednesday, August 24, 2022 3:21 PM
To: Spinosa, Stefan
Subject: Brent Spence Bridge Project - Reconnecting Cincinnati Westway Design Improvements

ODOT Capital Programs Administrator Stefan Spinosa,

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Connor Brand
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906 Main
Cincinnati , Ohio 45202

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From: Steve Farfsing <info@sg.actionnetwork.org>
Sent: Wednesday, August 24, 2022 7:35 AM
To: Spinosa, Stefan
Subject: Brent Spence Bridge Project - Reconnecting Cincinnati Westway Design Improvements

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Finally, having an engaged local owner of the Project will be critical to the success of Project design improvements. The City of Cincinnati and Hamilton County should be engaged as active partners to ODOT throughout the Project procurement, progressive design, and implementation processes. Specifically, the City of Cincinnati and Hamilton County should become Cooperating Agencies as well as have key votes during the selection of the design-build contractor and associated design concept. A new, City-assembled committee of local stakeholders should help craft the procurement documents and scoring criteria for the selection of a design-builder. Recently, the City of Covington and KYTC signed two MOUs related to the Project, giving Covington a significant seat at the table. ODOT, the City of Cincinnati, and Hamilton County should take similar steps.

Thank you for your attention. I pledge to support ODOT and the entire Cincinnati region in shaping the Project procurement process and ultimate end-product of this Project to all parties' best interests.

Very Respectfully,

Steve Farfsing
stevefarfsing@gmail.com

6013 Belmont Ave
cincinnati, Ohio 45224

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From: Seth Hershey <info@sg.actionnetwork.org>
Sent: Wednesday, August 24, 2022 8:28 AM
To: Spinosa, Stefan
Subject: Brent Spence Bridge Project - Reconnecting Cincinnati Westway Design Improvements

ODOT Capital Programs Administrator Stefan Spinosa,

I am thrilled that the Brent Spence Bridge Corridor Project (“the Project”) is gaining momentum, as a result of the recently passed Bipartisan Infrastructure Law. Earnestly, thank you for your leadership to date in helping our community reach this point. However, given the massive sum of federal and state funding being committed to the Cincinnati region for this Project, and given the lengthy lifespan of the physical infrastructure to be constructed, I am advocating for design improvements to the Project, so that the Cincinnati region receives the best possible outcome.

Before us stands a once-in-a-generation opportunity to shape the Cincinnati region’s relationship to its infrastructure. Unfortunately, the currently proposed Project design – Alternative I-W – which was substantially developed in 2012 and 2013, does not provide the best possible life-cycle value to local, public owners and end users of the Project. However, a Project design that reduces the interstate’s footprint and that improves local community connectivity between the east and west sides of I-75 would provide huge economic benefits to the Cincinnati region, thereby maximizing return on investment in this Project. The Cincinnati region should not miss out on this tremendous opportunity to advance its long-term urban and economic development priorities.

Generally, I advocate that the design of the Project be recalibrated in a way that:

- Does not require adherence to the Alternative I-W design
- Expands the original Purpose & Needs Statement to include the community priorities established here as primary goals
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- Lowers the alignment of I-75 through the urban core of Cincinnati, much like that of Fort Washington Way
- Extends the local street grid over the interstate in order to better connect the Cincinnati

CBD to Queensgate and the West End via multiple modes, and in order to accommodate local, urban, street-facing development

While I appreciate that the Project's current design has a smaller right-of-way footprint than previous designs, I believe that additional, significant right-of-way reduction is crucial to the future of Cincinnati. With a landlocked urban core, Cincinnati's urban growth is currently constrained, and the Cincinnati region's ability to develop urban-core real estate for those who desire it – including businesses and top talent – is limited. Improved connectivity between the east and west sides of I-75 in Cincinnati is also crucial. I believe a design solution that not only reduces the right-of-way footprint, but that also includes a recessed highway and an overpassing local street grid, like that of ODOT's celebrated Fort Washington Way project, is just one example of a configuration that could deliver important priorities for the community: the expansion of Cincinnati's central business district to the west of I-75, the elevation of the highest and best use for land, and the creation of new opportunities for economic development and housing development. Any design solution that is implemented should deliver these priorities and facilitate a new/improved network of safe, multi-modal local streets connecting the Cincinnati CBD, Queensgate, the West End, and Covington.

Specific design recommendations capturing the spirit of community priorities:

Specifically, I advocate for the alignment of the interstate through the urban core, from West 3rd Street to Ezzard Charles Drive, to be lowered and consolidated in a way that minimizes right-of-way width, thereby creating space for development and reducing environmental impacts of the interstate on the urban core. The final right-of-way width should be as small as possible, but no larger than the width of Fort Washington Way—about 155 feet. The local street grid should remain at-grade and extend over the interstate, improving community connectivity between currently bifurcated neighborhoods. This concept borrows from the successful urban interstate upgrades that have been ongoing in Columbus, Ohio, for years. Connections should be made across the interstate between as many local east-west streets as possible, but especially between West 4th, 5th, 6th, and 7th Streets. Improved connections should also be made north-south between Queensgate and the West End. Local streets extending over the interstate should be designed in a way that is safe and enjoyable for pedestrians and cyclists to navigate, and in a way that is supportive of local businesses

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Very Respectfully,
Seth A. Hershey
Northside, Cincinnati

Seth Hershey
seth.hershey@gmail.com
4373 Dane Ave
Cincinnati, Ohio 45223-1852

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From: Jeffrey Kramer <info@sg.actionnetwork.org>
Sent: Wednesday, August 24, 2022 4:12 PM
To: Spinosa, Stefan
Subject: Brent Spence Bridge Project - Reconnecting Cincinnati Westway Design Improvements

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Jeff G. Kramer

Jeffrey Kramer
jeff.g.kramer@gmail.com
610 Marietta Ave
Terrace Park, Ohio 45174

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From: Adam Nelson <info@sg.actionnetwork.org>
Sent: Wednesday, August 24, 2022 2:41 PM
To: Spinosa, Stefan
Subject: Brent Spence Bridge Project - Reconnecting Cincinnati Westway Design Improvements

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Adam Nelson

Adam Nelson
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From: Andrew Walsh <info@sg.actionnetwork.org>
Sent: Wednesday, August 24, 2022 10:52 AM
To: Spinosa, Stefan
Subject: Brent Spence Bridge Project - Reconnecting Cincinnati Westway Design Improvements

ODOT Capital Programs Administrator Stefan Spinosa,

I am thrilled that the Brent Spence Bridge Corridor Project (“the Project”) is gaining momentum, as a result of the recently passed Bipartisan Infrastructure Law. Earnestly, thank you for your leadership to date in helping our community reach this point. However, given the massive sum of federal and state funding being committed to the Cincinnati region for this Project, and given the lengthy lifespan of the physical infrastructure to be constructed, I am advocating for design improvements to the Project, so that the Cincinnati region receives the best possible outcome.

Before us stands a once-in-a-generation opportunity to shape the Cincinnati region’s relationship to its infrastructure. Unfortunately, the currently proposed Project design – Alternative I-W – which was substantially developed in 2012 and 2013, does not provide the best possible life-cycle value to local, public owners and end users of the Project. However, a Project design that reduces the interstate’s footprint and that improves local community connectivity between the east and west sides of I-75 would provide huge economic benefits to the Cincinnati region, thereby maximizing return on investment in this Project. The Cincinnati region should not miss out on this tremendous opportunity to advance its long-term urban and economic development priorities.

Generally, I advocate that the design of the Project be recalibrated in a way that:

- Does not require adherence to the Alternative I-W design
- Expands the original Purpose & Needs Statement to include the community priorities established here as primary goals
- Reduces the footprint of the interstate and associated ramping
- Lowers the alignment of I-75 through the urban core of Cincinnati, much like that of Fort Washington Way
- Extends the local street grid over the interstate in order to better connect the Cincinnati

CBD to Queensgate and the West End via multiple modes, and in order to accommodate local, urban, street-facing development

While I appreciate that the Project's current design has a smaller right-of-way footprint than previous designs, I believe that additional, significant right-of-way reduction is crucial to the future of Cincinnati. With a landlocked urban core, Cincinnati's urban growth is currently constrained, and the Cincinnati region's ability to develop urban-core real estate for those who desire it – including businesses and top talent – is limited. Improved connectivity between the east and west sides of I-75 in Cincinnati is also crucial. I believe a design solution that not only reduces the right-of-way footprint, but that also includes a recessed highway and an overpassing local street grid, like that of ODOT's celebrated Fort Washington Way project, is just one example of a configuration that could deliver important priorities for the community: the expansion of Cincinnati's central business district to the west of I-75, the elevation of the highest and best use for land, and the creation of new opportunities for economic development and housing development. Any design solution that is implemented should deliver these priorities and facilitate a new/improved network of safe, multi-modal local streets connecting the Cincinnati CBD, Queensgate, the West End, and Covington.

Specific design recommendations capturing the spirit of community priorities:

Specifically, I advocate for the alignment of the interstate through the urban core, from West 3rd Street to Ezzard Charles Drive, to be lowered and consolidated in a way that minimizes right-of-way width, thereby creating space for development and reducing environmental impacts of the interstate on the urban core. The final right-of-way width should be as small as possible, but no larger than the width of Fort Washington Way—about 155 feet. The local street grid should remain at-grade and extend over the interstate, improving community connectivity between currently bifurcated neighborhoods. This concept borrows from the successful urban interstate upgrades that have been ongoing in Columbus, Ohio, for years. Connections should be made across the interstate between as many local east-west streets as possible, but especially between West 4th, 5th, 6th, and 7th Streets. Improved connections should also be made north-south between Queensgate and the West End. Local streets extending over the interstate should be designed in a way that is safe and enjoyable for pedestrians and cyclists to navigate, and in a way that is supportive of local businesses

and conducive to urban development. A good example of this is the completed East Long Street bridge crossing I-71 in Columbus, Ohio. With respect to land recapture thanks to Project footprint reduction, enough space should be returned to local control to facilitate the development of long-awaited regional priorities: an expanded convention center and a new arena. Optimally, the interstate would be capped for as much road-length as possible between West 3rd Street and Ezzard Charles Drive. Interstate entry and exit ramps (i.e. Access Points) in the urban core should be simplified and consolidated, thereby reducing intensive, high-speed design requirements. Traffic forecasting assumptions used to drive the Project's design should be validated by actual, historic traffic counts and trends. Design input data could be based on a hybrid of econometric and network-based models built to consider individual trips that shift to other corridors, occur at a different time of day, involve a different mode of transportation, or disappear due to telecommuting or a shifted trip. When the specifics of these priorities cannot be implemented, the spirit of them should be.

These priorities can be incorporated within the scope of projects awarded funding through INFRA and/or Mega grants:

The Project priorities for which I am advocating are extremely consistent with USDOT's stated priorities for its INFRA and Mega grant programs, which are likely to fund the Project. The outcomes and design elements advocated for in this letter should be squarely within the scope of the Project, as made clear by the following passages in the grant programs' Notice of Funding Opportunity (NOFO):

- "Support integrated land use, economic development and transportation planning to improve the movement of people ... facilitate greater public and private investments and strategies in land-use productivity, ... [and] increase in the production or preservation of location-efficient housing"
- "Reduce transportation and housing cost burdens, including through public and private investments to support greater commercial and mixed-income residential development near public transportation ... in walkable neighborhoods"
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- “Improved multimodal and transit access across previously bifurcated disadvantaged neighborhoods”
- “Include[] physical-barrier-mitigating land bridges, caps, lids, linear parks, and multimodal mobility investments that either redress past barriers to opportunity or that proactively create new connections and opportunities for underserved communities that are underserved by transportation”
- “Include[] new or improved walking and bicycling infrastructure [and] reduce[] automobile dependence”
- “Address equity and barriers to opportunity, improve quality of life in ... urbanized areas, and benefit Historically Disadvantaged Communities”

o Note: Hamilton County, Ohio, Census Tracts 2 and 263 – both within the Project footprint, with one on each side of I-75 – are considered Transportation Disadvantaged, and Tract 2 is considered a Historically Disadvantaged Community by USDOT. Additionally Tract 2 and Tract 264, which is also adjacent to the Project, are both HUD-designated Racially or Ethnically Concentrated Areas of Poverty (R/ECAPs).

o The Project presents an opportunity for restorative justice. The construction of the Brent Spence Bridge corridor and the accompanying Queensgate industrial area in the early 1960’s eliminated a dense urban neighborhood and displaced 25,737 residents, 25,155 of whom were non-white. 49 CFR 21.5(b)(7) sets an expectation that the relevant agency “must take affirmative action to remove or overcome the effects of the prior discriminatory practice.”

A custom procurement and design-build processes can accommodate a design recalibration while maintaining Project budget and schedule:

Although ODOT selected a Preferred Alternative design in 2011, upgrades to this design are achievable before the Project’s targeted groundbreaking in late 2023. As local business, community, and political leaders, we expect flexibility from ODOT in designing the procurement process. ODOT should adopt a custom, alternative design-build process that is responsive to the community priorities established here. Such a procurement should be performance- and value-based rather than prescriptive, in the sense that the designer/contracting community should be proactively incentivized to provide creative alternative design concepts, not adhering to Alternative I-W, which are responsive to community goals. Importantly, in this value- and performance-based procurement, community

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Local stakeholders – especially the City of Cincinnati – should have power throughout the procurement and progressive design-build decision-making process:

Finally, having an engaged local owner of the Project will be critical to the success of Project design improvements. The City of Cincinnati and Hamilton County should be engaged as active partners to ODOT throughout the Project procurement, progressive design, and implementation processes. Specifically, the City of Cincinnati and Hamilton County should become Cooperating Agencies as well as have key votes during the selection of the design-build contractor and associated design concept. A new, City-assembled committee of local stakeholders should help craft the procurement documents and scoring criteria for the selection of a design-builder. Recently, the City of Covington and KYTC signed two MOUs related to the Project, giving Covington a significant seat at the table. ODOT, the City of Cincinnati, and Hamilton County should take similar steps.

Thank you for your attention. I pledge to support ODOT and the entire Cincinnati region in shaping the Project procurement process and ultimate end-product of this Project to all parties' best interests.

Very Respectfully,
Andrew Walsh
ENS USN

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andrewbwalsh21@gmail.com
821 Boyle Road
Hamilton, Ohio 45013

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From: Jocelyn Gibson <info@sg.actionnetwork.org>
Sent: Thursday, August 25, 2022 12:11 PM
To: Spinosa, Stefan
Subject: We need to consider a better design - Brent Spence Bridge Project - Reconnecting Cincinnati Westway Design Improvements

ODOT Capital Programs Administrator Stefan Spinosa,

I would like to start by saying that the Brent Spence Bridge Corridor Project (“the Project”) is still within a window where redesigns can occur, and the associated environmental approvals for a redesign are achievable. There are examples across the country. It is so exciting that as a result of the recently passed Bipartisan Infrastructure Law, there is genuine momentum around this Project. Earnestly, thank you for your leadership to date in helping our community reach this point. However, given the massive sum of federal and state funding being committed to the Cincinnati region for this Project, and given the lengthy lifespan of the physical infrastructure to be constructed, I am advocating for design improvements to the Project, so that the Cincinnati region receives the best possible outcome.

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Very Respectfully,

Jocelyn M. Gibson

Jocelyn Gibson

jocelyn.gibson@gmail.com

1211 Bates Avenue

Cincinnati, OH, Ohio 45225

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From: Jacob Witte <info@sg.actionnetwork.org>
Sent: Thursday, August 25, 2022 5:57 AM
To: Spinosa, Stefan
Subject: Brent Spence Bridge Project - Reconnecting Cincinnati Westway Design Improvements

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From: Aaron Kohlhepp <info@sg.actionnetwork.org>
Sent: Friday, August 26, 2022 12:43 PM
To: Spinosa, Stefan
Subject: Brent Spence Bridge Project - Reconnecting Cincinnati Westway Design Improvements

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Thank you for your attention. I pledge to support ODOT and the entire Cincinnati region in shaping the Project procurement process and ultimate end-product of this Project to all parties' best interests.

Very Respectfully,

Aaron Kohlhepp
ackohl00@hotmail.com

1010 Walnut Street, #1102
Cincinnati, Ohio 45202

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From: Hillary Wagner <info@sg.actionnetwork.org>
Sent: Friday, August 26, 2022 10:54 AM
To: Spinosa, Stefan
Subject: Brent Spence Bridge Project - Reconnecting Cincinnati Westway Design Improvements

ODOT Capital Programs Administrator Stefan Spinosa,

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Very Respectfully,

Hillary Wagner

hillaryruthwagner@gmail.com

2120 Saint James Ave, APT 3
Cincinnati, Ohio 45206

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From: Alex Hemmer <info@sg.actionnetwork.org>
Sent: Saturday, August 27, 2022 12:02 PM
To: Spinosa, Stefan
Subject: Brent Spence Bridge Project - Reconnecting Cincinnati Westway Design Improvements

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Very Respectfully,

Alex Hemmer

Alex Hemmer
859hemmer@gmail.com
903 Squire Oaks Dr
Villa Hills, Kentucky 41017

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From: Richard Wendel III <info@sg.actionnetwork.org>
Sent: Monday, August 29, 2022 8:35 AM
To: Spinosa, Stefan
Subject: Brent Spence Bridge Project - Reconnecting Cincinnati Westway Design Improvements

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Local stakeholders – especially the City of Cincinnati – should have power throughout the procurement and progressive design-build decision-making process:

Finally, having an engaged local owner of the Project will be critical to the success of Project design improvements. The City of Cincinnati and Hamilton County should be engaged as active partners to ODOT throughout the Project procurement, progressive design, and implementation processes. Specifically, the City of Cincinnati and Hamilton County should become Cooperating Agencies as well as have key votes during the selection of the design-build contractor and associated design concept. A new, City-assembled committee of local stakeholders should help craft the procurement documents and scoring criteria for the selection of a design-builder. Recently, the City of Covington and KYTC signed two MOUs related to the Project, giving Covington a significant seat at the table. ODOT, the City of Cincinnati, and Hamilton County should take similar steps.

Thank you for your attention. I pledge to support ODOT and the entire Cincinnati region in shaping the Project procurement process and ultimate end-product of this Project to all parties' best interests.

Very Respectfully,
Richard Wendel III

Richard Wendel III
richard.wendel@hey.com
427 Delta Ave.
Cincinnati, Ohio 45226

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From: Justin Herald <info@sg.actionnetwork.org>
Sent: Tuesday, August 30, 2022 8:05 PM
To: Spinosa, Stefan
Subject: Brent Spence Bridge Project - Reconnecting Cincinnati Westway Design Improvements

ODOT Capital Programs Administrator Stefan Spinosa,

Don't let it be a boondoggle, position our region to be competitive, not a misty flyover.

I am thrilled that the Brent Spence Bridge Corridor Project ("the Project") is gaining momentum, as a result of the recently passed Bipartisan Infrastructure Law. Earnestly, thank you for your leadership to date in helping our community reach this point. However, given the massive sum of federal and state funding being committed to the Cincinnati region for this Project, and given the lengthy lifespan of the physical infrastructure to be constructed, I am advocating for design improvements to the Project, so that the Cincinnati region receives the best possible outcome.

Before us stands a once-in-a-generation opportunity to shape the Cincinnati region's relationship to its infrastructure. Unfortunately, the currently proposed Project design – Alternative I-W – which was substantially developed in 2012 and 2013, does not provide the best possible life-cycle value to local, public owners and end users of the Project. However, a Project design that reduces the interstate's footprint and that improves local community connectivity between the east and west sides of I-75 would provide huge economic benefits to the Cincinnati region, thereby maximizing return on investment in this Project. The Cincinnati region should not miss out on this tremendous opportunity to advance its long-term urban and economic development priorities.

Generally, I advocate that the design of the Project be recalibrated in a way that:

- Does not require adherence to the Alternative I-W design
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- Reduces the footprint of the interstate and associated ramping
- Lowers the alignment of I-75 through the urban core of Cincinnati, much like that of Fort

Washington Way

- Extends the local street grid over the interstate in order to better connect the Cincinnati CBD to Queensgate and the West End via multiple modes, and in order to accommodate local, urban, street-facing development

While I appreciate that the Project's current design has a smaller right-of-way footprint than previous designs, I believe that additional, significant right-of-way reduction is crucial to the future of Cincinnati. With a landlocked urban core, Cincinnati's urban growth is currently constrained, and the Cincinnati region's ability to develop urban-core real estate for those who desire it – including businesses and top talent – is limited. Improved connectivity between the east and west sides of I-75 in Cincinnati is also crucial. I believe a design solution that not only reduces the right-of-way footprint, but that also includes a recessed highway and an overpassing local street grid, like that of ODOT's celebrated Fort Washington Way project, is just one example of a configuration that could deliver important priorities for the community: the expansion of Cincinnati's central business district to the west of I-75, the elevation of the highest and best use for land, and the creation of new opportunities for economic development and housing development. Any design solution that is implemented should deliver these priorities and facilitate a new/improved network of safe, multi-modal local streets connecting the Cincinnati CBD, Queensgate, the West End, and Covington.

Specific design recommendations capturing the spirit of community priorities:

Specifically, I advocate for the alignment of the interstate through the urban core, from West 3rd Street to Ezzard Charles Drive, to be lowered and consolidated in a way that minimizes right-of-way width, thereby creating space for development and reducing environmental impacts of the interstate on the urban core. The final right-of-way width should be as small as possible, but no larger than the width of Fort Washington Way—about 155 feet. The local street grid should remain at-grade and extend over the interstate, improving community connectivity between currently bifurcated neighborhoods. This concept borrows from the successful urban interstate upgrades that have been ongoing in Columbus, Ohio, for years. Connections should be made across the interstate between as many local east-west streets as possible, but especially between West 4th, 5th, 6th, and 7th Streets. Improved connections should also be made north-south between Queensgate and the West End. Local

streets extending over the interstate should be designed in a way that is safe and enjoyable for pedestrians and cyclists to navigate, and in a way that is supportive of local businesses and conducive to urban development. A good example of this is the completed East Long Street bridge crossing I-71 in Columbus, Ohio. With respect to land recapture thanks to Project footprint reduction, enough space should be returned to local control to facilitate the development of long-awaited regional priorities: an expanded convention center and a new arena. Optimally, the interstate would be capped for as much road-length as possible between West 3rd Street and Ezzard Charles Drive. Interstate entry and exit ramps (i.e. Access Points) in the urban core should be simplified and consolidated, thereby reducing intensive, high-speed design requirements. Traffic forecasting assumptions used to drive the Project's design should be validated by actual, historic traffic counts and trends. Design input data could be based on a hybrid of econometric and network-based models built to consider individual trips that shift to other corridors, occur at a different time of day, involve a different mode of transportation, or disappear due to telecommuting or a shifted trip. When the specifics of these priorities cannot be implemented, the spirit of them should be.

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Respectfully,
Justin Herald

Justin Herald
jrherald@me.com
713 Delmar PI
Covington , Kentucky 41014

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From: Matt Butler <info@sg.actionnetwork.org>
Sent: Wednesday, August 31, 2022 10:37 AM
To: Spinosa, Stefan
Subject: Brent Spence Bridge Project - Reconnecting Cincinnati Westway Design Improvements

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Matt Butler

matt@devougood.com

900 Edgehill Rd
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From: Noel Roldan Medina <info@sg.actionnetwork.org>
Sent: Wednesday, August 31, 2022 12:28 PM
To: Spinosa, Stefan
Subject: Brent Spence Bridge Project - Reconnecting Cincinnati Westway Design Improvements

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Thank you for your attention. I pledge to support ODOT and the entire Cincinnati region in shaping the Project procurement process and ultimate end-product of this Project to all parties' best interests.

Very Respectfully,

Noel Roldan Medina
noeld90@gmail.com

3559 Reading Rd Apt 223
Cincinnati , Saint Croix Island 45529

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From: John Laber-Kirst <info@sg.actionnetwork.org>
Sent: Wednesday, September 7, 2022 9:32 AM
To: Spinosa, Stefan
Subject: Brent Spence Bridge Project - Reconnecting Cincinnati Westway Design Improvements

ODOT Capital Programs Administrator Stefan Spinosa,

I am thrilled that the Brent Spence Bridge Corridor Project (“the Project”) is gaining momentum, as a result of the recently passed Bipartisan Infrastructure Law. Earnestly, thank you for your leadership to date in helping our community reach this point. However, given the massive sum of federal and state funding being committed to the Cincinnati region for this Project, and given the lengthy lifespan of the physical infrastructure to be constructed, I am advocating for design improvements to the Project, so that the Cincinnati region receives the best possible outcome.

Before us stands a once-in-a-generation opportunity to shape the Cincinnati region’s relationship to its infrastructure. Unfortunately, the currently proposed Project design – Alternative I-W – which was substantially developed in 2012 and 2013, does not provide the best possible life-cycle value to local, public owners and end users of the Project. However, a Project design that reduces the interstate’s footprint and that improves local community connectivity between the east and west sides of I-75 would provide huge economic benefits to the Cincinnati region, thereby maximizing return on investment in this Project. The Cincinnati region should not miss out on this tremendous opportunity to advance its long-term urban and economic development priorities.

Generally, I advocate that the design of the Project be recalibrated in a way that:

- Does not require adherence to the Alternative I-W design
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- Reduces the footprint of the interstate and associated ramping
- Lowers the alignment of I-75 through the urban core of Cincinnati, much like that of Fort Washington Way
- Extends the local street grid over the interstate in order to better connect the Cincinnati

CBD to Queensgate and the West End via multiple modes, and in order to accommodate local, urban, street-facing development

While I appreciate that the Project's current design has a smaller right-of-way footprint than previous designs, I believe that additional, significant right-of-way reduction is crucial to the future of Cincinnati. With a landlocked urban core, Cincinnati's urban growth is currently constrained, and the Cincinnati region's ability to develop urban-core real estate for those who desire it – including businesses and top talent – is limited. Improved connectivity between the east and west sides of I-75 in Cincinnati is also crucial. I believe a design solution that not only reduces the right-of-way footprint, but that also includes a recessed highway and an overpassing local street grid, like that of ODOT's celebrated Fort Washington Way project, is just one example of a configuration that could deliver important priorities for the community: the expansion of Cincinnati's central business district to the west of I-75, the elevation of the highest and best use for land, and the creation of new opportunities for economic development and housing development. Any design solution that is implemented should deliver these priorities and facilitate a new/improved network of safe, multi-modal local streets connecting the Cincinnati CBD, Queensgate, the West End, and Covington.

Specific design recommendations capturing the spirit of community priorities:

Specifically, I advocate for the alignment of the interstate through the urban core, from West 3rd Street to Ezzard Charles Drive, to be lowered and consolidated in a way that minimizes right-of-way width, thereby creating space for development and reducing environmental impacts of the interstate on the urban core. The final right-of-way width should be as small as possible, but no larger than the width of Fort Washington Way—about 155 feet. The local street grid should remain at-grade and extend over the interstate, improving community connectivity between currently bifurcated neighborhoods. This concept borrows from the successful urban interstate upgrades that have been ongoing in Columbus, Ohio, for years. Connections should be made across the interstate between as many local east-west streets as possible, but especially between West 4th, 5th, 6th, and 7th Streets. Improved connections should also be made north-south between Queensgate and the West End. Local streets extending over the interstate should be designed in a way that is safe and enjoyable for pedestrians and cyclists to navigate, and in a way that is supportive of local businesses

and conducive to urban development. A good example of this is the completed East Long Street bridge crossing I-71 in Columbus, Ohio. With respect to land recapture thanks to Project footprint reduction, enough space should be returned to local control to facilitate the development of long-awaited regional priorities: an expanded convention center and a new arena. Optimally, the interstate would be capped for as much road-length as possible between West 3rd Street and Ezzard Charles Drive. Interstate entry and exit ramps (i.e. Access Points) in the urban core should be simplified and consolidated, thereby reducing intensive, high-speed design requirements. Traffic forecasting assumptions used to drive the Project's design should be validated by actual, historic traffic counts and trends. Design input data could be based on a hybrid of econometric and network-based models built to consider individual trips that shift to other corridors, occur at a different time of day, involve a different mode of transportation, or disappear due to telecommuting or a shifted trip. When the specifics of these priorities cannot be implemented, the spirit of them should be.

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A custom procurement and design-build processes can accommodate a design recalibration while maintaining Project budget and schedule:

Although ODOT selected a Preferred Alternative design in 2011, upgrades to this design are achievable before the Project’s targeted groundbreaking in late 2023. As local business, community, and political leaders, we expect flexibility from ODOT in designing the procurement process. ODOT should adopt a custom, alternative design-build process that is responsive to the community priorities established here. Such a procurement should be performance- and value-based rather than prescriptive, in the sense that the designer/contracting community should be proactively incentivized to provide creative alternative design concepts, not adhering to Alternative I-W, which are responsive to community goals. Importantly, in this value- and performance-based procurement, community

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John Laber-Kirst

jlaberkirst@gmail.com

7282 Lawyer Rd
Cincinnati, Ohio 45244

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From: J.P. Burleigh <info@sg.actionnetwork.org>
Sent: Thursday, September 8, 2022 9:26 AM
To: Spinosa, Stefan
Subject: Brent Spence Bridge Project - Reconnecting Cincinnati Westway Design Improvements

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J.P. Burleigh

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3760 Indianview Avenue
Cincinnati, Ohio 45227

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From: Alex Hemmer <info@sg.actionnetwork.org>
Sent: Thursday, September 8, 2022 2:12 PM
To: Spinosa, Stefan
Subject: Brent Spence Bridge Project - Reconnecting Cincinnati Westway Design Improvements

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859hemmer@gmail.com
3675 Grape Street, Unit A
Denver, Colorado 80207

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Sent: Thursday, September 8, 2022 9:12 AM
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Specific design recommendations capturing the spirit of community priorities:

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Thank you for your attention. I pledge to support ODOT and the entire Cincinnati region in shaping the Project procurement process and ultimate end-product of this Project to all parties' best interests.

Very Respectfully,
Jason Taylor

Jason Taylor
jason.taylor@yahoo.com
243 Foote Ave
Bellevue, Kentucky 41073

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From: Jackie Smith <info@sg.actionnetwork.org>
Sent: Saturday, September 10, 2022 4:03 PM
To: Spinosa, Stefan
Subject: Brent Spence Bridge Project - Reconnecting Cincinnati Westway Design Improvements

ODOT Capital Programs Administrator Stefan Spinosa,

I am thrilled that the Brent Spence Bridge Corridor Project (“the Project”) is gaining momentum, as a result of the recently passed Bipartisan Infrastructure Law. Earnestly, thank you for your leadership to date in helping our community reach this point. However, given the massive sum of federal and state funding being committed to the Cincinnati region for this Project, and given the lengthy lifespan of the physical infrastructure to be constructed, I am advocating for design improvements to the Project, so that the Cincinnati region receives the best possible outcome.

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Thank you for your attention. I pledge to support ODOT and the entire Cincinnati region in shaping the Project procurement process and ultimate end-product of this Project to all parties' best interests.

I grew up in Cincinnati from 1930 until 1951. I saw how downtown and the West End looked before the interstate. The beauty. And going around on the streetcar. And coming back here

for a visit made me see how much was lost. This plan will make Cincinnati much better in the future.

Very Respectfully,

Jackie Smith

Jackie Smith

jjunkersmith@aol.com

75-6081 Alii Dr

Kona, Hawaii 96740

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From: Chip Laber-Kirst <info@sg.actionnetwork.org>
Sent: Sunday, September 11, 2022 12:37 PM
To: Spinosa, Stefan
Subject: Brent Spence Bridge Project - Reconnecting Cincinnati Westway Design Improvements

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Chip Laber-Kirst
claberkirst@gmail.com

7282 Lawyer Road
Cincinnati , Ohio 45244

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From: Kevin Zalac <info@sg.actionnetwork.org>
Sent: Sunday, September 11, 2022 2:23 PM
To: Spinosa, Stefan
Subject: Brent Spence Bridge Project - Reconnecting Cincinnati Westway Design Improvements

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Local stakeholders – especially the City of Cincinnati – should have power throughout the procurement and progressive design-build decision-making process:

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Thank you for your attention. I pledge to support ODOT and the entire Cincinnati region in shaping the Project procurement process and ultimate end-product of this Project to all parties' best interests.

Very Respectfully,

Kevin Zalac, PharmD

Kevin Zalac
zalackj@mail.uc.edu
4127 Lakeman St
Cincinnati, Ohio 45223

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From: Richard Wendel III <info@sg.actionnetwork.org>
Sent: Friday, September 23, 2022 10:23 AM
To: Spinosa, Stefan
Subject: Brent Spence Bridge Project - Reconnecting Cincinnati Westway Design Improvements

ODOT Capital Programs Administrator Stefan Spinosa,

I am thrilled that the Brent Spence Bridge Corridor Project (“the Project”) is gaining momentum, as a result of the recently passed Bipartisan Infrastructure Law. Earnestly, thank you for your leadership to date in helping our community reach this point. However, given the massive sum of federal and state funding being committed to the Cincinnati region for this Project, and given the lengthy lifespan of the physical infrastructure to be constructed, I am advocating for design improvements to the Project, so that the Cincinnati region receives the best possible outcome.

Before us stands a once-in-a-generation opportunity to shape the Cincinnati region’s relationship to its infrastructure. Unfortunately, the currently proposed Project design – Alternative I-W – which was substantially developed in 2012 and 2013, does not provide the best possible life-cycle value to local, public owners and end users of the Project. However, a Project design that reduces the interstate’s footprint and that improves local community connectivity between the east and west sides of I-75 would provide huge economic benefits to the Cincinnati region, thereby maximizing return on investment in this Project. The Cincinnati region should not miss out on this tremendous opportunity to advance its long-term urban and economic development priorities.

Generally, I advocate that the design of the Project be recalibrated in a way that:

- Does not require adherence to the Alternative I-W design
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CBD to Queensgate and the West End via multiple modes, and in order to accommodate local, urban, street-facing development

While I appreciate that the Project's current design has a smaller right-of-way footprint than previous designs, I believe that additional, significant right-of-way reduction is crucial to the future of Cincinnati. With a landlocked urban core, Cincinnati's urban growth is currently constrained, and the Cincinnati region's ability to develop urban-core real estate for those who desire it – including businesses and top talent – is limited. Improved connectivity between the east and west sides of I-75 in Cincinnati is also crucial. I believe a design solution that not only reduces the right-of-way footprint, but that also includes a recessed highway and an overpassing local street grid, like that of ODOT's celebrated Fort Washington Way project, is just one example of a configuration that could deliver important priorities for the community: the expansion of Cincinnati's central business district to the west of I-75, the elevation of the highest and best use for land, and the creation of new opportunities for economic development and housing development. Any design solution that is implemented should deliver these priorities and facilitate a new/improved network of safe, multi-modal local streets connecting the Cincinnati CBD, Queensgate, the West End, and Covington.

Specific design recommendations capturing the spirit of community priorities:

Specifically, I advocate for the alignment of the interstate through the urban core, from West 3rd Street to Ezzard Charles Drive, to be lowered and consolidated in a way that minimizes right-of-way width, thereby creating space for development and reducing environmental impacts of the interstate on the urban core. The final right-of-way width should be as small as possible, but no larger than the width of Fort Washington Way—about 155 feet. The local street grid should remain at-grade and extend over the interstate, improving community connectivity between currently bifurcated neighborhoods. This concept borrows from the successful urban interstate upgrades that have been ongoing in Columbus, Ohio, for years. Connections should be made across the interstate between as many local east-west streets as possible, but especially between West 4th, 5th, 6th, and 7th Streets. Improved connections should also be made north-south between Queensgate and the West End. Local streets extending over the interstate should be designed in a way that is safe and enjoyable for pedestrians and cyclists to navigate, and in a way that is supportive of local businesses

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Richard Wendel III

Richard Wendel III
richard.wendel@hey.com
427 Delta Ave.
Cincinnati, Ohio 45226

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From: Tony Bartish <info@sg.actionnetwork.org>
Sent: Tuesday, October 4, 2022 3:03 PM
To: Spinosa, Stefan
Subject: Brent Spence Bridge Project - Reconnecting Cincinnati Westway Design Improvements

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To: Spinosa, Stefan
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Finally, having an engaged local owner of the Project will be critical to the success of Project design improvements. The City of Cincinnati and Hamilton County should be engaged as active partners to ODOT throughout the Project procurement, progressive design, and implementation processes. Specifically, the City of Cincinnati and Hamilton County should become Cooperating Agencies as well as have key votes during the selection of the design-build contractor and associated design concept. A new, City-assembled committee of local stakeholders should help craft the procurement documents and scoring criteria for the selection of a design-builder. Recently, the City of Covington and KYTC signed two MOUs related to the Project, giving Covington a significant seat at the table. ODOT, the City of Cincinnati, and Hamilton County should take similar steps.

Thank you for your attention. I pledge to support ODOT and the entire Cincinnati region in shaping the Project procurement process and ultimate end-product of this Project to all parties' best interests.

Very Respectfully,

Andrea Benton
cincyrealm@yahoo.com

111 Towne Commons Way
Cincinnati, Ohio 45215

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From: Tom Blumena <info@sg.actionnetwork.org>
Sent: Tuesday, October 4, 2022 6:23 PM
To: Spinosa, Stefan
Subject: Brent Spence Bridge Project - Reconnecting Cincinnati Westway Design Improvements

ODOT Capital Programs Administrator Stefan Spinosa,

I am thrilled that the Brent Spence Bridge Corridor Project (“the Project”) is gaining momentum, as a result of the recently passed Bipartisan Infrastructure Law. Earnestly, thank you for your leadership to date in helping our community reach this point. However, given the massive sum of federal and state funding being committed to the Cincinnati region for this Project, and given the lengthy lifespan of the physical infrastructure to be constructed, I am advocating for design improvements to the Project, so that the Cincinnati region receives the best possible outcome.

Before us stands a once-in-a-generation opportunity to shape the Cincinnati region’s relationship to its infrastructure. Unfortunately, the currently proposed Project design – Alternative I-W – which was substantially developed in 2012 and 2013, does not provide the best possible life-cycle value to local, public owners and end users of the Project. However, a Project design that reduces the interstate’s footprint and that improves local community connectivity between the east and west sides of I-75 would provide huge economic benefits to the Cincinnati region, thereby maximizing return on investment in this Project. The Cincinnati region should not miss out on this tremendous opportunity to advance its long-term urban and economic development priorities.

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- Does not require adherence to the Alternative I-W design
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- Lowers the alignment of I-75 through the urban core of Cincinnati, much like that of Fort Washington Way
- Extends the local street grid over the interstate in order to better connect the Cincinnati

CBD to Queensgate and the West End via multiple modes, and in order to accommodate local, urban, street-facing development

While I appreciate that the Project's current design has a smaller right-of-way footprint than previous designs, I believe that additional, significant right-of-way reduction is crucial to the future of Cincinnati. With a landlocked urban core, Cincinnati's urban growth is currently constrained, and the Cincinnati region's ability to develop urban-core real estate for those who desire it – including businesses and top talent – is limited. Improved connectivity between the east and west sides of I-75 in Cincinnati is also crucial. I believe a design solution that not only reduces the right-of-way footprint, but that also includes a recessed highway and an overpassing local street grid, like that of ODOT's celebrated Fort Washington Way project, is just one example of a configuration that could deliver important priorities for the community: the expansion of Cincinnati's central business district to the west of I-75, the elevation of the highest and best use for land, and the creation of new opportunities for economic development and housing development. Any design solution that is implemented should deliver these priorities and facilitate a new/improved network of safe, multi-modal local streets connecting the Cincinnati CBD, Queensgate, the West End, and Covington.

Specific design recommendations capturing the spirit of community priorities:

Specifically, I advocate for the alignment of the interstate through the urban core, from West 3rd Street to Ezzard Charles Drive, to be lowered and consolidated in a way that minimizes right-of-way width, thereby creating space for development and reducing environmental impacts of the interstate on the urban core. The final right-of-way width should be as small as possible, but no larger than the width of Fort Washington Way—about 155 feet. The local street grid should remain at-grade and extend over the interstate, improving community connectivity between currently bifurcated neighborhoods. This concept borrows from the successful urban interstate upgrades that have been ongoing in Columbus, Ohio, for years. Connections should be made across the interstate between as many local east-west streets as possible, but especially between West 4th, 5th, 6th, and 7th Streets. Improved connections should also be made north-south between Queensgate and the West End. Local streets extending over the interstate should be designed in a way that is safe and enjoyable for pedestrians and cyclists to navigate, and in a way that is supportive of local businesses

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Tom Blumena
blumes33@gmail.com

731 Philadelphia St
Covington, Kentucky 41011

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From: Joe Bredestege <info@sg.actionnetwork.org>
Sent: Tuesday, October 4, 2022 2:27 PM
To: Spinosa, Stefan
Subject: Brent Spence Bridge Project - Reconnecting Cincinnati Westway Design Improvements

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Joe Bredestege

Joe Bredestege
jackoweese@gmail.com
1162 Rulison Ave
Cincinnati, Ohio 45238

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From: Burns Kayla <info@sg.actionnetwork.org>
Sent: Tuesday, October 4, 2022 10:09 PM
To: Spinosa, Stefan
Subject: Brent Spence Bridge Project - Reconnecting Cincinnati Westway Design Improvements

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Kayla

Burns Kayla
kaylabri95@gmail.com
1447 Walnut St Unit 201
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From: Ryan Cook <info@sg.actionnetwork.org>
Sent: Tuesday, October 4, 2022 2:36 PM
To: Spinosa, Stefan
Subject: Brent Spence Bridge Project - Reconnecting Cincinnati Westway Design Improvements

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Ryan Cook

Ryan Cook
Rmcook13@gmail.com
983 Windsor Street
Cincinnati, Ohio 45206

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From: Anna Derksen <info@sg.actionnetwork.org>
Sent: Tuesday, October 4, 2022 7:05 PM
To: Spinosa, Stefan
Subject: Brent Spence Bridge Project - Reconnecting Cincinnati Westway Design Improvements

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Anna Derksen
annaelisek04@gmail.com

3428 N Club Crest Ave
Cincinnati, Ohio 45209

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From: Barbara Didrichsen <info@sg.actionnetwork.org>
Sent: Tuesday, October 4, 2022 10:50 PM
To: Spinosa, Stefan
Subject: Brent Spence Bridge Project - Reconnecting Cincinnati Westway Design Improvements

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Barbara Didrichsen
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Thank you for your attention. I pledge to support ODOT and the entire Cincinnati region in shaping the Project procurement process and ultimate end-product of this Project to all parties' best interests.

Very Respectfully,

Aaron Earlywine
ajearlywine@gmail.com

341 Cassatt St
Cincinnati, Ohio 45219

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From: Katelyn Elliott <info@sg.actionnetwork.org>
Sent: Tuesday, October 4, 2022 3:38 PM
To: Spinosa, Stefan
Subject: Brent Spence Bridge Project - Reconnecting Cincinnati Westway Design Improvements

ODOT Capital Programs Administrator Stefan Spinosa,

I am thrilled that the Brent Spence Bridge Corridor Project (“the Project”) is gaining momentum, as a result of the recently passed Bipartisan Infrastructure Law. Earnestly, thank you for your leadership to date in helping our community reach this point. However, given the massive sum of federal and state funding being committed to the Cincinnati region for this Project, and given the lengthy lifespan of the physical infrastructure to be constructed, I am advocating for design improvements to the Project, so that the Cincinnati region receives the best possible outcome.

Before us stands a once-in-a-generation opportunity to shape the Cincinnati region’s relationship to its infrastructure. Unfortunately, the currently proposed Project design – Alternative I-W – which was substantially developed in 2012 and 2013, does not provide the best possible life-cycle value to local, public owners and end users of the Project. However, a Project design that reduces the interstate’s footprint and that improves local community connectivity between the east and west sides of I-75 would provide huge economic benefits to the Cincinnati region, thereby maximizing return on investment in this Project. The Cincinnati region should not miss out on this tremendous opportunity to advance its long-term urban and economic development priorities.

Generally, I advocate that the design of the Project be recalibrated in a way that:

- Does not require adherence to the Alternative I-W design
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- Reduces the footprint of the interstate and associated ramping
- Lowers the alignment of I-75 through the urban core of Cincinnati, much like that of Fort Washington Way
- Extends the local street grid over the interstate in order to better connect the Cincinnati

CBD to Queensgate and the West End via multiple modes, and in order to accommodate local, urban, street-facing development

While I appreciate that the Project's current design has a smaller right-of-way footprint than previous designs, I believe that additional, significant right-of-way reduction is crucial to the future of Cincinnati. With a landlocked urban core, Cincinnati's urban growth is currently constrained, and the Cincinnati region's ability to develop urban-core real estate for those who desire it – including businesses and top talent – is limited. Improved connectivity between the east and west sides of I-75 in Cincinnati is also crucial. I believe a design solution that not only reduces the right-of-way footprint, but that also includes a recessed highway and an overpassing local street grid, like that of ODOT's celebrated Fort Washington Way project, is just one example of a configuration that could deliver important priorities for the community: the expansion of Cincinnati's central business district to the west of I-75, the elevation of the highest and best use for land, and the creation of new opportunities for economic development and housing development. Any design solution that is implemented should deliver these priorities and facilitate a new/improved network of safe, multi-modal local streets connecting the Cincinnati CBD, Queensgate, the West End, and Covington.

Specific design recommendations capturing the spirit of community priorities:

Specifically, I advocate for the alignment of the interstate through the urban core, from West 3rd Street to Ezzard Charles Drive, to be lowered and consolidated in a way that minimizes right-of-way width, thereby creating space for development and reducing environmental impacts of the interstate on the urban core. The final right-of-way width should be as small as possible, but no larger than the width of Fort Washington Way—about 155 feet. The local street grid should remain at-grade and extend over the interstate, improving community connectivity between currently bifurcated neighborhoods. This concept borrows from the successful urban interstate upgrades that have been ongoing in Columbus, Ohio, for years. Connections should be made across the interstate between as many local east-west streets as possible, but especially between West 4th, 5th, 6th, and 7th Streets. Improved connections should also be made north-south between Queensgate and the West End. Local streets extending over the interstate should be designed in a way that is safe and enjoyable for pedestrians and cyclists to navigate, and in a way that is supportive of local businesses

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Thank you for your attention.

Very Respectfully,

Katelyn Elliott
moweryke@gmail.com

219 Crim St
Bowling Green, Ohio 43402

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From: JOSEPH GASTENVELD <info@sg.actionnetwork.org>
Sent: Tuesday, October 4, 2022 2:44 PM
To: Spinosa, Stefan
Subject: Brent Spence Bridge Project - Reconnecting Cincinnati Westway Design Improvements

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I am thrilled that the Brent Spence Bridge Corridor Project (“the Project”) is gaining momentum, as a result of the recently passed Bipartisan Infrastructure Law. Earnestly, thank you for your leadership to date in helping our community reach this point. However, given the massive sum of federal and state funding being committed to the Cincinnati region for this Project, and given the lengthy lifespan of the physical infrastructure to be constructed, I am advocating for design improvements to the Project, so that the Cincinnati region receives the best possible outcome.

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Very Respectfully,

Joseph Gastenveld

JOSEPH GASTENVELD
joegastenveld@gmail.com
1421 Race St, APT 1
Cincinnati, Ohio 45202

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From: Andrew Hanes-Bagford <info@sg.actionnetwork.org>
Sent: Tuesday, October 4, 2022 3:27 PM
To: Spinosa, Stefan
Subject: Please consider the future of Cincinnati's cityscape and walkability - Brent Spence Bridge Project

ODOT Capital Programs Administrator Stefan Spinosa,

Cincinnati has a beautiful riverscape and a downtown area that has seen vast growth in commerce and social activity over the past decade. The main obstacle to improving our city is the Interstate running through the heart of downtown, and dividing areas of our city as a result.

Before us stands a once-in-a-generation opportunity to shape the Cincinnati region's relationship to its infrastructure. Unfortunately, the currently proposed Project design – Alternative I-W – which was substantially developed in 2012 and 2013, does not provide the best possible life-cycle value to local, public owners and end users of the Project. However, a Project design that reduces the interstate's footprint and that improves local community connectivity between the east and west sides of I-75 would provide huge economic benefits to the Cincinnati region, thereby maximizing return on investment in this Project. The Cincinnati region should not miss out on this tremendous opportunity to advance its long-term urban and economic development priorities.

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Finally, having an engaged local owner of the Project will be critical to the success of Project design improvements. The City of Cincinnati and Hamilton County should be engaged as active partners to ODOT throughout the Project procurement, progressive design, and implementation processes. Specifically, the City of Cincinnati and Hamilton County should become Cooperating Agencies as well as have key votes during the selection of the design-build contractor and associated design concept. A new, City-assembled committee of local stakeholders should help craft the procurement documents and scoring criteria for the selection of a design-builder. Recently, the City of Covington and KYTC signed two MOUs related to the Project, giving Covington a significant seat at the table. ODOT, the City of Cincinnati, and Hamilton County should take similar steps.

Thank you for your attention. I pledge to support ODOT and the entire Cincinnati region in shaping the Project procurement process and ultimate end-product of this Project to all parties' best interests.

Very Respectfully,

A. Corbin Bagford
Proud Cincinnati and Ohioan

Andrew Hanes-Bagford
cbagford@kenwoodcc.com
11025 Labelle Ave.
Blue Ash, Ohio 45242

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From: Oliver Igney <info@sg.actionnetwork.org>
Sent: Tuesday, October 4, 2022 3:38 PM
To: info@brentspencebridgecorridor.com
Subject: Brent Spence Bridge Project - Design Improvements

Bi-State Management Team,

I am advocating that the Cincinnati region receive the best possible outcome from the Brent Spence Bridge Project (“the Project”). Such an outcome would achieve more than simply congestion reduction and safety improvements on the interstate. I support a recalibrated design for the Brent Spence Bridge Project (“the Project”) that follows the general vision of Reconnecting Cincinnati Westway. Specifically, I believe that the design of the Project should be recalibrated in a way that:

- Does not require adherence to the Alternative I-W design;
- Expands the original Purpose & Needs Statement to include the community priorities established here as primary goals;
- Reduces the footprint of the interstate and associated ramping in a way that provides enough land recapture to accommodate long-standing economic development goals such as an expanded convention center or a new arena;
- Minimizes the impact of the interstate and minimizes the barrier created by the interstate, perhaps by lowering the alignment of I-75 through the urban core of Cincinnati, much like that of Fort Washington Way;
- Extends the local street grid over the interstate in order to better connect the Cincinnati CBD to Queensgate and the West End via multiple modes, and in order to accommodate local, urban, street-facing development;
- When the specifics of these priorities cannot be accommodated, the spirit and the magnitude of them should be.

In order to accomplish this, design-builders should be incentivized to propose alternative designs addressing these priorities. The City of Cincinnati should be involved throughout the entire procurement and design process of the Project, and should have a key vote in selecting the design-builder of the Project.

Thank you for your attention. I pledge to support ODOT and the entire Cincinnati region in shaping the Project procurement process and ultimate end-product of this Project to all parties' best interests.

Very Respectfully,

Oliver Igney
oliverigney@gmail.com
4150 Marathon Street
Los Angeles , California 90029

From: Joe Mak <info@sg.actionnetwork.org>
Sent: Tuesday, October 4, 2022 8:05 PM
To: Spinosa, Stefan
Subject: Brent Spence Bridge Project - Reconnecting Cincinnati Westway Design Improvements

ODOT Capital Programs Administrator Stefan Spinosa,

I am thrilled that the Brent Spence Bridge Corridor Project (“the Project”) is gaining momentum, as a result of the recently passed Bipartisan Infrastructure Law. Earnestly, thank you for your leadership to date in helping our community reach this point. However, given the massive sum of federal and state funding being committed to the Cincinnati region for this Project, and given the lengthy lifespan of the physical infrastructure to be constructed, I am advocating for design improvements to the Project, so that the Cincinnati region receives the best possible outcome.

Before us stands a once-in-a-generation opportunity to shape the Cincinnati region’s relationship to its infrastructure. Unfortunately, the currently proposed Project design – Alternative I-W – which was substantially developed in 2012 and 2013, does not provide the best possible life-cycle value to local, public owners and end users of the Project. However, a Project design that reduces the interstate’s footprint and that improves local community connectivity between the east and west sides of I-75 would provide huge economic benefits to the Cincinnati region, thereby maximizing return on investment in this Project. The Cincinnati region should not miss out on this tremendous opportunity to advance its long-term urban and economic development priorities.

Generally, I advocate that the design of the Project be recalibrated in a way that:

- Does not require adherence to the Alternative I-W design
- Expands the original Purpose & Needs Statement to include the community priorities established here as primary goals
- Reduces the footprint of the interstate and associated ramping
- Lowers the alignment of I-75 through the urban core of Cincinnati, much like that of Fort Washington Way
- Extends the local street grid over the interstate in order to better connect the Cincinnati

CBD to Queensgate and the West End via multiple modes, and in order to accommodate local, urban, street-facing development

While I appreciate that the Project's current design has a smaller right-of-way footprint than previous designs, I believe that additional, significant right-of-way reduction is crucial to the future of Cincinnati. With a landlocked urban core, Cincinnati's urban growth is currently constrained, and the Cincinnati region's ability to develop urban-core real estate for those who desire it – including businesses and top talent – is limited. Improved connectivity between the east and west sides of I-75 in Cincinnati is also crucial. I believe a design solution that not only reduces the right-of-way footprint, but that also includes a recessed highway and an overpassing local street grid, like that of ODOT's celebrated Fort Washington Way project, is just one example of a configuration that could deliver important priorities for the community: the expansion of Cincinnati's central business district to the west of I-75, the elevation of the highest and best use for land, and the creation of new opportunities for economic development and housing development. Any design solution that is implemented should deliver these priorities and facilitate a new/improved network of safe, multi-modal local streets connecting the Cincinnati CBD, Queensgate, the West End, and Covington.

Specific design recommendations capturing the spirit of community priorities:

Specifically, I advocate for the alignment of the interstate through the urban core, from West 3rd Street to Ezzard Charles Drive, to be lowered and consolidated in a way that minimizes right-of-way width, thereby creating space for development and reducing environmental impacts of the interstate on the urban core. The final right-of-way width should be as small as possible, but no larger than the width of Fort Washington Way—about 155 feet. The local street grid should remain at-grade and extend over the interstate, improving community connectivity between currently bifurcated neighborhoods. This concept borrows from the successful urban interstate upgrades that have been ongoing in Columbus, Ohio, for years. Connections should be made across the interstate between as many local east-west streets as possible, but especially between West 4th, 5th, 6th, and 7th Streets. Improved connections should also be made north-south between Queensgate and the West End. Local streets extending over the interstate should be designed in a way that is safe and enjoyable for pedestrians and cyclists to navigate, and in a way that is supportive of local businesses

and conducive to urban development. A good example of this is the completed East Long Street bridge crossing I-71 in Columbus, Ohio. With respect to land recapture thanks to Project footprint reduction, enough space should be returned to local control to facilitate the development of long-awaited regional priorities: an expanded convention center and a new arena. Optimally, the interstate would be capped for as much road-length as possible between West 3rd Street and Ezzard Charles Drive. Interstate entry and exit ramps (i.e. Access Points) in the urban core should be simplified and consolidated, thereby reducing intensive, high-speed design requirements. Traffic forecasting assumptions used to drive the Project's design should be validated by actual, historic traffic counts and trends. Design input data could be based on a hybrid of econometric and network-based models built to consider individual trips that shift to other corridors, occur at a different time of day, involve a different mode of transportation, or disappear due to telecommuting or a shifted trip. When the specifics of these priorities cannot be implemented, the spirit of them should be.

These priorities can be incorporated within the scope of projects awarded funding through INFRA and/or Mega grants:

The Project priorities for which I am advocating are extremely consistent with USDOT's stated priorities for its INFRA and Mega grant programs, which are likely to fund the Project. The outcomes and design elements advocated for in this letter should be squarely within the scope of the Project, as made clear by the following passages in the grant programs' Notice of Funding Opportunity (NOFO):

- "Support integrated land use, economic development and transportation planning to improve the movement of people ... facilitate greater public and private investments and strategies in land-use productivity, ... [and] increase in the production or preservation of location-efficient housing"
- "Reduce transportation and housing cost burdens, including through public and private investments to support greater commercial and mixed-income residential development near public transportation ... in walkable neighborhoods"
- "Increase the walkability and accessibility for pedestrians and encourage thriving communities for individuals to work, live, and play by creating transportation choices for individuals to move freely with or without a car"

- “Improved multimodal and transit access across previously bifurcated disadvantaged neighborhoods”
- “Include[] physical-barrier-mitigating land bridges, caps, lids, linear parks, and multimodal mobility investments that either redress past barriers to opportunity or that proactively create new connections and opportunities for underserved communities that are underserved by transportation”
- “Include[] new or improved walking and bicycling infrastructure [and] reduce[] automobile dependence”
- “Address equity and barriers to opportunity, improve quality of life in ... urbanized areas, and benefit Historically Disadvantaged Communities”

o Note: Hamilton County, Ohio, Census Tracts 2 and 263 – both within the Project footprint, with one on each side of I-75 – are considered Transportation Disadvantaged, and Tract 2 is considered a Historically Disadvantaged Community by USDOT. Additionally Tract 2 and Tract 264, which is also adjacent to the Project, are both HUD-designated Racially or Ethnically Concentrated Areas of Poverty (R/ECAPs).

o The Project presents an opportunity for restorative justice. The construction of the Brent Spence Bridge corridor and the accompanying Queensgate industrial area in the early 1960’s eliminated a dense urban neighborhood and displaced 25,737 residents, 25,155 of whom were non-white. 49 CFR 21.5(b)(7) sets an expectation that the relevant agency “must take affirmative action to remove or overcome the effects of the prior discriminatory practice.”

A custom procurement and design-build processes can accommodate a design recalibration while maintaining Project budget and schedule:

Although ODOT selected a Preferred Alternative design in 2011, upgrades to this design are achievable before the Project’s targeted groundbreaking in late 2023. As local business, community, and political leaders, we expect flexibility from ODOT in designing the procurement process. ODOT should adopt a custom, alternative design-build process that is responsive to the community priorities established here. Such a procurement should be performance- and value-based rather than prescriptive, in the sense that the designer/contracting community should be proactively incentivized to provide creative alternative design concepts, not adhering to Alternative I-W, which are responsive to community goals. Importantly, in this value- and performance-based procurement, community

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Thank you for your attention. I pledge to support ODOT and the entire Cincinnati region in shaping the Project procurement process and ultimate end-product of this Project to all parties' best interests.

Very Respectfully,
Joe Mak

Joe Mak
jmak2222@gmail.com
426 Ada St
Cincinnati, Ohio 45219

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From: James McDermott <info@sg.actionnetwork.org>
Sent: Tuesday, October 4, 2022 6:00 PM
To: Spinosa, Stefan
Subject: Brent Spence Bridge Project - Reconnecting Cincinnati Westway Design Improvements

ODOT Capital Programs Administrator Stefan Spinosa,

I am thrilled that the Brent Spence Bridge Corridor Project (“the Project”) is gaining momentum, as a result of the recently passed Bipartisan Infrastructure Law. Earnestly, thank you for your leadership to date in helping our community reach this point. However, given the massive sum of federal and state funding being committed to the Cincinnati region for this Project, and given the lengthy lifespan of the physical infrastructure to be constructed, I am advocating for design improvements to the Project, so that the Cincinnati region receives the best possible outcome.

Before us stands a once-in-a-generation opportunity to shape the Cincinnati region’s relationship to its infrastructure. Unfortunately, the currently proposed Project design – Alternative I-W – which was substantially developed in 2012 and 2013, does not provide the best possible life-cycle value to local, public owners and end users of the Project. However, a Project design that reduces the interstate’s footprint and that improves local community connectivity between the east and west sides of I-75 would provide huge economic benefits to the Cincinnati region, thereby maximizing return on investment in this Project. The Cincinnati region should not miss out on this tremendous opportunity to advance its long-term urban and economic development priorities.

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- Lowers the alignment of I-75 through the urban core of Cincinnati, much like that of Fort Washington Way
- Extends the local street grid over the interstate in order to better connect the Cincinnati

CBD to Queensgate and the West End via multiple modes, and in order to accommodate local, urban, street-facing development

While I appreciate that the Project's current design has a smaller right-of-way footprint than previous designs, I believe that additional, significant right-of-way reduction is crucial to the future of Cincinnati. With a landlocked urban core, Cincinnati's urban growth is currently constrained, and the Cincinnati region's ability to develop urban-core real estate for those who desire it – including businesses and top talent – is limited. Improved connectivity between the east and west sides of I-75 in Cincinnati is also crucial. I believe a design solution that not only reduces the right-of-way footprint, but that also includes a recessed highway and an overpassing local street grid, like that of ODOT's celebrated Fort Washington Way project, is just one example of a configuration that could deliver important priorities for the community: the expansion of Cincinnati's central business district to the west of I-75, the elevation of the highest and best use for land, and the creation of new opportunities for economic development and housing development. Any design solution that is implemented should deliver these priorities and facilitate a new/improved network of safe, multi-modal local streets connecting the Cincinnati CBD, Queensgate, the West End, and Covington.

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Specifically, I advocate for the alignment of the interstate through the urban core, from West 3rd Street to Ezzard Charles Drive, to be lowered and consolidated in a way that minimizes right-of-way width, thereby creating space for development and reducing environmental impacts of the interstate on the urban core. The final right-of-way width should be as small as possible, but no larger than the width of Fort Washington Way—about 155 feet. The local street grid should remain at-grade and extend over the interstate, improving community connectivity between currently bifurcated neighborhoods. This concept borrows from the successful urban interstate upgrades that have been ongoing in Columbus, Ohio, for years. Connections should be made across the interstate between as many local east-west streets as possible, but especially between West 4th, 5th, 6th, and 7th Streets. Improved connections should also be made north-south between Queensgate and the West End. Local streets extending over the interstate should be designed in a way that is safe and enjoyable for pedestrians and cyclists to navigate, and in a way that is supportive of local businesses

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These priorities can be incorporated within the scope of projects awarded funding through INFRA and/or Mega grants:

The Project priorities for which I am advocating are extremely consistent with USDOT's stated priorities for its INFRA and Mega grant programs, which are likely to fund the Project. The outcomes and design elements advocated for in this letter should be squarely within the scope of the Project, as made clear by the following passages in the grant programs' Notice of Funding Opportunity (NOFO):

- "Support integrated land use, economic development and transportation planning to improve the movement of people ... facilitate greater public and private investments and strategies in land-use productivity, ... [and] increase in the production or preservation of location-efficient housing"
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- “Address equity and barriers to opportunity, improve quality of life in ... urbanized areas, and benefit Historically Disadvantaged Communities”

o Note: Hamilton County, Ohio, Census Tracts 2 and 263 – both within the Project footprint, with one on each side of I-75 – are considered Transportation Disadvantaged, and Tract 2 is considered a Historically Disadvantaged Community by USDOT. Additionally Tract 2 and Tract 264, which is also adjacent to the Project, are both HUD-designated Racially or Ethnically Concentrated Areas of Poverty (R/ECAPs).

o The Project presents an opportunity for restorative justice. The construction of the Brent Spence Bridge corridor and the accompanying Queensgate industrial area in the early 1960’s eliminated a dense urban neighborhood and displaced 25,737 residents, 25,155 of whom were non-white. 49 CFR 21.5(b)(7) sets an expectation that the relevant agency “must take affirmative action to remove or overcome the effects of the prior discriminatory practice.”

A custom procurement and design-build processes can accommodate a design recalibration while maintaining Project budget and schedule:

Although ODOT selected a Preferred Alternative design in 2011, upgrades to this design are achievable before the Project’s targeted groundbreaking in late 2023. As local business, community, and political leaders, we expect flexibility from ODOT in designing the procurement process. ODOT should adopt a custom, alternative design-build process that is responsive to the community priorities established here. Such a procurement should be performance- and value-based rather than prescriptive, in the sense that the designer/contracting community should be proactively incentivized to provide creative alternative design concepts, not adhering to Alternative I-W, which are responsive to community goals. Importantly, in this value- and performance-based procurement, community

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Thank you for your attention. I pledge to support ODOT and the entire Cincinnati region in shaping the Project procurement process and ultimate end-product of this Project to all parties' best interests.

Very Respectfully, James McDermott

James McDermott
mcdermje@mail.uc.edu

2301 Auburn Avenue
Cincinnati, Ohio 45219

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From: Bryce Mortera <info@sg.actionnetwork.org>
Sent: Tuesday, October 4, 2022 6:43 PM
To: Spinosa, Stefan
Subject: Brent Spence Bridge Project - Reconnecting Cincinnati Westway Design Improvements

ODOT Capital Programs Administrator Stefan Spinosa,

I am thrilled that the Brent Spence Bridge Corridor Project (“the Project”) is gaining momentum, as a result of the recently passed Bipartisan Infrastructure Law. Earnestly, thank you for your leadership to date in helping our community reach this point. However, given the massive sum of federal and state funding being committed to the Cincinnati region for this Project, and given the lengthy lifespan of the physical infrastructure to be constructed, I am advocating for design improvements to the Project, so that the Cincinnati region receives the best possible outcome.

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Thank you for your attention. I pledge to support ODOT and the entire Cincinnati region in shaping the Project procurement process and ultimate end-product of this Project to all parties' best interests.

Very Respectfully,

Bryce Mortera
morterb@mail.uc.edu

2669 highland Ave
Cincinnati, Ohio 45219

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From: Jessie Nelson <info@sg.actionnetwork.org>
Sent: Tuesday, October 4, 2022 2:53 PM
To: Spinosa, Stefan
Subject: Brent Spence Bridge Project - Reconnecting Cincinnati Westway Design Improvements

ODOT Capital Programs Administrator Stefan Spinosa,

I am thrilled that the Brent Spence Bridge Corridor Project (“the Project”) is gaining momentum, as a result of the recently passed Bipartisan Infrastructure Law. Earnestly, thank you for your leadership to date in helping our community reach this point. However, given the massive sum of federal and state funding being committed to the Cincinnati region for this Project, and given the lengthy lifespan of the physical infrastructure to be constructed, I am advocating for design improvements to the Project, so that the Cincinnati region receives the best possible outcome.

Before us stands a once-in-a-generation opportunity to shape the Cincinnati region’s relationship to its infrastructure. Unfortunately, the currently proposed Project design – Alternative I-W – which was substantially developed in 2012 and 2013, does not provide the best possible life-cycle value to local, public owners and end users of the Project. However, a Project design that reduces the interstate’s footprint and that improves local community connectivity between the east and west sides of I-75 would provide huge economic benefits to the Cincinnati region, thereby maximizing return on investment in this Project. The Cincinnati region should not miss out on this tremendous opportunity to advance its long-term urban and economic development priorities.

Generally, I advocate that the design of the Project be recalibrated in a way that:

- Does not require adherence to the Alternative I-W design
- Expands the original Purpose & Needs Statement to include the community priorities established here as primary goals
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- Lowers the alignment of I-75 through the urban core of Cincinnati, much like that of Fort Washington Way
- Extends the local street grid over the interstate in order to better connect the Cincinnati

CBD to Queensgate and the West End via multiple modes, and in order to accommodate local, urban, street-facing development

While I appreciate that the Project's current design has a smaller right-of-way footprint than previous designs, I believe that additional, significant right-of-way reduction is crucial to the future of Cincinnati. With a landlocked urban core, Cincinnati's urban growth is currently constrained, and the Cincinnati region's ability to develop urban-core real estate for those who desire it – including businesses and top talent – is limited. Improved connectivity between the east and west sides of I-75 in Cincinnati is also crucial. I believe a design solution that not only reduces the right-of-way footprint, but that also includes a recessed highway and an overpassing local street grid, like that of ODOT's celebrated Fort Washington Way project, is just one example of a configuration that could deliver important priorities for the community: the expansion of Cincinnati's central business district to the west of I-75, the elevation of the highest and best use for land, and the creation of new opportunities for economic development and housing development. Any design solution that is implemented should deliver these priorities and facilitate a new/improved network of safe, multi-modal local streets connecting the Cincinnati CBD, Queensgate, the West End, and Covington.

Specific design recommendations capturing the spirit of community priorities:

Specifically, I advocate for the alignment of the interstate through the urban core, from West 3rd Street to Ezzard Charles Drive, to be lowered and consolidated in a way that minimizes right-of-way width, thereby creating space for development and reducing environmental impacts of the interstate on the urban core. The final right-of-way width should be as small as possible, but no larger than the width of Fort Washington Way—about 155 feet. The local street grid should remain at-grade and extend over the interstate, improving community connectivity between currently bifurcated neighborhoods. This concept borrows from the successful urban interstate upgrades that have been ongoing in Columbus, Ohio, for years. Connections should be made across the interstate between as many local east-west streets as possible, but especially between West 4th, 5th, 6th, and 7th Streets. Improved connections should also be made north-south between Queensgate and the West End. Local streets extending over the interstate should be designed in a way that is safe and enjoyable for pedestrians and cyclists to navigate, and in a way that is supportive of local businesses

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- "Support integrated land use, economic development and transportation planning to improve the movement of people ... facilitate greater public and private investments and strategies in land-use productivity, ... [and] increase in the production or preservation of location-efficient housing"
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A custom procurement and design-build processes can accommodate a design recalibration while maintaining Project budget and schedule:

Although ODOT selected a Preferred Alternative design in 2011, upgrades to this design are achievable before the Project’s targeted groundbreaking in late 2023. As local business, community, and political leaders, we expect flexibility from ODOT in designing the procurement process. ODOT should adopt a custom, alternative design-build process that is responsive to the community priorities established here. Such a procurement should be performance- and value-based rather than prescriptive, in the sense that the designer/contracting community should be proactively incentivized to provide creative alternative design concepts, not adhering to Alternative I-W, which are responsive to community goals. Importantly, in this value- and performance-based procurement, community

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Very Respectfully,

Jessie Nelson, resident of Cincinnati

Jessie Nelson

jnelson1419@gmail.com

2351 Beechmont Ave, Apt. 14

Cincinnati, Ohio 45230

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From: Ryan Ponti-Zins <info@sg.actionnetwork.org>
Sent: Tuesday, October 4, 2022 8:35 PM
To: Spinosa, Stefan
Subject: Brent Spence Bridge Project - Reconnecting Cincinnati Westway Design Improvements

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Ryan

Ryan Ponti-Zins
rpzohio@gmail.com
1432 elm st
Cincinnati, Ohio 45202

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From: Demi Schoenherr <info@sg.actionnetwork.org>
Sent: Tuesday, October 4, 2022 2:23 PM
To: Spinosa, Stefan
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Cincinnati's neglected and decimated neighborhoods deserve better.

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Very Respectfully,
Demi Schoenherr

OTR Resident
West End Employee
Cincinnati Lover

Demi Schoenherr
demis9876@gmail.com
1447 Walnut St
Cincinnati , Ohio 45202

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From: Rene Thibodeaux
Date: Sat, Oct 8, 2022 at 7:03 AM
Subject: I-75 Brent Spence Bridge Project and alternative Future Interstate Corridors

The Brent Spence Bridge Corridor Project now has a chance to start because of the Bipartisan Infrastructure Law. However, this project is only the beginning of the necessary interstate infrastructure projects not only in the Cincinnati region but for the interstate system in Ohio, Kentucky, and across the nation, as even this project will reach traffic capacity design limits in the next 20 years, truth both for I-75 and I-70. I am advocating planning for alternatives to Interstate 75 and Interstate 70, both transcontinental routes, that will interconnect Midwestern states to the ports of the Atlantic and Pacific more directly.

When the urban freeway systems were planned in the 1940's and 1950's, they were based on the existing 'spoke' system of streets radiating from the central business district to the suburbs. The only problem was the neighborhoods around the downtown were already decades old with irregular street grids. The Interstate Highway System planned in the 1940's were RURAL routes to connect as many cities as possible above the population of 50,000. Mayors and city councils would only provide political support if the Interstate System included their existing urban freeway plans. That required the urban freeways would have to retrofit into the existing irregular street grid. The only way possible was to cut through neighborhoods as the concept of converting existing arterial streets to freeways was rejected in favor of running between the existing streets in straight routes as possible. Most of the routes were build using eminent domain but others were cancelled after neighborhood revolts, leaving significant gaps. The inner urban routes were built, but mostly with too few lanes and no alternate routes because of the gaps from cancelled freeways. To make up for these limitations, transportation departments have no alternative but to widen the highways or build even more expensive metro rails or subways, which eventually reach full capacity.

Cities like Cincinnati and Columbus built beltways, but even these have reached their design limits. Unfortunately, ODOT and KYTC have limited options when it comes to improving I-75 capacity. The Brent Spence Bridge is now 60 years old and functionally obsolete, and MUST be replaced, no question. I-75 will have to be widened by at least one lane in either direction through Cincinnati. However, this will also reach its limit in 20 years. Some proposals like the 4-lane Eastern Bypass are expensive and functionally obsolete from the beginning. Building I-75 subsurface 60 years after it was first build will be excessively expensive and produce a highway with a dangerous steep grade between the bridge and the subsurface section. ODOT was correct to reject both plans.

The I-75 corridor through both Ohio and Kentucky is a freight-truck traffic corridors with increasing in traffic as population grow. It is time to return to the original purpose of the Interstate System, a system of rural routes, this time bypassing the largest cities to reduce traffic through cities. In this case, it means reviving the Future Interstate 73 concept, but not the route proposed through the Columbus area that was rejected, and not the Interstate 74 concept through Cincinnati to Virginia that was also rejected. Three Future Interstate Corridors should be created by Congress as alternative traffic routes for I-75 and I-70:

1) Future Interstate 73: Congress needs to modify the 1991 ISTEA High Priority Corridor 5 to create Future I-73 Corridor from Michigan, through Ohio, Kentucky, Virginia to Tennessee. Future I-73 would follow U.S. 31 or U.S. 131 in Michigan to South Bend IN, then Ft Wayne IN, U.S. 127 west of Dayton, or U.S. 68 Springfield OH east of Dayton, OH 73 from Wilmington OH to Portsmouth, U.S. 23 through eastern Kentucky, Virginia to I-26 in Kingsport TN forming a traffic corridor from the port of South Charleston SC to Michigan.

2) Future Interstate 66: The existing I-66 should be extended west from I-81/I-66 in Virginia, using the new U.S. 48 Corridor H to either U.S. 50 Parkersburg WV and U.S. 50 to U.S. 35 or U.S. 33 to U.S. 35

near Point Pleasant OH. Future I-66 would use the existing U.S. 35 freeway to Dayton and through Dayton to then connect to I-70 west of Dayton.

3) Future Interstate 50/Future Interstate 60/Future Interstate 70 TransAmerica Corridor: Congress created the 1991 ISTEA “TransAmerica” High Priority Corridor 3 from Virginia to Kansas. None of the corridor was ever build, including the Future I-66 Southern Kentucky Corridor. FHWA cancelled the last “Corridor 66” environmental study in 2015. Separately, work is continuing to complete U.S. 460 ADHS Corridor Q highway in Kentucky and Virginia and the U.S. 121 Coalfield Expressway in West Virginia and Virginia. Both the future U.S. 121 and future U.S. 460 could be part of a new TransAmerica Corridor. The Kansas Department of Transportation is also proceeding with widening and eventually converting U.S. 400/U.S. 50/U.S. 54 to a freeway/expressway from I-44 to Wichita and Southwest Kansas. U.S. 400 was created for the original TransAmerica Corridor. Missouri is also upgrading U.S. 60 to a freeway/expressway from Sikeston to Springfield. Congress should modify the 1991 ISTEA High Priority Corridor 5 from Virginia to California. The new TransAmerica Corridor should be designated as I-50 from Virginia to Missouri, I-60 from Missouri to Colorado, and the extension of I-70 from Utah to California to follow current Interstate designation policies of Federal law and the AASHTO.

These new corridors will offer opportunities to demonstrate future traffic and electrification options for the future. As an example, solar farms could be built maybe every 50 to 100 miles to provide clean renewable energy for electric vehicle charging, independent of any fossil fuel power plant and not burdening the local electrical grid. The solar farms could then be interconnected with high voltage transmission, which could in fact supply the local power grids also.

Sincerely,
Mr. Rene Thibodeaux
rene.thibodeaux.1@betageneral.com
info@futureinterstatecorridors.com
Tel: 937-813-7757
3619 Eileen Rd, Kettering, OH, USA
Kettering, Ohio 45429

From: Rakshit Chandrasaha <info@sg.actionnetwork.org>
Sent: Wednesday, October 5, 2022 11:56 AM
To: Spinosa, Stefan
Subject: Brent Spence Bridge Project - Reconnecting Cincinnati Westway Design Improvements

ODOT Capital Programs Administrator Stefan Spinosa,

I am thrilled that the Brent Spence Bridge Corridor Project (“the Project”) is gaining momentum, as a result of the recently passed Bipartisan Infrastructure Law. Earnestly, thank you for your leadership to date in helping our community reach this point. However, given the massive sum of federal and state funding being committed to the Cincinnati region for this Project, and given the lengthy lifespan of the physical infrastructure to be constructed, I am advocating for design improvements to the Project, so that the Cincinnati region receives the best possible outcome.

Before us stands a once-in-a-generation opportunity to shape the Cincinnati region’s relationship to its infrastructure. Unfortunately, the currently proposed Project design – Alternative I-W – which was substantially developed in 2012 and 2013, does not provide the best possible life-cycle value to local, public owners and end users of the Project. However, a Project design that reduces the interstate’s footprint and that improves local community connectivity between the east and west sides of I-75 would provide huge economic benefits to the Cincinnati region, thereby maximizing return on investment in this Project. The Cincinnati region should not miss out on this tremendous opportunity to advance its long-term urban and economic development priorities.

Generally, I advocate that the design of the Project be recalibrated in a way that:

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CBD to Queensgate and the West End via multiple modes, and in order to accommodate local, urban, street-facing development

While I appreciate that the Project's current design has a smaller right-of-way footprint than previous designs, I believe that additional, significant right-of-way reduction is crucial to the future of Cincinnati. With a landlocked urban core, Cincinnati's urban growth is currently constrained, and the Cincinnati region's ability to develop urban-core real estate for those who desire it – including businesses and top talent – is limited. Improved connectivity between the east and west sides of I-75 in Cincinnati is also crucial. I believe a design solution that not only reduces the right-of-way footprint, but that also includes a recessed highway and an overpassing local street grid, like that of ODOT's celebrated Fort Washington Way project, is just one example of a configuration that could deliver important priorities for the community: the expansion of Cincinnati's central business district to the west of I-75, the elevation of the highest and best use for land, and the creation of new opportunities for economic development and housing development. Any design solution that is implemented should deliver these priorities and facilitate a new/improved network of safe, multi-modal local streets connecting the Cincinnati CBD, Queensgate, the West End, and Covington.

Specific design recommendations capturing the spirit of community priorities:

Specifically, I advocate for the alignment of the interstate through the urban core, from West 3rd Street to Ezzard Charles Drive, to be lowered and consolidated in a way that minimizes right-of-way width, thereby creating space for development and reducing environmental impacts of the interstate on the urban core. The final right-of-way width should be as small as possible, but no larger than the width of Fort Washington Way—about 155 feet. The local street grid should remain at-grade and extend over the interstate, improving community connectivity between currently bifurcated neighborhoods. This concept borrows from the successful urban interstate upgrades that have been ongoing in Columbus, Ohio, for years. Connections should be made across the interstate between as many local east-west streets as possible, but especially between West 4th, 5th, 6th, and 7th Streets. Improved connections should also be made north-south between Queensgate and the West End. Local streets extending over the interstate should be designed in a way that is safe and enjoyable for pedestrians and cyclists to navigate, and in a way that is supportive of local businesses

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These priorities can be incorporated within the scope of projects awarded funding through INFRA and/or Mega grants:

The Project priorities for which I am advocating are extremely consistent with USDOT's stated priorities for its INFRA and Mega grant programs, which are likely to fund the Project. The outcomes and design elements advocated for in this letter should be squarely within the scope of the Project, as made clear by the following passages in the grant programs' Notice of Funding Opportunity (NOFO):

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o The Project presents an opportunity for restorative justice. The construction of the Brent Spence Bridge corridor and the accompanying Queensgate industrial area in the early 1960’s eliminated a dense urban neighborhood and displaced 25,737 residents, 25,155 of whom were non-white. 49 CFR 21.5(b)(7) sets an expectation that the relevant agency “must take affirmative action to remove or overcome the effects of the prior discriminatory practice.”

A custom procurement and design-build processes can accommodate a design recalibration while maintaining Project budget and schedule:

Although ODOT selected a Preferred Alternative design in 2011, upgrades to this design are achievable before the Project’s targeted groundbreaking in late 2023. As local business, community, and political leaders, we expect flexibility from ODOT in designing the procurement process. ODOT should adopt a custom, alternative design-build process that is responsive to the community priorities established here. Such a procurement should be performance- and value-based rather than prescriptive, in the sense that the designer/contracting community should be proactively incentivized to provide creative alternative design concepts, not adhering to Alternative I-W, which are responsive to community goals. Importantly, in this value- and performance-based procurement, community

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Finally, having an engaged local owner of the Project will be critical to the success of Project design improvements. The City of Cincinnati and Hamilton County should be engaged as active partners to ODOT throughout the Project procurement, progressive design, and implementation processes. Specifically, the City of Cincinnati and Hamilton County should become Cooperating Agencies as well as have key votes during the selection of the design-build contractor and associated design concept. A new, City-assembled committee of local stakeholders should help craft the procurement documents and scoring criteria for the selection of a design-builder. Recently, the City of Covington and KYTC signed two MOUs related to the Project, giving Covington a significant seat at the table. ODOT, the City of Cincinnati, and Hamilton County should take similar steps.

Thank you for your attention. I pledge to support ODOT and the entire Cincinnati region in shaping the Project procurement process and ultimate end-product of this Project to all parties' best interests.

Very Respectfully,

Rakshit Chandrasana
r211093@gmail.com

802 Autumn Ln
Mason, Ohio 45040

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From: David Hoffman <info@sg.actionnetwork.org>
Sent: Wednesday, October 5, 2022 2:39 PM
To: Spinosa, Stefan
Subject: Brent Spence Bridge Project - Reconnecting Cincinnati Westway Design Improvements

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David Hoffman

David Hoffman
jhoff1525@gmail.com
2756 Willard Avenue
Cincinnati, Ohio 45209

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From: Mitch Mohan <info@sg.actionnetwork.org>
Sent: Wednesday, October 5, 2022 10:12 AM
To: Spinosa, Stefan
Subject: Brent Spence Bridge Project - Reconnecting Cincinnati Westway Design Improvements

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Finally, having an engaged local owner of the Project will be critical to the success of Project design improvements. The City of Cincinnati and Hamilton County should be engaged as active partners to ODOT throughout the Project procurement, progressive design, and implementation processes. Specifically, the City of Cincinnati and Hamilton County should become Cooperating Agencies as well as have key votes during the selection of the design-build contractor and associated design concept. A new, City-assembled committee of local stakeholders should help craft the procurement documents and scoring criteria for the selection of a design-builder. Recently, the City of Covington and KYTC signed two MOUs related to the Project, giving Covington a significant seat at the table. ODOT, the City of Cincinnati, and Hamilton County should take similar steps.

Thank you for your attention. I pledge to support ODOT and the entire Cincinnati region in shaping the Project procurement process and ultimate end-product of this Project to all parties' best interests.

Very Respectfully,
Mitch Mohan

Mitch Mohan
mitchell.mohan@gmail.com
621 E Mehring Way
Cincinnati, Ohio 45202

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From: John Wettengel <info@sg.actionnetwork.org>
Sent: Wednesday, October 5, 2022 11:33 AM
To: Spinosa, Stefan
Subject: Brent Spence Bridge Project - Reconnecting Cincinnati Westway Design Improvements

ODOT Capital Programs Administrator Stefan Spinosa,

I am thrilled that the Brent Spence Bridge Corridor Project (“the Project”) is gaining momentum, as a result of the recently passed Bipartisan Infrastructure Law. Earnestly, thank you for your leadership to date in helping our community reach this point. However, given the massive sum of federal and state funding being committed to the Cincinnati region for this Project, and given the lengthy lifespan of the physical infrastructure to be constructed, I am advocating for design improvements to the Project, so that the Cincinnati region receives the best possible outcome.

Before us stands a once-in-a-generation opportunity to shape the Cincinnati region’s relationship to its infrastructure. Unfortunately, the currently proposed Project design – Alternative I-W – which was substantially developed in 2012 and 2013, does not provide the best possible life-cycle value to local, public owners and end users of the Project. However, a Project design that reduces the interstate’s footprint and that improves local community connectivity between the east and west sides of I-75 would provide huge economic benefits to the Cincinnati region, thereby maximizing return on investment in this Project. The Cincinnati region should not miss out on this tremendous opportunity to advance its long-term urban and economic development priorities.

Generally, I advocate that the design of the Project be recalibrated in a way that:

- Does not require adherence to the Alternative I-W design
- Expands the original Purpose & Needs Statement to include the community priorities established here as primary goals
- Reduces the footprint of the interstate and associated ramping
- Lowers the alignment of I-75 through the urban core of Cincinnati, much like that of Fort Washington Way
- Extends the local street grid over the interstate in order to better connect the Cincinnati

CBD to Queensgate and the West End via multiple modes, and in order to accommodate local, urban, street-facing development

While I appreciate that the Project's current design has a smaller right-of-way footprint than previous designs, I believe that additional, significant right-of-way reduction is crucial to the future of Cincinnati. With a landlocked urban core, Cincinnati's urban growth is currently constrained, and the Cincinnati region's ability to develop urban-core real estate for those who desire it – including businesses and top talent – is limited. Improved connectivity between the east and west sides of I-75 in Cincinnati is also crucial. I believe a design solution that not only reduces the right-of-way footprint, but that also includes a recessed highway and an overpassing local street grid, like that of ODOT's celebrated Fort Washington Way project, is just one example of a configuration that could deliver important priorities for the community: the expansion of Cincinnati's central business district to the west of I-75, the elevation of the highest and best use for land, and the creation of new opportunities for economic development and housing development. Any design solution that is implemented should deliver these priorities and facilitate a new/improved network of safe, multi-modal local streets connecting the Cincinnati CBD, Queensgate, the West End, and Covington.

Specific design recommendations capturing the spirit of community priorities:

Specifically, I advocate for the alignment of the interstate through the urban core, from West 3rd Street to Ezzard Charles Drive, to be lowered and consolidated in a way that minimizes right-of-way width, thereby creating space for development and reducing environmental impacts of the interstate on the urban core. The final right-of-way width should be as small as possible, but no larger than the width of Fort Washington Way—about 155 feet. The local street grid should remain at-grade and extend over the interstate, improving community connectivity between currently bifurcated neighborhoods. This concept borrows from the successful urban interstate upgrades that have been ongoing in Columbus, Ohio, for years. Connections should be made across the interstate between as many local east-west streets as possible, but especially between West 4th, 5th, 6th, and 7th Streets. Improved connections should also be made north-south between Queensgate and the West End. Local streets extending over the interstate should be designed in a way that is safe and enjoyable for pedestrians and cyclists to navigate, and in a way that is supportive of local businesses

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These priorities can be incorporated within the scope of projects awarded funding through INFRA and/or Mega grants:

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A custom procurement and design-build processes can accommodate a design recalibration while maintaining Project budget and schedule:

Although ODOT selected a Preferred Alternative design in 2011, upgrades to this design are achievable before the Project’s targeted groundbreaking in late 2023. As local business, community, and political leaders, we expect flexibility from ODOT in designing the procurement process. ODOT should adopt a custom, alternative design-build process that is responsive to the community priorities established here. Such a procurement should be performance- and value-based rather than prescriptive, in the sense that the designer/contracting community should be proactively incentivized to provide creative alternative design concepts, not adhering to Alternative I-W, which are responsive to community goals. Importantly, in this value- and performance-based procurement, community

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Very Respectfully,

John Wettengel

John Wettengel
johnwettengel@gmail.com
2517 8 Mile Road
Cincinnati, Ohio 45244

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From: Jonathan Slone <info@sg.actionnetwork.org>
Sent: Wednesday, October 12, 2022 11:26 PM
To: Spinosa, Stefan
Subject: Brent Spence Bridge Project - Reconnecting Cincinnati Westway Design Improvements

ODOT Capital Programs Administrator Stefan Spinosa,

I am thrilled that the Brent Spence Bridge Corridor Project (“the Project”) is gaining momentum, as a result of the recently passed Bipartisan Infrastructure Law. Earnestly, thank you for your leadership to date in helping our community reach this point. However, given the massive sum of federal and state funding being committed to the Cincinnati region for this Project, and given the lengthy lifespan of the physical infrastructure to be constructed, I am advocating for design improvements to the Project, so that the Cincinnati region receives the best possible outcome.

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A custom procurement and design-build processes can accommodate a design recalibration while maintaining Project budget and schedule:

Although ODOT selected a Preferred Alternative design in 2011, upgrades to this design are achievable before the Project’s targeted groundbreaking in late 2023. As local business, community, and political leaders, we expect flexibility from ODOT in designing the procurement process. ODOT should adopt a custom, alternative design-build process that is responsive to the community priorities established here. Such a procurement should be performance- and value-based rather than prescriptive, in the sense that the designer/contracting community should be proactively incentivized to provide creative alternative design concepts, not adhering to Alternative I-W, which are responsive to community goals. Importantly, in this value- and performance-based procurement, community

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Thank you for your attention. I pledge to support ODOT and the entire Cincinnati region in shaping the Project procurement process and ultimate end-product of this Project to all parties' best interests.

Very Respectfully,

Jonathan Slone

jonathan_slone52@eku.edu

209 McCrae Ln, D
Fort Wright, Kentucky 41011

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From: Anthony Isaacs <info@sg.actionnetwork.org>
Sent: Tuesday, October 18, 2022 11:38 AM
To: Spinosa, Stefan
Subject: Brent Spence Bridge Project - Reconnecting Cincinnati Westway Design Improvements

ODOT Capital Programs Administrator Stefan Spinosa,

I am opposed to projects that waste public funding, maintain harmful levels pollution in our cities, and prevent Cincinnati from using extremely valuable land in the urban core.

I call on you to oppose the Brent Spence Bridge Corridor project, implement a tolling system, and use the existing I-275 loop for through-traffic.

If you insist on supporting the BSB Corridor project, I call on you to advocate on behalf of the Bridge Forward plan, whose rationale is detailed below.

Given the massive sum of federal and state funding being committed to the Cincinnati region for this Project, and given the lengthy lifespan of the physical infrastructure to be constructed, I am advocating for design improvements to the Project, so that the Cincinnati region receives the best possible outcome.

Before us stands a once-in-a-generation opportunity to shape the Cincinnati region's relationship to its infrastructure. Unfortunately, the currently proposed Project design – Alternative I-W – which was substantially developed in 2012 and 2013, does not provide the best possible life-cycle value to local, public owners and end users of the Project. However, a Project design that reduces the interstate's footprint and that improves local community connectivity between the east and west sides of I-75 would provide huge economic benefits to the Cincinnati region, thereby maximizing return on investment in this Project. The Cincinnati region should not miss out on this tremendous opportunity to advance its long-term urban and economic development priorities.

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Very Respectfully,
Anthony Isaacs
773-431-9775

Anthony Isaacs
isaacs.anthony@gmail.com
3619 Tamarack Avenue
Cincinnati, Ohio 45207

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From: Brian Larger <info@sg.actionnetwork.org>
Sent: Tuesday, October 18, 2022 3:05 AM
To: Spinosa, Stefan
Subject: Brent Spence Bridge Project - Reconnecting Cincinnati Westway Design Improvements

ODOT Capital Programs Administrator Stefan Spinosa,

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Very Respectfully,

Brian Larger

brianlarger@gmail.com

3230 16th Ave W Apt 732
SEATTLE, Washington 98119

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From: charles hanson <info@sg.actionnetwork.org>
Sent: Wednesday, October 19, 2022 10:44 PM
To: Spinosa, Stefan
Subject: Brent Spence Bridge Project - Reconnecting Cincinnati Westway Design Improvements

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charles hanson

cjhanson42@gmail.com

450 klotter

cincinnati , Ohio 45214

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From: Gaylord Crozier <info@sg.actionnetwork.org>
Sent: Thursday, October 20, 2022 8:42 PM
To: Spinosa, Stefan
Subject: Brent Spence Bridge Project - Reconnecting Cincinnati Westway Design Improvements

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Gaylord Crozier
zcrozier0@gmail.com

36 E 13th St. Apt. 309
Cincinnati, Ohio 45202

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From: Katrina Armstrong <info@sg.actionnetwork.org>
Sent: Friday, October 21, 2022 9:27 AM
To: Spinosa, Stefan
Subject: Brent Spence Bridge Project - Reconnecting Cincinnati Westway Design Improvements

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Local stakeholders – especially the City of Cincinnati – should have power throughout the procurement and progressive design-build decision-making process:

Finally, having an engaged local owner of the Project will be critical to the success of Project design improvements. The City of Cincinnati and Hamilton County should be engaged as active partners to ODOT throughout the Project procurement, progressive design, and implementation processes. Specifically, the City of Cincinnati and Hamilton County should become Cooperating Agencies as well as have key votes during the selection of the design-build contractor and associated design concept. A new, City-assembled committee of local stakeholders should help craft the procurement documents and scoring criteria for the selection of a design-builder. Recently, the City of Covington and KYTC signed two MOUs related to the Project, giving Covington a significant seat at the table. ODOT, the City of Cincinnati, and Hamilton County should take similar steps.

Thank you for your attention. I pledge to support ODOT and the entire Cincinnati region in shaping the Project procurement process and ultimate end-product of this Project to all parties' best interests.

Very Respectfully,

Katrina Armstrong
Ktfarfsing@gmail.com

3329 Wunder Ave
Cincinnati, Ohio 45211

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From: John Cavanaugh <info@sg.actionnetwork.org>
Sent: Friday, October 21, 2022 8:30 PM
To: Spinosa, Stefan
Subject: Brent Spence Bridge Project - Reconnecting Cincinnati Westway Design Improvements

ODOT Capital Programs Administrator Stefan Spinosa,

I am thrilled that the Brent Spence Bridge Corridor Project (“the Project”) is gaining momentum, as a result of the recently passed Bipartisan Infrastructure Law. Earnestly, thank you for your leadership to date in helping our community reach this point. However, given the massive sum of federal and state funding being committed to the Cincinnati region for this Project, and given the lengthy lifespan of the physical infrastructure to be constructed, I am advocating for design improvements to the Project, so that the Cincinnati region receives the best possible outcome.

Before us stands a once-in-a-generation opportunity to shape the Cincinnati region’s relationship to its infrastructure. Unfortunately, the currently proposed Project design – Alternative I-W – which was substantially developed in 2012 and 2013, does not provide the best possible life-cycle value to local, public owners and end users of the Project. However, a Project design that reduces the interstate’s footprint and that improves local community connectivity between the east and west sides of I-75 would provide huge economic benefits to the Cincinnati region, thereby maximizing return on investment in this Project. The Cincinnati region should not miss out on this tremendous opportunity to advance its long-term urban and economic development priorities.

Generally, I advocate that the design of the Project be recalibrated in a way that:

- Does not require adherence to the Alternative I-W design
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CBD to Queensgate and the West End via multiple modes, and in order to accommodate local, urban, street-facing development

While I appreciate that the Project's current design has a smaller right-of-way footprint than previous designs, I believe that additional, significant right-of-way reduction is crucial to the future of Cincinnati. With a landlocked urban core, Cincinnati's urban growth is currently constrained, and the Cincinnati region's ability to develop urban-core real estate for those who desire it – including businesses and top talent – is limited. Improved connectivity between the east and west sides of I-75 in Cincinnati is also crucial. I believe a design solution that not only reduces the right-of-way footprint, but that also includes a recessed highway and an overpassing local street grid, like that of ODOT's celebrated Fort Washington Way project, is just one example of a configuration that could deliver important priorities for the community: the expansion of Cincinnati's central business district to the west of I-75, the elevation of the highest and best use for land, and the creation of new opportunities for economic development and housing development. Any design solution that is implemented should deliver these priorities and facilitate a new/improved network of safe, multi-modal local streets connecting the Cincinnati CBD, Queensgate, the West End, and Covington.

Specific design recommendations capturing the spirit of community priorities:

Specifically, I advocate for the alignment of the interstate through the urban core, from West 3rd Street to Ezzard Charles Drive, to be lowered and consolidated in a way that minimizes right-of-way width, thereby creating space for development and reducing environmental impacts of the interstate on the urban core. The final right-of-way width should be as small as possible, but no larger than the width of Fort Washington Way—about 155 feet. The local street grid should remain at-grade and extend over the interstate, improving community connectivity between currently bifurcated neighborhoods. This concept borrows from the successful urban interstate upgrades that have been ongoing in Columbus, Ohio, for years. Connections should be made across the interstate between as many local east-west streets as possible, but especially between West 4th, 5th, 6th, and 7th Streets. Improved connections should also be made north-south between Queensgate and the West End. Local streets extending over the interstate should be designed in a way that is safe and enjoyable for pedestrians and cyclists to navigate, and in a way that is supportive of local businesses

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John Cavanaugh

johnthecavanaugh@gmail.com

544 terrace avenue
Cincinnati, Ohio 45220

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From: Nicholas Farfsing <info@sg.actionnetwork.org>
Sent: Friday, October 21, 2022 9:16 AM
To: Spinosa, Stefan
Subject: Brent Spence Bridge Project - Reconnecting Cincinnati Westway Design Improvements

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Nick Farfsing

Nicholas Farfsing
nick.farfsing@gmail.com
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Tucker, Georgia 30084

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From: David Sherman <info@sg.actionnetwork.org>
Sent: Saturday, October 22, 2022 7:01 AM
To: Spinosa, Stefan
Subject: Brent Spence Bridge Project - Reconnecting Cincinnati Westway Design Improvements

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Finally, having an engaged local owner of the Project will be critical to the success of Project design improvements. The City of Cincinnati and Hamilton County should be engaged as active partners to ODOT throughout the Project procurement, progressive design, and implementation processes. Specifically, the City of Cincinnati and Hamilton County should become Cooperating Agencies as well as have key votes during the selection of the design-build contractor and associated design concept. A new, City-assembled committee of local stakeholders should help craft the procurement documents and scoring criteria for the selection of a design-builder. Recently, the City of Covington and KYTC signed two MOUs related to the Project, giving Covington a significant seat at the table. ODOT, the City of Cincinnati, and Hamilton County should take similar steps.

Thank you for your attention. I pledge to support ODOT and the entire Cincinnati region in shaping the Project procurement process and ultimate end-product of this Project to all parties' best interests.

Very Respectfully,

David Sherman
dcs3939@gmail.com

819 Dayton Street,
Cincinnati, Ohio 45214

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From: Rebecca McWilliams <info@sg.actionnetwork.org>
Sent: Sunday, October 23, 2022 11:33 AM
To: Spinosa, Stefan
Subject: Brent Spence Bridge Project - Reconnecting Cincinnati Westway Design Improvements

ODOT Capital Programs Administrator Stefan Spinosa,

I am thrilled that the Brent Spence Bridge Corridor Project (“the Project”) is gaining momentum, as a result of the recently passed Bipartisan Infrastructure Law. Earnestly, thank you for your leadership to date in helping our community reach this point. However, given the massive sum of federal and state funding being committed to the Cincinnati region for this Project, and given the lengthy lifespan of the physical infrastructure to be constructed, I am advocating for design improvements to the Project, so that the Cincinnati region receives the best possible outcome.

Before us stands a once-in-a-generation opportunity to shape the Cincinnati region’s relationship to its infrastructure. Unfortunately, the currently proposed Project design – Alternative I-W – which was substantially developed in 2012 and 2013, does not provide the best possible life-cycle value to local, public owners and end users of the Project. However, a Project design that reduces the interstate’s footprint and that improves local community connectivity between the east and west sides of I-75 would provide huge economic benefits to the Cincinnati region, thereby maximizing return on investment in this Project. The Cincinnati region should not miss out on this tremendous opportunity to advance its long-term urban and economic development priorities.

Generally, I advocate that the design of the Project be recalibrated in a way that:

- Does not require adherence to the Alternative I-W design
- Expands the original Purpose & Needs Statement to include the community priorities established here as primary goals
- Reduces the footprint of the interstate and associated ramping
- Lowers the alignment of I-75 through the urban core of Cincinnati, much like that of Fort Washington Way
- Extends the local street grid over the interstate in order to better connect the Cincinnati

CBD to Queensgate and the West End via multiple modes, and in order to accommodate local, urban, street-facing development

While I appreciate that the Project's current design has a smaller right-of-way footprint than previous designs, I believe that additional, significant right-of-way reduction is crucial to the future of Cincinnati. With a landlocked urban core, Cincinnati's urban growth is currently constrained, and the Cincinnati region's ability to develop urban-core real estate for those who desire it – including businesses and top talent – is limited. Improved connectivity between the east and west sides of I-75 in Cincinnati is also crucial. I believe a design solution that not only reduces the right-of-way footprint, but that also includes a recessed highway and an overpassing local street grid, like that of ODOT's celebrated Fort Washington Way project, is just one example of a configuration that could deliver important priorities for the community: the expansion of Cincinnati's central business district to the west of I-75, the elevation of the highest and best use for land, and the creation of new opportunities for economic development and housing development. Any design solution that is implemented should deliver these priorities and facilitate a new/improved network of safe, multi-modal local streets connecting the Cincinnati CBD, Queensgate, the West End, and Covington.

Specific design recommendations capturing the spirit of community priorities:

Specifically, I advocate for the alignment of the interstate through the urban core, from West 3rd Street to Ezzard Charles Drive, to be lowered and consolidated in a way that minimizes right-of-way width, thereby creating space for development and reducing environmental impacts of the interstate on the urban core. The final right-of-way width should be as small as possible, but no larger than the width of Fort Washington Way—about 155 feet. The local street grid should remain at-grade and extend over the interstate, improving community connectivity between currently bifurcated neighborhoods. This concept borrows from the successful urban interstate upgrades that have been ongoing in Columbus, Ohio, for years. Connections should be made across the interstate between as many local east-west streets as possible, but especially between West 4th, 5th, 6th, and 7th Streets. Improved connections should also be made north-south between Queensgate and the West End. Local streets extending over the interstate should be designed in a way that is safe and enjoyable for pedestrians and cyclists to navigate, and in a way that is supportive of local businesses

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Rebecca McWilliams

Rebecca McWilliams
Beckyfarfsing@gmail.com
1522 Compton Rd
Cincinnati, Ohio 45231

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From: Jake McGinty <info@sg.actionnetwork.org>
Sent: Monday, October 24, 2022 11:06 PM
To: Spinosa, Stefan
Subject: Brent Spence Bridge Project - Reconnecting Cincinnati Westway Design Improvements

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Very Respectfully,
Jake McGinty

Jake McGinty
jm531614@ohio.edu
4529 plainville rd
Cincinnati, Ohio 45227

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From: Mitch Radakovich <info@sg.actionnetwork.org>
Sent: Monday, October 24, 2022 10:01 PM
To: Spinosa, Stefan
Subject: Brent Spence Bridge Project - Reconnecting Cincinnati Westway Design Improvements

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Mitch Radakovich

Mitch Radakovich
mitch@radakovichfamily.com
34 E 14th St
Cincinnati, Ohio 45202

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From: Charlie Schwartz <info@sg.actionnetwork.org>
Sent: Monday, October 24, 2022 9:48 AM
To: Spinosa, Stefan
Subject: Brent Spence Bridge Project - Reconnecting Cincinnati Westway Design Improvements

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Thank you for your attention. I pledge to support ODOT and the entire Cincinnati region in shaping the Project procurement process and ultimate end-product of this Project to all parties' best interests.

Very Respectfully,

Charlie Schwartz

c.schwartz.1983@gmail.com

2896 Victoria Ave
Cincinnati, Ohio 45208

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From: Colin Barge <info@sg.actionnetwork.org>
Sent: Tuesday, October 25, 2022 11:00 AM
To: Spinosa, Stefan
Subject: Brent Spence Bridge Project - Reconnecting Cincinnati Westway Design Improvements

ODOT Capital Programs Administrator Stefan Spinosa,

I am thrilled that the Brent Spence Bridge Corridor Project (“the Project”) is gaining momentum, as a result of the recently passed Bipartisan Infrastructure Law. Earnestly, thank you for your leadership to date in helping our community reach this point. However, given the massive sum of federal and state funding being committed to the Cincinnati region for this Project, and given the lengthy lifespan of the physical infrastructure to be constructed, I am advocating for design improvements to the Project, so that the Cincinnati region receives the best possible outcome.

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Colin Barge

bargecolin@gmail.com

2031 Dunlap Street
Cincinnati, Ohio 45214

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From: lauren E dameron <info@sg.actionnetwork.org>
Sent: Wednesday, October 26, 2022 10:25 PM
To: Spinosa, Stefan
Subject: Brent Spence Bridge Project - Reconnecting Cincinnati Westway Design Improvements

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Very Respectfully,

lauren E dameron
dinoteeth5672@gmail.com

730 Red Bud Ave
Cincinnati, Ohio 45229

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From: Richard Wendel <info@sg.actionnetwork.org>
Sent: Sunday, October 30, 2022 9:31 AM
To: Spinosa, Stefan
Subject: Brent Spence Bridge Project - Reconnecting Cincinnati Westway Design Improvements

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o Note: Hamilton County, Ohio, Census Tracts 2 and 263 – both within the Project footprint, with one on each side of I-75 – are considered Transportation Disadvantaged, and Tract 2 is considered a Historically Disadvantaged Community by USDOT. Additionally Tract 2 and Tract 264, which is also adjacent to the Project, are both HUD-designated Racially or Ethnically Concentrated Areas of Poverty (R/ECAPs).

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Thank you for your attention. I pledge to support ODOT and the entire Cincinnati region in shaping the Project procurement process and ultimate end-product of this Project to all parties' best interests.

Very Respectfully,
Richard G. Wendel MD, MBA

Richard Wendel
rgwendel4@gmail.com
6722 Hammerstone Way
Cincinnati, Ohio 45227

CAUTION: This is an external email and may not be safe. If the email looks suspicious, please do not click links or open attachments and forward the email to csc@ohio.gov or click the Phish Alert Button if available.

From: Noah Cable <info@sg.actionnetwork.org>
Sent: Thursday, November 10, 2022 3:59 PM
To: info@brentspencebridgecorridor.com
Subject: Brent Spence Bridge Project - Reconnecting Cincinnati Westway Design Improvements

Bi-State Management Team,

I am thrilled that the Brent Spence Bridge Corridor Project (“the Project”) is gaining momentum, as a result of the recently passed Bipartisan Infrastructure Law. Earnestly, thank you for your leadership to date in helping our community reach this point. However, given the massive sum of federal and state funding being committed to the Cincinnati region for this Project, and given the lengthy lifespan of the physical infrastructure to be constructed, I am advocating for design improvements to the Project, so that the Cincinnati region receives the best possible outcome.

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While I appreciate that the Project's current design has a smaller right-of-way footprint than previous designs, I believe that additional, significant right-of-way reduction is crucial to the future of Cincinnati. With a landlocked urban core, Cincinnati's urban growth is currently constrained, and the Cincinnati region's ability to develop urban-core real estate for those who desire it – including businesses and top talent – is limited. Improved connectivity between the east and west sides of I-75 in Cincinnati is also crucial. I believe a design solution that not only reduces the right-of-way footprint, but that also includes a recessed highway and an overpassing local street grid, like that of ODOT's celebrated Fort Washington Way project, is just one example of a configuration that could deliver important priorities for the community: the expansion of Cincinnati's central business district to the west of I-75, the elevation of the highest and best use for land, and the creation of new opportunities for economic development and housing development. Any design solution that is implemented should deliver these priorities and facilitate a new/improved network of safe, multi-modal local streets connecting the Cincinnati CBD, Queensgate, the West End, and Covington.

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Specifically, I advocate for the alignment of the interstate through the urban core, from West 3rd Street to Ezzard Charles Drive, to be lowered and consolidated in a way that minimizes right-of-way width, thereby creating space for development and reducing environmental impacts of the interstate on the urban core. The final right-of-way width should be as small as possible, but no larger than the width of Fort Washington Way—about 155 feet. The local street grid should remain at-grade and extend over the interstate, improving community connectivity between currently bifurcated neighborhoods. This concept borrows from the successful urban interstate upgrades that have been ongoing in Columbus, Ohio, for years. Connections should be made across the interstate between as many local east-west streets as possible, but especially between West 4th, 5th, 6th, and 7th Streets. Improved connections should also be made north-south between Queensgate and the West End. Local streets extending over the interstate should be designed in a way that is safe and enjoyable for pedestrians and cyclists to navigate, and in a way that is supportive of local businesses and conducive to urban development. A good example of this is the completed East Long Street bridge crossing I-71 in Columbus, Ohio. With respect to land recapture thanks to Project footprint reduction, enough space should be returned to local control to facilitate the development of long-awaited regional priorities: an expanded convention center and a new

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Thank you for your attention. I pledge to support ODOT and the entire Cincinnati region in shaping the Project procurement process and ultimate end-product of this Project to all parties' best interests.

Very Respectfully,

Noah D. Cable

Noah Cable

noahdcable@gmail.com

1404 E McMillan Street, #10

Cincinnati, Ohio 45206

From: Andrea Ray <info@sg.actionnetwork.org>
Sent: Friday, November 11, 2022 1:01 PM
To: info@brentspencebridgecorridor.com
Subject: Brent Spence Bridge Project - Reconnecting Cincinnati Westway Design Improvements

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Very Respectfully,

Andrea Ray

Andrea Ray
rayandrea99@gmail.com
2752 McKinley Ave
Cincinnati, Ohio 45211

From: Aditya Sharma <info@sg.actionnetwork.org>
Sent: Sunday, November 13, 2022 4:09 PM
To: info@brentspencebridgecorridor.com
Subject: Brent Spence Bridge Project - Reconnecting Cincinnati Westway Design Improvements

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Very Respectfully,

Aditya Sharma

Aditya Sharma

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2515 Burnet Ave

Cincinnati, Ohio 45219

**Appendix M:
Response to Cincinnati & Chamber Comments**





Response to Public Comments

City of Cincinnati BSB Corridor Concept I-W and Cincinnati USA Regional Chamber Footprint Minimization

TO: KYTC and ODOT
FROM: HNTB Corporation
DATE: November 2, 2022
RE: Brent Spence Bridge Corridor Project | ODOT PID 89068 | KYTC Project Item No. 6-17

Introduction

The purpose of this memorandum is to provide a response to comments on Concept I-W for the Brent Spence Bridge (BSB) Corridor Project provided by the City of Cincinnati on September 2, 2022 and the Cincinnati USA Regional Chamber of Commerce (Chamber) on July 11, 2022 (see Attachment 1). The Chamber is a member of the BSB Project Advisory Committee and provided feedback that was similar to comments received from the City. Responses to the City's comments are provided below.

Ramp Refinements

After receipt of the City's comments, ODOT and HNTB met with City staff to discuss potential refinements to Concept I-W in the area between 3rd Street and 7th Street. Following the coordination meeting, HNTB evaluated the following ramp refinements in downtown Cincinnati (see Attachment 2):

- Removing the entrance from 4th Street to northbound I-75.
- Adding an entrance from 3rd Street to northbound I-75.
- Removing the southbound I-75 exit to 5th Street and reducing the number of lanes on the southbound collector-distributor (C-D) road from three to two.
- Reconfiguring the northbound I-75 exit to 5th Street to create a new signalized intersection with the US 50 ramp approximately 300 feet west of Central Avenue.
- Reducing the number of lanes on the eastbound approach to the 5th Street/Central Avenue intersection from four to three.
- Removing the connection between 6th Street and Winchell Avenue.
- Providing a connection between 6th Street and the northbound C-D road and northbound I-75.
- Widening the southbound I-75 ramp to 7th Street from one to two lanes.

The above ramp refinements were found to operate acceptably in both the morning and evening peak travel periods (see Attachment 3), and the City concurred with the findings on October 20, 2022 (see Attachment 4). Therefore, the above refinements will be incorporated into Concept I-W, which is currently being evaluated in a Supplemental Environmental Assessment. Traffic operations at additional intersections in downtown Cincinnati will continue to be evaluated during the preparation of an *Interchange Modification Study* Update for the BSB Corridor Project. Public hearings will be scheduled in



June/July of 2023 to provide additional opportunities to comment on all the improvements included in Concept I-W.

Future Development

The ramp refinements discussed above will open up approximately 9.5 acres of additional land to help the City further its development goals. The BSB Corridor project will not preclude the City from using John Street to support future development, as requested in their comments.

Alternative Modes and Livability

The City also provided comments with respects to alternative transportation modes and promoting a “City feel” as part of the project’s design. The BSB project will install bicycle and pedestrian infrastructure on the following connections across I-75: 6th Street, 7th Street, 9th Street, Linn Street, Freeman Avenue, Ezzard Charles Drive, West Liberty Street, Findlay Street, Bank Street, and Harrison Avenue. In addition, new shared use path will be constructed along Winchell Avenue between 9th Street and Ezzard Charles Drive, including a pedestrian bridge connection to Freeman Avenue. To promote safety for bicycles and pedestrians, the ramp connections with local streets are being designed with lower speed limits. This includes efforts to minimize the footprint of the 5th Street ramps to better support alternative transportation modes and pedestrian crossings at Central Avenue.

Since 2012, KYTC and ODOT have been refining the preferred alternative for the BSB Corridor Project to further reduce the project footprint. Specifically, optimizing interchange geometry by utilizing the land formerly occupied by the Dunnhumby building (444 W. 3rd Street) resulted in a meaningful reduction in the project footprint in downtown Cincinnati. In addition, ODOT will continue to work with the City to identify opportunities for aesthetic improvements throughout the corridor.

Conclusion

KYTC and ODOT support the City’s goals of getting the project done; reclaiming land; improving green space, pedestrian safety, bike facilities, etc.; and keeping a “city feel” on or under bridges for I-75. Several refinements to the 3rd Street, 4th Street, 5th Street, and 6th Street ramps will be incorporated into Preferred Alternative I (Concept I-W) to further those goals. Based on the ramp refinements, there will be approximately 9.5 acres between 3rd Street and 6th Street that can be reclaimed for other uses after construction of the project. In addition, ODOT will continue to support the City of Cincinnati’s efforts to accommodate alternative modes and improve livability during the development of the BSB Corridor Project.



Attachment 1: City of Cincinnati and Cincinnati USA Chamber of
Commerce Comments

September 2, 2022

To: Stefan Spinosa, Ohio Department of Transportation
From: John Brazina, City of Cincinnati,
RE: Brent Spence Bridge Corridor Concept I-W comments

The City of Cincinnati has the following objectives/goals for the BSB:

1. Get the project done
2. Reclaiming land
3. Improving green space, pedestrian safety, bike facilities, etc.
4. Keep a "City feel" on or under bridges for I-75

With the above goals in mind, The City of Cincinnati has reviewed the Brent Spence Bridge Corridor Project – Concept I-W (BSB) and offers the following comments:

The City would like to evaluate options that would maximize the developable land between Central Avenue and the Interstate network. (Exhibit A) We would ask that ODOT evaluate the following options to help achieve that goal:

- Remove the ramps that access from 4th Street.
 - An additional ramp from 3rd Street to NB I-75.
- Minimize the footprint of the 5th Street ramps.
 - The NB ramp to 5th Street can create a T-intersection with the other ramps coming into 5th Street.
 - Shift the C-D road to the west since the 4th Street ramp is gone.
 - Potentially move to a two-way 6th Street.
- Direct connection from 6th Street to the mainline I75 northbound.
 - With the removal of the 4th Street ramp to the mainline a direct connection from 6th St should be created.
- Support future development
 - The City anticipates John Street being used to support future development in the space created in this area.
 - As design moves forward, ODOT and the City should make sure the interstate design doesn't preclude John St being used to support potential back of house uses for development.

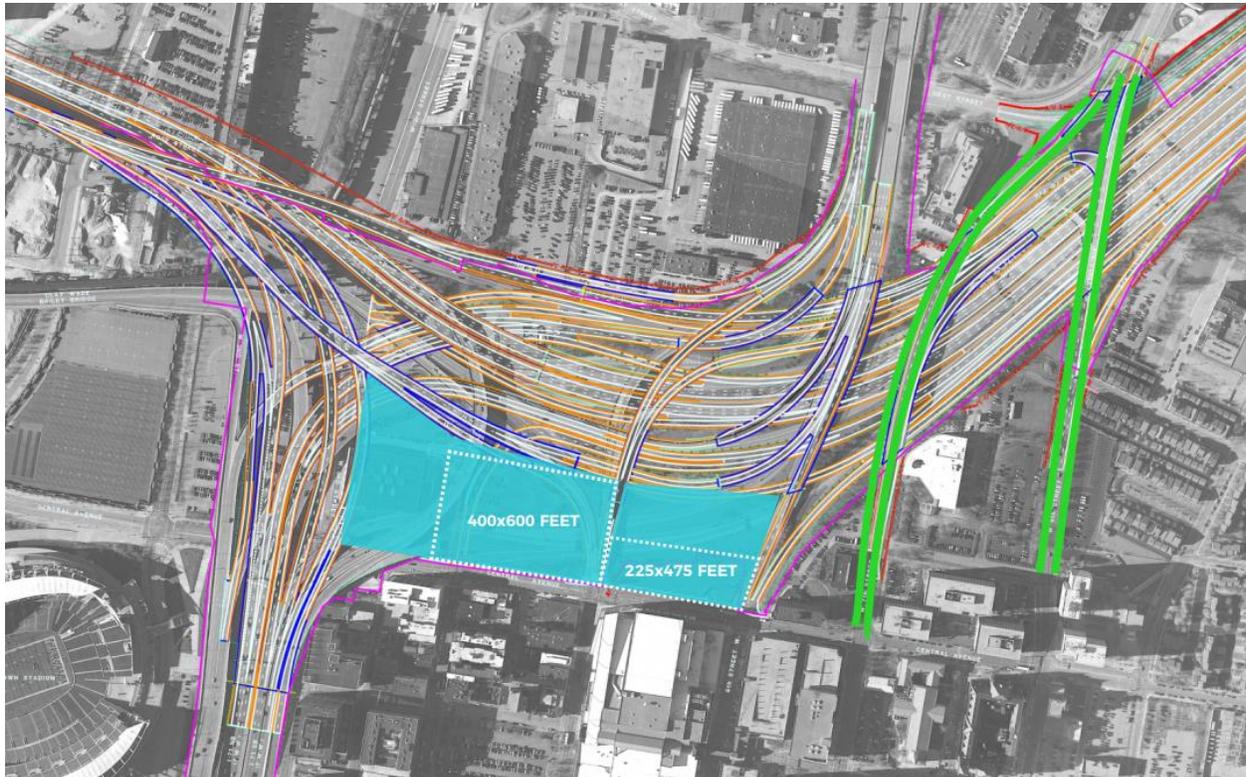
The City is also open to options that would increase alternative modes of transportation and improve livability of the communities along the I-75 corridor.

- City standard bike and pedestrian infrastructure should be installed on all connections across the interstate.
- Check the design speeds of all the ramps in this area. Especially the ones going to/from City street network.
 - Design all curves for the minimum design speed possible to help slow traffic and potentially minimize the footprint.
- The City is open to changes that allow for consolidation of entrances/exits that might shrink the footprint. These changes could allow changes to traffic flow in the urban core (ie: two-way conversions)

- To create an improved experience for non-motorists crossing the BSB footprint, including narrow caps that allow for green space and street trees. To the extent that there are opportunities to meaningfully shrink the footprint and decrease the distance required to cross from downtown to Queensgate or Queensgate to the West End.

Thank you for the opportunity to review and comment. The City looks forward to being a partner with ODOT as we move forward on this exciting project. If you have any questions on the comments provided, please contact Bryan Williams or me.

EXHIBIT A:



Spinosa, Stefan

From: Pete Metz [REDACTED]
Sent: Monday, July 11, 2022 9:19 AM
To: Spinosa, Stefan
Cc: Jill Meyer
Subject: BSB - Footprint Minimization Exercise
Attachments: bsbconcepts_6_30_2022v2.pdf

Stefan –

Attached please find the map that was developed over the last month+ with stakeholders we convened to identify and quantify the valuable minimum thresholds for developable land should it be possible to retain it in the design build phase of BSB. A few things worth noting:

1. The Blue sections are the desired areas, while the dotted lines delineate the minimum thresholds for Convention Center opportunities to the west.
2. The green lines indicate the areas where I understand the City and ODOT are discussing Long Street style treatment on connection points across the footprint. Throughout the room there was excitement for this concept and support for it. We included it here to make sure you understood that we were aligned in talking about both.

Thanks! Call with any questions,

Pete

PETE METZ

DIRECTOR, THE CONNECTED REGION

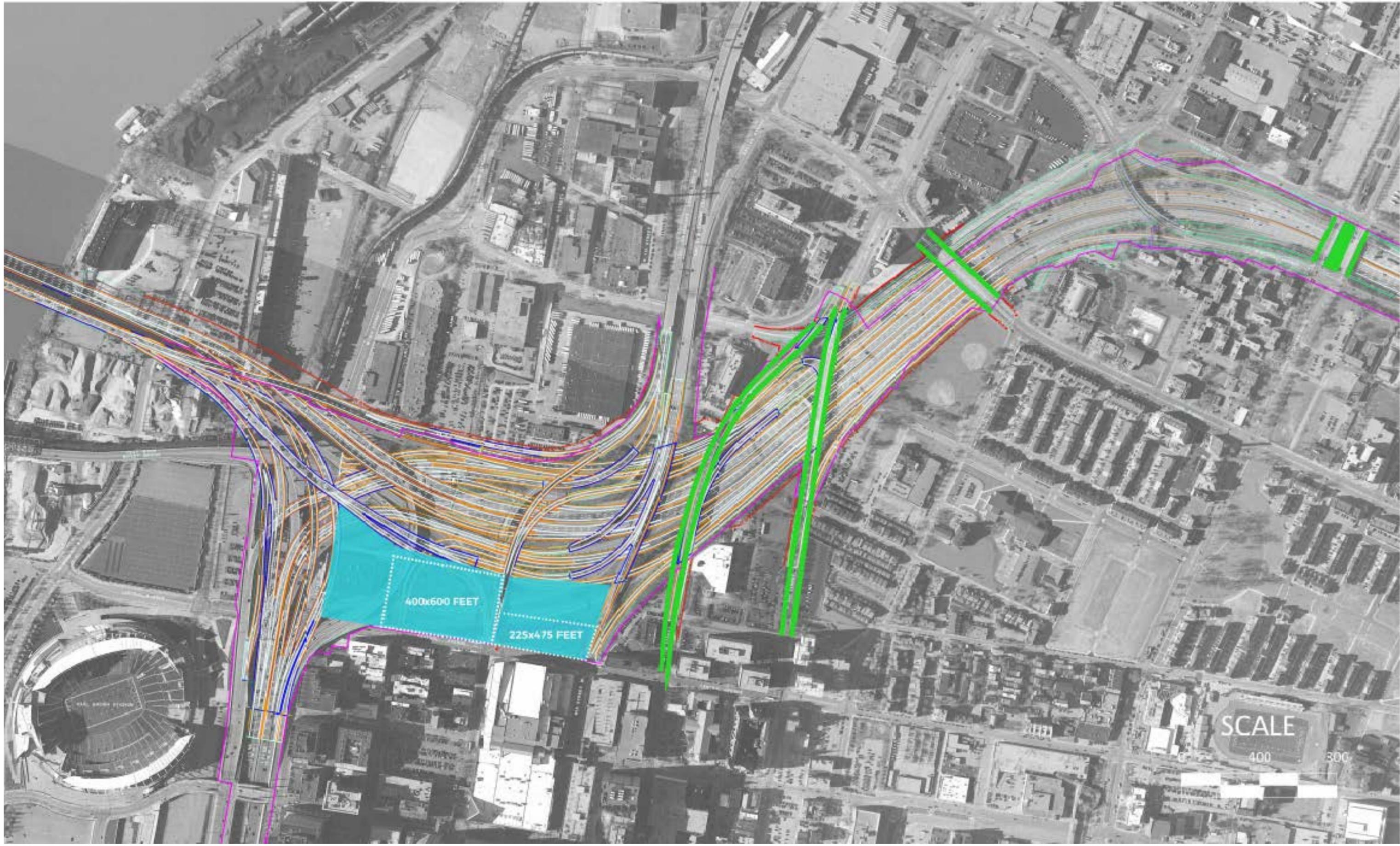
CINCINNATI USA REGIONAL CHAMBER

3 East Fourth Street, Suite 200, Cincinnati, OH 45202

cincinnati-chamber.com | theconnectedregion.com

CAUTION: This is an external email and may not be safe. If the email looks suspicious, please do not click links or open attachments and forward the email to csc@ohio.gov or click the Phish Alert Button if available.

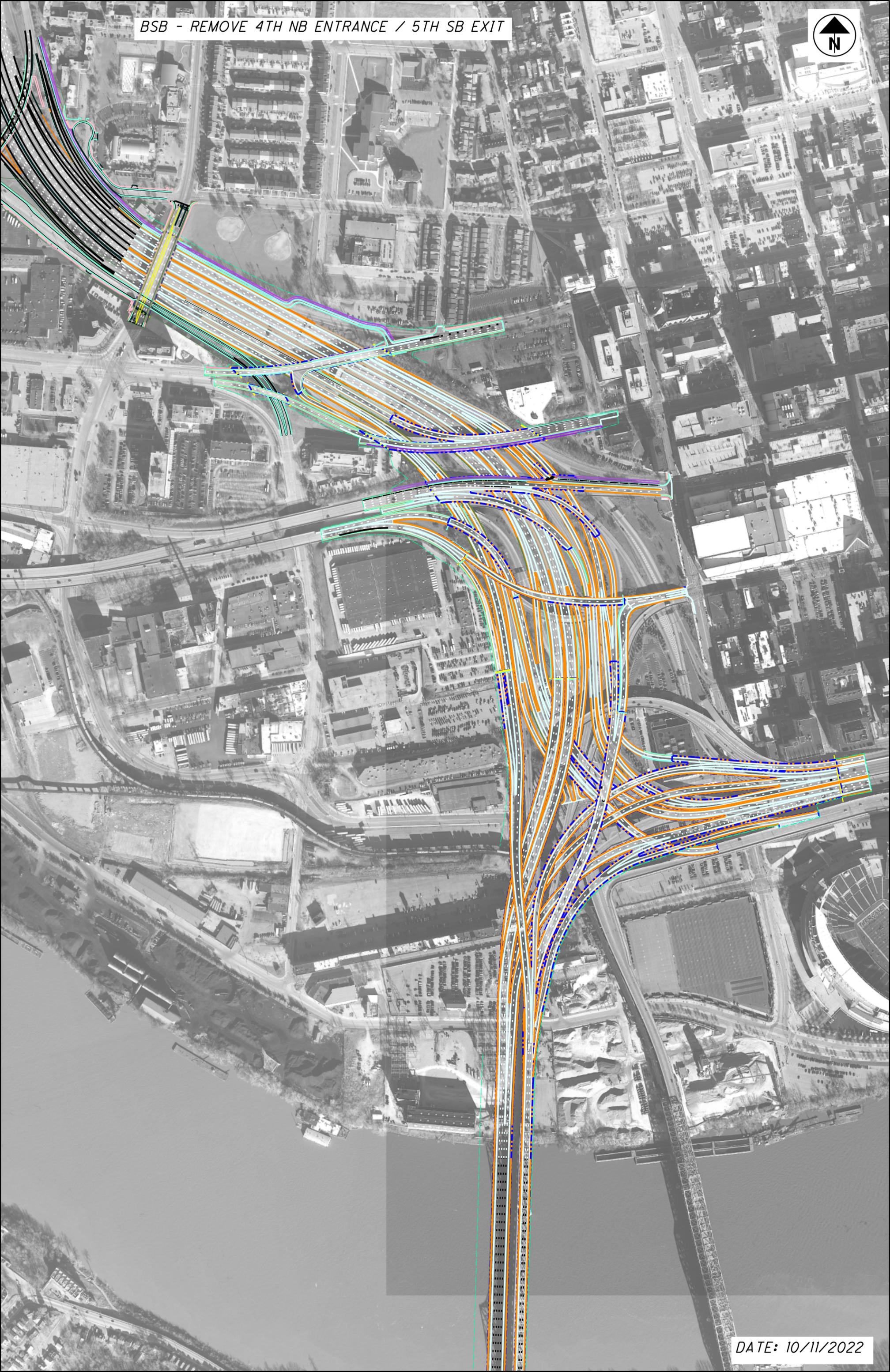
BRENT SPENCE BRIDGE CORRIDOR DESIGN BUILD PROJECT - CONCEPT I-W
(ODOT PID 116643 & KYTC KENTON COUNTY 6-17)





Attachment 2: Proposed Ramp Refinements

BSB - REMOVE 4TH NB ENTRANCE / 5TH SB EXIT





Attachment 3: Traffic Analysis

Concept I-W Traffic Analysis: Design Refinements based on City of Cincinnati Comments

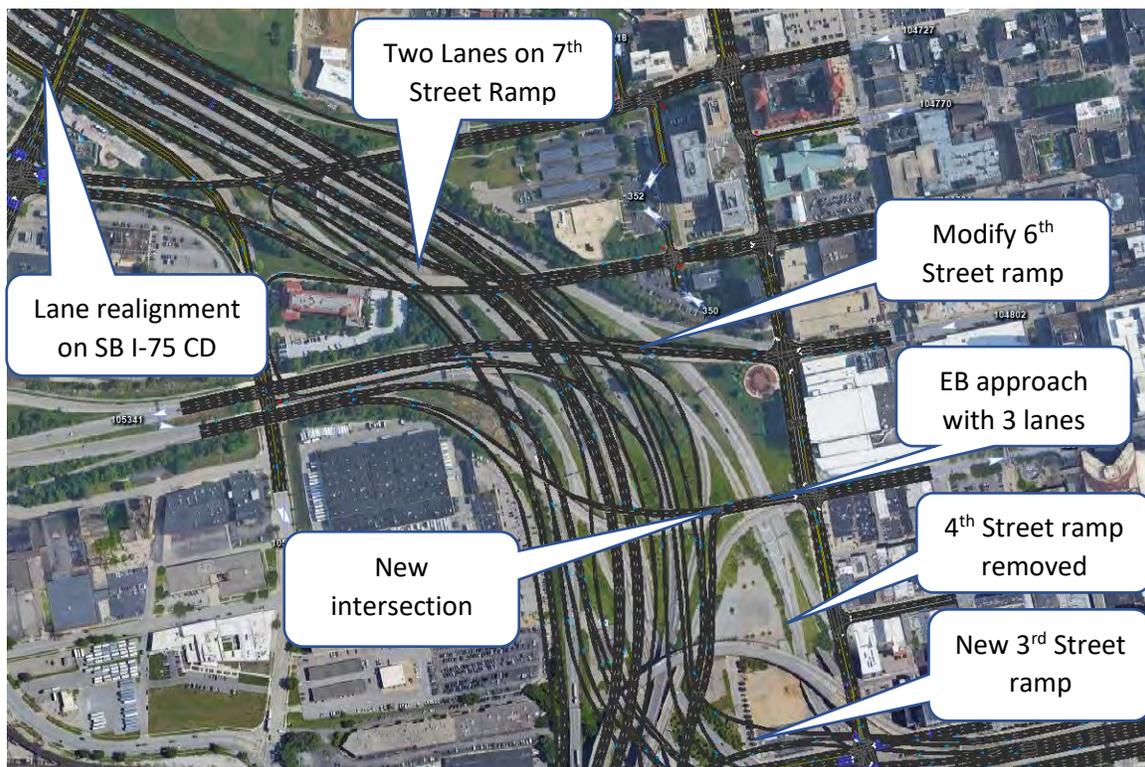
Summary

TransModeler traffic microsimulation models were used to evaluate proposed geometric changes to Concept I-W based on City of Cincinnati comments. The models use the methodology documented in the BSB Traffic Operations Report with 2050 planning level traffic volumes. The design modifications proposed by the City of Cincinnati include:

- Removal of SB I-75 Exit to 5th Street
- Removal of the NB I-75 entrance from 4th Street
- Addition of NB I-75 entrance ramp from 3rd Street
- Modification of 6th Street ramp geometry
- New signalized intersection between the NB I-75 exit ramp and US 50
- Reduced lanes for EB approach at 5th Street & Central Avenue Intersection

Due to traffic impacts from the 5th Street ramp removal, additional improvements are recommended for SB I-75 CD and SB I-75 exit ramp to 7th Street. With these proposed changes the traffic operations are acceptable in the project study area captured in the TransModeler network. The IMS forecasts and operations analysis will include additional intersections east of Central Avenue, which will provide a final analysis to understand the traffic impacts of the 4th Street ramp removal. The proposed geometric refinements for Concept I-W are shown in the figure below.

Figure 1: Concept I-W Design Refinements



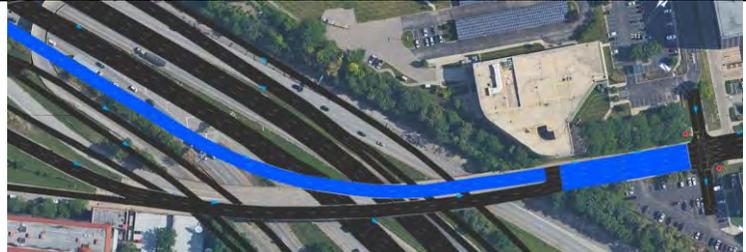
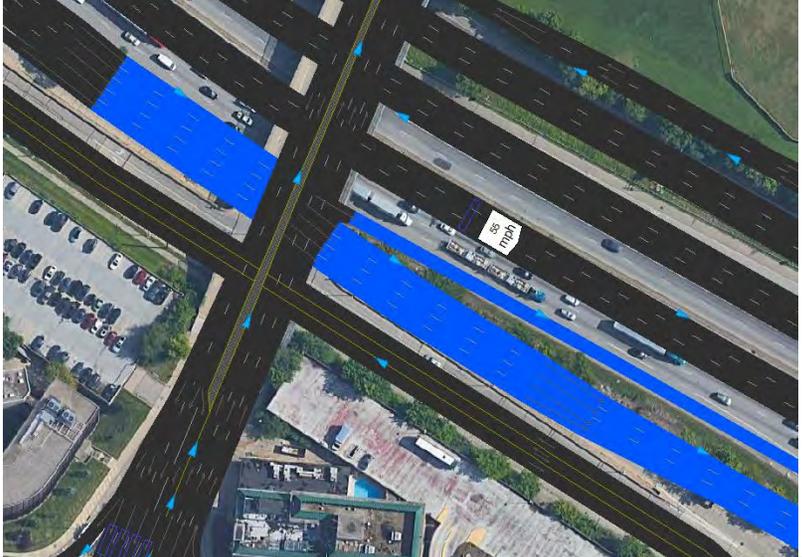
I-75 Exit Ramp Modifications

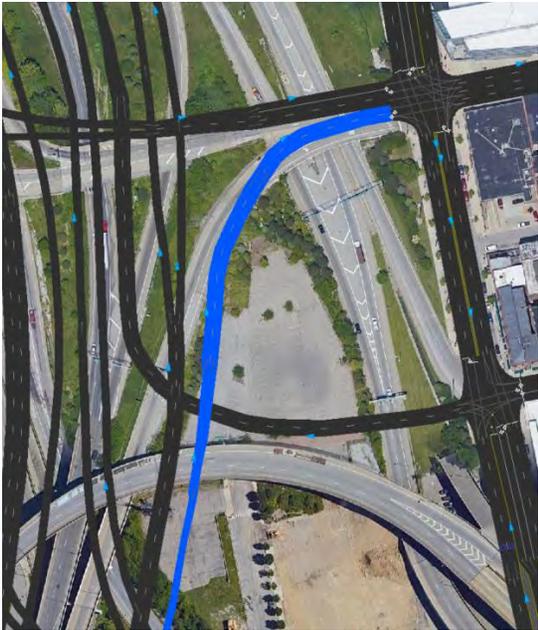
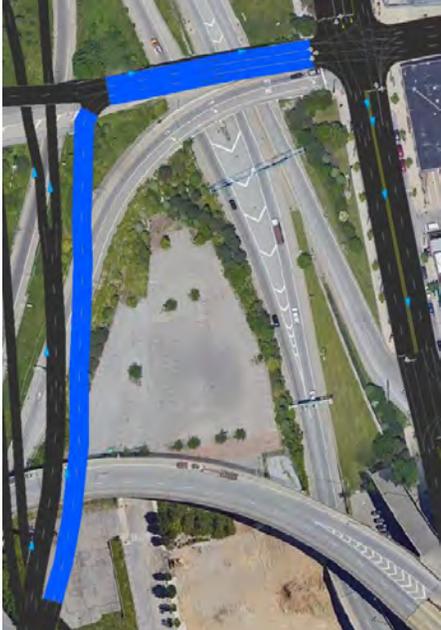
The SB I-75 exit onto 5th St was removed from the Concept I-W design. US 50 and the NB Exit to 5th Street was modified to form a signalized intersection west of Central Avenue. This signalized intersection operates at LOS B or better for both peak hours. The eastbound approach of the 5th Street and Central Avenue intersection is reduced from 4 lanes to 3 lanes. This intersection maintains acceptable operations of LOS B or better.

To accommodate the displaced exiting traffic to 5th Street, a second lane was added to the 7th Street exit ramp. This design change is different from the single lane in Concept I-W but consistent with the existing conditions. The LOS for the SB I-75 CD road before the 7th Street exit ramp changed from LOS B to LOS C during the AM period and remained at an LOS A for the PM period. The volume entering the ramp increased from 3410 vehicles to 6384 vehicles over the course of the 5-hour AM period and 1763 vehicles to 3431 vehicles for the 6-hour PM period. The LOS at the 7th Street and Central Avenue intersection remained at a LOS B for the AM and PM period for this design change.

On the SB I-75 CD road upstream of the 7th Street exit, the road diverges to either 7th Street or continues to 2nd Street, 3rd Street, and Brent Spence Bridge. It was also this section of road where the I-75 exit ramp to 5th Street was located. With the removal of the ramp, a lane was removed from the CD road. The traffic continuing south after the diverge now has three lanes that tapers to two, this tapering distance was shortened to match the distance used in the Concept I design. The LOS for this section changed from LOS B to LOS C during the AM period and remained at a LOS B for the PM period.

A comparison of the Concept I-W geometric changes for the I-75 exit ramps are shown on the following two pages.

Location	Concept I-W	Proposed Changes
SB I-75 Exit to 5 th Removal		
7 th St Exit Ramp		
I-75 S and 7 th St Ramp diverge		

Location	Concept I-W	Proposed Changes
5 th St Exit Ramp	 An aerial photograph of a highway interchange. A blue line highlights a specific exit ramp path that curves from the main highway towards the right side of the frame.	 An aerial photograph of the same highway interchange as the concept image. A blue line highlights a different exit ramp path, which is more direct and less curved than the one in the concept image.

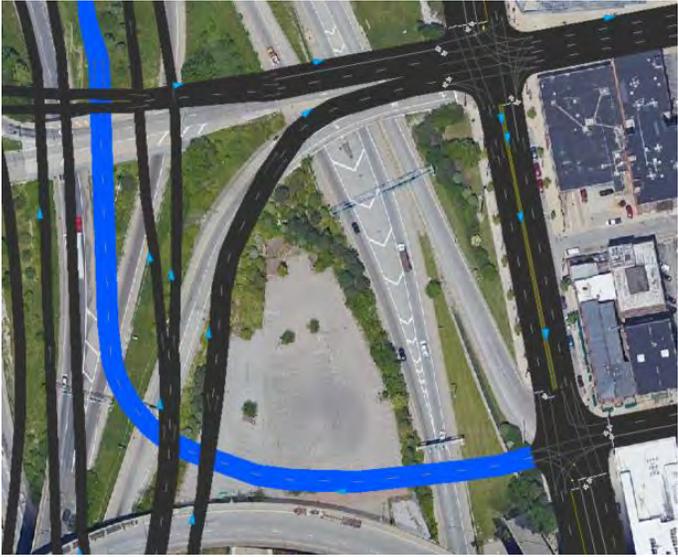
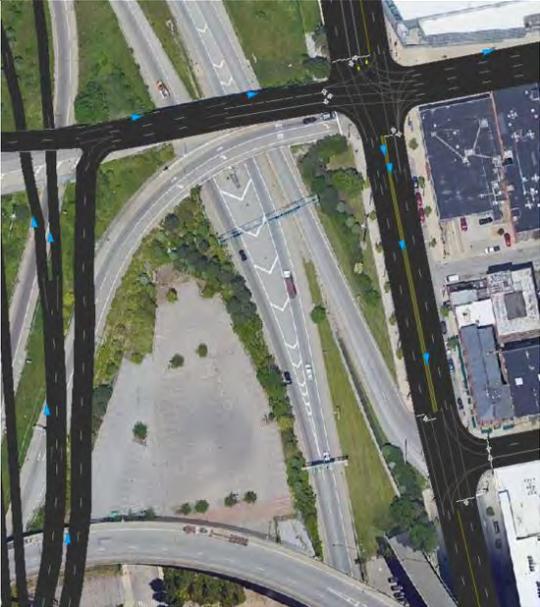
Northbound Entrance Ramp Modifications

The NB I-75 entrance ramp from 4th Street to the NB CD was removed. The entrance ramp from 3rd Street was modified to include an additional ramp leading to I-75 N to accommodate the displaced northbound traffic. This alteration increased the 3rd Street ramp volume from 829 vehicles to 2553 vehicles over the course of the AM period and 5424 vehicles to 9565 vehicles for the PM period.

The 6th Street entrance ramp no longer goes to the Winchell CD, instead it merges with the NB CD onto I-75 N. The traffic using the 6th St entrance increased from 784 vehicles to 1076 vehicles over the course of the AM period and 1327 vehicles to 1820 vehicles for the PM period. The LOS at the 6th Street and Central Avenue intersection changed from LOS A to LOS B for both the AM and PM period.

As part of the IMS addendum additional analysis will be completed for the 3rd Street intersections at Elm Street and Race Street.

The entrance ramp design modifications for Concept I-W are shown on the following two pages.

Location	Concept I-W	Proposed Changes
4 th St Entrance Ramp		
3 rd St Entrance Ramp		

Location	Concept I-W	Proposed Changes
6 th St Entrance Ramp		



Attachment 4: City Concurrence

From: Williams, Bryan (Urban Planning) [REDACTED]
Sent: Thursday, October 20, 2022 9:20 AM
To: Spinosa, Stefan <[REDACTED].gov>; Brazina, John <Joh[REDACTED].gov>
Cc: Eric Youngblom <[REDACTED]>; Mark Becherer [REDACTED]; Erica Johnson - IND [REDACTED]; Hans, Stacey D (KYTC-D06) [REDACTED]
Subject: RE: [External Email] RE: BSB - Concept I-W

Stefan,

All of the changes/modifications outlined below are acceptable to the City.
Please let us know if you need anything further from us at this time.

Bryan

From: [Stefan.Spinosa](#) [REDACTED]
Sent: Friday, October 14, 2022 12:15 PM
To: Williams, Bryan (Urban Planning) [REDACTED]; Brazina, John <[REDACTED].oh.gov>
Cc: Eric Youngblom <[REDACTED]>; Mark Becherer [REDACTED]; Erica Johnson - IND [REDACTED]; [REDACTED]
Subject: [External Email] RE: BSB - Concept I-W

External Email Communication

Bryan, John,

For your review is updated plan view and planning level traffic analysis for the changes we been discussing based on the attached September 2, 2022 comments.

Included is the following:

- Traffic/City Refinements – Short writeup on the traffic analysis based on City comments
- Concept I-W Segment Reference (for use with Operation Spreadsheets)
- Alt W AM and PM – Operation Spreadsheets for Concept I-W with changes
- 3rd and Elm AM and PM – HCS analysis at 3rd and ELM
- Plan view of changes

The overall changes from I-W resulting from these revisions are:

Remove:

4th NB entrance to NB I-75 CD
SB CD exit to 5th St
6th St access to Winchell

Add:

3rd St NB entrance to NB I-75 CD – merge with NB I-75 CD
6th St NB entrance to NB I-75 CD – add lane to NB I-75 CD
US 50 EB and 5th NB exit new intersection west of Central Ave

Changes:

US 50 EB adds 2nd lane coming into new intersection
SB CD exit to 7th street add lane (2 lanes) at exit from CD (match existing 4 lanes on 7th)

The changes made to Concept I-W shown in the attached are acceptable to ODOT. As a major stakeholder, we want the City's concurrence to include these changes in Concept I-W moving forward. We would be happy to discuss the information in the attached at your earliest convenience if necessary.

Thanks,

Stefan C. Spinosa, P.E.

Brent Spence Bridge Corridor Project Manager

ODOT District 8 Capital Programs

505 South SR 741, Lebanon, Ohio 45036

513.933.6639

transportation.ohio.gov

